



# SARASOTA/MANATEE US 41 CORRIDOR MOBILITY AND SAFETY STUDY (CMASS)

## Existing Conditions Report

FPID: 440154-21-01 | January 2024





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## 1. Project Description

Florida Department of Transportation (FDOT), District One is conducting the Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS) along US 41/US 41 Business from University Parkway to 17<sup>th</sup> Street W in Sarasota and Manatee Counties. The study limits are approximately 11 miles long starting at the south in the City of Sarasota and ending to the north in the City of Palmetto. The study corridor location map is shown in **Figure 1**. US 41, also known as the Tamiami Trail, generally consists of six lanes (three in each direction) and is functionally classified as an urban principle arterial. This roadway serves as a primary transportation corridor in Sarasota and Manatee Counties, which includes the Sarasota, Bradenton, and Palmetto communities. The corridor serves the statewide transportation network and regional corridor, providing connection to Interstate 275 (I-275) to the north and as well as complimenting Interstate 75 (I-75) to the east going north-south. The corridor also provides connection to the Sarasota Bradenton International Airport. The corridor also serves as an evacuation route from University Parkway to SR 684/Cortez Road.

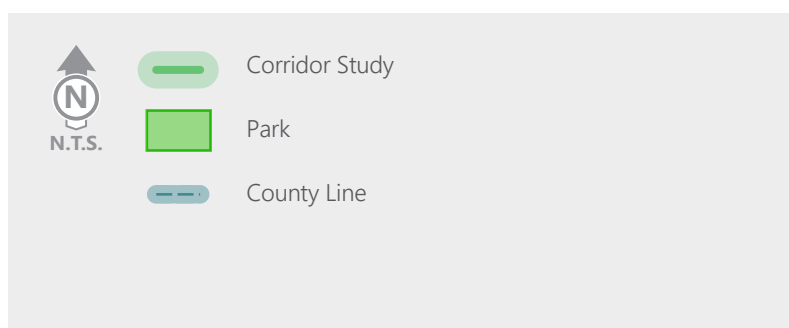
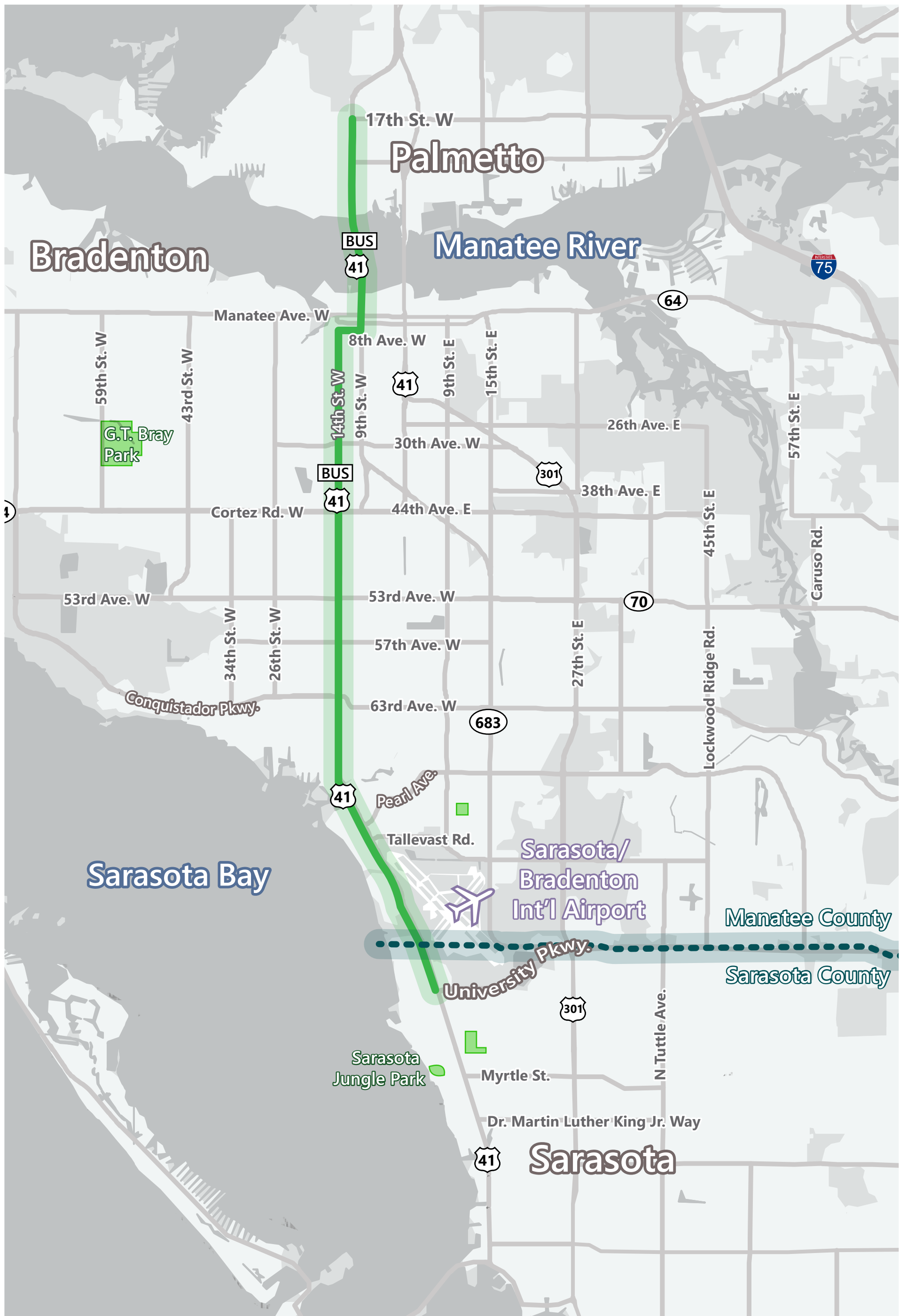
Sarasota/Manatee US 41 CMASS is part of the FDOT Connecting Communities Program, which includes several projects being conducted in partnership with Sarasota/Manatee Metropolitan Planning Organization (MPO) and local governments to holistically enhance the multimodal transportation network within Sarasota and Manatee Counties. Prior to this study, FDOT completed the “US 41 Complete Streets Corridor Planning Study”, the “Arterial Safety Study”, the “Access Management Study”, the “US 41 at Florida Boulevard Intersection Safety Study” and the “US 41 at SR 70 (53<sup>rd</sup> Avenue) Intersection Study”. These studies provide additional background for this project and serve as a basis for this corridor’s study.

Sarasota/Manatee US 41 CMASS is being conducted in two main phases. Phase I of the study includes community and agency outreach, data collection, evaluation of existing corridor conditions and corridor/modal usage, determine study goals, objectives, and measures of effectiveness, and identification of feasible improvement strategies for the corridor. Phase II includes the existing conditions report, additional community and agency outreach, analyzing improvement strategies, and recommending potential concepts with an implementation plan. The Sarasota/Manatee US 41 CMASS Existing Conditions Technical Memo presents the results of the existing conditions and stakeholder coordination conducted during Phase I and Phase II of the study.

### 1.1 Project Purpose

The purpose of this study is to develop concepts and implementation strategies to enhance multimodal mobility (specifically transit services and operations), interconnectivity within adjacent neighborhoods, and safety for all users along the US 41/US 41 Business corridor between University Parkway in Sarasota and 17<sup>th</sup> Street W in Palmetto.





**Figure 1**  
Project Location Map



## 2. Related Projects

To understand which relevant projects are planned and programmed in the area, the study team reviewed the following documents:

- Sarasota-Manatee MPO Adopted TIP FY 2023-24 (Last Updated May 22, 2023)
- Sarasota-Manatee MPO 2045 Long Range Transportation Plan (Adopted October 26, 2020)
- Sarasota-Manatee MPO 2023 Project Priorities (Adopted March 27, 2023)
- Sarasota-Manatee MPO 2022 Legislative Priorities
- Sarasota County Comprehensive Plan – Volume 1 (Last Updated May 22, 2023)
- Sarasota County Mobility Plan (Adopted October 25, 2016)
- Sarasota County Tentative Work Program FY23-27 (Last Updated November 17, 2022)
- Bradenton Comprehensive Plan (Adopted March 2, 2022)
- Manatee County Tentative Work Program FY 23-27 (Last Updated November 17, 2022)
- Manatee County Transportation Development Plan (TDP) 2019-2028 (Adopted August 2018)

On-going projects within or adjacent to the study area are summarized in **Table 1** and illustrated in **Figure 2**.

*Table 1: Summary of On-Going Projects*

FPID	Project Name	Description	Status
433592-4	SR 45 (US 41) Sidewalk Improvements from 69 <sup>th</sup> Avenue (Bay Drive) to Cortez Road	This project implements sidewalks and bicycle lanes along a segment of US 41 within the study limits.	Construction funded for FY 2024
444612-1	US 41 Resurfacing from Edwards Drive to Magellan Drive	This project mills and resurfaces a segment of US 41 within the study limits.	Currently in Design Construction funded for FY 2026
444807-3	US 41 Traffic Operation Improvements in Downtown Bradenton from 4 <sup>th</sup> Avenue W to 6 <sup>th</sup> Avenue W	This project improves mobility and reduces congestion along a segment of US 41 within the study limits.	Construction Let March 2024
444904-1	SR 45 (US 41 Business)/8th Avenue W Midblock Crossing	This project enhances pedestrian safety by implementing a midblock crossing at the intersection of US 41 Business and 8 <sup>th</sup> Avenue W within the study limits.	Currently in Design
449646-1	US 41 Safety Improvements from 53 <sup>rd</sup> Avenue to Cortez Road	This project provides safety improvements along a segment of US 41 within the study limits.	Design phase funded for FY 2024
451025-1	SR 45 (US 41) Resurfacing from north of 26 <sup>th</sup> Avenue W to north of 10 <sup>th</sup> Street W	This project mills and resurfaces a segment of US 41 within the study limits.	Design phase: TBD
448390-1	SR 45 (US 41) Safety Improvements from 63 <sup>rd</sup> Avenue to 53 <sup>rd</sup> Avenue	This project provides safety improvements along a segment of US 41 within the study limits.	ROW phase underway Construction Let March 2025

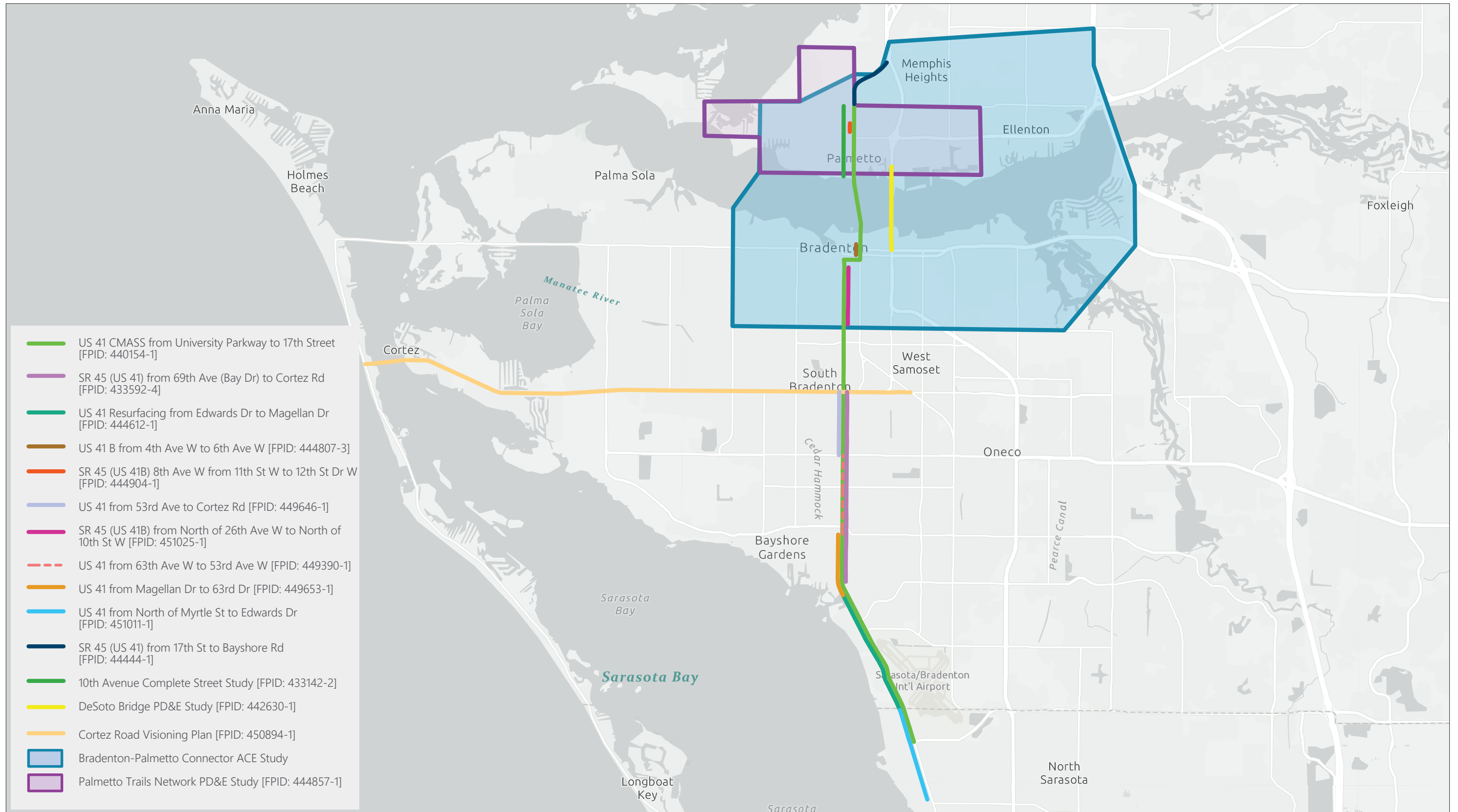




FPID	Project Name	Description	Status
449653-1	US 41 Safety Improvements from Magellan Drive to 63 <sup>rd</sup> Avenue	This project provides safety improvements along a segment of US 41 within the study limits.	Design phase underway Construction Let July 2026
451011-1	SR 45 (US 41) Resurfacing from north of Myrtle Street to Edwards Drive	This project mills and resurfaces US 41 beginning just south of the project limits and extending north along the study limits to Edwards Drive.	Design begin FY 2024 Construction Let July 2025
444440-1	SR 45 (US 41B) Resurfacing and Roundabout from 17 <sup>th</sup> Street to Bayshore Road	This project mills and resurfaces US 41 and provides a roundabout just north of the study limits.	Under construction
433142-2	10th Avenue from Riverside Drive to 17 <sup>th</sup> Street W Complete Streets Study	This study will evaluate complete street features adjacent to the study limits.	Design phase funded for FY 2024
442630-1	DeSoto Bridge from SR 64 (Manatee Avenue) to Haben Boulevard PD&E Study	This study will evaluate alternatives to replace the DeSoto Bridge adjacent to the study limits.	PD&E
444843-1	Bradenton-Palmetto Connector from City of Bradenton to City of Palmetto Alternative Corridor Evaluation Study	This study will evaluate feasible corridor alternatives to connect the Cities of Bradenton and Palmetto within the study limits.	Planning
444857-1	Palmetto Trails Network from MCAT Station to Lincoln Park to Washington Park PD&E Study	This study will identify alignments and evaluate connections through the communities near Lincoln Park, Washington Park, and the area's future regional trail network adjacent to the study limits.	PD&E
450894-1	Cortez Road Vision and Action Plan from 121 <sup>st</sup> St Ct to 301 Blvd E	This study will develop a transportation vision for the Cortez Road corridor and define multi-modal transportation strategies adjacent to the study limits.	Planning







**Figure 2**

**Current and Ongoing Projects**  
 Sarasota/Manatee US 41 CMASS

### 3. Roadway Characteristics

#### 3.1 FDOT Context Classification

Roadway context classification is used to develop build alternatives that meet the needs of current users while planning for future transportation needs. Context classification is used to define design criteria and standards that are used during the planning and design phases of a project.

The *FDOT GIS Preliminary Context Classification* feature class, published by FDOT Transportation Data and Analytics Office and updated October 14, 2023, was reviewed to determine context classifications for the US 41 corridor. **Table 2** summarizes the roadway context classifications of US 41 from University Parkway to 17<sup>th</sup> Street.

*Table 2: US 41 Current Context Classifications*

Roadway ID	From	To	Context Classification
17020000	University Parkway	Manatee County Line	Suburban Commercial (C3C)
13010000	Manatee County Line	69 <sup>th</sup> Avenue	Suburban Commercial (C3C)
13010000	69 <sup>th</sup> Avenue	53 <sup>rd</sup> Avenue	Suburban Residential (C3R)
13010000	53 <sup>rd</sup> Avenue	Cortez Road	Suburban Residential (C3R)
13010000	Cortez Road	10 <sup>th</sup> Street	Urban General (C4)
13010001	10 <sup>th</sup> Street	Green Bridge (southern end)	Urban General (C4)
13010001	Green Bridge (southern end)	Green Bridge (northern end)	Suburban Commercial (C3C)
13010001	Green Bridge (northern end)	Riverside Drive	Urban General (C4)
13010000	Riverside Drive	10 <sup>th</sup> Street	Urban General (C4)
13030000	10 <sup>th</sup> Street	17 <sup>th</sup> Street	Urban General (C4)

#### 3.2 Access Management Classification

FDOT uses a seven-tier access management system established in Rule 14-97 of the Florida Administrative Code (F.A.C.) to classify access on state roadways. The classification system ranges from Access Class 1, the most restrictive class reserved for limited access freeways, to Access Class 7, which is assigned to state highways in areas that are highly urbanized. The Access Classes and posted speeds establish the allowable spacing of signalized intersections, median openings, pedestrian crossing opportunities, and local street connections for corridors.

Access Management classifications for the US 41 study area were obtained from FDOT’s Roadway Characteristics Inventory (RCI), collected in July 2023. The US 41 study area is made up of Access Class 5, 6, and 7, as outlined below. It should be noted that current access management data does not reflect that several sections of the roadway have been modified from a bi-directional turn lane to a median. These changes were verified based on a field review of the US 41 corridor.

- **Access Class 5 from University Parkway north to Manatee County Line**
  - Minimum Connection Spacing:
    - Posted Speed greater than 45 miles per hour (mph): 440 feet
    - Posted Speed less than or equal to 45 mph: 245 feet
  - Minimum median opening spacing:



- Directional: 660 feet
  - Full: 1,320 feet (0.25 mile)
  - Minimum signal spacing: 1,320 feet (0.25 mile)
- **Access Class 6 from Manatee County Line to Florida Boulevard**
  - Minimum Connection Spacing:
    - Posted Speed greater than 45 mph: 440 feet
    - Posted Speed less than or equal to 45 mph: 245 feet
  - Minimum median opening spacing:
    - Directional: Not applicable
    - Full: Not applicable
  - Minimum signal spacing: 1,320 feet (0.25 mile)
- **Access Class 7 from Florida Boulevard to 17<sup>th</sup> Street**
  - Minimum Connection Spacing: 125 feet
    - Posted Speed greater than 45 mph: 125 feet
    - Posted Speed less than or equal to 45 mph: 125 feet
  - Minimum median opening spacing:
    - Directional: 330 feet
    - Full: 660 feet
  - Minimum signal spacing: 1,320 feet (0.25 mile)

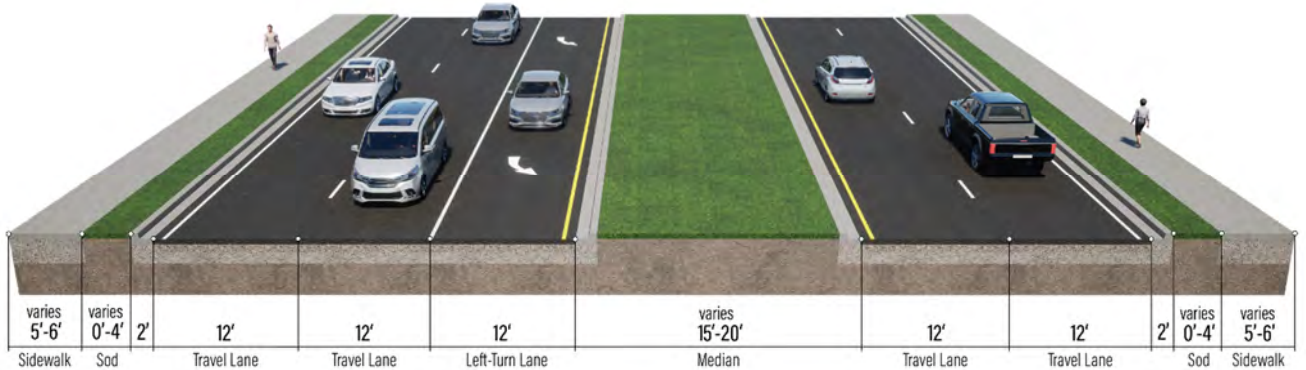
### 3.3 Typical Sections

The characteristics of the various typical sections along US 41 through the study area were determined based on FDOT Straight Line Diagrams (SLDs), Google Earth, and field reviews. All existing typical sections found within the study area are illustrated below in **Figure 3** through **Figure 15**.

Existing Typical Section 1: From University Parkway to North of General Spaatz Boulevard the corridor consists of five lanes. There are two 12-foot travel lanes in the northbound direction, two 12-foot travel lanes and one 12-foot continuous left-turn lane in the southbound direction, and a raised median. There is curb and gutter and five-to-six-foot sidewalks on both sides of the roadway. The posted speed limit for this typical section is 45 mph.

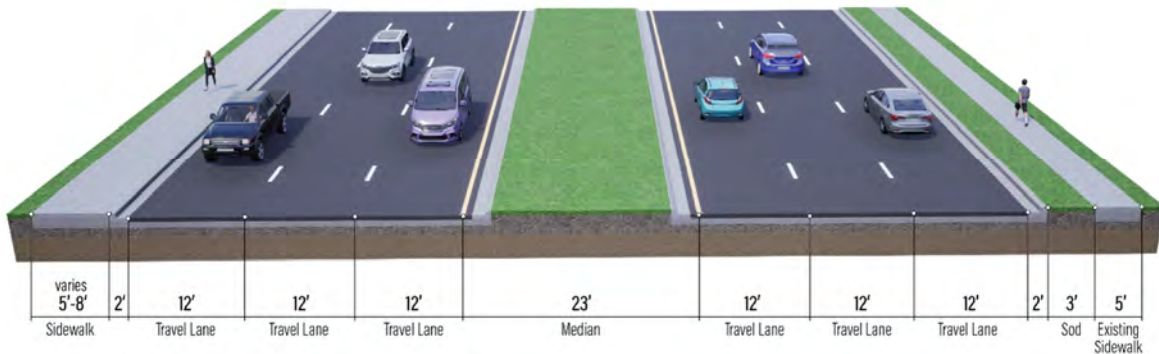


Figure 3: Existing Typical Section 1 - US 41 from University Parkway to North of General Spatz Boulevard



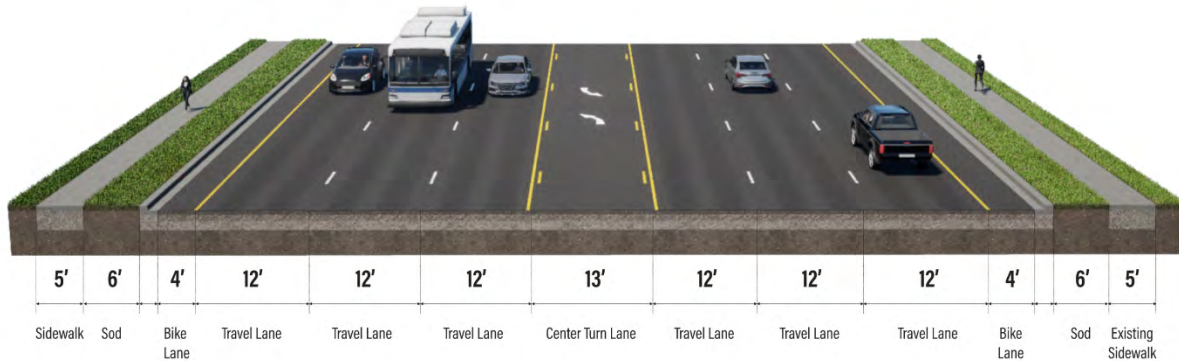
**Existing Typical Section 2:** From North of General Spatz Boulevard to Edwards Drive the corridor transitions to six 12-foot travel lanes (three lanes in each direction) with a raised grass median. There is curb and gutter, a five-foot sidewalk on the northbound side of the roadway and a five-to-eight-foot sidewalk on the southbound side of the roadway. The posted speed limit for this typical section is 45 mph.

Figure 4: Existing Typical Section 2 - US 41 from North of General Spatz Boulevard to Edwards Drive



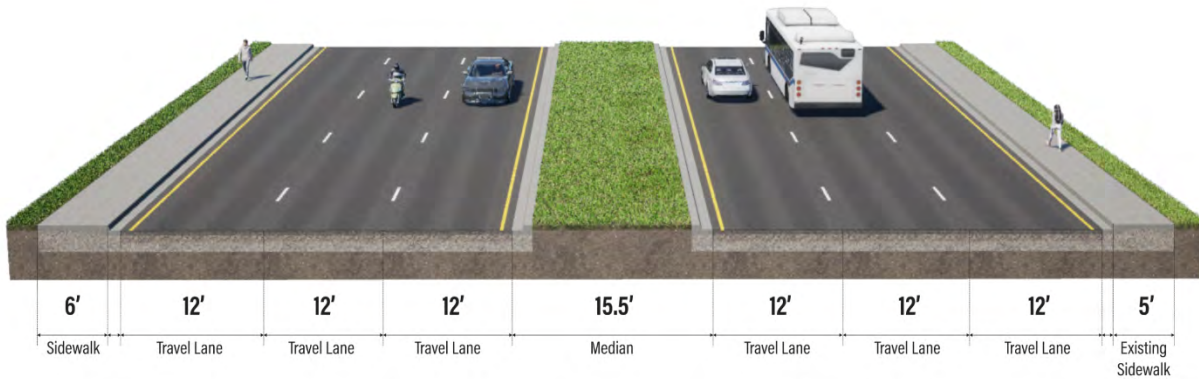
**Existing Typical Section 3:** From Edwards Drive to Montgomery Avenue, US 41 transitions to have six 12-foot travel lanes (three in each direction). There is a 13-foot bi-directional center turn lane, curb and gutter, four-foot bike lanes on both sides of the roadway, and a five-foot sidewalk on the north and south sides of the roadway. The posted speed limit for this typical section is 50 mph.

Figure 5: Existing Typical Section 3 - US 41 from Edwards Drive to Montgomery Avenue



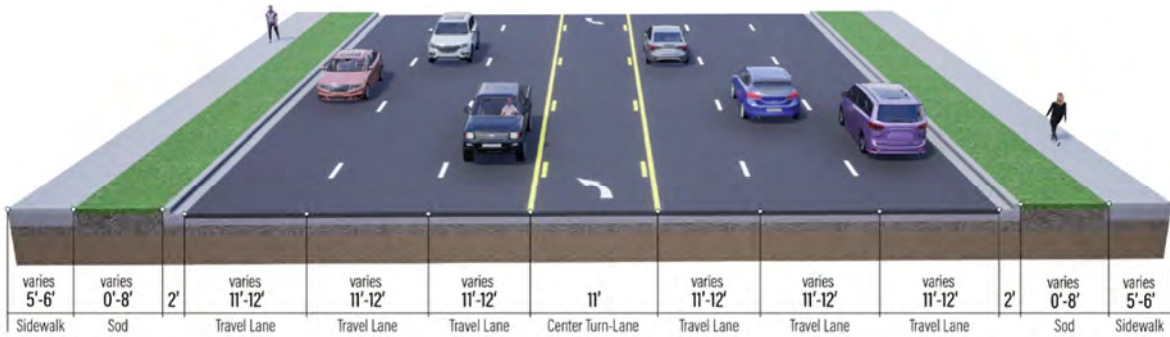
**Existing Typical Section 4:** From Montgomery Avenue to Magellan Drive, there are six 12-foot travel lanes (three lanes per direction) with a raised median. There is curb and gutter, a five-foot sidewalk on the east side of the roadway, and a six-foot sidewalk on the west side of the roadway. The posted speed limit for this typical section is 50 mph.

Figure 6: Existing Typical Section 4 - US 41 from Montgomery Avenue to Magellan Drive



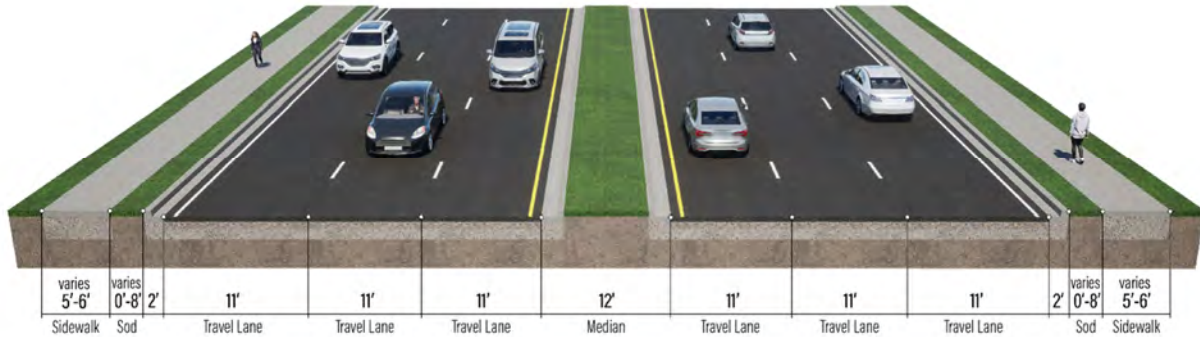
**Existing Typical Section 5:** From Magellan Drive to 63<sup>rd</sup> Avenue there are six 11-12-foot travel lanes (three lanes in each direction) with a bi-directional center turn lane. There is curb and gutter and five-to-six-foot sidewalks on both sides of the roadway. The posted speed limit for this typical section is 45 mph.

Figure 7: Existing Typical Section 5 - US 41 from Magellan Drive to 63rd Avenue



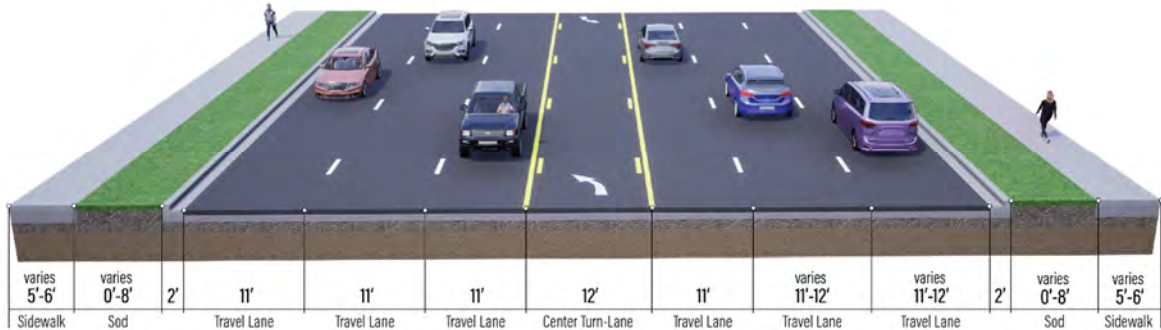
**Existing Typical Section 6:** From 63rd Avenue to 53rd Avenue W the corridor consists of six 11-foot travel lanes (three lanes in each direction) with a raised median, curb and gutter, and five-to-six-foot sidewalks on both sides of the roadway. The posted speed limit for this typical section is 45 mph.

Figure 8: Existing Typical Section 6 - US 41 from 63rd Avenue to 53rd Avenue W



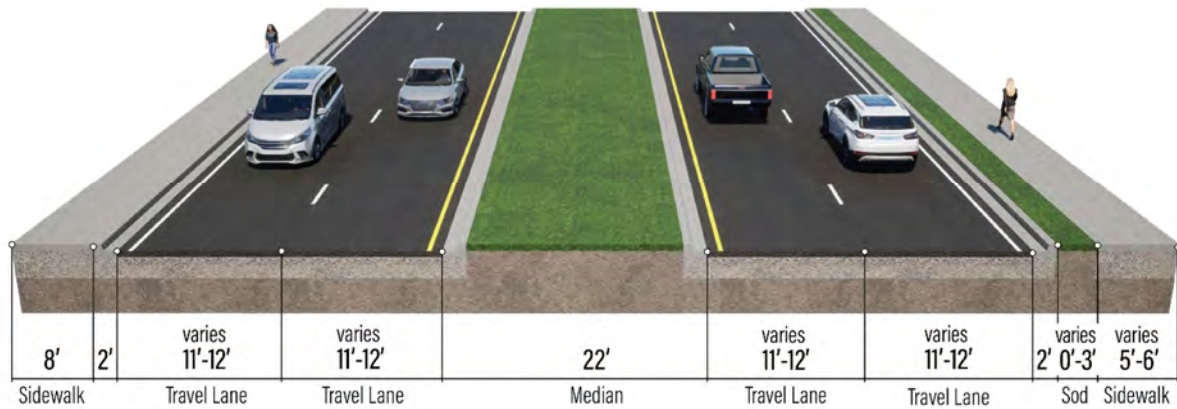
**Existing Typical Section 7:** From 53rd Avenue W to Cortez Road (SR 684/44th Avenue) the corridor has six travel lanes (three lanes in each direction) – the three southbound lanes and innermost northbound lane are 11-feet wide, and the outer two northbound lanes are 11-12-feet wide. This segment also has a bi-directional center turn lane, curb and gutter, and five-to-six-foot-wide sidewalks on both sides of the roadway. The posted speed limit for this typical section is 45 mph between 53rd Avenue W to 600 feet south of Cortez Road, and 40 mph between 600 feet south of Cortez Road to Cortez Road.

Figure 9: Existing Typical Section 7 - US 41 from 53rd Avenue to Cortez Road (SR 684/44th Avenue)



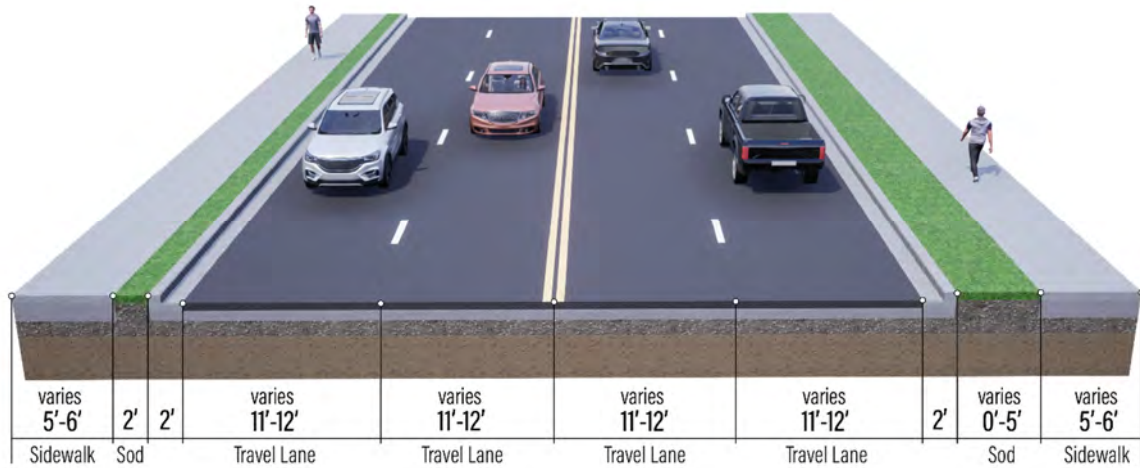
**Existing Typical Section 8:** Along US 41 Business from Cortez Road (SR 684/44<sup>th</sup> Avenue) to 26<sup>th</sup> Avenue W the corridor transitions to four 11-12-foot travel lanes with a raised grass median and curb and gutter. There is a five-to-six-foot sidewalk on the north side of the roadway and an eight-foot sidewalk on the south side of the roadway. The posted speed limit for this typical section is 40 mph.

Figure 10: Existing Typical Section 8 - US 41 Business from Cortez Road (SR 684/44th Avenue) to 26th Avenue



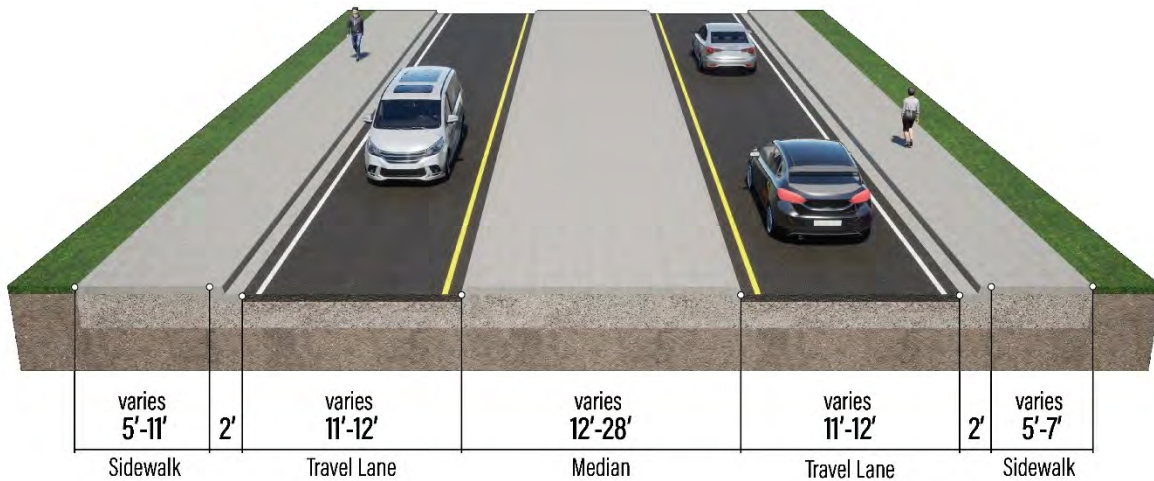
**Existing Typical Section 9:** Along US 41 Business from 26<sup>th</sup> Avenue to 8<sup>th</sup> Avenue the corridor is undivided with four 11-12-foot travel lanes, curb and gutter, and five-to-six-foot sidewalks on both sides of the roadway separated by a grass buffer. The posted speed limit for this typical section is 40 mph between 26<sup>th</sup> Avenue to 300 feet north of 26<sup>th</sup> Avenue, and 35 mph between 300 feet north of 26<sup>th</sup> Avenue and 8<sup>th</sup> Avenue.

Figure 11: Existing Typical Section 9 - US 41 Business from 26th Avenue to 8th Avenue



**Existing Typical Section 10:** Along 8<sup>th</sup> Avenue W from US 41 Business to 9<sup>th</sup> Street, there are two 11-12-foot travel lanes divided by a raised grass median. There is curb and gutter, and 5-11-foot sidewalks on both sides of the roadway. The posted speed limit for this typical section is 25 mph.

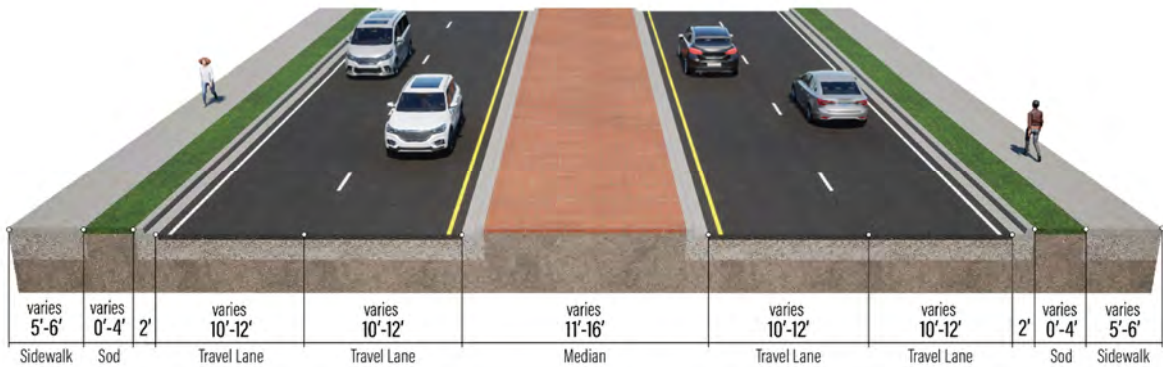
Figure 12: Existing Typical Section 10 - 8th Avenue in Bradenton from US 41 Business to 9th Street



**Existing Typical Section 11:** Along 9<sup>th</sup> Street from 8<sup>th</sup> Avenue to the Green Bridge, the corridor transitions to four 10-12-foot travel lanes (two in each direction) divided by a raised paved median. There is curb and gutter and five-to-six-foot sidewalks on both sides of the roadway. The posted speed limit for this typical section is 35 mph.

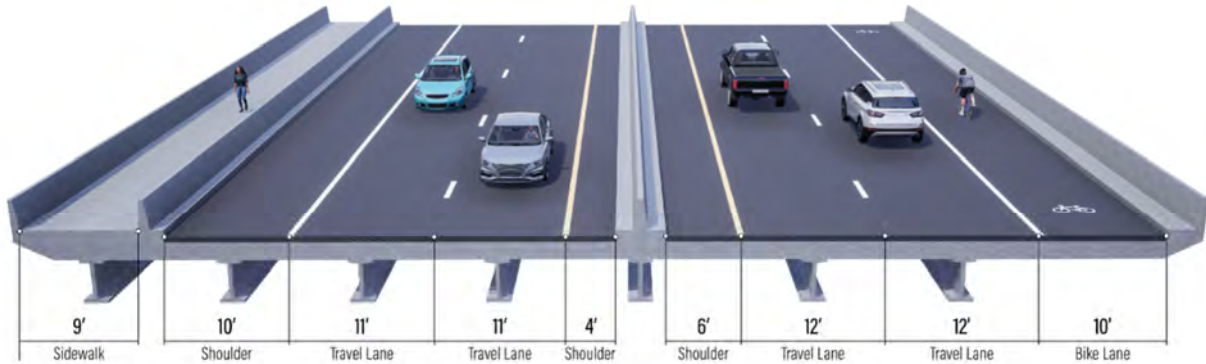


Figure 13: Existing Typical Section 11 - 9th Street from 8th Avenue to the Green Bridge



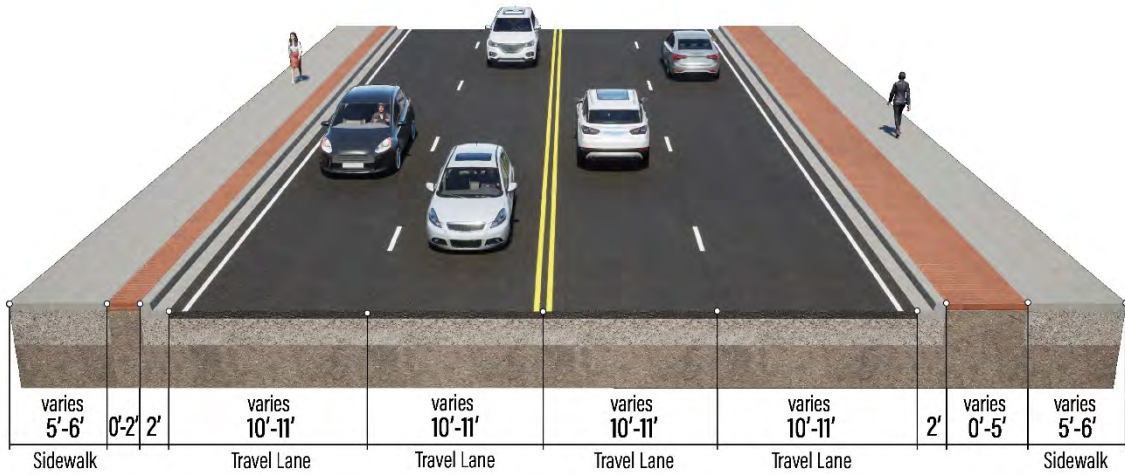
**Existing Typical Section 12:** The Green Bridge consists of four 11-12-foot travel lanes (2 in each direction) with a four-foot inside shoulder, 10-foot outside shoulder, and a nine-foot sidewalk on the southbound side of the roadway. There is a six-foot inside shoulder and 10-foot bike lane on the northbound side of the roadway. The posted speed limit for this typical section is 45 mph.

Figure 14: Existing Typical Section 12 - US 41 Business (Green Bridge)



**Existing Typical Section 13:** From the Green Bridge to just north of 17<sup>th</sup> Street W there are four 11-12-foot travel lanes, curb and gutter, and five-to-six-foot sidewalks separated by a brick-paved buffer on both sides of the roadway. The brick-paver buffer on the west side of the roadway varies between 0'-2' wide, while the brick-paver buffer on the east side of the roadway varies between 0'-5' wide. The posted speed limit for this typical section is 45 mph between the Green Bridge to 500 feet south of Riverside Drive, 35 mph between 500 feet south of Riverside Drive and Riverside Drive, 30 mph between Riverside Drive and 11<sup>th</sup> Street, and 35 mph between 11<sup>th</sup> Street and 17<sup>th</sup> Street W.

Figure 15: Existing Typical Section 13 - US 41 Business (8th Avenue in Palmetto) from the Green Bridge to Just North of 17th Street W





### 3.4 Posted Speed

Posted speed limit data was collected from the FDOT RCI in October 2023, and field verified in August 2023. The posted speed limits along US 41 are summarized in **Table 3**.

*Table 3: Posted Speeds Along US 41*

From	To	Posted Speed Limit
University Parkway	Manatee County Line	45 mph
Manatee County Line	Magellan Drive	50 mph
Magellan Drive	~600 feet south of Cortez Road	45 mph
~600 feet south of Cortez Road	~300 feet north of 26 <sup>th</sup> Avenue	40 mph
~300 feet north of 26 <sup>th</sup> Avenue	8 <sup>th</sup> Avenue	35 mph
8 <sup>th</sup> Avenue	9 <sup>th</sup> Street	25 mph
9 <sup>th</sup> Street	Green Bridge	35 mph
Green Bridge	~500 feet south of Riverside Drive	45 mph
~500 feet south of Riverside Drive	Riverside Drive	35 mph
Riverside Drive	11 <sup>th</sup> Street	30 mph
11 <sup>th</sup> Street	17 <sup>th</sup> Street	35 mph

### 3.5 Jurisdiction

The US 41 study area is under FDOT jurisdiction. The entire project is located within the FDOT District 1 and Sarasota/Manatee MPO’s planning area. Additionally, from south to north the US 41 study area travels through the City of Sarasota, Sarasota County, Manatee County, City of Bradenton, and the City of Palmetto. **Table 4** summarizes the roadway segments by Roadway ID, mileposts, and jurisdiction of adjacent land uses, based on FDOT SLDs.

*Table 4: Roadway Identification Segments*

Roadway ID	Begin Milepost	End Milepost	Jurisdiction
17020000	21.879	22.221	City of Sarasota
17020000	22.221	22.390	Sarasota County
13010000	0.000	6.330	Manatee County
13010000	6.330	7.552	City of Bradenton
13010001 (Bridge)	0.000	0.045	City of Bradenton
13010001 (Bridge)	0.045	1.379	Manatee County
13010000	8.878	9.498	City of Palmetto
13030000	0.000	0.505	City of Palmetto

### 3.6 Median

The existing median varies along the study corridor, with some areas having a raised median, other areas have a two-way left-turn lane, while some areas lack any sort of median. See **Table 5** below for the limits of median types along the study corridor.



Table 5: Roadway Median Segments

From	To	Median Type	Median Width	Source
University Parkway	General Spaatz Boulevard	Raised Median	15'-20'	Straight Line Diagram (17020000)
General Spaatz Boulevard	Edwards Drive	Raised Median	23'	Straight Line Diagram (17020000)
Edwards Drive	Magellan Drive	Two Way Left Turn Lane	N/A	FPID 444612-1-52-01
Magellan Drive	63 <sup>rd</sup> Avenue	Two Way Left Turn Lane	N/A	Google Earth/Straight Line Diagram (13010000)
63 <sup>rd</sup> Avenue	53 <sup>rd</sup> Avenue	Two Way Left Turn Lane	N/A	FPID 444049-1-52-01
53 <sup>rd</sup> Avenue	Cortez Road	Two Way Left Turn Lane	N/A	Google Earth/Straight Line Diagram (13010000)
Cortez Road	26 <sup>th</sup> Avenue	Raised Median	18'	Straight Line Diagram (13010000)
26 <sup>th</sup> Avenue	8 <sup>th</sup> Avenue	No Median	N/A	Google Earth/Straight Line Diagram (13010000)
8 <sup>th</sup> Avenue	9 <sup>th</sup> Street	Two Way Left Turn Lane	N/A	Google Earth
9 <sup>th</sup> Street	Green Bridge	Brick, raised	16'	Straight Line Diagram (13010001)
Green Bridge	Green Bridge	Paved shoulder/ Median Barrier	12'	Straight Line Diagram (13010001)
Green Bridge	17 <sup>th</sup> Street	No Median	N/A	Google Earth/Straight Line Diagram (13030000)

### 3.7 Lighting

The existing lighting varies along the study corridor. Stretches of the corridor have lighting, while others do not have any lighting. See **Table 6** below for the limits of lighting types and locations along the study corridor.

Table 6: US 41 Existing Lighting

From	To	Lighting Type	Location	Source
University Parkway	General Spaatz Boulevard	Conventional Lighting	Northbound and Southbound	Field Verified
Seagate Drive	Tower Road	Conventional Lighting	Northbound	Field Verified
Tower Road	Bernard Avenue	Conventional Lighting	Northbound and Southbound	Field Verified
Ponce De Leon Street	Pearl Avenue	Conventional Lighting	Northbound and Southbound	Field Verified
Pearl Avenue	200' north of Magellan Drive	Conventional Lighting	Northbound	Field Verified
63 <sup>rd</sup> Avenue	53 <sup>rd</sup> Avenue	Conventional Lighting	Northbound	Field Verified
North of Cortez Road	26 <sup>th</sup> Avenue	Conventional Lighting	Southbound	Field Verified
26 <sup>th</sup> Avenue	8 <sup>th</sup> Avenue	Conventional Lighting	Northbound	Field Verified
Tamiami Trail	9 <sup>th</sup> Street	Conventional Lighting	Eastbound	Field Verified
8 <sup>th</sup> Avenue	Green Bridge	Conventional Lighting	Northbound and Southbound	Field Verified
Green Bridge	Green Bridge	Conventional Lighting	Median	Field Verified
Green Bridge	17 <sup>th</sup> Street	Conventional Lighting	Northbound and Southbound	Field Verified



### 3.8 Drainage Type

US 41 along the study corridor is a closed drainage system, with water being collected through drainage inlets in the curb and gutter. No ponds are located adjacent to the US 41 study corridor.

### 3.9 Utilities

A variety of utility types were identified based on a field review of the US 41 study corridor. These utility types include but are not limited to telephone, fiber, gas, and overhead electric. A Sunshine 811 design ticket was processed to identify a listing of potential utilities along the study corridor. Utility owners were listed in the design ticket with utility types including telephone, communication lines, fiber, CATV, stormwater, wastewater, reclaimed water, chilled water, gas, traffic lights/signals, and water. The list of utilities and providers are listed in **Table 7**.

Table 7: Utility Agency/Owners

Utility Agency/Owner	Utility Type	Contact
<b>Sarasota County</b>		
Charter Communications	CATV	Gene Gregory / (863) 333-4763
City of Sarasota Utilities	Electric, Sewer, Water	Josh Welch / (941) 263-6177
Comcast	CATV	Chad Evener / (941) 356-1564
Crown Castle NG	Fiber	Fiber Dig Team / (800) 654-3110
Florida Power & Light--Sarasota	Electric	Joel Bray / (386) 586-6403
Frontier Communications	CATV, Communication Lines	Toni Cannon / (813) 875-1014
Manatee County Transportation Department	Traffic Lights, Traffic Signals	Utility Design Requests / (941) 792-8811
Manatee County Utility Operations	Reclaimed Water, Sewer, Water	Utility Design Requests / (941) 792-8811
MCI	Communication Lines, Fiber	Investigations Team / (800) 624-9675 X2
New College of Florida - Electric	Electric	Curtis Davis / (941) 487-4240
New College of Florida - Gas	Gas	Curtis Davis / (941) 487-4240
New College of Florida - Sewer	Sewer	Curtis Davis / (941) 487-4240
New College of Florida - Water	Water	Curtis Davis / (941) 487-4240
New College of Florida- Chilled Water	Chilled Water	Curtis Davis / (941) 487-4240
New College of Florida- Telephone And Fiber	Fiber, Telephone	Curtis Davis / (941) 487-4240
Sarasota County Traffic	Traffic Signals	Dean Young / (941) 264-8288
Sarasota County Utils	Sewer, Water	Karla Sohaili / (941) 861-0525
TECO Peoples Gas- Sarasota	Gas	Joan Domning / (813) 275-3783
Uniti Fiber LLC	Fiber	Charlie Croft / (251) 214-7059
<b>Manatee County</b>		
A T & T Mobility	Telephone	Mason Porter / (321) 388-9541
Advanced Cable Communications DbA Blue Stream	CATV	Sean Hayden / (772) 607-2203
Charter Communications	CATV	Gene Gregory / (863) 333-4763
City Of Bradenton	Sewer, Water	Jim McLellan / (941) 462-1989





Utility Agency/Owner	Utility Type	Contact
City Of Palmetto	Sewer, Water	Mohammed Rayan / (941) 723-4580 X2124
City Of Sarasota Utilities	Electric, Sewer, Water	Josh Welch / (941) 263-6177
Comcast	CATV	Chad Evener / (941) 356-1564
Crown Castle	Electric	Fiber Dig Team / (800) 654-3110
Crown Castle Ng	Fiber	Fiber Dig Team / (800) 654-3110
Florida Power & Light - Subaqueous	Electric	Edgar Aguilar / (386) 586-6403
Florida Power & Light--Manatee	Electric	Joel Bray / (386) 586-6403
Florida Power & Light--Sarasota	Electric	Joel Bray / (386) 586-6403
Frontier Communications	CATV, Communication Lines	Toni Cannon / (813) 875-1014
Hotwire Communications	CATV, Fiber, Telephone	Walter Davila / (954) 699-0900
Manatee County Transportation Department	Traffic Lights, Traffic Signals	Utility Design Requests / (941) 792-8811
Manatee County Utility Operations	Reclaimed Water, Sewer, Water	Utility Design Requests / (941) 792-8811
MCI	Communication Lines, Fiber	Investigations Team / (800) 624-9675 X2
New College Of Florida - Electric	Electric	Curtis Davis / (941) 487-4240
New College Of Florida - Gas	Gas	Curtis Davis / (941) 487-4240
New College Of Florida - Sewer	Sewer	Curtis Davis / (941) 487-4240
New College Of Florida - Water	Water	Curtis Davis / (941) 487-4240
New College Of Florida- Chilled Water	Chilled Water	Curtis Davis / (941) 487-4240
New College Of Florida- Telephone And Fiber	Fiber, Telephone	Curtis Davis / (941) 487-4240
TECO Peoples Gas- Sarasota	Gas	Joan Domning / (813) 275-3783
Uniti Fiber LLC	Fiber	Charlie Croft / (251) 214-7059
Verizon Wireless	Fiber	Nick Wiekliniski
Zayo Group / Formerly Lightwave, LLC	Fiber	Henry Klobucar / (406) 496-6510

## 4. Pedestrian & Bicycle Facilities

### 4.1 Bicycle Facilities

According to the FDOT RCI bicycle lane dataset (collected October 2023) and field verified, there are several segments of US 41 with designated bicycle lanes, as well as a segment of sharrows. Designated bicycle lanes are provided on both sides of the US 41 corridor from the Sarasota/Manatee County Line to Montgomery Avenue. Designated bicycle lanes are also provided on the northbound (right) side of the US 41 “Green Bridge.” The east/west segment of US 41, along 8<sup>th</sup> Avenue, contains sharrows from Tamiami Trail (US 41 Business) to 9<sup>th</sup> Street. There are no other bicycle facilities identified along the US 41 Business corridor.

### 4.2 Sidewalks

Sidewalk inventory was identified using FDOT RCI database information, aerial imagery via Google Maps and Esri base map aerials, and was field verified. Sidewalks are generally present on both sides of the roadway for the majority of the US 41 study corridor. These sidewalks are typically located within four (4) feet or less of the roadway.





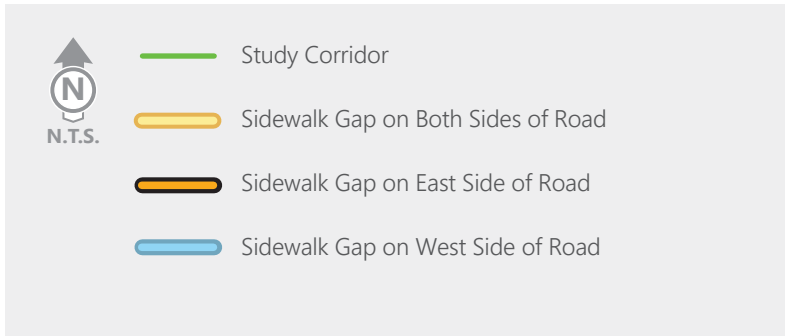
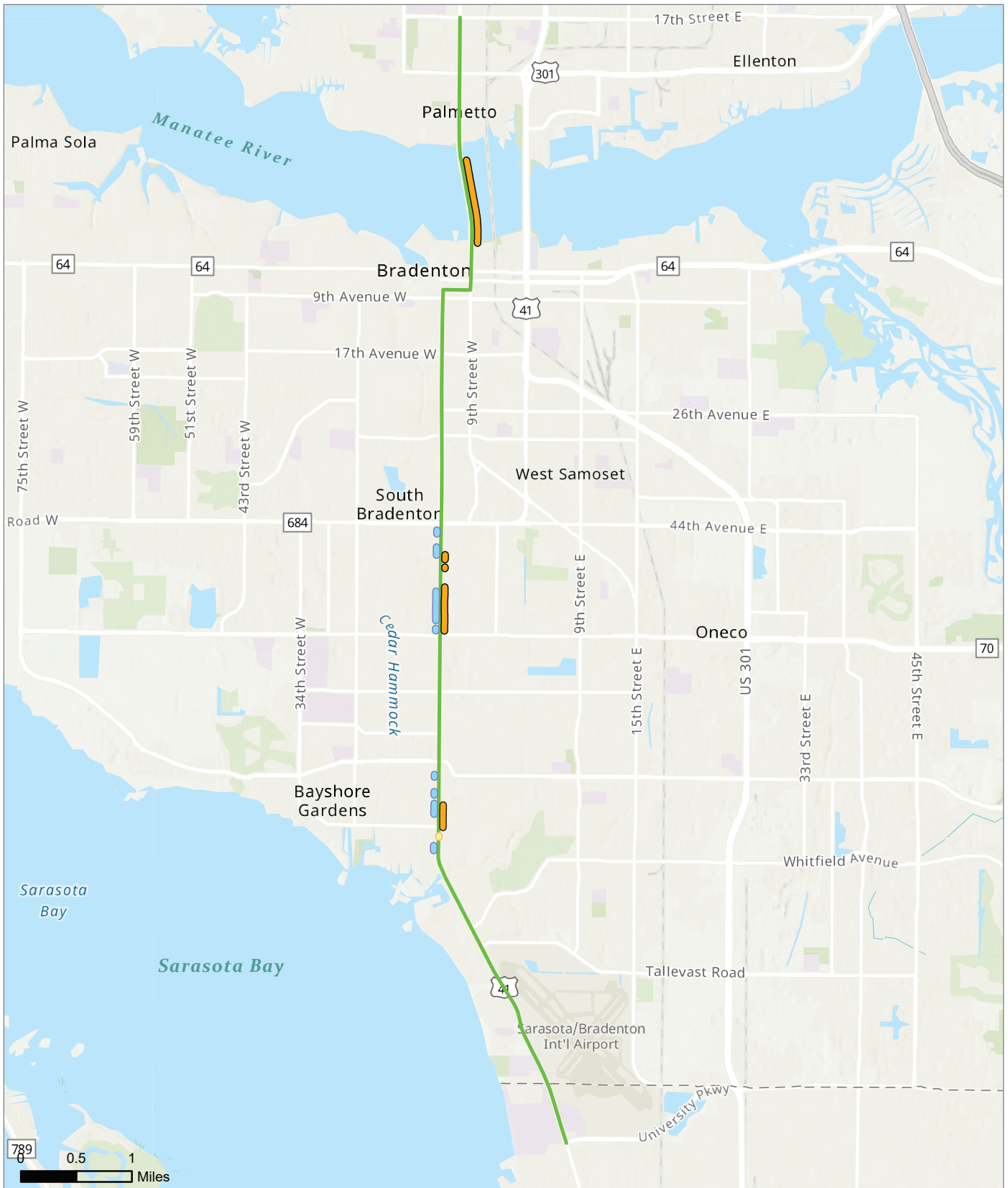
There are 13 segments along US 41 where there is no sidewalk present on one side or both sides of the roadway. See **Table 8** below for the locations of sidewalk gaps along the study corridor.

Sidewalk gaps along the study corridor are illustrated in **Figure 16**.

*Table 8: US 41 Sidewalk Gaps*

From	To	Location	Length (ft)	Source
68 <sup>th</sup> Avenue W	66 <sup>th</sup> Avenue W	Northbound	100'	Field Verified
Florida Boulevard	700' North of 66 <sup>th</sup> Avenue W	Northbound	1,100'	Field Verified
53 <sup>rd</sup> Avenue	49 <sup>th</sup> Avenue Drive W	Northbound	2,200'	Field Verified
300' South of Eagle Creek Village	Eagle Creek Village	Northbound	300'	Field Verified
150' North of Eagle Creek Village	Orlando Avenue	Northbound	300'	Field Verified
550' North of 3 <sup>rd</sup> Avenue W	750' South of Riverside Drive	Northbound	4,100'	Field Verified
300' South of 68 <sup>th</sup> Avenue W	68 <sup>th</sup> Avenue W	Southbound	300'	Field Verified
100' North of 66 <sup>th</sup> Avenue W	550' South of South Palm W	Southbound	400'	Field Verified
100' South of 63 <sup>rd</sup> Avenue W	63 <sup>rd</sup> Avenue W	Southbound	100'	Field Verified
150' North of 53 <sup>rd</sup> Avenue W	450' North of 53 <sup>rd</sup> Avenue W	Southbound	300'	Field Verified
650' North of 53 <sup>rd</sup> Avenue W	350' North of 50 <sup>th</sup> Avenue Plaza W	Southbound	1,600'	Field Verified
50' South of Orlando Avenue	550' North of Orlando Avenue	Southbound	600'	Field Verified
550' South of Cortez Road	250' South of Cortez Road	Southbound	300'	Field Verified





**Figure 16**  
**Sidewalk Gaps Along Corridor**  
 Sarasota/Manatee US 41 CMASS



### 4.3 Pedestrian Accommodation at Signalized Intersections

Pedestrian accommodations are present at all signalized intersections along the US 41 study corridor. These accommodations include sidewalks, curb ramps, clearly marked crosswalks, accessible pedestrian buttons, detectable warning strips, and ample flat landing space surrounding each curb ramp. However, some of the signalized intersections are missing all of these features. For example, both the intersection of US 41 and University Parkway and the intersection of US 41 and Braden Avenue are missing a marked crosswalk on the north leg of the intersection. Additionally, the intersection of US 41 and Braden Avenue is missing a marked crosswalk on the south leg of the intersection.

Some of the crosswalks only have longitudinal pavement markings, other crosswalks only have the special emphasis pavement markings, while other crosswalks have both perpendicular and special emphasis pavement markings. Additionally, some of the crosswalk pavement markings are older and are not fully visible.

## 5. Transit Service and Infrastructure

### 5.1 Transit Routes

Manatee County Area Transit (MCAT) and Breeze Transit (FKA SCAT) provide fixed route service throughout the study area with Route 99 being the primary route that travels along the US 41/US 41 Business corridor. As shown in **Figure 17**, Route 99 begins south of University Parkway and continues north along the study corridor to Downtown Station located adjacent to 8<sup>th</sup> Avenue W and is operated by MCAT and Breeze Transit. MCAT Route 201 North County Connection continues north along the study corridor to Palmetto Station located just north of 17<sup>th</sup> Street W. A copy of MCAT and Breeze Transit maps can be found in **Appendix A**.

### 5.2 Transit Stop Accommodations

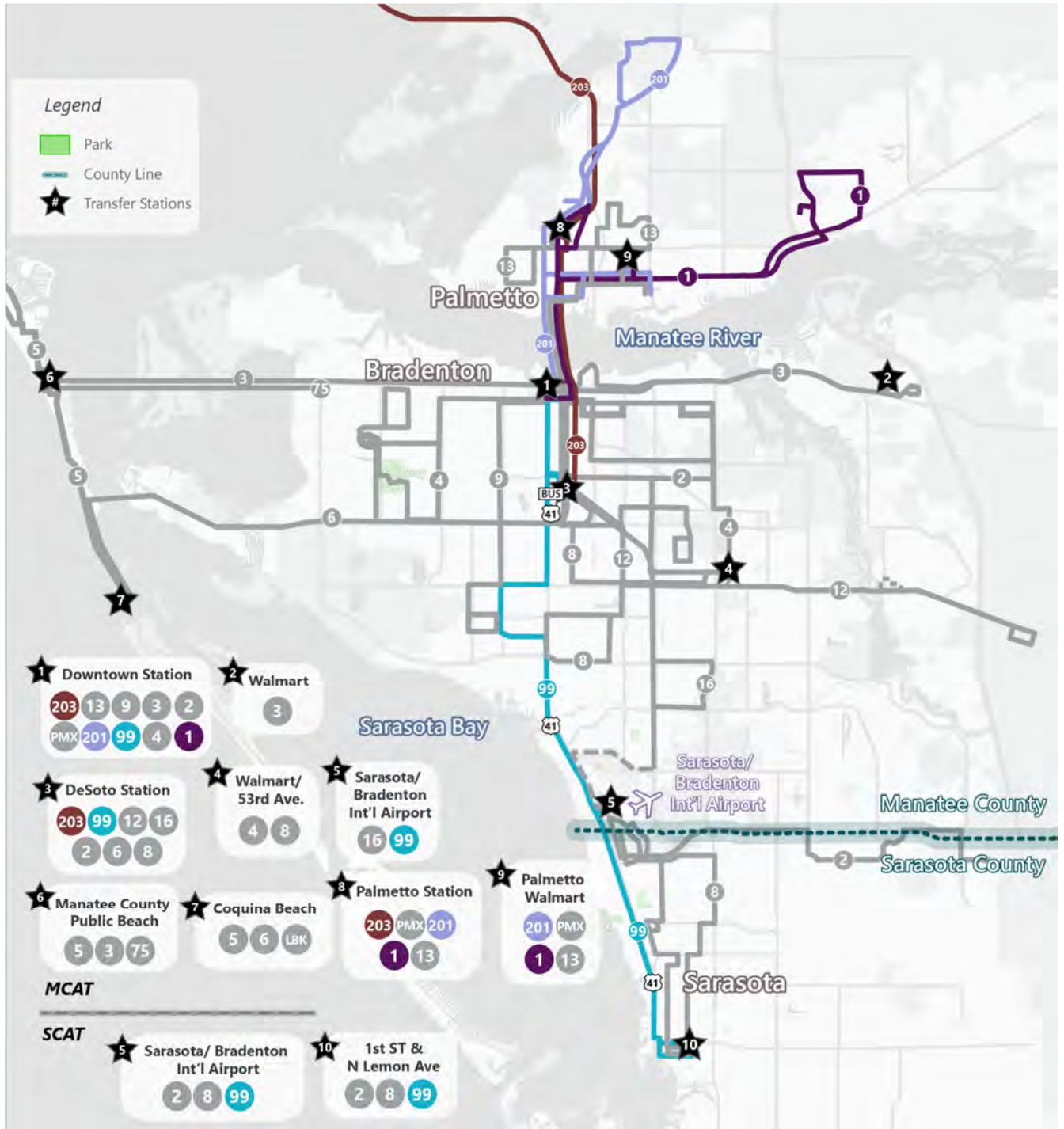
There are approximately 84 stops situated along the study corridor. Transit stations located near the study corridor include Palmetto Station, Bradenton Downtown Station, DeSoto Station, and Sarasota-Bradenton International Airport Station.

### 5.3 Ridership and Timing

Service for Route 99 runs Monday through Saturday with no service on Sundays or select holidays and operates every 20 minutes. Route 201 North County ConneXion runs Monday through Saturday with no service on Sunday or select holidays and operates every other hour.

Ridership data for MCAT and Breeze Transit for fiscal year (FY) 2021 was gathered for Route 99. The total annual ridership recorded for the route was 52,540.

Figure 17: Transit Routes Overview





## 6. Land Use Characteristics

### 6.1 Existing Land Use

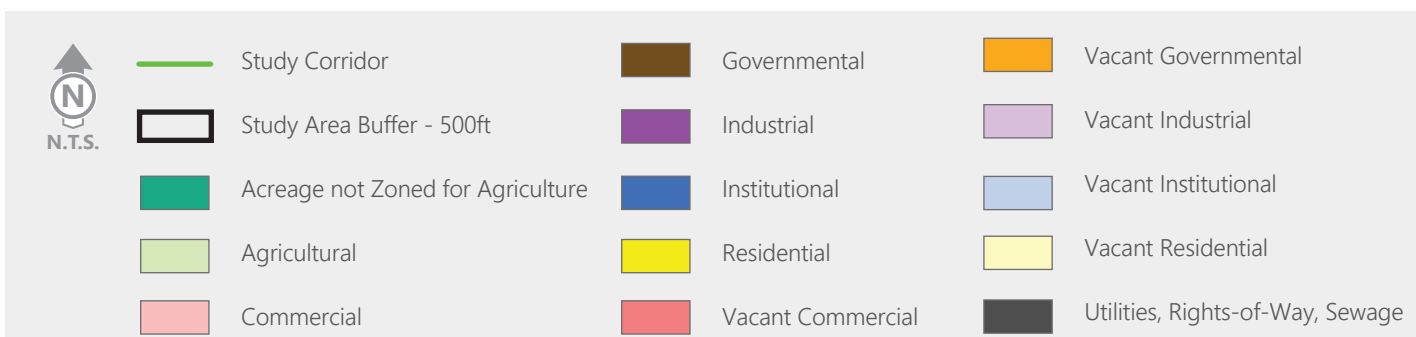
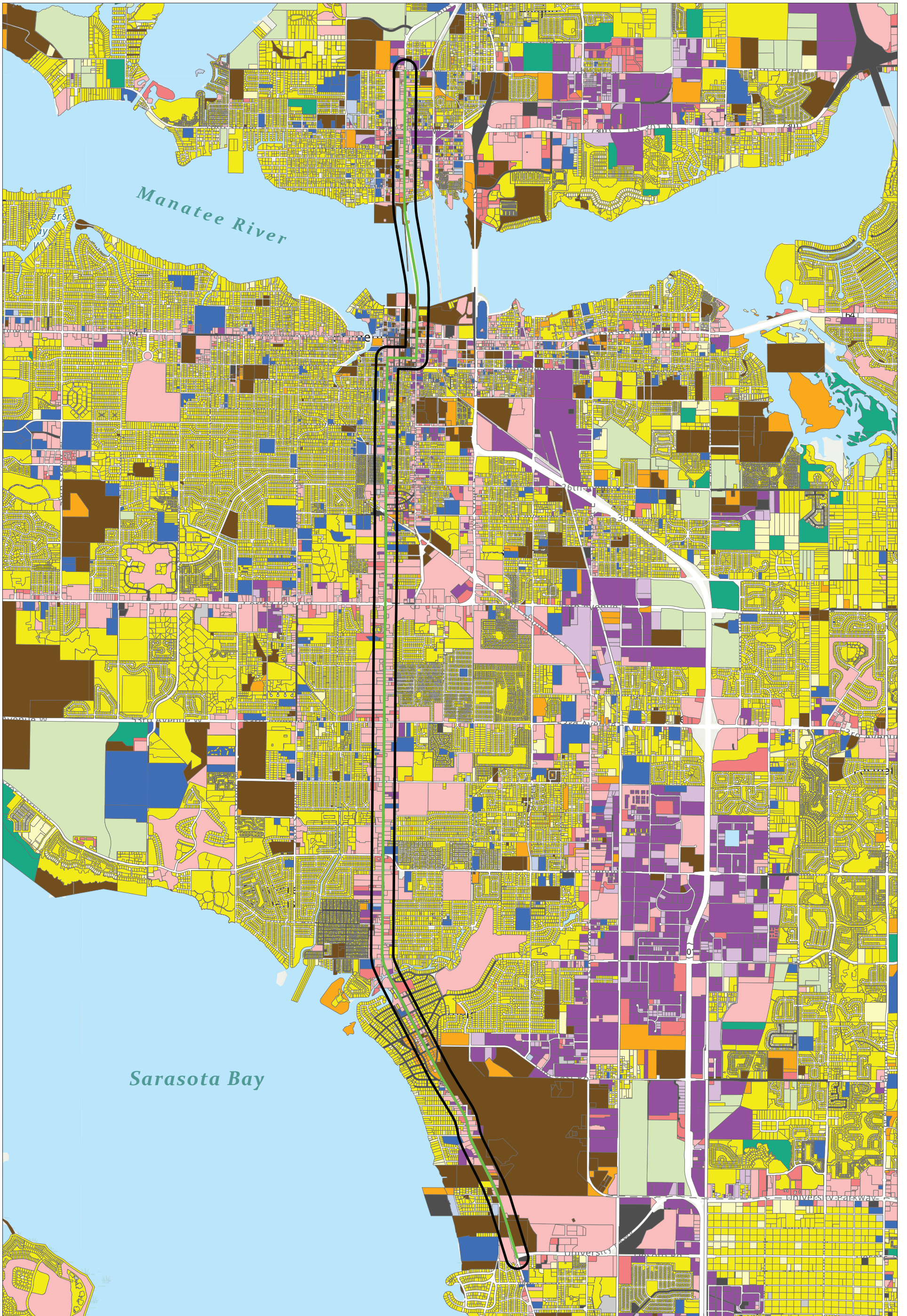
Existing Land Uses along the US 41 study corridor and surrounding area were derived from the *Florida Parcel Data Statewide – 2022* dataset, published by the Florida Department of Revenue in cooperation with the Manatee County Property Appraiser and the Sarasota County Property Appraiser. This dataset was collected from the Florida Geographic Data Library (FGDL) in July 2023. Using the Department of Revenue (DOR) codes provided in the dataset, general land uses were identified for each parcel in the region within 500 feet of the US 41 study area. Each general land use is summarized in **Table 9** and illustrated in **Figure 18**.

*Table 9: Existing Land Use within 500 feet*

Existing Land Use (500' Buffer)	Parcels	Acres	Percent
Residential	1,192	767.96	44.7%
Commercial	540	527.98	30.8%
Governmental	56	127.13	7.4%
Industrial	62	56.16	3.3%
Institutional	49	37.94	2.2%
Utilities, Rights-of-Way, Sewage	26	72.85	4.2%
Parcels with No Value	2	3.51	0.2%
Vacant Residential	102	23.56	1.4%
Vacant Commercial	130	62.50	3.6%
Vacant Governmental	96	32.19	1.9%
Vacant Institutional	5	4.76	0.3%
<b>Total</b>	<b>2,260</b>	<b>1,716.537</b>	<b>100.0%</b>

Land uses along the US 41 corridor are primarily comprised of Commercial (30.8%) properties fronting the roadway with Residential (44.7%) properties situated behind. Other significant land uses include Governmental (7.4%), Industrial (3.3%), and Institutional (2.2%). Vacant properties make up approximately 7.2% of all land uses within 500 feet of the US 41 study area.





**Figure 18**  
**Existing Land Use**  
Sarasota/Manatee US 41 CMASS

## 6.2 Future Land Use/Growth

Future Land Uses along the US 41 study corridor and surrounding area were collected from the statewide *Florida Level 2 Future Land Use 2020 – April 2022* dataset published by the University of Florida GeoPlan Center, derived from local government documentation in 2020. Generalized future land uses within 500 feet of the US 41 study corridor is summarized in **Table 10** and illustrated in **Figure 19**. Nearly 47% of the study corridor is comprised of Mixed Use – General future land use. Other major future land uses include Institutional/Governmental (15.9%), Mixed Use – Downtown/Urban Core (9.6%), Mixed Use – Neighborhood/Activity Center (7%), and Residential High (4.5%).

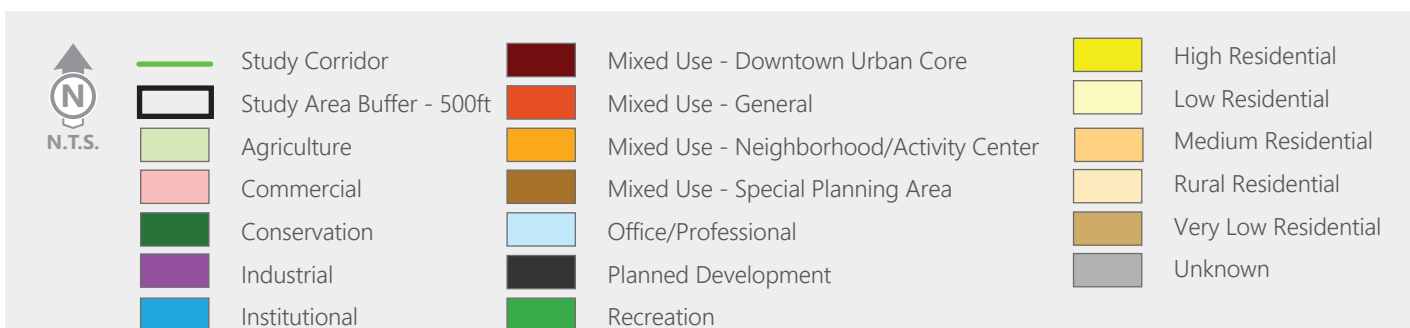
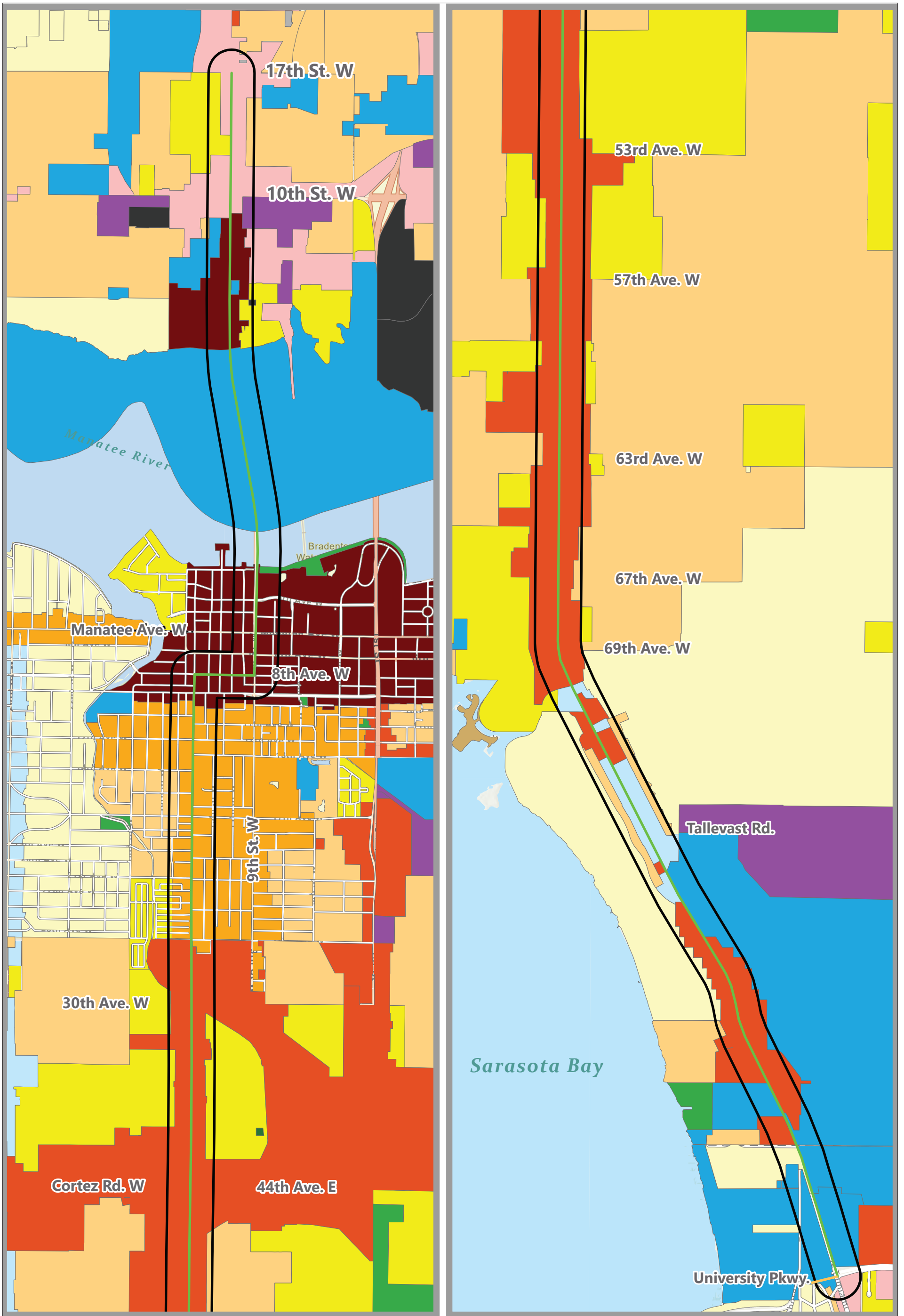
*Table 10: Future Land use within 500 feet*

General Future Land Use (500' buffer)	Acres	Percent
Commercial	65.13	5.3%
Industrial	3.74	0.3%
Institutional / Governmental	196.19	15.9%
Mixed Use - Downtown / Urban Core	118.35	9.6%
Mixed Use - General	573.57	46.6%
Mixed Use - Neighborhood / Activity Center	85.68	7.0%
Office / Professional	45.82	3.7%
Planned Development	0.23	0.0%
Recreation	1.41	0.1%
Residential High (> 12 DU/AC)	55.09	4.5%
Residential Low (2.01 - 5 DU/AC)	37.40	3.0%
Residential Medium (5.01 - 12 DU/AC)	47.98	3.9%
Unknown	0.22	0.02%
<b>Total</b>	<b>1,230.82</b>	<b>100%</b>

At the southern end of the US 41 study corridor, within the City of Sarasota and unincorporated Sarasota County, the primary future land use is Institutional, derived from the New College of Florida and the Sarasota Bradenton International Airport. From the Manatee County Line to 69<sup>th</sup> Avenue, the primary future land uses include Mixed Use – General and Office/Professional, with some Institutional and Residential Low Density future land uses.

From 69<sup>th</sup> Avenue to 26<sup>th</sup> Avenue, the primary future land use is Mixed Use – General, surrounded by Residential High Density and Residential Medium Density beyond the 500-foot buffer. Within the City of Bradenton, Mixed Use – Neighborhood/Activity Center and Mixed Use – Downtown/Urban Core are the primary future land uses within 500 feet of the US 41 study corridor.

In the City of Palmetto, Mixed Use – Downtown/Urban Core and Commercial are the primary future land uses within 500 feet of US 41, with some Residential High Density uses as well. Industrial future land uses are just outside the 500-foot buffer.



**Figure 19**  
**Future Land Use**  
 Sarasota/Manatee US 41 CMASS

### 6.3 Development Patterns

The land surrounding US 41 is almost entirely built out, as such there are eight development projects currently in progress along the study corridor. Of these four, the most significant is Orange on 14<sup>th</sup>, a 174-unit apartment complex located in the southwest quadrant of US 41 and 37<sup>th</sup> Avenue West, for which the building permit applications are currently in the review process. A summary of the current development projects is provided in **Table 11**.

Table 11: Current Planned Developments along US 41

Project	Project Type	Location	Acreage	Status
<b>Sarasota County</b>				
SRQ West Commercial Apron and Employee Parking Lot Relocation and Improvements	Airport Parking Lot Improvements	East side of US 41 between General Spaatz Boulevard and Edwards Drive	5.95	Construction in Progress
<b>Manatee County</b>				
Bayshore Gardens Free Standing Emergency Room (FSER)	Free-Standing Emergency Department (Medical)	East side of US 41 just south of Bayshore Gardens Parkway	1.83	Final Site Plans Approved No Building Permit Application submitted (as of 11/2023)
Tommy's Car Wash	Car Wash	Southeast quadrant of US 41 and 53 <sup>rd</sup> Avenue West	1.10	Construction Complete – Business scheduled to open 11/17/2023
Orange On 14 <sup>th</sup>	174-Unit Apartments	Southwest quadrant of US 41 and 37 <sup>th</sup> Avenue West	5.26	Final Site Plans Approved Building Permit Applications are in Review Process (as of 11/2023)
Unknown <sup>1</sup>	Unknown	Southwest quadrant of US 41 and 19 <sup>th</sup> Avenue West	0.52	Construction in Progress
The Metropolitan	Apartments	Southeast quadrant of US 41 and 13 <sup>th</sup> Avenue West	3.15	Construction in Progress
Riverview 6 – Downtown Bradenton	Commercial Retail	Southeast quadrant of 9 <sup>th</sup> Street West and 6 <sup>th</sup> Avenue W	Unknown	Construction in Progress
Unknown <sup>1</sup>	Commercial	Southwest quadrant of 9 <sup>th</sup> Street West and 4 <sup>th</sup> Avenue Drive West	.66	Construction in Progress

Sources:

Sarasota: Sarasota County GIS Site And Development Area Shapefile (11/3/2023)

Manatee: Manatee County GIS Land Development Portal (11/3/2023)

<sup>1</sup>Unknown developments were observed during field review but no additional information on the project name or type was found



There is one active Development of Regional Impact (DRI) within one mile of the study corridor; The Sarasota Bradenton International Airport (SRQ). This development is mostly built out, but does have some ongoing development in the outparcels. The outparcel development related to this project is detailed in **Table 12**. In the latest ordinance issued by Manatee County for SRQ (ORD #21-05), the airport is also approved to add an 800-space parking garage by 6/7/2026. However, no site plans or building permit applications for said parking garage have been submitted to the county as of November, 2023.

Table 12: SRQ Outparcel Development Summary

Outparcel Number	Location	Size	Development	Anticipated Build-Out Date
4	Southeast quadrant of US 41 and Braden Avenue	11.07 acres	Hotel, Office, and Auto-Sales	6/7/2026

Source: Manatee County Ordinance #21-05 – SRQ Airport

### 6.4 Zoning

The zoning data for the US 41 study corridor was obtained from the following sources:

- Sarasota County GIS
- Manatee County GIS
- The City of Sarasota GIS
- The City of Bradenton GIS
- The City of Palmetto GIS

At the southern end of the study corridor, from University Parkway to just south of the Sarasota/Manatee County line, the three primary zoning types are Governmental, Medical Charitable Institutional, and Airport Authority. Throughout the majority of the study corridor, from just south of the Sarasota/Manatee County line to north of 30<sup>th</sup> Avenue West, the predominant zoning surrounding US 41 is for General Commercial. The one exception through this segment is from Scott Avenue to Magellan Drive, where the primary zoning is for Professional Office and various Residential zonings. At the north end of the study area where the study corridor passes through downtown Bradenton and downtown Palmetto, the primary zoning is for Urban Center/Core and Commercial Core; once the study corridor passes the Palmetto downtown area the primary zoning returns to Business/Light Commercial. The zoning along US 41 through the study area is illustrated in **Figure 20**.





US 41 - Study Area 500ft Buffer

### Palmetto City Zoning

- Public
- Residential - Single Family Detached
- Residential - Single Family and Duplex
- Residential - Multi-Family
- Residential - Mobile Home Park
- Planned Development - Housing
- Commercial Neighborhood
- General Office
- Commercial Core
- Business/Light Commercial
- Heavy Commercial/Light Industrial
- Rezoned

### Bradenton City Zoning

- Residential-Moderate Density
- Residential-Mobile Home
- Urban Village
- General Urban Open
- General Urban Restricted
- Urban Center
- Urban Core

### Manatee County Zoning

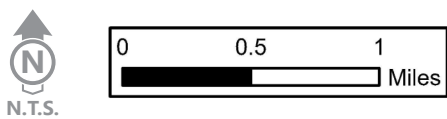
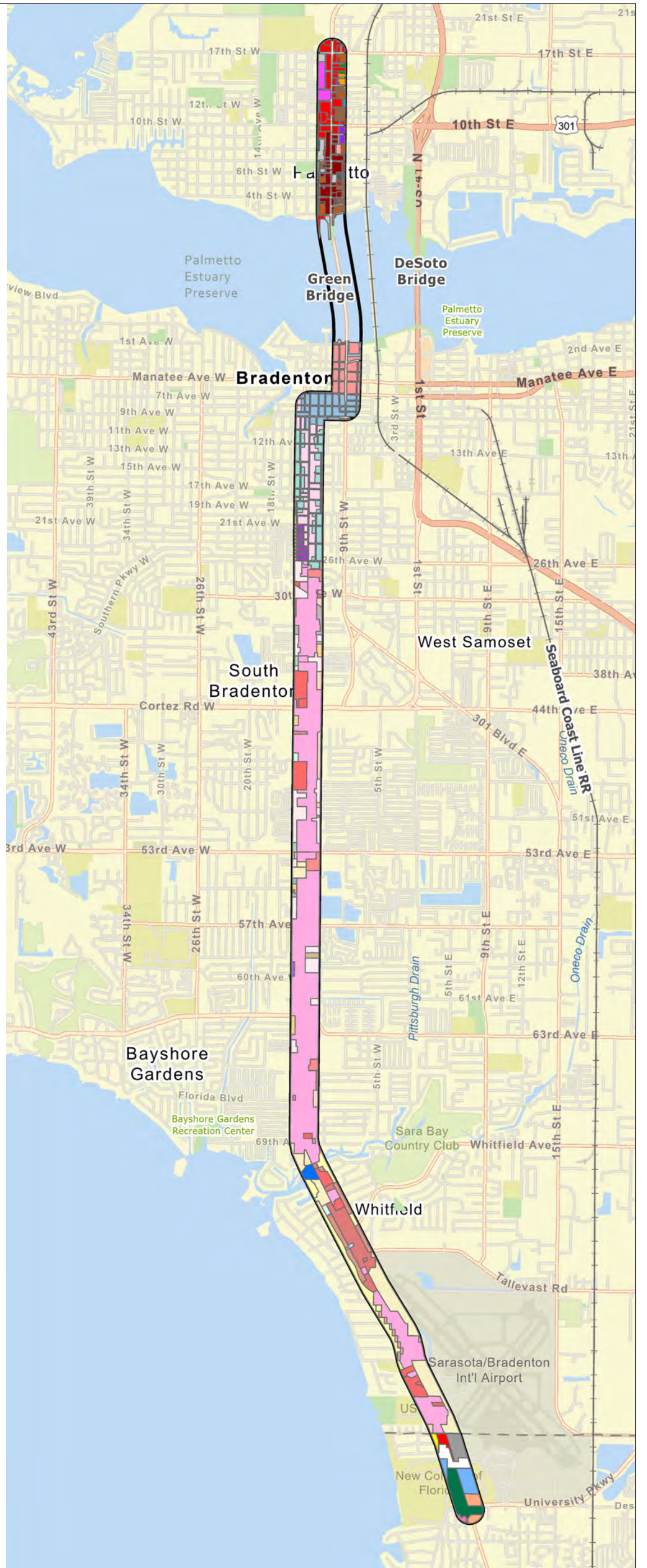
- General Commercial
- Heavy Commercial
- Neighborhood Commercial - Medium
- Planned Development - Commercial
- Planned Development - Multi-Use
- Planned Development - Office
- Planned Development - Residential
- Planned Development - Waterfront
- Professional Office - Medium
- Professional Office - Small
- Residential Duplex - 3
- Residential Duplex - 4.5
- Residential Duplex - 6
- Residential Multi-Family - 9
- Residential Multi-Family - 16
- Residential Single Family - 2
- Residential Single Family - 3
- Residential Single Family - 4.5
- Residential Single Family - 6
- Residential Mobile Home - 6

### Sarasota County Zoning

- Airport Authority
- Commercial General
- Residential Single Family 1
- Sarasota

### Sarasota City Zoning

- Governmental
- Medical Charitable Institutional
- North Trail
- Residential Single Family 1
- Residential Single Family 2
- Residential Single Family 3



**Figure 20**  
**Study Area Zoning**  
 Sarasota/Manatee US 41 CMASS



## 6.5 Schools and Public Facilities

Primary, secondary, and post-secondary schools were identified within one (1) mile of the US 41 study corridor. Data was gathered from two datasets: *School Facilities (Public and Post-Secondary) Boundaries in Florida – 2023*, and *School Facilities (Private) Boundaries in Florida – 2017*, published by the University of Florida (UF) GeoPlan Center on FGDL. **Table 13** identifies the schools within one mile of the US 41 study area.

*Table 13: Schools within One Mile of US 41 Study Area*

School	Type	Operation
Sun Academy	Elementary	Private
Rogers Garden-Bullock Elementary	Elementary	Public
Ballard Elementary School	Elementary	Public
Bayshore Elementary School	Elementary	Public
Palmetto Elementary School	Elementary	Public
Manatee Community Action Agency - Head Start	Elementary	Public
Center Montessori School	Elementary and Secondary	Private
Saint Joseph Catholic School	Elementary and Secondary	Private
The Broach School	Elementary and Secondary	Private
Peace Lutheran School	Elementary and Secondary	Private
Providence Community School	Elementary and Secondary	Private
The Broach School - Bradenton	Elementary and Secondary	Private
Queen of Angels Academy	Elementary and Secondary	Private
New Path Academy	Elementary and Secondary	Private
The ABA Academy	Elementary and Secondary	Private
Next Generation Academics	Elementary and Secondary	Private
Palmetto Charter School	Elementary and Secondary	Charter
Team Success - A School of Excellence	Elementary and Secondary	Charter
EasterSeals	Elementary and Secondary	Public
Hospital Homebound Instruction	Elementary and Secondary	Public
Manatee Virtual School	Elementary and Secondary	Public
Lincoln Memorial Middle School	Secondary (Middle)	Charter
Just for Girls Middle School	Secondary (Middle)	Public
Rowlett Middle Academy	Secondary (Middle)	Charter
Pace Center for Girls	Secondary (High School)	Public
Palmetto High School	Secondary (High School)	Public
State College of Florida Collegiate School	Secondary (High School)	Charter
East West College of Natural Medicine	College / University	Private
University of South Florida - Sarasota-Manatee Campus	College / University	State
New College of Florida	College / University	State
State College of Florida - Manatee-Sarasota Campus	College / University	Private





## 7. Demographics

Demographic data was gathered for Manatee and Sarasota Counties to determine the population characteristics of the study corridor and its surroundings. Data gathered includes population characteristics and socioeconomic data for block groups intersecting a 300-foot buffer surrounding the study corridor and was collected from the US Census Bureau 2021 American Community Survey (ACS) Five-Year Estimates. The ACS identified 19,103 households and a population of 47,439 people within the study area, which includes 36 intersecting block groups.

An overview of the demographics in Manatee and Sarasota County and the study area block groups is provided in **Table 14**.

*Table 14: Demographics Overview*

Category	Measure		
	Manatee County	Sarasota County	Study Area
<b>Population</b>			
Total Population	394,824	429,336	47,439
<b>Households</b>			
Total Households	156,254	193,923	19,103
Average Household Size <sup>2</sup>	2.49	2.18	2.41
<b>Age</b>			
Median Age	50 years	56 years	48 years
Population 65 years and older	27.48%	36.5%	24.5%
<b>Sex</b>			
Male	48.6%	47.9%	49.7%
Female	51.4%	52.1%	50.3%
<b>Race</b>			
White	80.80%	87.95%	78.10%
Black or African American	8.29%	4.35%	11.30%
American Indian and Alaska Native	0.25%	0.24%	0.17%
Asian	2.17%	1.78%	0.74%
Native Hawaiian and Other Pacific Islander	0.08%	0.03%	0.07%
Two or More Races	5.64%	1.31%	6.25%
Other	2.78%	4.34%	3.36%
<b>Ethnicity</b>			
Hispanic or Latino (of any race)	16.74%	9.68%	25.76%
Not Hispanic or Latino	83.26%	90.32%	74.24%
<b>Income</b>			
Median Household Income	\$69,736	\$74,599	\$49,074
Persons Below Poverty	10.36%	8.83%	23.18%
<b>Housing</b>			
Total Housing Units	202,950	249,418	25,257
Owner-Occupied	57.43%	59.00%	47.68%
Renter-Occupied	19.56%	18.75%	27.95%
Vacant	23.0%	22.2%	24.4%
<b>Limited English Proficiency (LEP)</b>			





Category	Measure		
	Manatee County	Sarasota County	Study Area
Percent of Spanish LEP surrounding study area	1.7%	1.4%	3.6%
<b>Vehicle Ownership</b>			
Households with No Vehicles Available	4.0%	4.6%	7.51%
<b>Employment for Civilians 16 and Older</b>			
Employed	94.52%	96.42%	90.55%
Unemployed	5.48%	3.58%	9.45%

Source: ACS 2021 Block Group Data, <sup>2</sup>ACS 2021 Census Tract Data

### 7.1 Age

The median age of persons in the study area is 48 years, with persons aged 65 and older comprising approximately 24.5% of the population, which is lower than both Manatee and Sarasota Counties. Approximately 56% of the population falls between the ages of 18 and 64, which is higher than Manatee and Sarasota Counties.

### 7.2 Race and Ethnicity

The majority race within the study area is White (78.1%), followed by Black or African American (11.3%), and Two or More Races (6.25%). The majority race for Manatee County is White (80.8%), followed by Black or African American (8.29%), and Two or More Races (5.64%). In Sarasota County, the majority race is White (87.95%), followed by Black or African American (4.35%), and Two or More Races (1.31%). The remaining 4.34% of people in the study area identify as Some Other Race (3.36%), Asian (0.74%), American Indian and Alaska Native (0.17%), and Native Hawaiian and Other Pacific Islander (0.07%). In total, 25.76% of the population in the study area are Hispanic.

### 7.3 Income

The study area has a median household income of \$49,074, with 23.18% of households below the federal poverty line. This is higher than in Manatee County, where 10.36% of the population fall under the federal poverty line, and Sarasota County, where 8.83% of the population fall under the federal poverty line.

### 7.4 LEP

Demographics acquired for the study area determined the Limited English Proficiency (LEP) populations do not meet the Department of Justice’s (DOJ’s) Safe Harbor Provision threshold for written translations (minimum LEP population of 1,000 persons or 5% of the area population). Accommodations will not likely be required as only 3.6% of the population within the study area speak “English less than very well”.

### 7.5 Education

For the population between the ages of 18 and 24, 33.4% of the population within the study area have at least a high school diploma (or equivalent), which is slightly lower than Manatee and Sarasota





Counties. Between the ages of 18 and 24, 7.3% of the population have a bachelor's degree or higher, which is lower than Manatee and Sarasota Counties. For the population aged 25 and older, 34.6% have at least a high school diploma (or equivalent), and 21.3% have a bachelor's degree or higher.

### 7.6 Employment

Approximately 90.6% of the study area population aged 16 years and older are in the civilian labor force, which is slightly lower than Manatee and Sarasota Counties.

### 7.7 Vehicles per Household

Within the study area, there are approximately 1.62 vehicles per household. About 7.51% of households have no vehicle available. Overall, there are less vehicles per household and a slightly higher percentage of households with no vehicle available when compared to Manatee and Sarasota Counties.

## 8. Traffic Data

This section presents the Annual Average Daily Traffic (AADT), peak hour volumes, intersection and roadway Level of Service (LOS) for US 41 and study side streets as part of Sarasota/Manatee US 41 CMASS.

### 8.1 Existing Traffic Conditions and Design Characteristics

**Table 15** shows the existing year (2022) AADT, peak hour flow to daily ratio (K factor), directional split (D factor), and daily truck percentages (T-daily factor) for the US 41 study corridor taken from the latest Florida Traffic Online (FTO) information (included in **Appendix B**).

Volume to Capacity (V/C) ratios were calculated utilizing roadway segment capacities based on guidelines found in the latest 2023 FDOT Multimodal Quality/Level of Service (Q/LOS) handbook and obtained from the latest D1 LOS Update spreadsheet (**Appendix B**). All roadway segments along US 41 were found to be operating with an acceptable V/C ratio with the exception of the segment from SR 64/Manatee Avenue to Riverside Drive.

The historical K factors, D factors, and T-daily factors collected from the relevant historical AADT reports can be found in **Table 16**, **Table 17** and **Table 18**, respectively. The measured K factors with available synopsis reports are also included in **Table 16**. The average of all available measured K factors is 8.7%.





Table 15: 2022 AADT and Traffic Factors

Roadway / Segment	CoSite/ Source	FTO 2022 AADT Data				Capacity* (Daily)	Volume/ Capacity Ratio (Daily)
		AADT	"K"	"D"	"T <sub>Daily</sub> "		
<b>US 41 (South to North)</b>							
University Parkway to Sarasota-Manatee County Line	170001	39,000	9.0%	52.8%	3.0%	47,600	0.82
Sarasota-Manatee County Line to Tallevast Road/Gaines Avenue	135035	38,500	9.0%	54.8%	3.0%	56,800	0.68
Tallevast Road/Gaines Avenue to 63rd Avenue West	135079	43,000	9.0%	54.8%	3.2%	55,000	0.78
63rd Avenue West to 53rd Avenue West/SR 70	135037	43,500	9.0%	54.8%	3.0%	55,100	0.79
53rd Avenue West/SR 70 to North of SR 70	135038	44,500	9.0%	54.8%	2.8%	55,100	0.81
SR 684/Cortez Road to 30th Avenue West	130003	26,500	9.0%	54.8%	3.2%	37,900	0.70
30th Avenue West to 26th Avenue West	135078	18,900	9.0%	54.8%	2.9%	36,100	0.52
26th Avenue West to 9th Avenue West/MLK Jr Avenue West	135001	19,000	9.0%	54.8%	2.6%	27,100	0.70
9th Avenue West/MLK Jr Avenue West to 10th Street West	135014	11,600	9.0%	54.8%	2.8%	26,300	0.44
10th Street West to SR 64/Manatee Avenue	135017	25,000	9.0%	54.8%	3.2%	33,800	0.74
SR 64/Manatee Avenue to Riverside Drive	130009	39,000	9.0%	54.8%	4.1%	38,400	1.02
Riverside Drive to 7th Street West	135002	31,500	9.0%	54.8%	4.1%	34,300	0.92
7th Street West to 10th Street West	135003	30,500	9.0%	54.8%	4.1%	34,300	0.89
10th Street West to 17th Street West	135004	28,500	9.0%	54.8%	3.6%	34,300	0.83
<b>Side Streets</b>							
University Parkway, East of US 41	TMC - 5/17/23	17,500	-	-	-	40,500	0.43
University Parkway, West of US 41	TMC - 5/17/23	2,500	-	-	-	-	-
Bayshore Gardens Parkway, East of US 41	TMC - 5/16/23	13,500	-	-	-	-	-
Bayshore Gardens Parkway, West of US 41	TMC - 5/16/23	18,500	-	-	-	-	-
SR 70/53rd Avenue W, East of US 41	TMC - 5/16/23	23,000	-	-	-	37,200	0.62
SR 70/53rd Avenue W, West of US 41	TMC - 5/16/23	22,500	-	-	-	-	-
SR 684/Cortez Road, East of US 41	TMC - 5/17/23	35,500	-	-	-	56,800	0.63
SR 684/Cortez Road, West of US 41	TMC - 5/17/23	39,500	-	-	-	56,800	0.70
SR 64/Manatee Avenue, East of US 41	TMC - 5/10/23	18,000	-	-	-	34,000	0.53
SR 64/Manatee Avenue, West of US 41	TMC - 5/10/23	19,500	-	-	-	35,800	0.54
10th Street W, East of US 41	TMC - 5/10/23	11,500	-	-	-	36,100	0.32
10th Street W, West of US 41	TMC - 5/10/23	9,600	-	-	-	-	-

\*Capacities were computed based on guidelines from the FDOT Multimodal Q/LOS Handbook - accounting for factors including Context Classification, number of through lanes, turn lanes, presence of median, signal density, and posted speed limits





Table 16: Existing and Historical K Factors

Roadway / Segment	Context Classification	2016 - 2022	Recommended Standard K Factor Range**	Measure K from available Synopsis Reports***
<b>US 41 (South to North)</b>				
University Parkway to Sarasota-Manatee County Line	C3C	9.0%	7.5% - 9.5%	9.0%
Sarasota-Manatee County Line to Tallevast Road/Gaines Avenue	C3C	9.0%	7.5% - 9.5%	
Tallevast Road/Gaines Avenue to 63rd Avenue West	C3C/C3R	9.0%	7.5% - 9.5%	9.0%
63rd Avenue West to 53rd Avenue West/SR 70	C3R	9.0%	7.5% - 9.5%	
53rd Avenue West/SR 70 to North of SR 70	C3R	9.0%	7.5% - 9.5%	7.8%
SR 684/Cortez Road to 30th Avenue West	C4	9.0%	7.5% - 9.5%	8.8%
30th Avenue West to 26th Avenue West	C4	9.0%	7.5% - 9.5%	
26th Avenue West to 9th Avenue West/MLK Jr Avenue West	C4	9.0%	7.5% - 9.5%	8.6%
9th Avenue West/MLK Jr Avenue West to 10th Street West	C4	9.0%	7.5% - 9.5%	
10th Street West to SR 64/Manatee Avenue	C4	9.0%	7.5% - 9.5%	
SR 64/Manatee Avenue to Riverside Drive	C3C	9.0%	7.5% - 9.5%	9.2%
Riverside Drive to 7th Street West	C4	9.0%	7.5% - 9.5%	
7th Street West to 10th Street West	C4	9.0%	7.5% - 9.5%	
10th Street West to 17th Street West	C4	9.0%	7.5% - 9.5%	8.5%

\*Historical K Factors obtained from FTO historical data

\*\*Obtained from Table 3 of 2023 Multimodal Q/LOS Handbook

\*\*\*Average Measured K factor from available Synopsis Reports is 8.7%





Table 17: Existing and Historical D Factors

Roadway / Segment	D Factor*						
	2016	2017	2018	2019	2020	2021	2022
<b>US 41 (South to North)</b>							
University Parkway to Sarasota-Manatee County Line	52.6%	52.6%	52.5%	52.2%	52.2%	52.6%	52.8%
Sarasota-Manatee County Line to Tallevast Road/Gaines Avenue	56.2%	56.0%	55.6%	55.8%	52.6%	54.0%	54.8%
Tallevast Road/Gaines Avenue to 63rd Avenue West	56.2%	56.0%	55.6%	55.8%	52.6%	54.0%	54.8%
63rd Avenue West to 53rd Avenue West/SR 70	56.2%	56.0%	55.6%	55.8%	52.6%	54.0%	54.8%
53rd Avenue West/SR 70 to North of SR 70	56.2%	56.0%	55.6%	55.8%	52.6%	54.0%	54.8%
SR 684/Cortez Road to 30th Avenue West	56.2%	56.0%	55.6%	55.8%	52.6%	54.0%	54.8%
30th Avenue West to 26th Avenue West	56.2%	56.0%	55.6%	55.8%	52.6%	54.0%	54.8%
26th Avenue West to 9th Avenue West/MLK Jr Avenue West	56.2%	56.0%	55.6%	55.8%	52.6%	54.0%	54.8%
9th Avenue West/MLK Jr Avenue West to 10th Street West	56.2%	56.0%	55.6%	55.8%	52.6%	54.0%	54.8%
10th Street West to SR 64/Manatee Avenue	56.2%	56.0%	55.6%	55.8%	52.6%	54.0%	54.8%
SR 64/Manatee Avenue to Riverside Drive	56.2%	56.0%	55.6%	55.8%	52.6%	54.0%	54.8%
Riverside Drive to 7th Street West	56.2%	56.0%	55.6%	55.8%	52.6%	54.0%	54.8%
7th Street West to 10th Street West	56.2%	56.0%	55.6%	55.8%	52.6%	54.0%	54.8%
10th Street West to 17th Street West	56.2%	56.0%	55.6%	55.8%	52.6%	54.0%	54.8%

\*Historical D Factors obtained from FTO historical data





Table 18: Existing and Historical T-Daily Factors

Roadway / Segment	T Factor*						
	2016	2017	2018	2019	2020	2021	2022
<b>US 41 (South to North)</b>							
University Parkway to Sarasota-Manatee County Line	2.9%	2.8%	3.0%	3.1%	3.7%	3.2%	3.0%
Sarasota-Manatee County Line to Tallevast Road/Gaines Avenue	2.8%	2.9%	3.1%	3.1%	3.6%	3.3%	3.0%
Tallevast Road/Gaines Avenue to 63rd Avenue West	3.0%	3.0%	3.1%	3.1%	3.6%	3.3%	3.2%
63rd Avenue West to 53rd Avenue West/SR 70	2.6%	2.9%	3.4%	3.1%	3.4%	3.0%	3.0%
53rd Avenue West/SR 70 to North of SR 70	2.6%	3.0%	2.9%	3.2%	3.7%	3.5%	2.8%
SR 684/Cortez Road to 30th Avenue West	3.4%	3.4%	2.9%	2.9%	3.4%	3.0%	3.2%
30th Avenue West to 26th Avenue West	2.9%	2.5%	2.9%	2.5%	3.7%	2.9%	2.9%
26th Avenue West to 9th Avenue West/MLK Jr Avenue West	3.0%	2.2%	2.7%	2.4%	3.0%	2.9%	2.6%
9th Avenue West/MLK Jr Avenue West to 10th Street West	2.0%	2.4%	2.9%	3.5%	3.5%	2.8%	2.8%
10th Street West to SR 64/Manatee Avenue	3.2%	2.9%	3.2%	3.3%	3.5%	3.2%	3.2%
SR 64/Manatee Avenue to Riverside Drive	3.8%	3.2%	3.0%	3.6%	4.0%	3.5%	4.1%
Riverside Drive to 7th Street West	2.6%	2.6%	3.0%	3.6%	4.0%	3.5%	4.1%
7th Street West to 10th Street West	2.4%	2.4%	3.0%	3.6%	4.0%	3.5%	4.1%
10th Street West to 17th Street West	3.3%	3.4%	4.2%	3.4%	4.9%	4.0%	4.1%

\*Historical T Factors obtained from FTO historical data

As shown in **Table 16**, the average of measured K factors from the available Synopsis Reports was 8.7%, and based on Context classification, the K factor can be between 7.5% – 9.5%. As such, an average of the K factors, 8.5%, is recommended for the study corridor. Based on the latest historical D factors shown in **Table 17**, a D factor of 53% will be used for Sarasota County, and a D factor of 55% will be used for Manatee County. Based on the latest FTO historical data, a 3% T factor will be used for US 41 between University Parkway and Manatee Avenue, and a 4% T factor will be used for US 41 between Manatee Avenue at 17<sup>th</sup> Street W.

## 8.2 Traffic Volume Data – Turning Movement Counts

The 8-hour Turning Movement Counts (TMCs) (7-9 AM, 12-2 PM and 3-7 PM) provided by FDOT are listed in **Table 19**. These counts were collected between May and July 2023. Further, the TMCs were seasonally adjusted using the factors from the 2022 FTO. The raw counts are provided in **Appendix B**.

Table 19: 8-Hour TMC Roadway Intersections

Roadway/Segments along US 41	
US 41 at University Parkway/Ringling Plaza	US 41 at College Drive/General Spatz Boulevard
US 41 at Braden Avenue	US 41 at Tallevast Road/Gaines Avenue
US 41 at Whitfield Avenue	US 41 at Pearl Avenue
US 41 at 69th Avenue W/Bay Drive	US 41 at Florida Boulevard
US 41 & Pine Street N	US 41 at Bayshore Gardens Parkway
US 41 at 60th Avenue W	US 41 at 57th Avenue W
US 41 at 53rd Avenue W	US 41 at 49th Drive W
US 41 at Orlando Avenue/46th Avenue Drive W	US 41 & Business Driveway
US 41 at 44th Avenue W/Cortez Road W	USB 41 at 39th Avenue W
USB 41 & 35th Avenue W	USB 41 & 34th Avenue W
USB 41 & 33rd Avenue W	USB 41 at 30th Avenue W
USB 41 (14th St W) at 29th Avenue W Turnout (NB to EB)	USB 41 (14th St W) & 29th Avenue W
USB 41 & 26th Avenue W	USB 41 at 21st Avenue W
USB 41 & 18th Avenue W (South)	USB 41 & 18th Avenue W (North)
USB 41 & 17th Avenue Drive W	USB 41 at 17th Avenue W
USB 41 at 12th Avenue W	USB 41 at 9th Avenue W
USB 41 & Ballard Park Drive	USB 41 at 8th Avenue W
8th Avenue W and 9th Street W	USB 41 & 7th Avenue W
9th Street W and 6th Avenue W	9th Street W and Manatee Avenue W
9th Street W and 3rd Avenue W	8th Avenue W and 5th Street W
USB 41 & Parkway Drive	8th Avenue W and 7th Street W
USB 41 & 8th Street W	USB 41 & 9th Street Drive W
8th Avenue W and 10th Street W	USB 41 & 10th Street Drive W
USB 41 & 13th Street W	8th Avenue W and 17th Street W/Memphis Road

### 8.3 Existing Multimodal Operations Analysis

Synchro 11 software was used to calculate existing intersection delay and LOS information for the 30 signalized intersections within the study corridor. HCM 6<sup>th</sup> Edition based delay and LOS information can be found in **Table 20** below. Synchro outputs can be found in **Appendix B**. The following signalized intersections exceed the targeted LOS D in the existing conditions, US 41 and University Parkway/Ringling Plaza, Bayshore Gardens Parkway, 53<sup>rd</sup> Avenue W, 44<sup>th</sup> Avenue W/Cortez Road W, and 6<sup>th</sup> Avenue W. Stop-controlled intersections along US 41 at Pine Street N, Business Driveway, 29<sup>th</sup> Avenue W, 26<sup>th</sup> Avenue W, 8<sup>th</sup> Street W and 13<sup>th</sup> Street W were found to exceed the target LOS for a minor street approach in the existing conditions. The stop-controlled intersection at US 41 and Pine Street N is found to exceed the target LOS for both a major and minor street approaches under the existing conditions.



Table 20: Existing Conditions Intersection Operational Analysis

Study Intersections along US 41	Control Type	Target LOS	Existing Year 2023			
			AM Peak Hour		PM Peak Hour	
			Delay (s)	LOS	Delay (s)	LOS
University Parkway/Ringling Plaza	Signalized	D	35.7	D	93.5	F
College Drive/General Spaatz Boulevard	Signalized	D	8.1	A	16.9	B
Braden Avenue	Signalized	D	15.4	B	8.2	A
Tallevast Road/Gaines Avenue	Signalized	D	15.4	B	40.9	D
Whitfield Avenue	Signalized	D	6.1	A	2.8	A
Pearl Avenue	Signalized	D	8.7	A	6.6	A
69th Avenue W/Bay Drive	Signalized	D	13.8	B	13.7	B
Florida Boulevard*	Signalized	D	28.8	C	30.0	C
Pine Street N	TWSC	D	15.3/138.4	C/F	45.7/109.3	E/F
Bayshore Gardens Parkway	Signalized	D	54.6	D	63.2	E
60th Avenue W	Signalized	D	23.3	C	20.4	C
57th Avenue W	Signalized	D	37.8	D	34.3	C
53rd Avenue W	Signalized	D	71.1	E	59.8	E
49th Drive W*	Signalized	D	8.4	A	4.6	A
Orlando Avenue/46th Avenue Drive W	Signalized	D	18.6	B	8.9	A
Business Driveway	TWSC	D	28.4/114.3	D/F	19.0/53.0	C/F
44th Avenue W/Cortez Road W	Signalized	D	58.5	E	71.0	E
39th Avenue W	Signalized	D	17.9	B	14.6	B
35th Avenue W	TWSC	D	9.5/24.8	A/C	9.9/31.1	A/D
34th Avenue W	TWSC	D	10.2/32.4	B/D	10.0/28.7	A/D
33rd Avenue W	TWSC	D	9.4/15.2	A/C	9.9/20.5	A/C
30th Avenue W	Signalized	D	37.4	D	36.5	D
29th Avenue W Turnout (NB to EB)	TWSC	D	-	-	-	-
29th Avenue W	TWSC	D	9.4/33.8	A/D	9.1/45.1	A/E
26th Avenue W	TWSC	D	8.9/20.8	A/C	9.6/40.3	A/E
21st Avenue W*	Signalized	D	18.6	B	17.9	B
18th Avenue W (South)	TWSC	D	9.0/17.9	A/C	9.5/20.9	A/C
18th Avenue W (North)	TWSC	D	8.9/11.0	A/B	9.1/12.7	A/B
17th Avenue Drive W	TWSC	D	8.8/10.3	A/B	9.0/19.1	A/C
17th Avenue W*	Signalized	D	18.1	B	20.1	C
12th Avenue W*	Signalized	D	6.8	A	10.8	B
9th Avenue W*	Signalized	D	19.5	B	28.6	C
Ballard Park Drive	TWSC	D	8.3/11.2	A/B	8.6/12.0	A/B
8th Avenue W	Signalized	D	21.6	C	36.5	D
9th Street W	Signalized	D	20.5	C	30.9	C
7th Avenue W	TWSC	D	9.0/11.2	A/B	10.1/12.1	B/B





Study Intersections along US 41	Control Type	Target LOS	Existing Year 2023			
			AM Peak Hour		PM Peak Hour	
			Delay (s)	LOS	Delay (s)	LOS
6th Avenue W	Signalized	D	30.7	C	58.3	E
Manatee Avenue W	Signalized	D	34.1	C	46.8	D
3rd Avenue W	Signalized	D	18.9	B	32.7	C
5th Street W*	Signalized	D	21.3	C	6.8	A
Parkway Drive	TWSC	D	14.9/19.6	B/C	10.3/15.5	B/C
7th Street W	Signalized	D	13.2	B	11.9	B
8th Street W	TWSC	D	9.4/29.5	A/D	13.0/39.3	B/E
9th Street Drive W	TWSC	D	11.4/14.2	B/B	10.1/12.2	B/B
10th Street W	Signalized	D	34.5	C	34.6	C
10th Street Drive W	TWSC	D	0.0/14.4	A/B	9.5/12.6	A/B
13th Street W	TWSC	D	10.5/37.8	B/E	12.9/152.5	B/F
17th Street W/Memphis Road	Signalized	D	45.7	D	32.3	C

Notes:

- 1) \*Synchro results utilized instead of HCM 6th due to signal timing incompatibility
- 2) TWSC – Two-way Stop Control
- 3) Overall delay and LOS are reported for signalized intersections, whereas worst-case main street/minor street delay and LOS are reported for TWSC intersections

### 8.4 Multimodal Segment Level of Traffic Stress (LTS) Analysis

The performance of alternative transportation modes, such as walking, cycling, and transit, were evaluated based on new guidelines from the latest 2023 FDOT Multimodal Q/LOS Handbook. New in 2023, the handbook introduces a new Measure of Effectiveness (MOE) for pedestrians and bicyclists called Level of Traffic Stress (LTS), which quantifies the amount of discomfort felt by travelers walking or cycling along a roadway. In general, LTS 1 indicates the highest comfort level, appropriate for all users, including, but not limited to, vulnerable users such as children, the elderly, and the handicapped. LTS 4 indicates the lowest comfort level, which is typically inaccessible for vulnerable users such as children or wheeled mobility device users with limitations in movement. Facilities at LTS 4 are most likely used only by users with limited route and mode choice.

The criteria for LTS determination include factors such as provision of pedestrian and bicyclist facilities, AADT on the adjacent roadway, and speed limits on the adjacent roadway. **Table 21, Table 22, Table 23,** and **Table 24** below demonstrate the LTS determination and criteria for pedestrians, bicyclists with no bike facilities, bicyclists with bike facilities, and transit, respectively. As shown in these tables, segments varied from LTS 1 to LTS 4 for the pedestrian analysis while all bicycle analysis resulted in the LTS 4, the lowest comfort levels, except for 2 segments, Riverside Drive to 7th Street West and 7th Street West to 10th Street West. The major contributing factors to the low scores are the lack of bicycle lanes, high AADTs, high posted speed limits, and lack of separation from the vehicular travel lanes.



Table 21: Existing Conditions Pedestrian Level of Traffic Stress (TLS) Analysis

Roadway/Segment	Segment Length (mi)	Continuous Sidewalk	Posted Speed	Separation from Vehicular Travel Lane	Vertical Separation	Level of Traffic Stress
University Parkway to Sarasota-Manatee County Line	0.55	✓	45	✗	✓	LTS 4
Sarasota-Manatee County Line to Tallevast Road/Gaines Avenue	1.39	✓	50	✓	✓	LTS 2
Tallevast Road/Gaines Avenue to Whitfield Avenue	0.19	✓	50	✓	✓	LTS 2
Whitfield Avenue to 63rd Avenue West	1.44	✓	45	✓	✓	LTS 2
63rd Avenue West to 60th Avenue West	0.74	✓	45	✓	✓	LTS 2
60th Avenue West to 53rd Avenue West/SR 70*	0.89	✓	45	✓	✓	LTS 2
53rd Avenue West/SR 70 to North of SR 70	0.11	✓	45	✓	✓	LTS 2
North of SR 70 to North of 45th Avenue Circle	0.74	✗	45	✓	✓	LTS 4
North of 45th Avenue Circle to SR 684/Cortez Road	0.16	✓	45	✗	✓	LTS 4
SR 684/Cortez Road to 30th Avenue West	0.79	✓	40	✗	✓	LTS 4
30th Avenue West to 26th Avenue West	0.23	✓	40	✗	✓	LTS 4
26th Avenue West to 9th Avenue West/MLK Jr Avenue West	1.01	✓	35	✓	✓	LTS 1
9th Avenue West/MLK Jr Avenue West to 10th Street West	0.28	✓	35	✗	✓	LTS 3
10th Street West to SR 64/Manatee Avenue	0.22	✓	35	✗	✓	LTS 3
SR 64/Manatee Avenue to Riverside Drive	1.17	✓	45	✓	✓	LTS 2
Riverside Drive to 7th Street West	0.37	✓	30	✗	✓	LTS 3
7th Street West to 10th Street West	0.25	✓	30	✗	✓	LTS 3
10th Street West to 17th Street West	0.51	✓	35	✗	✓	LTS 3

Source: 2023 Q/LOS Handbook - Pedestrian Level of Traffic Stress Flow Chart

Table 22: Existing Conditions Bicyclist LTS Analysis - No Facilities

Roadway/Segment	Segment Length (mi)	Posted Speed	AADT	Vehicular Travel Lanes	Posted Speed ≥ 35 mph	Travel Lanes ≥ 4 lanes	AADT ≥ 3,000	Level of Traffic Stress
University Parkway to Sarasota-Manatee County Line	0.55	45	39,000	2	✓	✗	✓	LTS 4
Whitfield Avenue to 63rd Avenue West	1.44	45	43,000	3	✓	✗	✓	LTS 4
63rd Avenue West to 60th Avenue West	0.74	45	43,500	3	✓	✗	✓	LTS 4
60th Avenue West to 53rd Avenue West/SR 70*	0.89	45	43,500	3	✓	✗	✓	LTS 4
53rd Avenue West/SR 70 to North of SR 70	0.11	45	44,500	3	✓	✗	✓	LTS 4
North of SR 70 to North of 45th Avenue Circle	0.74	45	44,500	3	✓	✗	✓	LTS 4
North of 45th Avenue Circle to SR 684/Cortez Road	0.16	45	44,500	3	✓	✗	✓	LTS 4
SR 684/Cortez Road to 30th Avenue West	0.79	40	26,500	2	✓	✗	✓	LTS 4
30th Avenue West to 26th Avenue West	0.23	40	18,900	2	✓	✗	✓	LTS 4
26th Avenue West to 9th Avenue West/MLK Jr Avenue West	1.01	35	19,000	2	✓	✗	✓	LTS 4
9th Avenue West/MLK Jr Avenue West to 10th Street West	0.28	35	11,600	1	✓	✗	✓	LTS 4
10th Street West to SR 64/Manatee Avenue	0.22	35	25,000	2	✓	✗	✓	LTS 4
SR 64/Manatee Avenue to Riverside Drive	1.17	45	39,000	2	✓	✗	✓	LTS 4
Riverside Drive to 7th Street West	0.37	30	31,500	2	✗	✗	✓	LTS 3
7th Street West to 10th Street West	0.25	30	30,500	2	✗	✗	✓	LTS 3
10th Street West to 17th Street West	0.51	35	28,500	2	✓	✗	✓	LTS 4

Source: 2023 Q/LOS Handbook - Bicycle Level of Traffic Stress Flow Chart to use When Bicycle Facility is Present

Note: The flowchart includes more considerations such as bicycle lane width, on-street parking, and buffering. However, LTS 4 is assigned to any segment with posted speeds exceeding 40 mph.

Table 23: Existing Conditions Bicyclist LTS Analysis - With Facilities

Roadway/Segment	Segment Length (mi)	Facility Type	Posted Speed	AADT	Posted Speed ≥ 40 mph	Level of Traffic Stress
Sarasota-Manatee County Line to Tallevast Road/Gaines Avenue	1.39	Bicycle Lane	50	38,500	✓	LTS 4
Tallevast Road/Gaines Avenue to Whitfield Avenue	0.19	Bicycle Lane	50	43,000	✓	LTS 4

Source: 2023 Q/LOS Handbook - Bicycle Level of Traffic Stress Flow Chart to use When No Bicycle Facility is Present or When There are Sharrows Present

Table 24: Existing Conditions Transit Level of Service Analysis

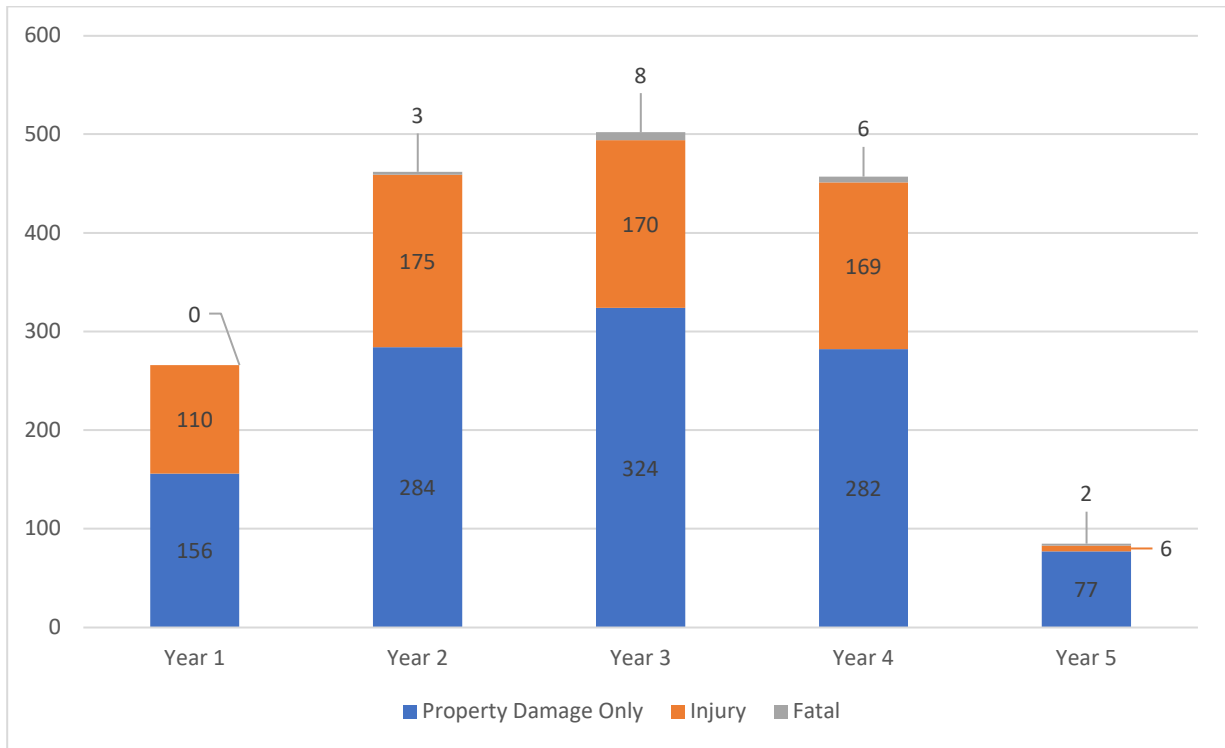
Roadway/Segment	Segment Length (mi)	Service Frequency (Vehicles/Hour)	Headway (Minutes)	Level of Service
University Parkway to Sarasota-Manatee County Line	0.55	1	20	E
Sarasota-Manatee County Line to Tallevast Road/Gaines Avenue	1.39	1	20	E
Tallevast Road/Gaines Avenue to Whitfield Avenue	0.19	1	20	E
Whitfield Avenue to 63rd Avenue West	1.44	2	20	D
63rd Avenue West to 60th Avenue West	0.74	2	20	D
60th Avenue West to 53rd Avenue West/SR 70*	0.89	2	20	D
53rd Avenue West/SR 70 to North of SR 70	0.11	1	20	E
North of SR 70 to North of 45th Avenue Circle	0.74	1	20	E
North of 45th Avenue Circle to SR 684/Cortez Road	0.16	1	20	E
SR 684/Cortez Road to 30th Avenue West	0.79	1	20	E
30th Avenue West to 26th Avenue West	0.23	2	20	D
26th Avenue West to 9th Avenue West/MLK Jr Avenue West	1.01	2	20	D
9th Avenue West/MLK Jr Avenue West to 10th Street West	0.28	8	<10	A
10th Street West to SR 64/Manatee Avenue	0.22	4	15	C
SR 64/Manatee Avenue to Riverside Drive	1.17	4	15	C
Riverside Drive to 7th Street West	0.37	4	15	C
7th Street West to 10th Street West	0.25	4	15	C
10th Street West to 17th Street West	0.51	4	15	C

Source: 2023 Q/LOS Handbook - Pedestrian Level of Traffic Stress Flow Chart

### 8.5 Safety and Crash Analysis

Crash analysis for the US 41 corridor was conducted for the most recent 4 years using data from University of Florida’s Signal 4 Analytics. The preliminary crash data shows 1,796 total crashes during the 4-year analysis period along the study corridor. **Figure 21** provides a general summary of the crash types by year. In total, the highest crash types are rear end (676), left turn (281), and sideswipe (271). Altogether, there were 19 fatal crashes, 654 injury crashes, and 1,123 property damage only crashes. Overall, there were 1,332 (74%) crashes that occurred in daytime lighting conditions and 463 (26%) crashes that occurred in nighttime lighting conditions. There were 1,682 (94%) crashes that occurred in dry roadway conditions and 114 (6%) crashes that occurred in wet or other roadway conditions. Year 3 had the highest overall number of crashes and fatal crashes.

Figure 21: Crash Summary Chart by Type and Severity in Previous 4 Years (2019-2023)



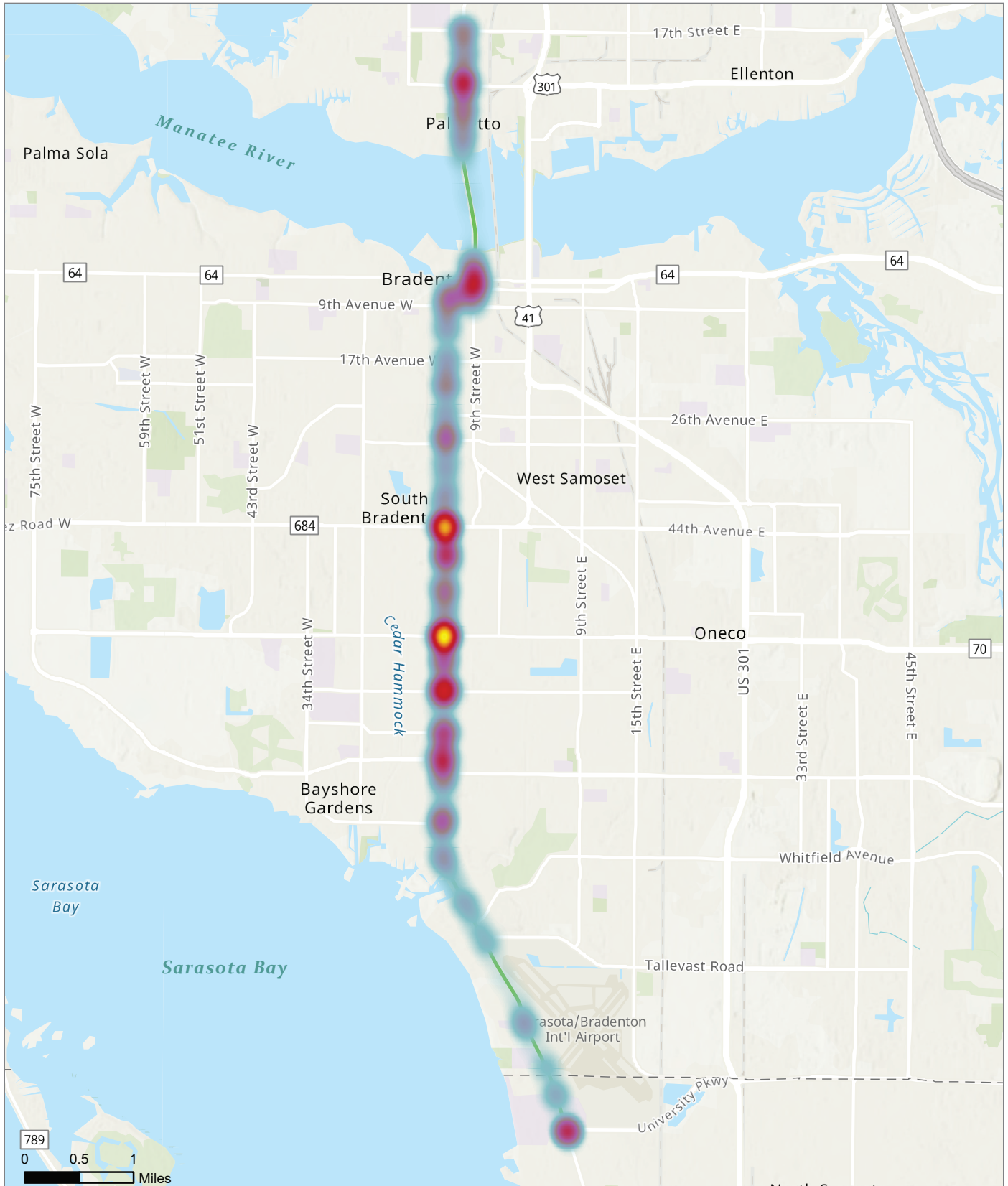
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






Figure 22 illustrates a heat map of all crash locations along US 41 between 2019 and 2023.





-  Study Corridor
-  Low Crash Frequency
-  High Crash Frequency

**Figure 22**  
**Crash Frequencies**  
Sarasota/Manatee US 41 CMASS

## 8.6 Overview of Pedestrian and Bicycle Crashes

There were a total of 79 pedestrian or bicycle crashes. Among the bicyclist and pedestrian crashes, 40 (50.6%) were bicyclist crashes and 39 (49.4%) were pedestrian crashes. The majority of pedestrian and bicycle crashes along the corridor occur predominately in three key hotspots. The first hotspot is at the intersection of US 41 and 66<sup>th</sup> Avenue W. The second hotspot is the roadway segment between 57<sup>th</sup> Avenue W and Cortez Road W/44<sup>th</sup> Avenue E. The third hotspot is the intersection at 66<sup>th</sup> Avenue W. Out of the pedestrian and bicycle crashes there were 10 fatal pedestrian crashes and three fatal bicyclist crashes. The details of fatal crashes are provided in the next section.

## 8.7 Fatalities

Nineteen (19) crashes along the US 41 corridor in the most recent four years (2019 - 2023) were fatal. **Table 25** provides a summary of fatal crashes along the corridor and is followed by a narrative of fatal crashes along the corridor.

*Table 25: Summary of Fatal Crashes*

Crash Type	Report Number	Intersection	Weather Condition	Lighting Condition	Road Surface Condition	Drug/Alcohol Related
Angle	24910130	21 <sup>st</sup> Street E	Clear	Daylight	Dry	No
Pedestrian	88572833	~57 feet north of 27 <sup>th</sup> Avenue E	Cloudy	Dark - Lighted	Dry	Yes
Bicycle	88570144	~52 feet north of 32 <sup>nd</sup> Avenue W	Clear	Dark - Lighted	Dry	No
Bicycle	25003967	~111 feet north of 45 <sup>th</sup> Avenue Circle W	Clear	Daylight	Dry	No
Pedestrian	88440573	~104 feet north 51 <sup>st</sup> Avenue Drive W	Clear	Dark - Not Lighted	Dry	No
Pedestrian	25070144	~149 feet south of 51 <sup>st</sup> Avenue Drive W	Clear	Dark - Not Lighted	Dry	No
Pedestrian	88336003	51 <sup>st</sup> Avenue Drive W	Clear	Dark - Not Lighted	Dry	No
Pedestrian	88569072	~27 feet south 54 <sup>th</sup> Avenue W	Cloudy	Dark - Lighted	Wet	Yes
Pedestrian	24946520	~134 feet north 55 <sup>th</sup> Avenue W	Clear	Dark - Lighted	Dry	Yes
Left Turn	24894145	~87 feet north of 58 <sup>th</sup> Avenue W	Clear	Daylight	Dry	Yes
Pedestrian	88449119	~326 feet north of 58 <sup>th</sup> Avenue W	Fog, Smog, Smoke	Dark - Lighted	Dry	No
Pedestrian	88383756	~287 feet north of 66 <sup>th</sup> Street W	Clear	Dark - Not Lighted	Dry	Yes
Left Turn	25039289	Braden Avenue	Clear	Dark - Lighted	Dry	Yes
Off-Road	24969116	~327 feet south of Edwards Drive	Clear	Daylight	Dry	No
Rear End	89577066	~63 feet south of Florida Boulevard	Clear	Daylight	Dry	No

Crash Type	Report Number	Intersection	Weather Condition	Lighting Condition	Road Surface Condition	Drug/Alcohol Related
Pedestrian	88547993	~215 feet south of Orlando Avenue	Clear	Dark - Not Lighted	Dry	No
Pedestrian	88526916	~365 feet north of Orlando Avenue	Clear	Dark - Not Lighted	Dry	Yes
Off-Road	87129154	Pearl Avenue	Rain	Dark - Lighted	Wet	Yes
Bicycle	89391158	SR 45/9 <sup>th</sup> Street W	Clear	Dark - Lighted	Dry	No

Fatal Crash Narrative:

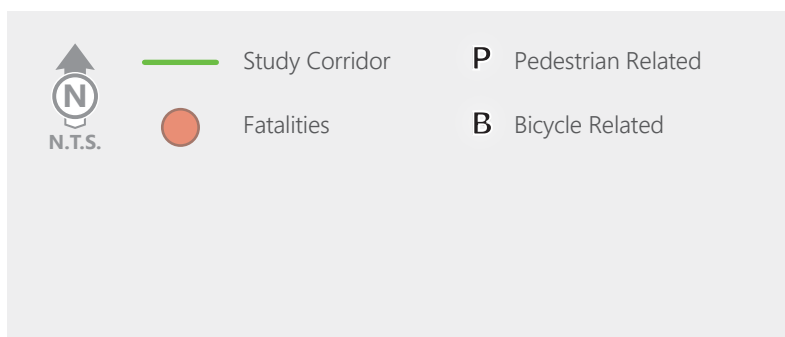
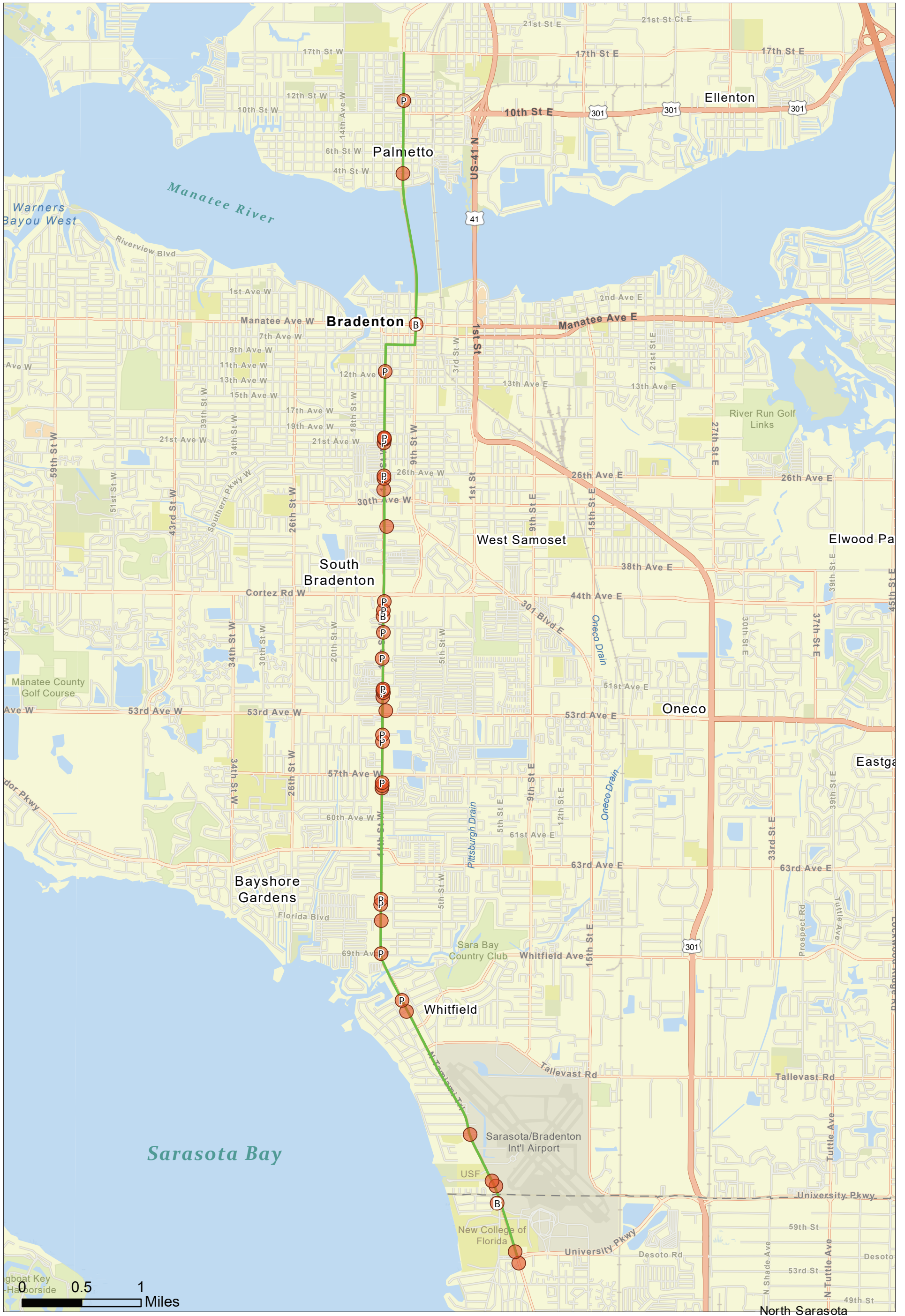
- 24910130 – Angle:** Vehicle 1 – a passenger van – was traveling westbound on 21<sup>st</sup> Street E, approaching the intersection at US 41. Vehicle 2 – a motorcycle was traveling southbound on US 41. Vehicle 1 failed to yield to right-of-way which resulted in the front of Vehicle 2 striking the right side of Vehicle 1. Vehicle 2 spun and then overturned. The driver of Vehicle 2 was transported to the hospital and sustained fatal injuries. The crash occurred during daylight, clear, and dry conditions. No use of alcohol or drugs were involved in the crash.
- 88572833 – Pedestrian:** Vehicle 1 – a passenger car – was traveling northbound on US 41 just north of 27<sup>th</sup> Avenue E and struck a pedestrian darting across the roadway with no marked crosswalk. Vehicle 1 then swerved and hit a utility pole. The pedestrian was transported to the hospital and sustained fatal injuries. The crash occurred during dark-lighted, cloudy, and dry conditions. The driver of Vehicle 1 was reported to be under the influence of alcohol at the time of the crash.
- 88570144 – Bicycle:** Vehicle 1 – an SUV, and Vehicle 2 – a passenger car, were traveling southbound on US 41 just north of 32<sup>nd</sup> Avenue W. Vehicle 1 struck the undercarriage of vehicle 2 with the front left bumper, which resulted in a collision with a bicyclist who was traveling in the roadway with the flow of traffic. The bicyclist sustained fatal injuries. The crash occurred during dark-lighted, clear, and dry conditions. No use of alcohol or drugs were reported.
- 25003967 – Bicycle:** Vehicle 1 – a light truck – was traveling southbound on US 41 just north of 45<sup>th</sup> Avenue Circle W and struck a bicycle traveling in the roadway with the flow of traffic. The bicyclist sustained fatal injuries. The crash occurred during daylight, clear, and dry conditions. The bicyclist was reported to be under the influence of drugs at the time of the crash.
- 88440573 – Pedestrian:** Vehicle 1 – a passenger car – was traveling northbound on US 41 just north of 51<sup>st</sup> Avenue Drive and struck a pedestrian who was standing in the roadway improperly. The driver of Vehicle 1 reported trees or bushes obscured their vision causing the collision. The pedestrian was transported to the hospital and sustained fatal injuries. The crash occurred during dark-not lighted, clear, and dry conditions. No use of drugs or alcohol were reported.
- 25070144 – Pedestrian:** Vehicle 1 – an SUV, and Vehicle 2 – a passenger car, were traveling southbound on US 41 just north of 51<sup>st</sup> Avenue Drive W. Vehicle 1 struck the undercarriage of Vehicle 2 with the front center bumper, which resulted in a collision with a pedestrian who was crossing the roadway with no marked crosswalk. The pedestrian sustained fatal injuries. The crash occurred during dark-not lighted, clear, and dry conditions. No use of alcohol or drugs were reported.

- **88336003 – Pedestrian:** Vehicle 1 – a passenger car – was traveling northbound on US 41 approaching 51<sup>st</sup> Avenue Drive W and struck a pedestrian crossing the roadway with no marked crosswalk. The pedestrian sustained fatal injuries. The crash occurred during dark-not lighted, clear, and dry conditions. No use of alcohol or drugs were reported.
- **88569072 – Pedestrian:** Vehicle 1 – a passenger car – was traveling northbound on US 41 just south of 54<sup>th</sup> Avenue W. The driver changed lanes and struck a pedestrian who darted across the roadway with no marked crosswalk. The pedestrian was transported to the hospital and sustained fatal injuries. The crash occurred in dark-lighted, cloudy, and wet conditions. According to the report, the pedestrian was under the influence of alcohol at the time of the crash.
- **24946520 – Pedestrian:** Vehicle 1 – a passenger car – was traveling northbound on US 41 just north of 55<sup>th</sup> Avenue W and struck a pedestrian who darted across the roadway with no marked crosswalk. The pedestrian sustained fatal injuries. The crash occurred in dark-lighted, clear, and dry conditions. According to the report, the pedestrian was under the influence of drugs at the time of the crash.
- **24894145 – Left Turn:** Vehicle 1 – a passenger car – was traveling northbound on US 41 turning left onto 58<sup>th</sup> Avenue W. Vehicle 2 – a motorcycle – was traveling southbound on US 41 approaching 58<sup>th</sup> Avenue W. Vehicle 1 failed to yield to right-of-way which resulted in the front of Vehicle 2 striking the right side of Vehicle 1. Vehicle 2 then spun and overturned. The driver of Vehicle 2 sustained fatal injuries. The crash occurred in daylight, clear, and dry conditions. According to the report, the driver of Vehicle 2 was under the influence of drugs at the time of the crash.
- **88449119 – Pedestrian:** Vehicle 1 – unknown – was traveling southbound on US 41 just north of 58<sup>th</sup> Avenue W and struck a pedestrian crossing the roadway with no marked crosswalk. The pedestrian failed to yield to right-of-way resulting in fatal injuries. The crash occurred in dark-lighted and dry conditions. According to the report, fog, smog, and smoke was present. No alcohol or drugs were involved in the crash.
- **88383756 – Pedestrian:** Vehicle 1 – a pickup truck – was traveling northbound on US 41 just north of 66<sup>th</sup> Street W and struck a pedestrian who darted across the roadway with no marked crosswalk. The pedestrian was transported to the hospital and sustained fatal injuries. The crash occurred in dark-not lighted, clear, and dry conditions. According to the report, the pedestrian was under the influence of drugs at the time of the crash.
- **25039289 – Left Turn:** Vehicle 1 – a pickup truck – was traveling southbound on US 41 at Braden Avenue. Vehicle 2 – a pickup truck – was traveling northbound on US 41. Vehicle 1 failed to yield to right-of-way while turning on to Braden Avenue which resulted in the front of Vehicle 2 striking the right side of Vehicle 1. The driver of Vehicle 1 was transported to the hospital and sustained fatal injuries. The crash occurred in dark-lighted, clear, and dry conditions. According to the report, the drivers of Vehicle 1 and Vehicle 2 were under the influence of alcohol at the time of the crash.
- **24969116 – Off Road:** Vehicle 1 – a motorcycle – was traveling northbound on US 41 toward Edwards Drive. It was reported that the driver was exceeding the speed limit and operating the motor vehicle in a careless manner. The vehicle ran off the road and collided with a utility pole. The driver was transported to the hospital and sustained fatal injuries. The crash occurred in

dark-lighted, clear, and dry conditions. According to the report, no alcohol or drugs were involved in the crash.

- **89577066 – Rear End:** Vehicle 1 – a motorcycle – was traveling northbound on US 41 approaching Florida Boulevard. Vehicle 2 – an SUV – was stopped in traffic heading north on US 41. It was reported that the driver of Vehicle 1 was exceeding the speed limit and operating the motor vehicle in a careless manner, which resulted in Vehicle 1 rear ending Vehicle 2. Vehicle 1 then spun and overturned. The driver of vehicle 1 was transported to the hospital and sustained fatal injuries. The crash occurred in daylight, clear, and dry conditions. No alcohol or drugs were involved in the crash.
- **88547993 – Pedestrian:** Vehicle 1 – a pickup truck – was heading northbound on US 41 approaching Orlando Avenue and struck a pedestrian who darted in the roadway with no marked crosswalk. The pedestrian sustained fatal injuries. The crash occurred in dark-not lighted, clear, and dry conditions. According to the report, no alcohol or drugs were involved in the crash.
- **88526916 – Pedestrian:** Vehicle 1 – a passenger car, and Vehicle 2 – a pickup truck, were traveling northbound on US 41 approaching Orlando Avenue and struck a pedestrian crossing the roadway improperly with no marked crosswalk. The pedestrian sustained fatal injuries. The crash occurred in dark-not lighted, clear, and dry conditions. According to the report, the pedestrian was under the influence of drugs and alcohol at the time of the crash.
- **87129154 – Off-Road:** Vehicle 1 – a passenger car – was traveling northbound on US 41 approaching Pearl Avenue. According to the report, the driver was operating the motor vehicle in a careless manner which resulted in the vehicle running off the road. The driver sustained fatal injuries. The crash occurred in dark-lighted, rainy, and wet conditions. According to the report, the driver was under the influence of alcohol at the time of the crash.
- **89391158 – Bicycle:** Vehicle 1 – a pickup truck – was traveling eastbound on SR 64/6<sup>th</sup> Avenue W. Vehicle 1 ran a red light, which resulted in a collision with a bicyclist crossing the roadway with no marked crosswalk. The cyclist was transported to the hospital and sustained fatal injuries. The crash occurred in dark-lighted, clear, and dry conditions. According to the report, no alcohol or drugs were involved in the crash.

Fatal crashes along the corridor are illustrated in **Figure 23**.



**Figure 23**  
**Fatal Crash Locations**  
**Along the Study Corridor**  
Sarasota/Manatee US 41  
CMASS

## 8.8 Summary of Safety Conditions

In an ongoing effort to improve road safety, FDOT District One has identified a list of the top 20 fatal roadway segments for the years 2014 - 2018. Of those, the following five segments and their ranking are located within the study limits:

- 63<sup>rd</sup> Avenue W to 55<sup>th</sup> Avenue W; Ranked #1
- 55th Avenue W to Orlando Avenue; Ranked #2
- Magellan Drive to 63<sup>rd</sup> Avenue W; Ranked #8
- County line to Scott Avenue; Ranked #9
- Orlando Avenue to 30<sup>th</sup> Avenue W; Ranked #11

Using the results of the study's crash analysis for the four most recent years, , the following patterns are noted which can help identify potential mitigation strategies and enhanced safety along the study corridor:

- A majority of pedestrian/bicycle related crashes occurred along the roadway segment between 57th Avenue W and Cortez Road W/44th Avenue E, where the corridor consists of high-speed traffic and surrounded by commercial land use.
- Pedestrian fatalities are concentrated along US 41 between 51st Avenue Drive W and 58th Avenue W. Three pedestrian fatalities occurred at or near the intersection of US 41 and 51st Avenue Drive W. This could be due to a lack of lighting in this area and no marked crosswalks.
- The highest frequency crash location is at Cortez Road W/44th Avenue W and US 41 intersection and are mostly due to careless driving. Careless driving approaching the intersection could be due to high traffic volume and the transition from a six-lane divided roadway to a four-lane divided roadway.
- The highest frequency crash type is rear end. Congestion and high speeds along the corridor may contribute to the high number of rear end crashes.

## 9. Stakeholder Coordination

Local input was sought throughout the study process to ensure that the needs and desires of the surrounding community were accounted for. Engagement with local stakeholders began early in Phase 1 and will continue throughout Phase 2A of the study. This section provides a summary of all stakeholder outreach held over the course of the study.

### 9.1 Local Agency Presentations

- Sarasota/Manatee MPO Bike/Ped/Trails Advisory Committee Presentation on US 41 CMASS (April 19, 2022)
- Sarasota/Manatee MPO Citizens Advisory Network Presentation on US 41 CMASS - May 9, 2022
- Sarasota/Manatee MPO Technical Advisory Committee Presentation on US 41 CMASS - May 9, 2022
- Sarasota/Manatee MPO Board Presentation on US 41 CMASS - May 23, 2022



- Manatee County Board of County Commissioners Presentation on US 41 CMASS - June 7, 2022
- Bradenton City Council Presentation on US 41 CMASS - June 15, 2022
- Palmetto City Commission Presentation on US 41 CMASS - June 27, 2022

## 9.2 Key Stakeholders

Key Stakeholders for the study were established during Phase I of the study to represent the local community and provide input during the study. The key stakeholders consisted of local colleges, groups, and community leaders. Representatives of the following stakeholders were invited to participate in stakeholder coordination meetings:

- Bradenton Area Convention & Visitors Bureau
- New College of Florida
- Sarasota Manatee Airport Authority (SMAA)
- Sarasota Chamber of Commerce
- University of South Florida Sarasota-Manatee Campus
- NAACP Manatee County Chapter
- Manasota Black Chamber of Commerce
- Bradenton Area Economic Development Council (EDC)

A series of stakeholder meetings were held during Phase I of the study. The purpose of these meetings was to provide an overview of the study and conduct an open discussion between the study team and each individual stakeholder. **Table 26** below summarizes the feedback received during each of these meetings. A full summary of the feedback received during each meeting along with copies of the presentations are included in **Appendix C**.

*Table 26: Summary of Feedback Received*

Stakeholder Representative	Date of Meeting	Summary of Feedback
Bradenton Area Convention & Visitors Bureau	January 17, 2023	Concerns for lack of safety measures, lighting, and pedestrian accommodations along the corridor in addition to high-speed traffic. Additional suggestions for speed management and implementation of bicycle and pedestrian features along the corridor.
New College of Florida	January 18, 2023	Concerns for congestion, lack of safety, varying traffic speeds, and increased number of accidents near New College. Stakeholder representatives expressed the need for enhanced multimodal mobility and bike lanes along US 41.
Sarasota Manatee Airport Authority	January 19, 2023	Concerns about high-speed traffic along the corridor. Stakeholder representatives expressed the need for enhanced pedestrian safety features and wayfinding signs and discussed future development along US 41.
Greater Sarasota Chamber of Commerce	January 20, 2023	Concerns for high-speed traffic and lack of access to transit stops. Stakeholder representatives expressed the need for aesthetic

Stakeholder Representative	Date of Meeting	Summary of Feedback
		improvements, medians, and transit hubs that provide access to workplaces and affordable housing.
University of South Florida Sarasota-Manatee	January 20, 2023	Concern for congestion and lack of multi-modal safety. Stakeholder representatives provided suggestions for pedestrian and bicycle features and expressed the need for speed control measures along the corridor.
Manatee NAACP	January 24, 2023	Concerns for lack of lighting along the corridor. Stakeholder representatives expressed interest in the addition of bike lanes along the corridor.
Manasota Black Chamber of Commerce	February 1, 2023	Concerns for congestion, lack of pedestrian crossings, and potential impacts to historic Black and African American churches along the corridor. Stakeholder representatives expressed the desire for aesthetic improvements and intentional planning on behalf of the homeless population located along the corridor.
Bradenton Area Economic Council	February 3, 2023	Concern for lack of pedestrian safety and high-speed traffic. Stakeholder representatives expressed the need for advanced transit service and operations, and aesthetic improvements along the corridor.

## 10. Next Steps

The next steps for the US 41 CMASS are to conduct additional community and agency engagement, analyze future conditions and identify alternative improvement strategies. Then, a recommended alternative will be selected. A concept plan will be developed along with an implementation plan.



## Appendix A: Transit Routes



## Cash Fare and Bus Passes Pases de Autobús y Tarifa en Efectivo

PMX

### Port Manatee ConneXion

	Regular Card Tarjeta Regular	Discount Card Tarjeta de Descuento
Single Ride Cash Fare Tarifa en efectivo para un solo viaje	\$1.50	\$0.75
1-day M Card (available on bus) Tarjeta M de 1 día (disponible en el bus)	\$4.00	\$2.00
7-day M Card Tarjeta M de 7 días	\$12.00	\$6.00
31-day M Card Tarjeta M de 31 días	\$40.00	\$20.00
30-Day R Card Tarjeta R de 31 días	\$60.00	\$30.00

Riders aged 5 years and under, or aged 80 years and older, ride free on all MCAT routes.

Los pasajeros de 5 años o menos, o de 80 años o más, viajan gratis en todas las rutas MCAT.

\* M Cards are valid on MCAT vehicles. R Cards are valid on MCAT and SCAT vehicles.

\* Las tarjetas M son válidas en vehículos MCAT. Las tarjetas R son válidas en vehículos MCAT y SCAT.



#### Discount Eligibility

Upon proof of qualification, the following riders are eligible for discounted fares: aged 60-79 years, Mobility Card holder (medical condition), Medicare Card holder, active duty or military veteran, or enrolled student. Photo ID and proof of age, disability, military, veteran or student status is required for discount cards/fares at the time of purchase and when boarding the bus.

#### Elegibilidad de Descuento

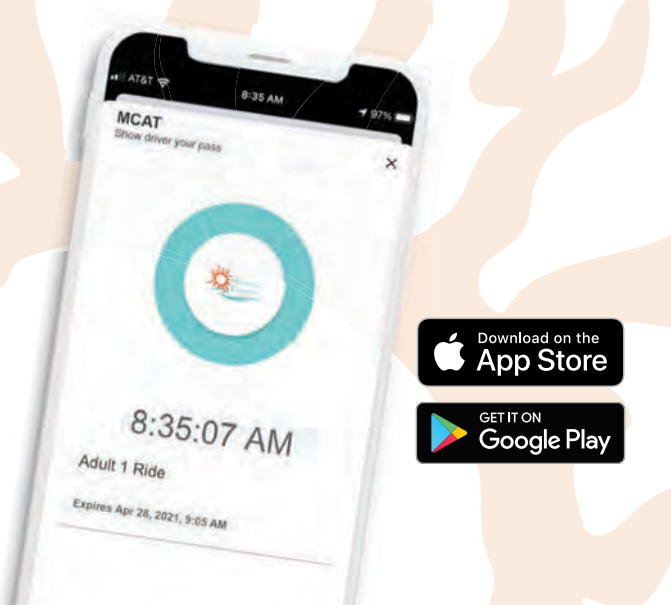
Con la prueba de calificación, los siguientes pasajeros son elegibles para tarifas con descuento: de 60 a 79 años, titular de la tarjeta de movilidad (condición médica), titular de la tarjeta de Medicare, miembro militar en servicio activo o veterano, o estudiante matriculado. Se requiere una identificación con foto y un comprobante de edad, discapacidad, servicio militar o veterano, o estudiante, para las tarjetas de descuento al momento de hacer de la compra y al abordar el autobús.

## Buy Tickets with Your Mobile Phone/ Compre boletos con su teléfono móvil



### TokenTransit Mobile App and Moovit Mobile App

Proof of qualification must be presented to the bus operator to use a discounted fare.  
Se debe presentar prueba de calificación al operador del autobús para usar una tarifa con descuento.



## CONTACT US:

Rider information 941-749-7116  
Text Telephone (TDD) 941-747-7868  
Email [mcat@mymanatee.org](mailto:mcat@mymanatee.org)  
Website [www.mymanatee.org/mcat](http://www.mymanatee.org/mcat)

Manatee County Transit Fleet Facility  
MCAT Administrative Office: 941-747-8621  
2411 Tallevast Road  
Sarasota, FL 34243



@manatee.county.fl



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ManateeCountyFla



# MCAT

MANATEE COUNTY AREA TRANSIT

PMX

## Port Manatee ConneXion

Monday - Saturday

Effective Date: December 4, 2021



## DESTINATIONS

Port Manatee area  
Palmetto Station  
Palmetto Walmart

## Tips for Riders

### Consejos para pasajeros

- Arrive 10 minutes prior to the scheduled time
- Ensure the driver can see you at the bus stop
- Have exact cash fare, bus pass or mobile ticket ready before boarding
- Let the operator know if you require additional assistance
- Llegue 10 minutos antes de la hora programada
- Asegúrese de que el conductor pueda verlo en la parada del autobús
- Tenga lista su tarifa en efectivo exacto, pase para el bus, o boleto en su móvil, antes de abordar.
- Informe al operador si necesita asistencia adicional

## Bus Passes and Services

### Pases y Servicios de Autobús

M Cards and R Cards are available by mail order or as listed at the following locations:

Las tarjetas M y las tarjetas R están disponibles por pedido por correo o como se enumeran en las siguientes ubicaciones:

#### Downtown Bradenton Transfer Station

1 2 3 4 9 13 99 201 203 PMX

601 13th St West, Bradenton

On 13th St W between 8th Ave and 6th Ave

Customer Service Hours: Monday - Friday, 7am to 6pm

Purchase regular & discounted bus passes (no bills over \$20).

Compre pases de autobús regulares y con descuento (no se aceptan billetes de más de \$20).

#### DeSoto Transfer Station

2 6 8 12 16 99 203

820 301 Boulevard West, Bradenton

On the corner of 301 Blvd W and 9th St W

Customer Service Hours:

Monday - Friday, 7am - 11:30am and 12:30pm - 4pm

Purchase regular & discounted bus passes (no bills over \$20). Park and Ride spaces available.

Compre pases de autobús regulares y con descuento (no se aceptan billetes de más de \$20).

#### Palmetto Transfer Station

1 13 201 203 PMX

1802 8th Ave West, Palmetto

On the corner of Business 41 and 19th St

Bus information displays, schedules, and park and ride lot available.

Pantallas de información de autobuses, horarios y estacionamiento disponible.

#### Goodwill Stores\*

• 3465 Cortez Rd West, Bradenton

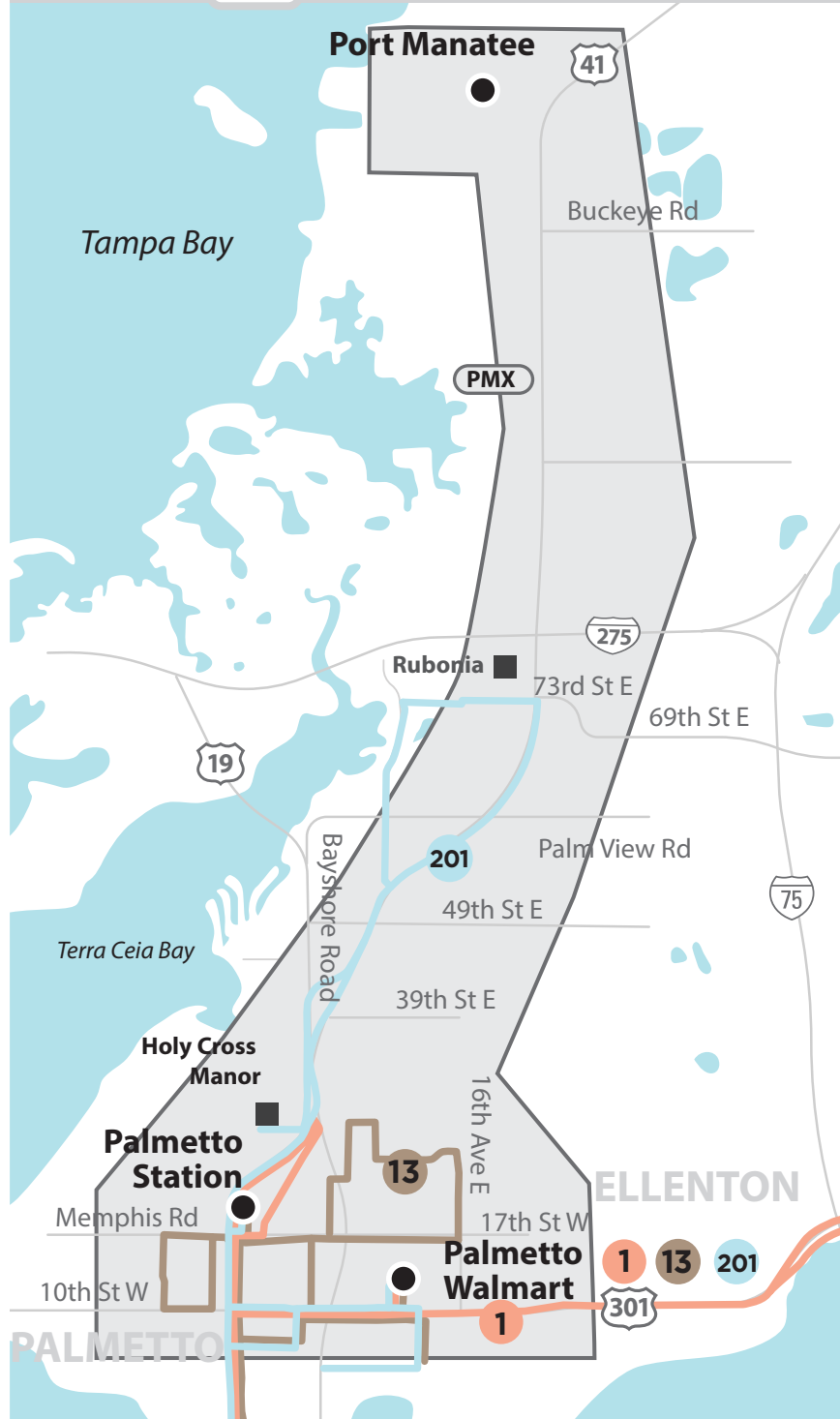
• 2715 51st Ave East, Bradenton

• 8750 State Road 70, Bradenton

\*Discounted tickets are not available at Goodwill Stores.

\*Los boletos con descuento no están disponibles en las tiendas Goodwill.

# ROUTE PMX Port Manatee ConneXion



## INTRODUCTION:

The Port Manatee ConneXion Shuttle is a mobility on-demand/call ahead service available within a defined area of northern Manatee County. This new service was designed specifically for residents, visitors and those who work in the Port Manatee area. The Port Manatee ConneXion transfer points include the Palmetto Transit Station (Routes 1, 13, 201 and 203), the Palmetto Wal-Mart (Routes 1, 13, and 201) and the Rubonia area (Route 201).

Port Manatee ConneXion services are available Monday – Saturday between 5:30am and 7:30pm.

The Port Manatee ConneXion cash fare is \$1.50 for each ride. (Change is NOT available.)

- All valid MCAT and SCAT fixed route bus passes are accepted on the Port Manatee ConneXion Shuttle. The fare can also be paid using the Token Transit mobile phone application.
- A Pre-paid Fare program is available for the ease of buying a pre-set number of Port Manatee ConneXion Shuttle trips. The Pre-paid account must be established in advance and then replenished as desired.



## INTRODUCCIÓN:

El Port Manatee ConneXion Shuttle es un servicio de movilidad por encargo/llamando antes, que está disponible dentro de un área definida del norte del Condado Manatee. Este servicio nuevo fue diseñado específicamente para residentes, visitantes, y trabajadores en el área de Port Manatee. Los puntos de transferencia del Port Manatee ConneXion incluyen la Estación de Tránsito de Palmetto (Rutas 1, 13, 201 y 203), el Walmart de Palmetto (Rutas 1, 13, and 201) y la zona de Rubonia (Ruta 201).

Los servicios de Port Manatee ConneXion están disponibles lunes – sábado entre 5:30am y 7:30pm.

La tarifa en efectivo para Port Manatee ConneXion es \$1.50 por viaje. (NO hay cambio disponible.)

- Todos los pases de bus de ruta fija de MCAT y SCAT se aceptan en el Port Manatee ConneXion Shuttle. La tarifa también se puede pagar usando la aplicación de teléfono móvil Token Transit.
- Está disponible un programa de tarifas prepagadas para la conveniencia de comprar un número preestablecido de viajes en el Port Manatee ConneXion Shuttle. La cuenta prepagada debe ser establecida con antelación y después recargada como se desee.

## TO USE THE PORT MANATEE CONNEXION SHUTTLE

1. Make your reservation by 5:00pm the day before service is desired.
2. Call 941-748-2317, Monday – Friday, 8:00am to 5:00pm up to 14 days before your trip.
3. If you plan to use the Port Manatee ConneXion regularly, subscription service is available.
4. Walk-up service is available at designated transfer points and bus stops when a Port Manatee ConneXion Shuttle vehicle is already in operation. Please “flag” the Port Manatee ConneXion Shuttle operator to stop for you.
5. Southbound from the Port Manatee area, the driver can drop you off in Rubonia, at the Palmetto Transit Station or at the Palmetto Walmart.
6. Northbound from Palmetto, the driver will drop you at a designated MCAT bus stop in the Port Manatee Area.
7. Port Manatee ConneXion Shuttle vehicles can transport bicycles and are accessible for people using wheelchairs or other mobility devices. Please note use of a bike or mobility aid when scheduling a ride.



## PARA USAR EL PORT MANATEE CONNEXION SHUTTLE:

1. Haga su reservación hasta las 5 p.m. del día antes del cuando quiera el servicio.
2. Llame al 941-748-2317, de lunes a viernes, 8am a 5pm hasta 14 días antes de su viaje.
3. Si planea usar el Port Manatee ConneXion frecuentemente, está disponible el servicio de suscripción.
4. Está disponible el servicio por ventanilla en puntos de transferencia designados y en paradas de bus, cuando un vehículo de Port Manatee ConneXion Shuttle esté ya operando. Por favor hágale señas al operador del Port Manatee ConneXion Shuttle para que pare.
5. En rumbo al sur desde el área de Port Manatee, el conductor puede dejarlo bajar en Rubonia, en la Estación de Tránsito de Palmetto, o en el Walmart de Palmetto.
6. En rumbo al norte desde Palmetto, el conductor lo dejará bajar en la parada de bus designada de MCAT, en el área de Port Manatee.
7. Los vehículos de Port Manatee ConneXion Shuttle pueden transportar bicicletas, y son accesibles para gente que use sillas de rueda u otros aparatos de movilidad. Por favor note que va a usar una bicicleta o aparato de movilidad al reservar su viaje.

## RIDER RESPONSIBILITIES RESPONSABILIDADES DE LOS PASAJEROS

We want your trip to be a pleasant experience. To ensure a safe, clean, and comfortable ride for our passengers, please follow these rules: Queremos que su viaje sea una experiencia agradable. Para garantizar un viaje seguro, limpio y cómodo para nuestros pasajeros, por favor siga estas reglas:

### ✓ DO

- Wear appropriate clothing (shirt and shoes required)
- Practice hygiene that reduces personal odors
- Speak in quiet voices
- Use headphones to listen to electronic devices
- Keep food and drinks in closed containers
- Secure bicycles onto the front bike rack
- Store personal items under your seat or on your lap
- Secure pets in a cage/carrier that fits under your seat or on your lap. Service animals may accompany their owners as needed.

### ✗ DON'T

- Board the bus under the influence of alcohol or narcotics
- Lay on bus seats or put feet on bus seats
- Use profane language
- Distract the driver
- Attempt to buy or sell products or services. This includes distributing or posting materials.
- Smoke or vape while riding
- Eat or drink from an open container
- Bring open alcoholic beverages on the bus
- Bring large items (including strollers, scooters, etc) that block bus aisles
- Bring weapons or flammable/explosive materials
- Allow animals out of their carrier

### ✓ HACER

- Use ropa adecuada (se requieren camisa y zapatos)
- Practique una higiene que reduzca olores personales
- Hable en voz baja
- Use audífonos para escuchar dispositivos electrónicos
- Mantenga alimentos y bebidas en recipientes cerrados
- Asegure las bicicletas en el portabicicletas delantero
- Guarde artículos personales debajo de su asiento o en su regazo
- Asegure a las mascotas en una jaula/portador que quepa debajo de su asiento o sobre sus piernas. Los animales de servicio pueden acompañar a sus dueños según sea necesario

### ✗ NO HACER

- Abordar el autobús bajo la influencia del alcohol o narcóticos
- Acostarse en los asientos del autobús o poner los pies en los asientos del autobús
- Usar lenguaje profano
- Distraer al conductor
- Intentar comprar o vender productos o servicios. Esto incluye distribuir o publicar materiales
- Fumar o usar vaporizador en el autobús
- Comer o beber de un recipiente abierto
- Traer bebidas alcohólicas abiertas en el autobús
- Traer artículos grandes (incluyendo coches, patinetes, etc.) que bloqueen los pasillos del autobús
- Llevar armas o materiales inflamables / explosivos
- Permitir que los animales salgan de su portador

Passengers who do not follow these rules may be asked to leave the bus or bus stop area. Refusal to leave the bus or bus stop area may result in an arrest for trespassing pursuant to Florida Statute 810.08. All MCAT vehicles and facilities are monitored 24 hours per day, 7 days per week.

A los pasajeros que no sigan estas reglas se les puede pedir que se bajen del autobús o del área de la parada del autobús. Negarse a abandonar el autobús o el área de la parada del autobús puede resultar en un arresto por invasión de propiedad privada, conforme al Estatuto de la Florida 810.08. Todos los vehículos e instalaciones de MCAT son monitoreados 24 horas al día, 7 días a la semana.

## Tips for Riders Consejos para pasajeros

- Arrive 10 minutes prior to the scheduled time
- Ensure the driver can see you at the bus stop
- Let the operator know if you require additional assistance
- Llegue 10 minutos antes de la hora programada
- Asegúrese de que el conductor pueda verlo en la parada del autobús
- Informe al operador si necesita asistencia adicional

## Bus Stations and Customer Service Estaciones de Autobuses y Atención al Cliente

### Downtown Bradenton Transfer Station

1 2 3 4 9 13 99 201 203 PMX

601 13th St West, Bradenton  
On 13th St W between 8th Ave and 6th Ave  
Customer Service Hours: Subject to change  
Bus information displays and schedules are available.

### DeSoto Transfer Station

2 6 8 12 16 99 203

820 301 Boulevard West, Bradenton  
On the corner of 301 Blvd W and 9th St W  
Customer Service Hours: Subject to change  
Bus information displays, schedules and park and ride spaces are available.

### Palmetto Transfer Station

1 13 201 203 PMX

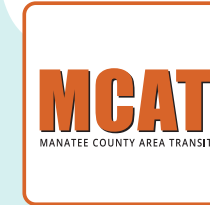
1802 8th Ave West, Palmetto  
On the corner of Business 41 and 19th St  
Bus information displays, schedules, and park and ride lot available.  
Pantallas de información de autobuses, horarios y estacionamiento disponible.



## Where's my Bus?

### Find the bus from your phone

1. Get the **Manatee MyStop** App
2. Check real-time bus information
3. Set service alerts
4. Plan your trip



## ¿Dónde está mi autobús?

### Encuentra el autobús desde su teléfono

1. Obtenga la aplicación **Manatee MyStop**
2. Verifique la información del autobús en tiempo real
3. Reciba alertas de servicio
4. Planifique su viaje



## CONTACT US:

Rider information 941-749-7116  
Text Telephone (TDD) 941-747-7868  
Email [mcat@mymanatee.org](mailto:mcat@mymanatee.org)  
Website [www.mymanatee.org/mcat](http://www.mymanatee.org/mcat)

Manatee County Transit Fleet Facility  
Administration Offices 941-747-8621  
2411 Tallevast Road  
Sarasota, FL 34243

@ManateeGov manateegov

@PW\_ManateeGov ManateeCountyFla



# MCAT

MANATEE COUNTY AREA TRANSIT

## TRANSIT/TROLLEY SYSTEM MAP

**Local Fixed Route Bus**  
Monday - Saturday

**Anna Maria Island Trolley**  
7 days a week/365 days a year

Effective Date: February 1, 2023



## SERVING

Anna Maria Island  
Bradenton  
Cortez  
Longboat Key  
Manatee County  
Palmetto  
Pinellas County  
Port Manatee  
Sarasota County

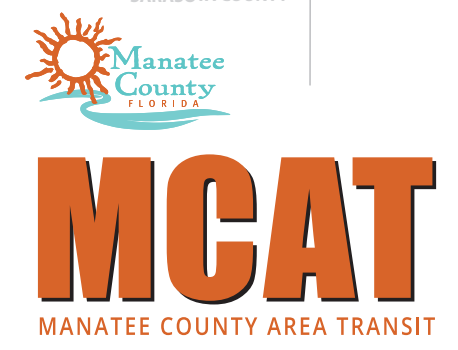
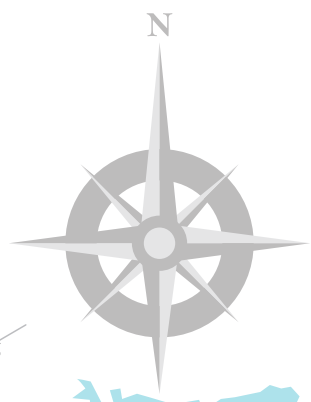
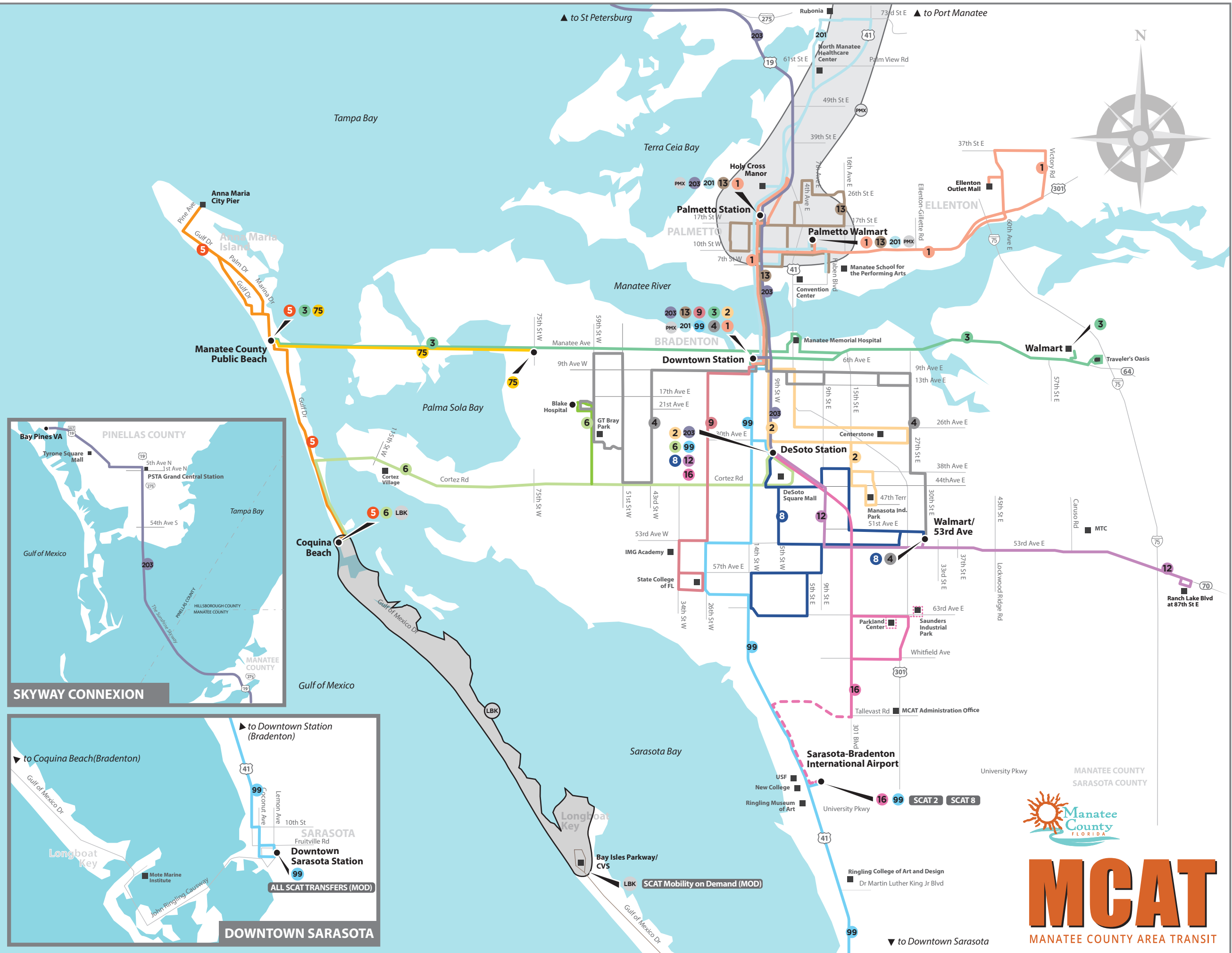
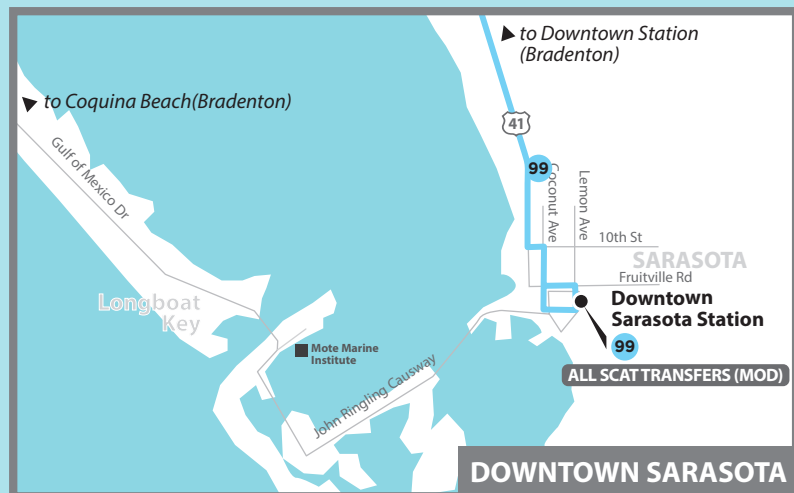
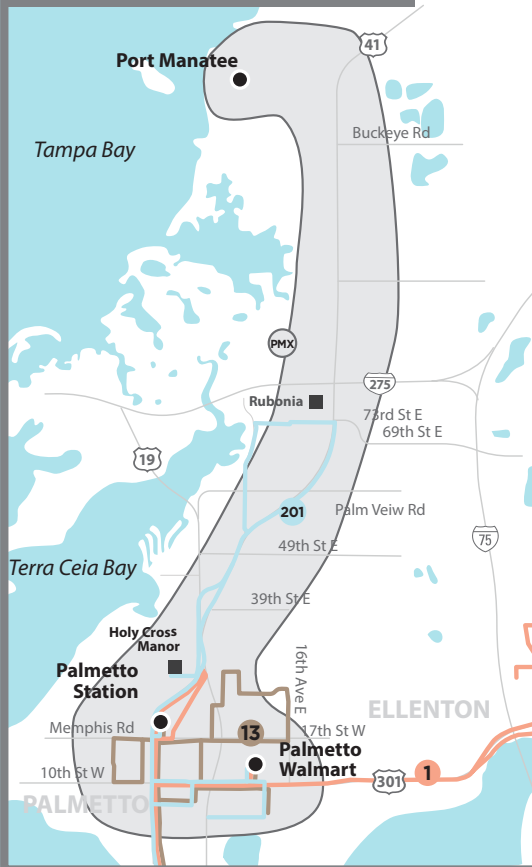
**LEGEND**

- 1 Ellenton Outlet Mall
- 2 East Bradenton
- 3 Manatee Avenue
- 4 9th Avenue East and West
- 5 Anna Maria Island Trolley
- 6 Cortez Road
- 8 Oneco-Bayshore
- 9 9th Avenue W/26th Street
- 12 State Road 70
- 13 Palmetto
- 16 15th Street East
- 75 Beach ConneXion
- 99 Bradenton-Sarasota
- 201 North County ConneXion
- 203 Skyway ConneXion
- LBK Longboat Key Shuttle
- PMX Port Manatee ConneXion
- Intermittent Service
- Transfer Points
- Points of Interest

**LONGBOAT KEY SHUTTLE  
PORT MANATEE CONNESSION**

- ON DEMAND, CURB-TO-CURB SERVICE
- CALL TO SCHEDULE A RIDE
- PHONE: 941-748-2317

**PORT MANATEE CONNESSION**



## RIDER RESPONSIBILITIES RESPONSABILIDADES DE LOS PASAJEROS

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### ✓ DO

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- Bring weapons or flammable/explosive materials
- Allow animals out of their carrier

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- Guarde artículos personales debajo de su asiento o en su regazo
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- Permitir que los animales salgan de su portador

Passengers who do not follow these rules may be asked to leave the bus or bus stop area. Refusal to leave the bus or bus stop area may result in an arrest for trespassing pursuant to Florida Statute 810.08. All MCAT vehicles and facilities are monitored 24 hours per day, 7 days per week.  
A los pasajeros que no sigan estas reglas se les puede pedir que se bajen del autobús o del área de la parada del autobús. Negarse a abandonar el autobús o el área de la parada del autobús puede resultar en un arresto por invasión de propiedad privada, conforme al Estatuto de la Florida 810.08. Todos los vehículos e instalaciones de MCAT son monitoreados 24 horas al día, 7 días a la semana.



## Where's my Bus?

Find the bus from your phone

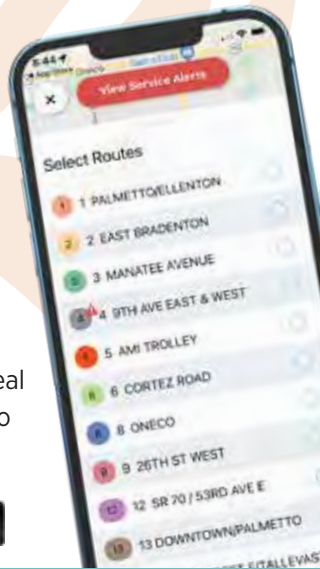
1. Get the **Manatee MyStop** App
2. Check real-time bus information
3. Set service alerts
4. Plan your trip



## ¿Dónde está mi autobús?

Encuentra el autobús desde su teléfono

1. Obtenga la aplicación **Manatee MyStop**
2. Verifique la información del autobús en tiempo real
3. Reciba alertas de servicio
4. Planifique su viaje



## CONTACT US:

Rider information 941-749-7116  
Text Telephone (TDD) 941-747-7868  
Email [mcat@mymanatee.org](mailto:mcat@mymanatee.org)  
Website [www.mymanatee.org/mcat](http://www.mymanatee.org/mcat)

Manatee County Transit Fleet Facility  
MCAT Administration 941-747-8621  
2411 Tallevast Road  
Sarasota, FL 34243

[@manatee.county.fl](https://www.facebook.com/manatee.county.fl) [manateegov](https://www.instagram.com/manateegov)  
 [@PW\\_ManateeGov](https://twitter.com/PW_ManateeGov) [ManateeCountyFla](https://www.youtube.com/ManateeCountyFla)



# MCAT

MANATEE COUNTY AREA TRANSIT

## 99

### Bradenton-Sarasota

Monday - Saturday

Effective Date: April 29, 2023



## MCAT FARE FREE Pilot Program

### DESTINATIONS

Downtown Bradenton Station  
Desoto Station  
State College of Florida  
University of South Florida  
New College of Florida  
Sarasota Bradenton Airport  
Ringling College of Art and Design  
Downtown Sarasota

## Tips for Riders

### Consejos para los pasajeros

- Arrive 10 minutes prior to the scheduled time
- Ensure the driver can see you at the bus stop
- Have your belongings ready before boarding
- Let the operator know if you require additional assistance
- All passengers are required to exit the bus at the end of each trip
- *Llegue 10 minutos antes de la hora programada*
- *Asegúrese de que el conductor pueda verlo en la parada de autobuses*
- *Tenga sus pertenencias listas antes de abordar*
- *Informe al operador si necesita asistencia adicional*
- *Todos los pasajeros deben desembarcar el autobús al final del viaje*

## Bus Stations and Customer Service Estaciones de Autobuses y Atención al Cliente

### Downtown Bradenton Transfer Station

**1 2 3 4 9 13 99 201 203 PMX**

601 13th St West, Bradenton

On 13th St W between 8th Ave and 6th Ave

Customer Service Hours: Subject to change.

### DeSoto Transfer Station

**2 6 8 12 16 99 203**

820 301 Boulevard West, Bradenton

On the corner of 301 Blvd W and 9th St W

Bus information displays, schedules, and park and ride spaces available.

Customer Service Hours: Subject to change.

### Palmetto Transfer Station

**1 13 201 203 PMX**

1802 8th Ave West, Palmetto

On the corner of Business 41 and 19th St

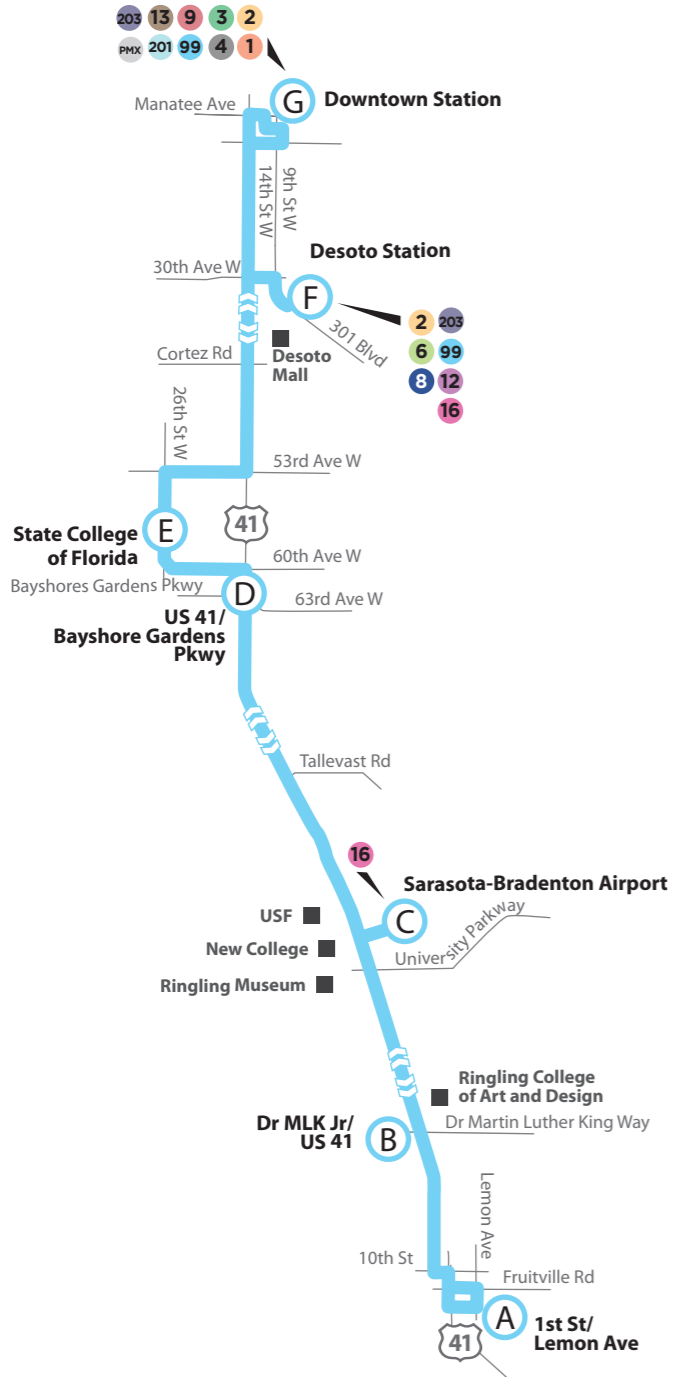
Bus information displays, schedules, and park and ride lot available.

*Pantallas de información de autobuses, horarios y estacionamiento disponible.*

# MCAT FARE FREE PILOT PROGRAM



# ROUTE 99 Bradenton-Sarasota



○ Time Points  
■ Points of interest

## SOUTH to Downtown Sarasota

## NORTH to Downtown Bradenton

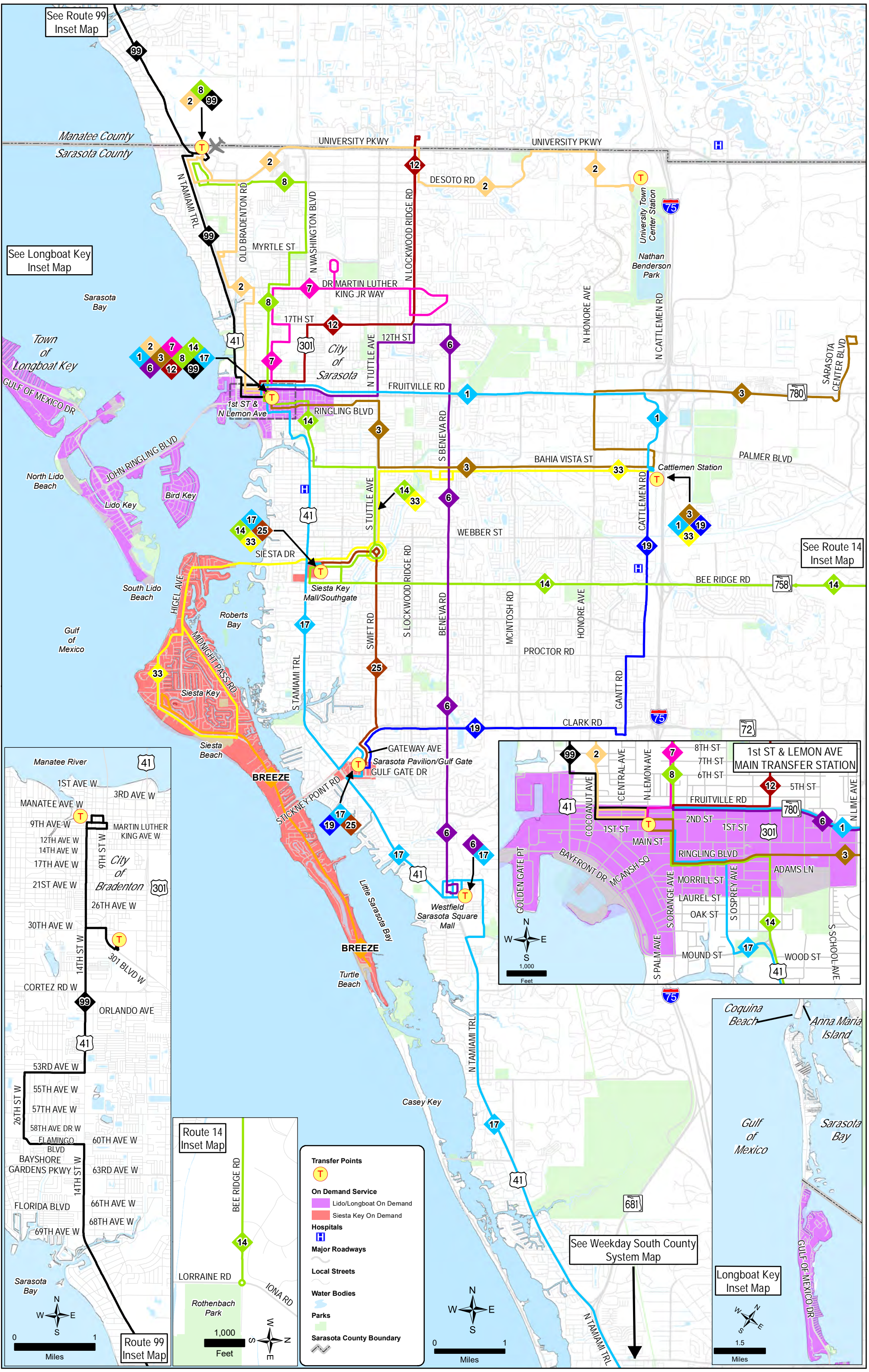
Bus Number	SOUTH to Downtown Sarasota						NORTH to Downtown Bradenton								
	G	F	E	D	C	B	A	A	B	C	D	E	F	G	
	Downtown Station	Desoto Station/US 301 Blvd	State College of Florida	US 41/Bayshore Gardens Parkway	Sarasota/Bradenton Airport	US 41/MLK Blvd	1st St/Lemon Ave Downtown Sarasota	1st St/Lemon Ave Downtown Sarasota	US 41/MLK Blvd	Sarasota/Bradenton Airport	US 41/Bayshore Gardens Parkway	State College of Florida	Desoto Station/US 301 Blvd	Downtown Station	Bus Number
								5:15a	5:21a	5:30a	5:37a			5:50a	SCAT
										5:35a	5:42a	5:46a	6:05a	6:13a	993
								5:35a	5:41a	5:50a	5:57a	6:01a	6:25a	6:33a	992
								5:55a	6:01a	6:10a	6:17a	6:21a	6:45a	6:53a	SCAT
991		5:15a	5:25a	5:30a	5:45a	5:52a	6:00a	6:15a	6:23a	6:33a	6:41a	6:46a	7:05a	7:15a	991
								6:35a	6:43a	6:53a	7:01a	7:06a	7:25a	7:35a	SCAT
994	5:40a	5:55a	6:10a	6:15a	6:26a	6:33a	6:41a	6:55a	7:03a	7:13a	7:21a	7:26a	7:45a	7:55a	994
SCAT	6:00a	6:15a	6:30a	6:35a	6:46a	6:53a	7:01a	7:15a	7:23a	7:33a	7:41a	7:46a	8:05a	8:15a	SCAT
993	6:20a	6:35a	6:51a	6:57a	7:08a	7:15a	7:23a	7:35a	7:43a	7:53a	8:01a	8:06a	8:25a	8:35a	993
992	6:40a	6:55a	7:11a	7:17a	7:28a	7:35a	7:43a	7:55a	8:03a	8:13a	8:21a	8:26a	8:45a	8:55a	992
SCAT	7:00a	7:15a	7:31a	7:37a	7:48a	7:55a	8:03a	8:15a	8:23a	8:33a	8:41a	8:46a	9:05a	9:15a	SCAT
991	7:20a	7:35a	7:51a	7:57a	8:08a	8:15a	8:23a	8:35a	8:43a	8:53a	9:01a	9:06a	9:25a	9:35a	991
SCAT	7:40a	7:55a	8:11a	8:17a	8:28a	8:35a	8:43a	8:55a	9:03a	9:13a	9:21a	9:26a	9:45a	9:55a	SCAT
994	8:00a	8:15a	8:31a	8:37a	8:48a	8:55a	9:03a	9:15a	9:23a	9:33a	9:41a	9:46a	10:05a	10:15a	994
SCAT	8:20a	8:35a	8:51a	8:57a	9:08a	9:15a	9:23a	9:45a	9:53a	10:03a	10:11a	10:16a	10:35a	10:45a	SCAT
993	8:50a	9:05a	9:21a	9:27a	9:38a	9:45a	9:53a	10:15a	10:23a	10:33a	10:41a	10:46a	11:05a	11:15a	993
SCAT	9:20a	9:35a	9:51a	9:57a	10:08a	10:15a	10:23a	10:45a	10:53a	11:03a	11:11a	11:16a	11:35a	11:45a	SCAT
991	9:50a	10:05a	10:21a	10:27a	10:38a	10:45a	10:53a	11:15a	11:23a	11:33a	11:41a	11:46a	12:05p	12:15p	991
SCAT	10:20a	10:35a	10:51a	10:57a	11:08a	11:15a	11:23a	11:45a	11:53a	12:03p	12:11p	12:16p	12:35p	12:45p	SCAT
SCAT	10:50a	11:05a	11:21a	11:27a	11:38a	11:45a	11:53a	12:15p	12:23p	12:33p	12:41p	12:46p	1:05p	1:15p	SCAT
993	11:20a	11:35a	11:51a	11:57a	12:08p	12:15p	12:23p	12:45p	12:53p	1:03p	1:11p	1:16p	1:35p	1:45p	993
SCAT	11:50a	12:05p	12:21p	12:27p	12:38p	12:45p	12:53p	1:15p	1:23p	1:33p	1:41p	1:46p	2:05p	2:15p	SCAT
991	12:20p	12:35p	12:51p	12:57p	1:08p	1:15p	1:23p	1:45p	1:53p	2:03p	2:11p	2:16p	2:35p	2:45p	991
SCAT	12:50p	1:05p	1:21p	1:27p	1:38p	1:45p	1:53p	2:15p	2:23p	2:33p	2:41p	2:46p	3:05p	3:15p	SCAT
SCAT	1:20p	1:35p	1:51p	1:57p	2:08p	2:15p	2:23p	2:45p	2:53p	3:03p	3:11p	3:16p	3:35p	3:45p	SCAT
993	1:50p	2:05p	2:21p	2:27p	2:38p	2:45p	2:53p	3:15p	3:23p	3:33p	3:41p	3:46p	4:05p	4:15p	993
SCAT	2:20p	2:35p	2:51p	2:57p	3:08p	3:15p	3:23p	3:35p	3:43p	3:53p	4:01p	4:06p	4:25p	4:35p	SCAT
995	2:50p	3:05p	3:21p	3:27p	3:38p	3:45p	3:53p	3:55p	4:03p	4:13p	4:21p	4:26p	4:45p	4:55p	995
991	3:05p	3:15p	3:31p	3:37p	3:48p	3:55p	4:03p	4:15p	4:23p	4:33p	4:41p	4:46p	5:05p	5:15p	991
SCAT	3:20p	3:35p	3:51p	3:57p	4:08p	4:15p	4:23p	4:35p	4:43p	4:53p	5:01p	5:06p	5:25p	5:35p	SCAT
996	3:40p	3:55p	4:11p	4:17p	4:28p	4:35p	4:43p	4:55p	5:03p	5:13p	5:21p	5:26p	5:45p	5:55p	996
SCAT	4:00p	4:15p	4:31p	4:37p	4:48p	4:55p	5:03p	5:15p	5:23p	5:33p	5:41p	5:46p	6:05p	6:15p	SCAT
993	4:20p	4:35p	4:51p	4:57p	5:08p	5:15p	5:23p	5:35p	5:43p	5:53p	6:01p	6:06p	6:25p	6:35p	993
SCAT	4:40p	4:55p	5:11p	5:17p	5:28p	5:35p	5:43p	5:55p	6:03p	6:13p	6:21p	6:26p	6:45p	6:55p	SCAT
995	5:00p	5:15p	5:31p	5:37p	5:48p	5:55p	6:03p	6:15p	6:23p	6:33p	6:41p	6:46p	7:05p	7:15p	995
991	5:20p	5:35p	5:51p	5:57p	6:08p	6:15p	6:23p	6:35p	6:43p	6:53p	7:01p	7:06p	7:25p	7:35p	991
SCAT	5:40p	5:55p	6:11p	6:17p	6:28p	6:35p	6:43p	6:55p	7:03p	7:13p	7:21p	7:26p	7:45p	7:55p	SCAT
996	6:00p	6:15p	6:31p	6:37p	6:48p	6:55p	7:03p	7:15p	7:23p	7:33p	7:41p	7:46p	8:05p	8:15p	996
SCAT	6:20p	6:35p	6:51p	6:57p	7:08p	7:15p	7:23p	7:35p	7:43p	7:53p	8:01p	8:06p	8:25p	8:35p	993
993	6:40p	6:55p	7:11p	7:17p	7:28p	7:35p	7:43p	7:55p	8:03p	8:13p	8:21p	8:26p	8:45p	8:55p	993
SCAT	7:00p	7:15p	7:30p	7:35p	7:46p	7:52p	8:00p	8:15p	8:23p	8:33p	8:41p	8:46p	9:05p	9:15p	991
995	7:30p	7:45p	8:00p	8:05p	8:16p	8:22p	8:30p	8:45p	8:52p	9:02p	9:10p	9:15p	9:35p	9:45p	995
991	8:00p	8:15p	8:30p	8:35p	8:46p	8:52p	9:00p	9:15p	9:23p	9:33p	9:41p	9:46p	10:05p	10:15p	991
SCAT	8:30p	8:45p	9:00p	9:05p	9:14p	9:20p	9:28p								

PLAN YOUR RIDE WITH



### HOLIDAYS:

Fixed route buses are not in service on the following holidays: **New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day and Christmas Day.**

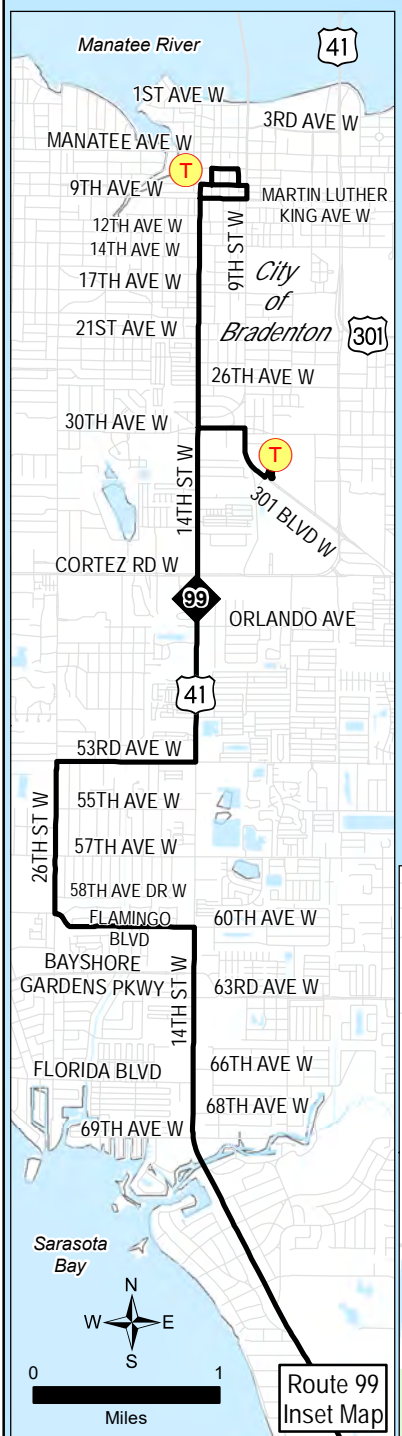


See Route 99 Inset Map

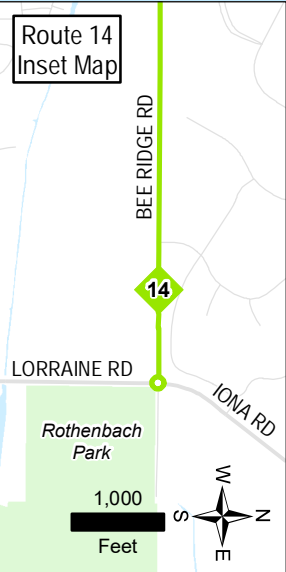
See Longboat Key Inset Map

See Route 14 Inset Map

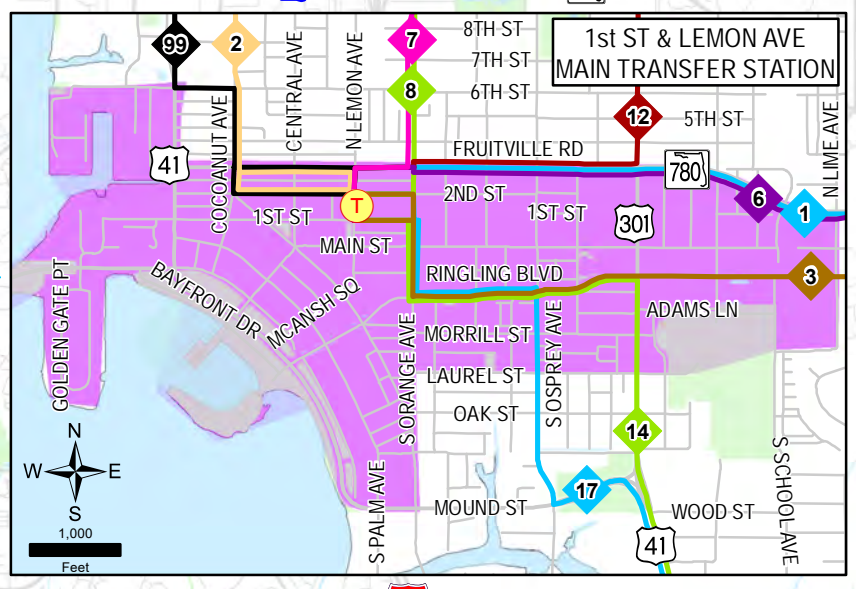
See Weekday South County System Map



Route 99 Inset Map



Route 14 Inset Map



1st ST & LEMON AVE MAIN TRANSFER STATION

**Transfer Points**

**On Demand Service**

- Lido/Longboat On Demand
- Siesta Key On Demand

**Hospitals**

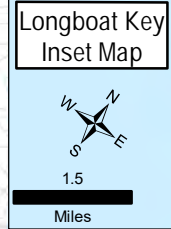
**Major Roadways**

**Local Streets**

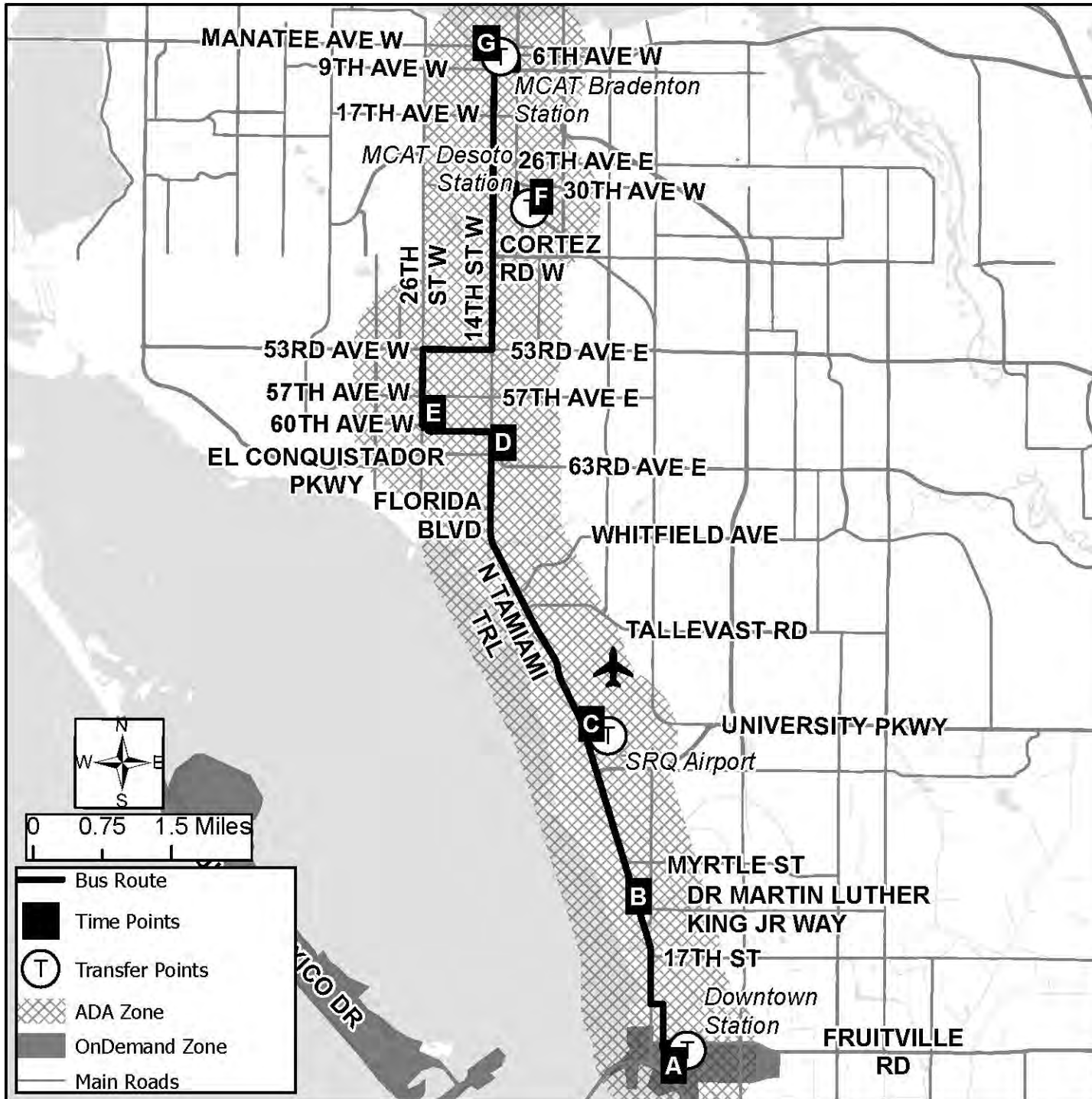
**Water Bodies**

**Parks**

**Sarasota County Boundary**



Longboat Key Inset Map



Go paperless with the **Breeze Rider App!**

- Enter a starting and ending location to plan your bus trip.
- Track the bus in real time.
- Subscribe to notifications about service changes or interruptions.

Available in app stores or online at [scgov.net/BreezeRiderApp](http://scgov.net/BreezeRiderApp).

What is **OnDemand**?

- Curb-to-curb service upon request to/from any location within a zone.
- Zones in 1) Downtown/Lido Key/Longboat Key, 2) Siesta Key, 3) Venice/Englewood, and 4) North Port.
- Request an OnDemand ride through the *OnDemand by Sarasota County* mobile app, web portal, or at 941-300-1553.

*Our transit services will soon be united under the name Breeze.*

**Need additional information?**

Call 311 or visit [scgov.net/scat](http://scgov.net/scat) for details about fares, hours, holidays, paratransit eligibility, and more.

Route  
**99**  
 Monday thru Saturday

**MCAT Bradenton Station to Sarasota Downtown Station via US 41**

*Effective May 2023*



**Route 99 Monday thru Saturday Time Points, Effective May 2023**

Southbound to Downtown Station					Northbound to MCAT Bradenton Station															
Service Provider	G: MCAT Bradenton Station 601 13th Street West	ARRIVE F: MCAT DeSoto Station 820 301 Boulevard West	DEPART F: MCAT DeSoto Station 820 301 Boulevard West	E: State College of Florida (SCF) 26th Street West @ 60th Avenue West	D: US 41 @ Bayshore Gardens Parkway	ARRIVE C: SRQ Airport Airport Circle @ Bradenton Connector	DEPART C: SRQ Airport Airport Circle @ Bradenton Connector	B: US 41 @ Dr MLK Jr Way	A: (Sarasota) Downtown Station 150 North Lemon Avenue		Service Provider	A: (Sarasota) Downtown Station 150 North Lemon Avenue	B: US 41 @ Dr MLK Jr Way	ARRIVE C: SRQ Airport Airport Circle @ Bradenton Connector	DEPART C: SRQ Airport Airport Circle @ Bradenton Connector	D: US 41 @ Bayshore Gardens Parkway	E: State College of Florida (SCF) 26th Street West @ 60th Avenue West	ARRIVE F: MCAT DeSoto Station 820 301 Boulevard West	DEPART F: MCAT DeSoto Station 820 301 Boulevard West	G: MCAT Bradenton Station 601 13th Street West
MCAT	-	-	-	5:15 AM	5:25 AM	5:28 AM	5:35 AM	5:35 AM	5:42 AM		MCAT	5:15 AM	5:21 AM	5:28 AM	5:30 AM	5:37 AM	-	-	-	5:50 AM
MCAT	-	5:40 AM	5:53 AM	5:55 AM	6:10 AM	6:15 AM	6:24 AM	6:26 AM	6:33 AM		MCAT	-	-	-	-	5:35 AM	5:42 AM	5:46 AM	5:58 AM	6:05 AM
SCAT	6:00 AM	6:13 AM	6:15 AM	6:30 AM	6:35 AM	6:44 AM	6:46 AM	6:53 AM	7:01 AM		MCAT	-	5:35 AM	5:41 AM	5:48 AM	5:50 AM	5:57 AM	6:01 AM	6:13 AM	6:25 AM
MCAT	6:20 AM	6:34 AM	6:35 AM	6:51 AM	6:57 AM	7:06 AM	7:08 AM	7:15 AM	7:23 AM		SCAT	5:55 AM	6:01 AM	6:08 AM	6:10 AM	6:17 AM	6:21 AM	6:33 AM	6:45 AM	6:53 AM
MCAT	6:40 AM	6:54 AM	6:55 AM	7:11 AM	7:17 AM	7:26 AM	7:28 AM	7:35 AM	7:43 AM		MCAT	6:15 AM	6:23 AM	6:31 AM	6:33 AM	6:41 AM	6:46 AM	7:03 AM	7:05 AM	7:15 AM
SCAT	7:00 AM	7:14 AM	7:15 AM	7:31 AM	7:37 AM	7:46 AM	7:48 AM	7:55 AM	8:03 AM		SCAT	6:35 AM	6:43 AM	6:51 AM	6:53 AM	7:01 AM	7:06 AM	7:23 AM	7:25 AM	7:35 AM
MCAT	7:20 AM	7:34 AM	7:35 AM	7:51 AM	7:57 AM	8:06 AM	8:08 AM	8:15 AM	8:23 AM		MCAT	6:55 AM	7:03 AM	7:11 AM	7:13 AM	7:21 AM	7:26 AM	7:43 AM	7:45 AM	7:55 AM
SCAT	7:40 AM	7:54 AM	7:55 AM	8:11 AM	8:17 AM	8:26 AM	8:28 AM	8:35 AM	8:43 AM		SCAT	7:15 AM	7:23 AM	7:31 AM	7:33 AM	7:41 AM	7:46 AM	8:03 AM	8:05 AM	8:15 AM
MCAT	8:00 AM	8:14 AM	8:15 AM	8:31 AM	8:37 AM	8:46 AM	8:48 AM	8:55 AM	9:03 AM		MCAT	7:35 AM	7:43 AM	7:51 AM	7:53 AM	8:01 AM	8:06 AM	8:23 AM	8:25 AM	8:35 AM
SCAT	8:20 AM	8:34 AM	8:35 AM	8:51 AM	8:57 AM	9:06 AM	9:08 AM	9:15 AM	9:23 AM		MCAT	7:55 AM	8:03 AM	8:11 AM	8:13 AM	8:21 AM	8:26 AM	8:43 AM	8:45 AM	8:55 AM
MCAT	8:50 AM	9:04 AM	9:05 AM	9:21 AM	9:27 AM	9:36 AM	9:38 AM	9:45 AM	9:53 AM		SCAT	8:15 AM	8:23 AM	8:31 AM	8:33 AM	8:41 AM	8:46 AM	9:03 AM	9:05 AM	9:15 AM
SCAT	9:20 AM	9:34 AM	9:35 AM	9:51 AM	9:57 AM	10:06 AM	10:08 AM	10:15 AM	10:23 AM		MCAT	8:35 AM	8:43 AM	8:51 AM	8:53 AM	9:01 AM	9:06 AM	9:23 AM	9:25 AM	9:35 AM
MCAT	9:50 AM	10:04 AM	10:05 AM	10:21 AM	10:27 AM	10:36 AM	10:38 AM	10:45 AM	10:53 AM		SCAT	8:55 AM	9:03 AM	9:11 AM	9:13 AM	9:21 AM	9:26 AM	9:43 AM	9:45 AM	9:55 AM
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SCAT	10:50 AM	11:04 AM	11:05 AM	11:21 AM	11:27 AM	11:36 AM	11:38 AM	11:45 AM	11:53 AM		SCAT	9:45 AM	9:53 AM	10:01 AM	10:03 AM	10:11 AM	10:16 AM	10:33 AM	10:35 AM	10:45 AM
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SCAT	11:50 AM	12:04 PM	12:05 PM	12:21 PM	12:27 PM	12:36 PM	12:38 PM	12:45 PM	12:53 PM		SCAT	10:45 AM	10:53 AM	11:01 AM	11:03 AM	11:11 AM	11:16 AM	11:33 AM	11:35 AM	11:45 AM
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SCAT	12:50 PM	1:04 PM	1:05 PM	1:21 PM	1:27 PM	1:36 PM	1:38 PM	1:45 PM	1:53 PM		SCAT	11:45 AM	11:53 AM	12:01 PM	12:03 PM	12:11 PM	12:16 PM	12:33 PM	12:35 PM	12:45 PM
SCAT	1:20 PM	1:34 PM	1:35 PM	1:51 PM	1:57 PM	2:06 PM	2:08 PM	2:15 PM	2:23 PM		SCAT	12:15 PM	12:23 PM	12:31 PM	12:33 PM	12:41 PM	12:46 PM	1:03 PM	1:05 PM	1:15 PM
MCAT	1:50 PM	2:04 PM	2:05 PM	2:21 PM	2:27 PM	2:36 PM	2:38 PM	2:45 PM	2:53 PM		MCAT	12:45 PM	12:53 PM	1:01 PM	1:03 PM	1:11 PM	1:16 PM	1:33 PM	1:35 PM	1:45 PM
SCAT	2:20 PM	2:34 PM	2:35 PM	2:51 PM	2:57 PM	3:06 PM	3:08 PM	3:15 PM	3:23 PM		SCAT	1:15 PM	1:23 PM	1:31 PM	1:33 PM	1:41 PM	1:46 PM	2:03 PM	2:05 PM	2:15 PM
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MCAT	7:30 PM	7:43 PM	7:45 PM	8:00 PM	8:05 PM	8:14 PM	8:16 PM	8:22 PM	8:30 PM		SCAT	6:55 PM	7:03 PM	7:11 PM	7:13 PM	7:21 PM	7:26 PM	7:43 PM	7:45 PM	7:55 PM
MCAT	8:00 PM	8:13 PM	8:15 PM	8:30 PM	8:35 PM	8:44 PM	8:46 PM	8:52 PM	9:00 PM		MCAT	7:15 PM	7:23 PM	7:31 PM	7:33 PM	7:41 PM	7:46 PM	8:03 PM	8:05 PM	8:15 PM
SCAT	8:30 PM	8:43 PM	8:45 PM	9:00 PM	9:05 PM	9:14 PM	9:14 PM	9:20 PM	9:28 PM		MCAT	7:45 PM	7:53 PM	8:00 PM	8:00 PM	8:07 PM	8:12 PM	8:27 PM	8:29 PM	8:39 PM
											MCAT	8:30 PM	8:38 PM	8:45 PM	8:45 PM	8:52 PM	8:57 PM	9:12 PM	-	-
											MCAT	9:00 PM	9:08 PM	9:15 PM	-	-	-	-	-	-

Time points are estimated under typical travel conditions. Riders should be at the bus stop at least 5 minutes prior to the listed time. View real-time information for all bus stop locations in the Breeze Rider app.



## Appendix B: Existing Operational Analysis



## Historical FDOT AADT Counts

FLORIDA DEPARTMENT OF TRANSPORTATION  
 TRANSPORTATION STATISTICS OFFICE  
 2022 HISTORICAL AADT REPORT

COUNTY: 13 - MANATEE

SITE: 0003 - SR 45/US BUS 41, NORTH OF SR 684/CORTEZ ROAD

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2022	26500	C	N 14000		S 12500	9.00	54.80	3.20
2021	29000	C	N 14500		S 14500	9.00	54.00	3.00
2020	28000	C	N 14000		S 14000	9.00	52.60	3.40
2019	30000	C	N 15000		S 15000	9.00	55.80	2.90
2018	26500	C	N 13500		S 13000	9.00	55.60	2.90
2017	28000	C	N 13500		S 14500	9.00	56.00	3.40
2016	32000	C	N 16000		S 16000	9.00	56.20	3.40
2015	29000	C	N 14500		S 14500	9.00	55.60	3.40
2014	25500	C	N 13000		S 12500	9.00	55.00	3.40
2013	28500	C	N 14000		S 14500	9.00	54.80	3.00
2012	26500	C	N 13000		S 13500	9.00	55.50	3.20
2011	26000	C	N 13000		S 13000	9.00	55.50	2.80
2010	27500	C	N 13500		S 14000	9.85	54.33	3.00
2009	27500	C	N 13500		S 14000	9.90	55.60	2.50
2008	30500	C	N 15000		S 15500	10.11	54.86	2.80
2007	29500	C	N 15000		S 14500	9.73	56.20	3.00

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE  
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE  
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

\*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

FLORIDA DEPARTMENT OF TRANSPORTATION  
 TRANSPORTATION STATISTICS OFFICE  
 2022 HISTORICAL AADT REPORT

COUNTY: 13 - MANATEE

SITE: 0009 - SR 45/US 41B, N OF GREEN BRIDGE/S OF RIVERSIDE DR

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2022	39000	C	N 20000		S 19000	9.00	54.80	4.10
2021	37000	C	N 18500		S 18500	9.00	54.00	3.50
2020	34000	C	N 16500		S 17500	9.00	52.60	4.00
2019	36000	C	N 18000		S 18000	9.00	55.80	3.60
2018	35000	C	N 18000		S 17000	9.00	55.60	3.00
2017	34500	C	N 17500		S 17000	9.00	56.00	3.20
2016	35000	C	N 17500		S 17500	9.00	56.20	3.80
2015	33000	C	N 16500		S 16500	9.00	55.60	2.90
2014	33000	C	N 16500		S 16500	9.00	55.00	3.40
2013	32000	C	N 16000		S 16000	9.00	54.80	3.30
2012	31000	C	N 15500		S 15500	9.00	55.50	3.20
2011	31500	C	N 15500		S 16000	9.00	55.50	2.90
2010	31500	C	N 15500		S 16000	9.85	54.33	2.70
2009	33500	C	N 16500		S 17000	9.90	55.60	3.60
2008	34000	C	N 17000		S 17000	10.11	54.86	4.00
2007	34000	C	N 17000		S 17000	9.73	56.20	3.50

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE  
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE  
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

\*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES



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COUNTY: 13 - MANATEE

SITE: 5001 - SR 45/US BUS 41, SOUTH OF 17TH AVENUE WEST

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2022	19000	C	N 10000		S 9000	9.00	54.80	2.60
2021	16800	C	N 8700		S 8100	9.00	54.00	2.90
2020	15800	C	N 7800		S 8000	9.00	52.60	3.00
2019	16400	C	N 8000		S 8400	9.00	55.80	2.40
2018	17700	C	N 9000		S 8700	9.00	55.60	2.70
2017	17400	C	N 9100		S 8300	9.00	56.00	2.20
2016	21000	C	N 11000		S 10000	9.00	56.20	3.00
2015	18200	C	N 9500		S 8700	9.00	55.60	2.20
2014	18700	C	N 9400		S 9300	9.00	55.00	2.30
2013	19900	C	N 10000		S 9900	9.00	54.80	1.90
2012	19300	C	N 10000		S 9300	9.00	55.50	2.10
2011	18200	C	N 9500		S 8700	9.00	55.50	1.80
2010	19400	C	N 9900		S 9500	9.85	54.33	2.00
2009	19900	C	N 10500		S 9400	9.90	55.60	2.00
2008	21500	C	N 11000		S 10500	10.11	54.86	2.50
2007	22500	C	N 11500		S 11000	9.73	56.20	2.50

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE  
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE  
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

\*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

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COUNTY: 13 - MANATEE

SITE: 5002 - SR-45/US-41B, S OF 7TH STREET WEST

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2022	31500	F	N 15500		S 16000	9.00	54.80	4.10
2021	31500	C	N 15500		S 16000	9.00	54.00	3.50
2020	28000	C	N 14000		S 14000	9.00	52.60	4.00
2019	29500	C	N 14500		S 15000	9.00	55.80	3.60
2018	29500	C	N 15000		S 14500	9.00	55.60	3.00
2017	32000	C	N 16000		S 16000	9.00	56.00	2.60
2016	31500	C	N 15500		S 16000	9.00	56.20	2.60
2015	29000	C	N 14500		S 14500	9.00	55.60	2.60
2014	28000	C	N 14000		S 14000	9.00	55.00	2.60
2013	28000	C	N 14000		S 14000	9.00	54.80	3.00
2012	29000	C	N 14000		S 15000	9.00	55.50	2.50
2011	26500	C	N 13000		S 13500	9.00	55.50	2.50
2010	27500	C	N 13500		S 14000	9.85	54.33	2.70

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE  
S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE  
V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

\*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

FLORIDA DEPARTMENT OF TRANSPORTATION  
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COUNTY: 13 - MANATEE

SITE: 5003 - SR 45/US BUS 41, S OF 10TH ST WEST/SR 43 PALMETTO

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2022	30500	F	N 15500		S 15000	9.00	54.80	4.10
2021	30500	C	N 15500		S 15000	9.00	54.00	3.50
2020	28000	C	N 14000		S 14000	9.00	52.60	4.00
2019	29000	C	N 14500		S 14500	9.00	55.80	3.60
2018	28500	C	N 14500		S 14000	9.00	55.60	3.00
2017	30000	C	N 15000		S 15000	9.00	56.00	2.40
2016	26500	C	N 13500		S 13000	9.00	56.20	2.40
2015	29500	C	N 14500		S 15000	9.00	55.60	2.40
2014	27500	C	N 13500		S 14000	9.00	55.00	2.60
2013	28000	C	N 14000		S 14000	9.00	54.80	2.70
2012	29500	C	N 14500		S 15000	9.00	55.50	2.80
2011	25000	C	N 12500		S 12500	9.00	55.50	2.20
2010	28500	C	N 14000		S 14500	9.85	54.33	2.70
2009	29000	C	N 14000		S 15000	9.90	55.60	2.20
2008	29500	C	N 14500		S 15000	10.11	54.86	3.40
2007	28500	C	N 14500		S 14000	9.73	56.20	1.90

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE  
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE  
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

\*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

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COUNTY: 13 - MANATEE

SITE: 5004 - SR 45/US BUS 41, N OF 10TH ST WEST/SR 43 PALMETTO

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2022	28500	C	N 14500		S 14000	9.00	54.80	3.60
2021	25500	C	N 13000		S 12500	9.00	54.00	4.00
2020	23000	C	N 11500		S 11500	9.00	52.60	4.90
2019	24000	C	N 12000		S 12000	9.00	55.80	3.40
2018	25000	C	N 12500		S 12500	9.00	55.60	4.20
2017	25000	C	N 12500		S 12500	9.00	56.00	3.40
2016	26000	C	N 13000		S 13000	9.00	56.20	3.30
2015	24000	C	N 11500		S 12500	9.00	55.60	2.80
2014	25000	C	N 12000		S 13000	9.00	55.00	3.00
2013	23500	C	N 11500		S 12000	9.00	54.80	3.20
2012	23000	C	N 11000		S 12000	9.00	55.50	2.80
2011	22500	C	N 11000		S 11500	9.00	55.50	2.70
2010	24000	C	N 11500		S 12500	9.85	54.33	3.30
2009	24500	C	N 12500		S 12000	9.90	55.60	2.90
2008	24500	C	N 12000		S 12500	10.11	54.86	3.30
2007	24500	C	N 12000		S 12500	9.73	56.20	3.20

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE  
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE  
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

\*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

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COUNTY: 13 - MANATEE

SITE: 5014 - SR 45/US BUS 41, SOUTH OF 8TH AVENUE WEST

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR	
2022	11600	F	N	6100	S	5500	9.00	54.80	2.80
2021	11600	C	N	6100	S	5500	9.00	54.00	2.80
2020	11100	C	N	5900	S	5200	9.00	52.60	3.50
2019	11200	C	N	5600	S	5600	9.00	55.80	3.50
2018	11500	C	N	6000	S	5500	9.00	55.60	2.90
2017	13200	C	N	7300	S	5900	9.00	56.00	2.40
2016	15900	C	N	8600	S	7300	9.00	56.20	2.00
2015	13700	C	N	7600	S	6100	9.00	55.60	2.60
2014	14200	C	N	7400	S	6800	9.00	55.00	2.50
2013	14900	C	N	7900	S	7000	9.00	54.80	2.10
2012	14900	C	N	7900	S	7000	9.00	55.50	2.60
2011	13700	C	N	6500	S	7200	9.00	55.50	3.00
2010	14200	C	N	7500	S	6700	9.85	54.33	2.90
2009	14700	C	N	7700	S	7000	9.90	55.60	2.10
2008	16500	C	N	8600	S	7900	10.11	54.86	2.30
2007	17600	C	N	9500	S	8100	9.73	56.20	2.30

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE  
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE  
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

\*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

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COUNTY: 13 - MANATEE

SITE: 5017 - SR 45/US BUS 41, SOUTH OF SR 64/MANATEE AVENUE WB

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2022	25000	F	N 14500		S 10500	9.00	54.80	3.20
2021	25000	C	N 14500		S 10500	9.00	54.00	3.20
2020	26000	C	N 15500		S 10500	9.00	52.60	3.50
2019	27000	C	N 15500		S 11500	9.00	55.80	3.30
2018	26500	C	N 15500		S 11000	9.00	55.60	3.20
2017	26500	C	N 15000		S 11500	9.00	56.00	2.90
2016	29000	C	N 16500		S 12500	9.00	56.20	3.20
2015	23000	C	N 11000		S 12000	9.00	55.60	3.10
2014	25500	C	N 15000		S 10500	9.00	55.00	2.90
2013	26400	C	N 16500		S 9900	9.00	54.80	3.00
2012	24500	C	N 14500		S 10000	9.00	55.50	2.60
2011	24000	C	N 14000		S 10000	9.00	55.50	2.70
2010	25000	C	N 14500		S 10500	9.85	54.33	2.40
2009	26500	C	N 15500		S 11000	9.90	55.60	3.60
2008	26000	C	N 15500		S 10500	10.11	54.86	3.30
2007	27000	C	N 16000		S 11000	9.73	56.20	2.80

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE  
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE  
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

\*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

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COUNTY: 13 - MANATEE

SITE: 5035 - SR-45/US-41B, S OF GAINES AVE/TALLEVAST RD

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2022	38500	F	N 19000		S 19500	9.00	54.80	3.00
2021	38500	C	N 19000		S 19500	9.00	54.00	3.30
2020	34000	C	N 16500		S 17500	9.00	52.60	3.60
2019	39500	C	N 19500		S 20000	9.00	55.80	3.10
2018	39500	C	N 19500		S 20000	9.00	55.60	3.10
2017	40000	C	N 20000		S 20000	9.00	56.00	2.90
2016	39500	C	N 19500		S 20000	9.00	56.20	2.80
2015	39500	C	N 20000		S 19500	9.00	55.60	3.10
2014	41500	F	N 21000		S 20500	9.00	55.00	3.00
2013	40500	C	N 20500		S 20000	9.00	54.80	3.00
2012	36500	C	N 18500		S 18000	9.00	55.50	2.90
2011	35000	C	N 17000		S 18000	9.00	55.50	2.90
2010	38500	C	N 19000		S 19500	9.85	54.33	2.90

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE  
S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE  
V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

\*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

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COUNTY: 13 - MANATEE

SITE: 5037 - SR 45/US 41, NORTH OF 57TH AVENUE WEST

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2022	43500	F	N 21500		S 22000	9.00	54.80	3.00
2021	43500	C	N 21500		S 22000	9.00	54.00	3.00
2020	42500	C	N 21500		S 21000	9.00	52.60	3.40
2019	44500	C	N 22000		S 22500	9.00	55.80	3.10
2018	47500	C	N 23500		S 24000	9.00	55.60	3.40
2017	45000	C	N 22500		S 22500	9.00	56.00	2.90
2016	47500	S	N 23500		S 24000	9.00	56.20	2.60
2015	46500	F	N 23000		S 23500	9.00	55.60	2.60
2014	45500	C	N 22500		S 23000	9.00	55.00	2.60
2013	47000	C	N 23000		S 24000	9.00	54.80	2.30
2012	42500	C	N 21500		S 21000	9.00	55.50	2.30
2011	49000	C	N 24500		S 24500	9.00	55.50	2.70
2010	47000	C	N 23000		S 24000	9.85	54.33	2.90
2009	45000	C	N 22500		S 22500	9.90	55.60	2.60
2008	51500	C	N 25500		S 26000	10.11	54.86	2.80
2007	52500	C	N 26000		S 26500	9.73	56.20	2.80

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE  
S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE  
V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

\*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES



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COUNTY: 13 - MANATEE

SITE: 5038 - SR 45/US 41, NORTH OF SR 70/53RD AVENUE WEST

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2022	44500	C	N 23000		S 21500	9.00	54.80	2.80
2021	44500	C	N 22500		S 22000	9.00	54.00	3.50
2020	42500	C	N 21500		S 21000	9.00	52.60	3.70
2019	44500	C	N 22500		S 22000	9.00	55.80	3.20
2018	45000	C	N 22500		S 22500	9.00	55.60	2.90
2017	44500	C	N 22500		S 22000	9.00	56.00	3.00
2016	49500	C	N 25000		S 24500	9.00	56.20	2.60
2015	48000	F	N 24000		S 24000	9.00	55.60	3.60
2014	47000	C	N 23500		S 23500	9.00	55.00	3.60
2013	44500	C	N 22500		S 22000	9.00	54.80	3.40
2012	45000	C	N 22500		S 22500	9.00	55.50	3.00
2011	43500	C	N 21500		S 22000	9.00	55.50	2.70
2010	47000	C	N 23000		S 24000	9.85	54.33	2.90
2009	46500	C	N 24000		S 22500	9.90	55.60	2.60
2008	56000	C	N 29000		S 27000	10.11	54.86	2.70
2007	46500	C	N 22500		S 24000	9.73	56.20	3.00

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE  
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE  
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

\*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

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COUNTY: 13 - MANATEE

SITE: 5078 - SR 45/BUS US 41, SOUTH OF 26TH AVENUE WEST

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2022	18900	F	N 9800		S 9100	9.00	54.80	2.90
2021	18800	C	N 9700		S 9100	9.00	54.00	2.90
2020	20300	C	N 10500		S 9800	9.00	52.60	3.70
2019	20300	C	N 10500		S 9800	9.00	55.80	2.50
2018	17600	C	N 8800		S 8800	9.00	55.60	2.90
2017	21000	C	N 11000		S 10000	9.00	56.00	2.50
2016	23500	C	N 12000		S 11500	9.00	56.20	2.90
2015	21500	C	N 11000		S 10500	9.00	55.60	3.20
2014	20500	C	N 10500		S 10000	9.00	55.00	3.20
2013	21500	C	N 11000		S 10500	9.00	54.80	2.80
2012	21500	C	N 11000		S 10500	9.00	55.50	2.70
2011	21500	C	N 11000		S 10500	9.00	55.50	2.30
2010	21500	C	N 11000		S 10500	9.85	54.33	2.30
2009	23500	C	N 11500		S 12000	9.90	55.60	2.40
2008	23500	C	N 12000		S 11500	10.11	54.86	2.40
2007	25000	C	N 12500		S 12500	9.73	56.20	2.80

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE  
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE  
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

\*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

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COUNTY: 13 - MANATEE

SITE: 5079 - SR 45/BUS US 41, SOUTH OF 63RD AVE W/SAUNDERS RD

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2022	43000	C	N 21500		S 21500	9.00	54.80	3.20
2021	42000	C	N 21000		S 21000	9.00	54.00	3.30
2020	39500	C	N 19500		S 20000	9.00	52.60	3.60
2019	45000	C	N 22500		S 22500	9.00	55.80	3.10
2018	45500	C	N 22500		S 23000	9.00	55.60	3.10
2017	44500	C	N 22000		S 22500	9.00	56.00	3.00
2016	42000	C	N 20500		S 21500	9.00	56.20	3.00
2015	43000	C	N 21000		S 22000	9.00	55.60	3.00
2014	43500	C	N 21500		S 22000	9.00	55.00	2.40
2013	41500	C	N 20500		S 21000	9.00	54.80	3.00
2012	40500	C	N 20000		S 20500	9.00	55.50	2.60
2011	42000	C	N 21000		S 21000	9.00	55.50	2.60
2010	43000	C	N 21000		S 22000	9.85	54.33	2.90
2009	41000	C	N 21000		S 20000	9.90	55.60	2.60
2008	49000	C	N 25000		S 24000	10.11	54.86	2.70
2007	47000	C	N 23500		S 23500	9.73	56.20	3.00

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE  
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE  
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

\*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

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COUNTY: 17 - SARASOTA

SITE: 0001 - SR 45/US 41 @ MANATEE/SARASOTA CTY LINE, SARASOTA

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2022	39000	C	N 19500		S 19500	9.00	52.80	3.00
2021	36500	C	N 18000		S 18500	9.00	52.60	3.20
2020	32500	C	N 16000		S 16500	9.00	52.20	3.70
2019	38500	C	N 18500		S 20000	9.00	52.20	3.10
2018	40500	C	N 20000		S 20500	9.00	52.50	3.00
2017	39500	C	N 19500		S 20000	9.00	52.60	2.80
2016	45000	C	N 22500		S 22500	9.00	52.60	2.90
2015	40500	C	N 20000		S 20500	9.00	52.80	2.90
2014	41500	F	N 20500		S 21000	9.00	52.50	2.80
2013	40500	C	N 20000		S 20500	9.00	52.40	2.80
2012	38500	C	N 19500		S 19000	9.00	52.70	2.30
2011	38500	F	N 19000		S 19500	9.00	53.10	2.70
2010	38500	C	N 19000		S 19500	10.54	52.03	2.70
2009	37000	C	N 18000		S 19000	10.59	53.04	2.70
2008	42500	C	N 20500		S 22000	10.48	52.68	2.70
2007	43500	C	N 21500		S 22000	10.00	54.96	3.00

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE  
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE  
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

\*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

**Relevant D1 LOS Spreadsheets**

# **Appendix B: Florida's Generalized Service Volume Tables**

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# Limited Access

## Freeway Generalized Service Volume Tables

### Peak Hour Directional

	B	C	D	E
2 Lane	2,400	3,170	3,970	4,150
3 Lane	3,390	4,600	5,810	6,130
4 Lane	4,340	6,060	7,700	8,170
5 Lane	5,480	7,450	9,680	10,390
6 Lane	6,630	9,220	11,520	12,760

### Peak Hour Two-Way

	B	C	D	E
4 Lane	4,360	5,760	7,220	7,550
6 Lane	6,160	8,360	10,560	11,150
8 Lane	7,890	11,020	14,000	14,850
10 Lane	9,960	13,550	17,600	18,890
12 Lane	12,050	16,760	20,950	23,200

### AADT

	B	C	D	E
4 Lane	51,300	67,800	84,900	88,800
6 Lane	72,500	98,400	124,200	131,200
8 Lane	92,800	129,600	164,700	174,700
10 Lane	117,200	159,400	207,100	222,200
12 Lane	141,800	197,200	246,500	272,900

(Core Urbanized)

	B	C	D	E
2 Lane	2,500	3,300	4,070	4,240
3 Lane	3,570	4,900	6,080	6,360
4 Lane	4,720	6,500	8,090	8,490
5 Lane	5,790	8,020	10,020	10,610

	B	C	D	E
4 Lane	4,550	6,000	7,400	7,710
6 Lane	6,490	8,910	11,050	11,560
8 Lane	8,580	11,820	14,710	15,440
10 Lane	10,530	14,580	18,220	19,290

	B	C	D	E
4 Lane	50,600	66,700	82,200	85,700
6 Lane	72,100	99,000	122,800	128,400
8 Lane	95,300	131,300	163,400	171,600
10 Lane	117,000	162,000	202,400	214,300

(Urbanized)

	B	C	D	E
2 Lane	2,430	3,180	3,790	3,910
3 Lane	3,520	4,670	5,610	5,870
4 Lane	4,630	6,170	7,440	7,830
5 Lane	5,690	7,640	9,220	9,800

	B	C	D	E
4 Lane	4,420	5,780	6,890	7,110
6 Lane	6,400	8,490	10,200	10,670
8 Lane	8,420	11,220	13,530	14,240
10 Lane	10,350	13,890	16,760	17,820

	B	C	D	E
4 Lane	45,100	59,000	70,300	72,600
6 Lane	65,300	86,600	104,100	108,900
8 Lane	85,900	114,500	138,100	145,300
10 Lane	105,600	141,700	171,000	181,800

(Transitioning)

	B	C	D	E
2 Lane	2,010	2,770	3,270	3,650
3 Lane	2,820	3,990	4,770	5,470
4 Lane	3,630	5,220	6,260	7,300

	B	C	D	E
4 Lane	3,650	5,040	5,950	6,640
6 Lane	5,130	7,250	8,670	9,950
8 Lane	6,600	9,490	11,380	13,270

	B	C	D	E
4 Lane	34,800	48,000	56,700	63,200
6 Lane	48,900	69,000	82,600	94,800
8 Lane	62,900	90,400	108,400	126,400

(Rural)

### Adjustment Factors

Auxiliary Lanes Present in Analysis Direction Adjustment: +1,000  
Ramp Metering Present Adjustment: Multiply by 1.05

Auxiliary Lanes Present in Analysis Direction Adjustment: +1,800  
Ramp Metering Present Adjustment: Multiply by 1.05

Auxiliary Lanes Present in Analysis Direction Adjustment: +20,000  
Ramp Metering Present Adjustment: Multiply by 1.05

This table does not constitute a standard and should be used only for general planning applications. The table should not be used for corridor or intersection design, where more refined techniques exist.

# Limited Access

## Freeway Generalized Service Volume Tables

### Input Parameters

#### Roadway Characteristics

	Core Urbanized	Urbanized	Transitioning	Rural
Number of Lanes (one direction)	2-6	2.5	2-5	2-4
Posted Speed (mph)	65	70	70	70
Auxiliary Lanes	No	No	No	No
Lane Width (feet)	12	12	12	12
Total Ramp Density (ramps/mile)	1.33	2.67	0.50	0.17
Facility Length (miles)	3	3	6	18
Terrain	Level	Level	Level	Level

#### Traffic Characteristics

	Core Urbanized	Urbanized	Transitioning	Rural
Planning Analysis Hour Factor (K)	0.085	0.090	0.098	0.105
Directional Distribution Factor (D)	0.55	0.55	0.55	0.55
Peak Hour Factor (PHF)	0.95	0.95	0.92	0.88
Base Free Flow Speed (mph)	70	75	75	75
Heavy Vehicle Percent (%)	4%	4%	9%	12%
Speed Adjustment Factor (SAF)	0.975	0.975	0.975	0.975
Capacity Adjustment Factor (CAF)	0.968	0.968	0.968	0.968



# C1 & C2

## Motor Vehicle Highway Generalized Service Volume Tables



(C1-Natural & C2-Rural)

### Peak Hour Directional

	B	C	D	E
1 Lane	240	430	730	1,490
2 Lane	1,670	2,390	2,910	3,340
3 Lane	2,510	3,570	4,370	5,010

### Peak Hour Two-Way

	B	C	D	E
2 Lane	440	780	1,330	2,710
4 Lane	3,040	4,350	5,290	6,070
6 Lane	4,560	6,490	7,950	9,110

### AADT

	B	C	D	E
2 Lane	4,600	8,200	14,000	28,500
4 Lane	32,000	45,800	55,700	63,900
6 Lane	48,000	68,300	83,700	95,900

### Adjustment Factors

- 2 Lane Divided Roadway with Exclusive Left Turn Adjustment: Multiply by 1.05
- Multilane Undivided Highway with Exclusive Left Turn Adjustment: Multiply by 0.95
- Multilane Undivided Highway without Exclusive Left Turn Adjustment: Multiply by 0.75

# C1 & C2

## Motor Vehicle Highway Generalized Service Volume Tables

### Input Parameters

#### Roadway Characteristics

	C1	C2
Number of Lanes (one direction)	1	2-3
Posted Speed (mph)	55	55
Base Free Flow Speed (mph)	60	60
Median Type	Undivided	Divided
Shoulder Width (feet)	3	6
Lane Width (feet)	12	12
% No Passing Zone	20%	
Access-Point Density (access/mile)	2	2
Terrain	Level	Level

#### Traffic Characteristics

	C1	C2
Planning Analysis Hour Factor (K)	0.095	0.095
Directional Distribution Factor (D)	0.55	0.55
Peak Hour Factor (PHF)	0.88	0.88
Heavy Vehicle Percent (%)	5%	10%
Speed Adjustment Factor (SAF)	0.975	0.975
Capacity Adjustment Factor (CAF)	0.968	0.968

# C3C & C3R

## Motor Vehicle Arterial Generalized Service Volume Tables

### Peak Hour Directional



	B	C	D	E
1 Lane	*	760	1,070	**
2 Lane	*	1,520	1,810	**
3 Lane	*	2,360	2,680	**
4 Lane	*	3,170	3,180	**

### Peak Hour Two-Way

	B	C	D	E
2 Lane	*	1,380	1,950	**
4 Lane	*	2,760	3,290	**
6 Lane	*	4,290	4,870	**
8 Lane	*	5,760	5,780	**

### AADT

	B	C	D	E
2 Lane	*	15,300	21,700	**
4 Lane	*	30,700	36,600	**
6 Lane	*	47,700	54,100	**
8 Lane	*	64,000	64,200	**



	B	C	D	E
1 Lane	*	970	1,110	**
2 Lane	*	1,700	1,850	**
3 Lane	*	2,620	2,730	**

	B	C	D	E
2 Lane	*	1,760	2,020	**
4 Lane	*	3,090	3,360	**
6 Lane	*	4,760	4,960	**

	B	C	D	E
2 Lane	*	19,600	22,400	**
4 Lane	*	34,300	37,300	**
6 Lane	*	52,900	55,100	**

This table does not constitute a standard and should be used only for general planning applications. The table should not be used for corridor or intersection design, where more refined techniques exist.

# C3C & C3R

## Motor Vehicle Arterial Generalized Service Volume Tables

### Input Parameters

#### Roadway Characteristics

	C3C	C3R
Number of Lanes (one direction)	1-4	1-3
Posted Speed (mph)	45	45
Facility Length (miles)	3.98	2.57

#### Traffic Characteristics

	C3C		C3R	
Planning Analysis Hour Factor (K)	0.09		0.09	
Directional Distribution Factor (D)	0.55		0.55	
Peak Hour Factor (PHF)	0.95		0.92	
Base Saturation Flow Rate	1,950		1,950	
Heavy Vehicle Percent (%)	4		4	
Lane Width	12		12	
Median Type	Non Restrictive (1 lane)	Restrictive (2,3,4 lanes)	Non Restrictive (1 lane)	Restrictive (2,3 lanes)
Roadway Edge Type	Curbed		Flush	
On-Street Parking	None		None	

#### Control Characteristics

	C3C		C3R
Cycle Length	160		190
Major Street Through g/c	0.5 (1,2,3 lanes)	0.45 (4 lanes)	0.5
Yellow Change Interval	5.1		5.1
Red Change Interval	2		2
Number of Signals	10		5

# C2T, C4, C5, & C6

## Motor Vehicle Arterial Generalized Service Volume Tables



(C2T-Rural Town)

### Peak Hour Directional

	B	C	D	E
1 Lane	*	720	940	**
2 Lane	*	1,140	1,640	**
3 Lane	*	2,120	2,510	**

### Peak Hour Two-Way

	B	C	D	E
2 Lane	*	1,310	1,710	**
4 Lane	*	2,070	2,980	**
6 Lane	*	3,850	4,560	**

### AADT

	B	C	D	E
2 Lane	*	13,800	18,000	**
4 Lane	*	21,800	31,400	**
6 Lane	*	40,500	48,000	**



(C4-Urban General)

	B	C	D	E
1 Lane	*	*	870	1,190
2 Lane	*	1,210	1,790	2,020
3 Lane	*	2,210	2,810	2,990
4 Lane	*	2,590	3,310	3,510

	B	C	D	E
2 Lane	*	*	1,580	2,160
4 Lane	*	2,200	3,250	3,670
6 Lane	*	4,020	5,110	5,440
8 Lane	*	4,710	6,020	6,380

	B	C	D	E
2 Lane	*	*	17,600	24,000
4 Lane	*	24,400	36,100	40,800
6 Lane	*	44,700	56,800	60,400
8 Lane	*	52,300	66,900	70,900



(C5-Urban Center)

	B	C	D	E
1 Lane	*	*	690	1,080
2 Lane	*	1,290	1,900	2,130
3 Lane	*	1,410	2,670	3,110
4 Lane	*	2,910	3,560	3,640

	B	C	D	E
2 Lane	*	*	1,250	1,960
4 Lane	*	2,350	3,450	3,870
6 Lane	*	2,560	4,850	5,650
8 Lane	*	5,290	6,470	6,620

	B	C	D	E
2 Lane	*	*	13,900	21,800
4 Lane	*	26,100	38,300	43,000
6 Lane	*	28,400	53,900	62,800
8 Lane	*	58,800	71,900	73,600



(C6-Urban Core)

	B	C	D	E
1 Lane	*	***	790	1,030
2 Lane	*	***	1,490	1,920
3 Lane	*	***	2,730	2,940
4 Lane	*	***	3,250	3,490

	B	C	D	E
2 Lane	*	***	1,440	1,870
4 Lane	*	***	2,710	3,490
6 Lane	*	***	4,960	5,350
8 Lane	*	***	5,910	6,350

	B	C	D	E
2 Lane	*	***	16,000	20,800
4 Lane	*	***	30,100	38,800
6 Lane	*	***	55,100	59,400
8 Lane	*	***	65,700	70,600

### Adjustment Factors

The peak hour directional service volumes should be adjusted by multiplying by 1.2 for one-way facilities  
 The AADT service volumes should be adjusted by multiplying 0.6 for one way facilities  
 2 Lane Divided Roadway with an Exclusive Left Turn Lane(s): Multiply by 1.05  
 2 lane Undivided Roadway with No Exclusive Left Turn Lane(s): Multiply by 0.80

Exclusive right turn lane(s): Multiply by 1.05  
 Multilane Undivided Roadway with an Exclusive Left Turn Lane(s): Multiply by 0.95  
 Multilane Roadway with No Exclusive Left Turn Lane(s): Multiply by 0.75  
 Non-State Signalized Roadway: Multiply by 0.90

This table does not constitute a standard and should be used only for general planning applications. The table should not be used for corridor or intersection design, where more refined techniques exist.

\* Cannot be achieved using table input value defaults.

\*\* Not applicable for that level of service letter grade. For the automobile mode, volumes greater than level of service D become F because intersection capacities have been reached.

# C2T, C4, C5, & C6

## Motor Vehicle Arterial Generalized Service Volume Tables

### Input Parameters

#### Roadway Characteristics

	C2T	C4	C5	C6
Number of Lanes (one direction)	1-3	1-4	1-4	1-4
Posted Speed (mph)	40	45	35	30
Facility Length (miles)	0.78	1.83	1.18	0.74
Number of Signals	4	9	9	7

#### Traffic Characteristics

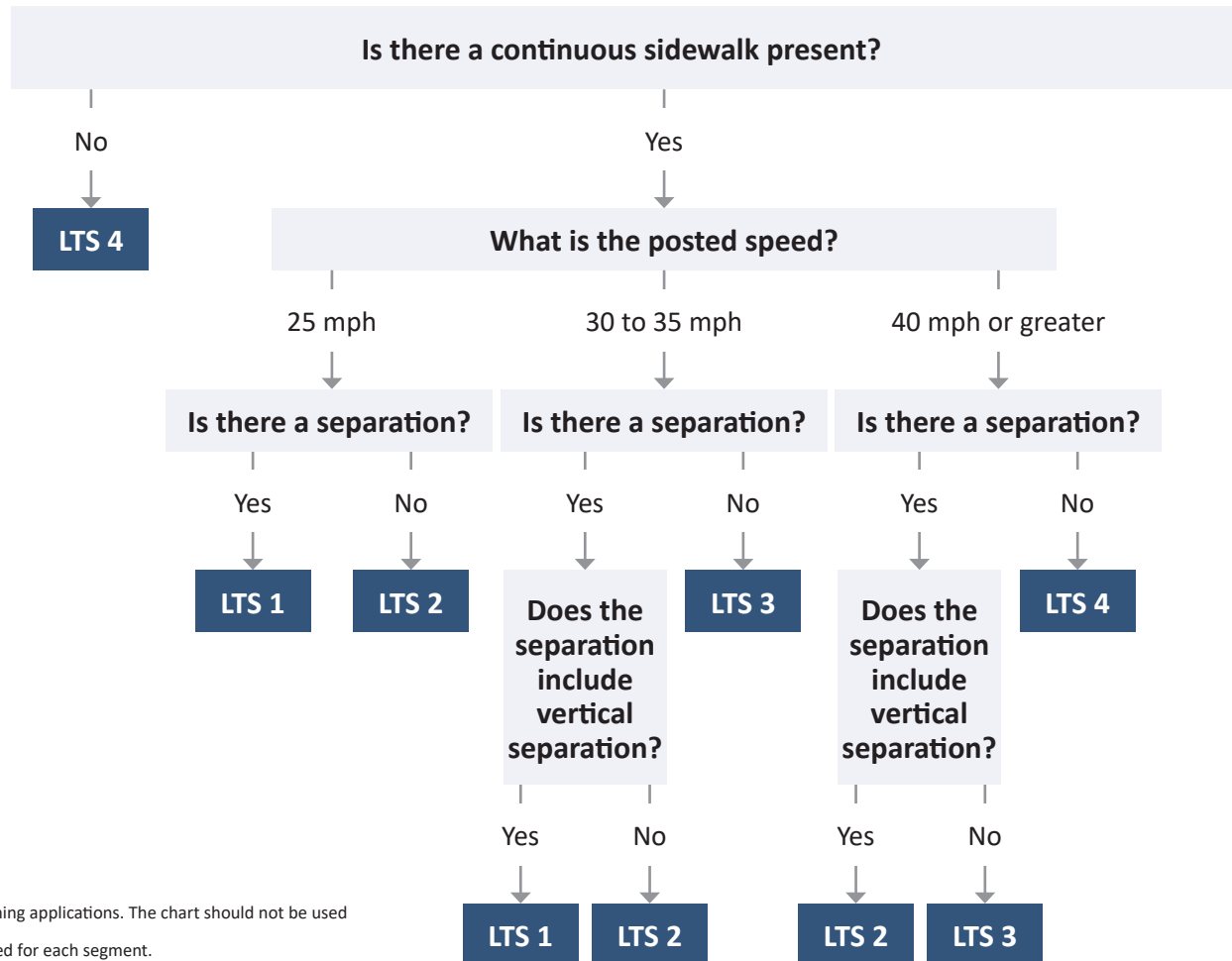
	C2T	C4	C5	C6
Planning Analysis Hour Factor (K)	0.095	0.09	0.09	0.09
Directional Distribution Factor (D)	0.55	0.55	0.55	0.55
Peak Hour Factor (PHF)	0.92	0.95	0.95	0.95
Base Saturation Flow Rate	1,700	1,950	1,950	1,950
Heavy Vehicle Percent (%)	5	3	2	2
Lane Width	11	11	10	10
Median Type	Non Restrictive	Non Restrictive	Non Restrictive	Non Restrictive
Roadway Edge Type	Curb	Curb	Curb	Curb
On-Street Parking	50%	100%	100%	100%

#### Signal Characteristics

	C2T	C4	C5	C6
Cycle Length	90	170	150	120
Major Street Through g/c	0.47	0.52 (1,2,3 lanes)    0.47 (4 lanes)	0.55 (1,2,3 lanes)    0.48 (4 lanes)	0.52 (1,2,3 lanes)    0.46 (4 lanes)
Yellow Change Interval	4.4	4.8	4	3.7
Red Change Interval	2	2	2	2

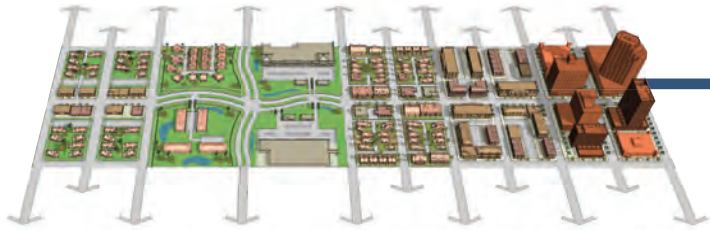
# Pedestrian Level of Traffic Stress Flow Chart

## C2T, C3C, C3R, C4, C5, & C6



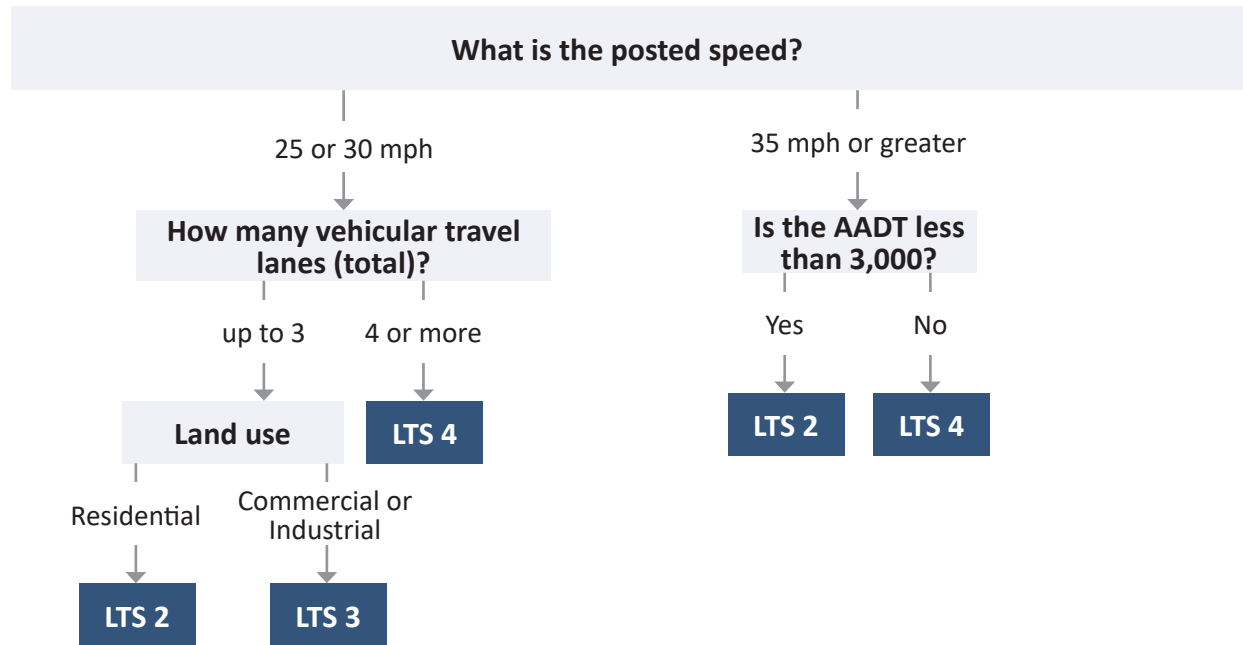
- Notes:
- 1) This chart does not constitute a standard and should be used only for general planning applications. The chart should not be used for corridor or intersection design, where more refined techniques exist.
  - 2) This analysis is conducted for each side of the road and the worst side PLTS is applied for each segment.
  - 3) If the sidewalk width is less than or equal to 5 feet, reduce the PLTS by 1.
  - 4) Separation is defined by space between the outside vehicular travel lane and sidewalk and can include bicycle lanes, unmarked shoulders, street furniture, vertical separation, landscaping, or utility strips. Vertical separation in the separation includes tubular markers, islands, on-street parking, rigid barriers, and landscaping.
  - 5) Sidewalk space over 6 feet can be evaluated as part of the separation.





# Bicycle Level of Traffic Stress Flow Chart to use When No Bicycle Facility is Present or When There are Sharrows Present

C2T, C3C, C3R, C4, C5, & C6



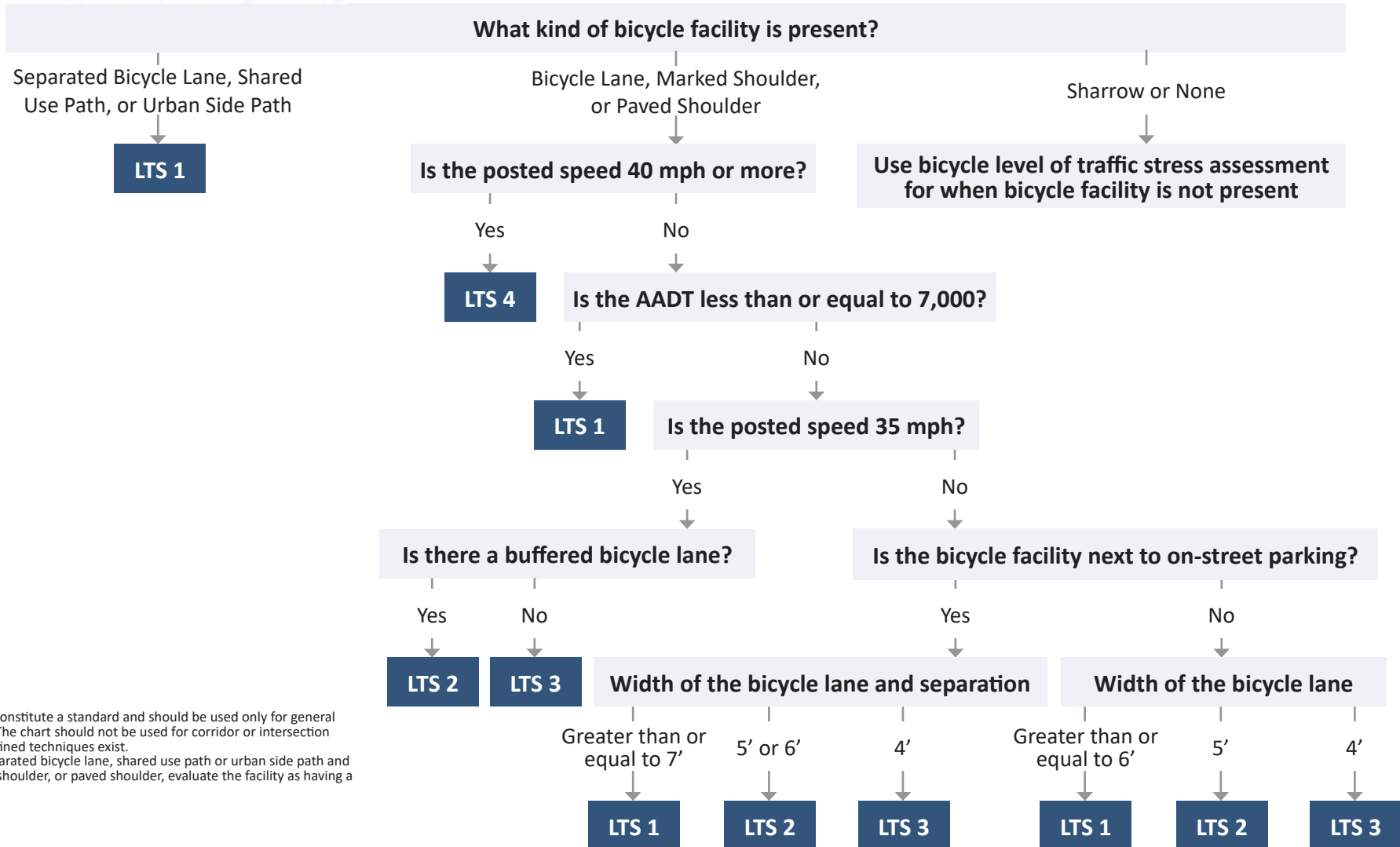
Notes:  
 1) This chart does not constitute a standard and should be used only for general planning applications. The chart should not be used for corridor or intersection design, where more refined techniques exist.





# Bicycle Level of Traffic Stress Flow Chart to use When Bicycle Facility is Present

C2T, C3C, C3R, C4, C5, & C6



Notes:  
 1) This chart does not constitute a standard and should be used only for general planning applications. The chart should not be used for corridor or intersection design, where more refined techniques exist.  
 2) If there is both a separated bicycle lane, shared use path or urban side path and a bicycle lane, marked shoulder, or paved shoulder, evaluate the facility as having a shared use path



## **Manatee County**

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -DAILY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	SIS	Existing Context Class	Future Context Class	Functional Classification	Posted Speed	Area Type	Facility Type	FDOT LOS Std.	County LOS Std.	City LOS Std.	Year 2022										Deficiency Determination
																		Divided/UnDivided	One/Two Way	Left Turn Bays	Right Turn Bays	Thru Lanes	Daily					
																							Capacity	Volume	LOS			
13010000	US 41	TAMIAMI TRAIL	Sarasota County Line	0.000	63rd Ave W/Bay Dr	3.007	3.007		C3C	C4	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	6	56,805	40,938	C			
13010000	US 41	14TH ST W.	63rd Ave W/Bay Dr	3.007	SR 684 /Cortez Rd	5.284	2.277		C3R	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	57,855	43,942	C			
13010000	USB 41	14TH ST W.	SR 684 /Cortez Rd	5.284	9th Ave W/MLK Jr Ave W	7.320	2.036		C4	C4	Principal Arterial-other	40	UA	A	D	D	D	U	2W	WL	WR	4	36,010	21,917	C			
13010000	USB 41	14TH ST W./8TH AVE W.	9th Ave	7.320	10th St W	7.552	0.232		C4	C4	Principal Arterial-other	35	UA	A	D	D	D	U	2W	WL	WR	4	36,010	11,600	C			
13010000	USB 41	8TH AVE W.	Riverside Dr	8.878	10th St W	9.498	0.620		C4	C4	Principal Arterial-other	30	UA	A	D	D	D	U	2W	WL	OR	4	34,295	31,097	D	Near Capacity		
13010001	USB 41	8TH AVE W./9TH ST W.	10th St W	0.000	Riverside Dr	1.379	1.379		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	38,430	36,838	D	Near Capacity		
** 13020000	SR 43/US 301	10TH ST W.	SR 45/US 41 Bus	0.000	US 41/Tamiami Trail	0.643	0.643		C4	C4	Principal Arterial-other	40	UA	A	D	D	D	D	2W	WL	OR	4	36,100	18,286	C			
13020000	US 301	SR 43/US 301	US 41/Tamiami Trail	0.643	CR 683/36th Ave	2.745	2.102		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	38,430	36,500	D	Near Capacity		
13020000	US 301	SR 43/US 301	CR 683/36th Ave	2.745	19th St/51st Ave	3.842	1.097		C3R	C3R	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	39,165	34,222	C			
13020000	US 301	SR 43/US 301	19th St/51st Ave	3.842	Old Tampa Rd/Erie Rd	6.310	2.468		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	38,430	39,835	F	Over Capacity		
** 13020000	US 301	SR 43/US 301	Old Tampa Rd/Erie Rd	6.310	Chin Rd/Palm Blvd	8.528	2.218		C3R	C3R	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	4	39,165	23,674	C			
** 13020000	US 301	SR 43/US 301	Chin Rd/Palm Blvd	8.528	Fort Hamer Rd/121st Ave E	10.054	1.526		C3R	C3R	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	4	39,165	16,200	C			
13020000	US 301	SR 43/US 301	Fort Hamer Rd	10.054	Moccasin Wallow Rd	11.646	1.592		C3R	C3R	Principal Arterial-other	45	UA	H	D	C	D	D	2W	WL	OR	4	37,300	17,114	C			
13020000	US 301	SR 43/US 301	Dickey Rd	11.646	Hillsborough County Line	16.066	4.420		C2	C2	Principal Arterial-other	60	RDA	H	C	C	D	U	2W	WL	WR	2	8,200	8,800	D	Over Capacity		
13030000	USB 41	8TH AVE W.	SR 43/10th St W	0.000	26th St W	1.309	1.309		C4	C4	Principal Arterial-other	35	UA	A	D	D	D	D	2W	WL	WR	4	37,905	26,043	D			
13030103	USB 41	8TH AVE W.	US 41 Bus/SR 45	0.000	26th St W	0.385	0.385		C3C	C3C	Principal Arterial-other	50	UA	A	D	D	D	U	1W	WL	WR	2	23,058	12,000	C			
13030000	US 41	TAMIAMI TRAIL	SR 55/US 19 NB On Ramp	2.225	0.021 mi. S of 45th St E	2.582	0.357		C3C	C3R	Principal Arterial-other	55	UA	A	D	D	D	U	1W	WL	OR	2	21,960	19,500	D			
13030000	US 41	TAMIAMI TRAIL	0.021 mi. S of 45th St E	2.582	I-275	5.202	2.620		C2	C3R	Principal Arterial-other	60	UA	A	D	D	D	D	2W	WL	WR	4	55,700	33,500	C			
13030000	US 41	TAMIAMI TRAIL	I-275	5.202	Piney Point Rd/Inland Transp St	8.542	3.340	SC	C2	C2	Principal Arterial-other	60	UA	H	D	D	D	D	2W	WL	WR	4	55,700	17,900	B			
13030000	US 41	TAMIAMI TRAIL	Piney Point Rd/Inland Transp St	8.542	Hillsborough County Line	9.782	1.240		C2	C2	Principal Arterial-other	60	UA	H	D	D	D	D	2W	WL	WR	4	55,700	15,400	B			
13030101	USB 41 -NB	SR 45/USB 41	26th St W/US 41/SR 45	0.000	SR 55/US 19	0.560	0.560		C3C	C3C	Principal Arterial-other	50	UA	A	D	D	D	U	1W	OL	OR	2	21,960	12,500	C			
13030102	USB 41 -SB	SR 45/US 41	SR 45/US 41/45th St E	0.000	SR 55/US 19	0.457	0.457		C4	C3C	Principal Arterial-other	55	UA	A	D	D	D	U	1W	OL	OR	2	21,660	18,000	D			
13040000	SR 684	CORTEZ RD	SR 789/Gulf Dr	0.000	119th St W	1.188	1.188		C3R	C3R	Principal Arterial-other	35	UA	A	D	D	D	U	2W	WL	WR	2	23,520	17,863	C			
13040000	SR 684	CORTEZ RD	119th St W	1.188	75th St W	3.971	2.783		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	38,430	26,000	C			
13040000	SR 684	CORTEZ RD	75th St W	3.971	37th St W	6.372	2.401		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	38,430	28,500	C			
13040000	SR 684	CORTEZ RD	37th St W	6.372	9th St W	8.011	1.639		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	56,805	51,805	D	Near Capacity		
13040000	US 41	CORTEZ RD	9th St W	8.011	US 41 (Cortez Rd)	8.440	0.429		C3C	C3C	Principal Arterial-other	35	UA	A	D	D	D	D	2W	WL	WR	4	38,430	40,000	F	Over Capacity		
13040000	US 41	44TH AVE E	US 41 (Cortez Rd)	8.440	0.064 mi. E of 1st St	8.574	0.134		C3C	C3C	Minor Arterial	35	UA	A	D	D	D	U	1W	OL	OR	1	10,937	8,100	D			
13040001	US 41	TAMIAMI TRAIL	SR 684/Cortez Rd/44th Ave E	0.000	41st Ave E	0.158	0.158		C3C	C3C	Principal Arterial-other	35	UA	A	D	D	D	D	2W	WL	OR	4	36,600	34,987	D	Near Capacity		
13040002	SR 684	44TH AVE CONNECTOR	44th Ave	0.000	US 41/Tamiami Trail	0.073	0.073		C3C	C3C	Minor Arterial	35	UA	A	D	D	D	D	2W	WL	WR	2	23,924	11,600	C			
13050000	SR 64	6TH AVE E/7TH AVE	US 41B (10th St W)	0.000	10th St E	1.122	1.122		C3C	C4	Principal Arterial-other	35	UA	A	D	D	D	U	1W	WL	WR	3	34,083	17,791	C			

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -DAILY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	Year 2028 (E+C)					Year 2045					Standard K Factor	Existing D Factor	Existing T <sub>24</sub> Factor		
								Thru Lanes	LOS Std	Daily			Thru Lanes	LOS Std	Daily							
										Capacity	Volume	LOS			Capacity	Trend Volume	Trend LOS				Model Volume	Model LOS
13010000	US 41	TAMIAMI TRAIL	Sarasota County Line	0.000	63rd Ave W/Bay Dr	3.007	3.007	6	D	56,805	43,400	C	6	D	59,640	50,380	D	53,765	D	9.00%	54.80%	3.20%
13010000	US 41	14TH ST W.	63rd Ave W/Bay Dr	3.007	SR 684 /Cortez Rd	5.284	2.277	6	D	57,855	46,586	C	6	D	56,805	54,031	D	52,097	D	9.00%	54.80%	2.80%
13010000	USB 41	14TH ST W.	SR 684 /Cortez Rd	5.284	9th Ave W/MLK Jr Ave W	7.320	2.036	4	D	36,010	23,212	C	4	D	36,010	26,970	D	29,954	D	9.00%	54.80%	2.73%
13010000	USB 41	14TH ST W./8TH AVE W.	9th Ave	7.320	10th St W	7.552	0.232	4	D	36,010	12,300	C	4	D	36,010	14,300	C	14,891	C	9.00%	54.80%	2.80%
13010000	USB 41	8TH AVE W.	Riverside Dr	8.878	10th St W	9.498	0.620	4	D	34,295	32,956	D	4	D	34,295	38,216	E	36,153	E	9.00%	54.80%	4.10%
13010001	USB 41	8TH AVE W./9TH ST W.	10th St W	0.000	Riverside Dr	1.379	1.379	4	D	38,430	39,944	F	4	D	38,430	48,641	F	51,082	F	9.00%	54.80%	3.96%
** 13020000	SR 43/US 301	10TH ST W.	SR 45/US 41 Bus	0.000	US 41/Tamiami Trail	0.643	0.643	4	D	36,100	19,407	C	4	D	36,100	22,468	C	17,066	C	9.00%	54.80%	4.90%
13020000	US 301	SR 43/US 301	US 41/Tamiami Trail	0.643	CR 683/36th Ave	2.745	2.102	4	D	38,430	38,700	F	4	D	38,430	44,900	F	43,181	F	9.00%	54.80%	6.90%
13020000	US 301	SR 43/US 301	CR 683/36th Ave	2.745	19th St/51st Ave	3.842	1.097	4	D	39,165	38,000	D	4	D	39,165	48,700	F	40,161	F	9.00%	56.80%	6.50%
13020000	US 301	SR 43/US 301	19th St/51st Ave	3.842	Old Tampa Rd/Erie Rd	6.310	2.468	4	D	38,430	42,446	F	4	D	38,430	49,836	F	48,430	F	9.00%	54.80%	5.10%
** 13020000	US 301	SR 43/US 301	Old Tampa Rd/Erie Rd	6.310	Chin Rd/Palm Blvd	8.528	2.218	4	D	39,165	28,857	C	4	D	39,165	43,657	F	21,357	C	9.00%	54.80%	5.80%
** 13020000	US 301	SR 43/US 301	Chin Rd/Palm Blvd	8.528	Fort Hamer Rd/121st Ave E	10.054	1.526	4	D	39,165	19,400	C	4	D	39,165	28,500	C	11,992	C	9.00%	54.80%	9.30%
13020000	US 301	SR 43/US 301	Fort Hamer Rd	10.054	Moccasin Wallow Rd	11.646	1.592	4	D	37,300	21,151	C	4	D	37,300	32,687	C	18,268	C	9.00%	54.80%	12.88%
13020000	US 301	SR 43/US 301	Dickey Rd	11.646	Hillsborough County Line	16.066	4.420	2	C	8,200	11,200	D	2	C	8,200	18,100	E	14,729	E	9.50%	56.10%	10.70%
13030000	USB 41	8TH AVE W.	SR 43/10th St W	0.000	26th St W	1.309	1.309	4	D	37,905	28,112	D	4	D	37,905	33,933	D	35,371	D	9.00%	54.80%	4.83%
13030103	USB 41	8TH AVE W.	US 41 Bus/SR 45	0.000	26th St W	0.385	0.385	2	D	23,058	13,100	C	2	D	23,058	16,400	C	19,101	C	9.00%	99.90%	5.50%
13030000	US 41	TAMIAMI TRAIL	SR 55/US 19 NB On Ramp	2.225	0.021 mi. S of 45th St E	2.582	0.357	2	D	21,960	22,800	F	2	D	22,380	32,300	F	22,495	F	9.00%	99.90%	10.90%
13030000	US 41	TAMIAMI TRAIL	0.021 mi. S of 45th St E	2.582	I-275	5.202	2.620	4	D	55,700	39,700	C	4	D	55,700	57,300	F	35,653	C	9.00%	54.80%	9.40%
13030000	US 41	TAMIAMI TRAIL	I-275	5.202	Piney Point Rd/Inland Transp St	8.542	3.340	4	D	55,700	21,400	B	4	D	55,700	31,100	B	39,369	C	9.00%	54.80%	16.20%
13030000	US 41	TAMIAMI TRAIL	Piney Point Rd/Inland Transp St	8.542	Hillsborough County Line	9.782	1.240	4	D	55,700	19,400	B	4	D	55,700	30,800	B	30,729	B	9.00%	54.80%	12.60%
13030101	USB 41 -NB	SR 45/USB 41	26th St W/US 41/SR 45	0.000	SR 55/US 19	0.560	0.560	2	D	21,960	14,200	C	2	D	21,960	19,100	D	19,743	D	9.00%	99.90%	5.80%
13030102	USB 41 -SB	SR 45/US 41	SR 45/US 41/45th St E	0.000	SR 55/US 19	0.457	0.457	2	D	21,660	20,900	D	2	D	21,960	28,900	F	20,057	D	9.00%	99.90%	6.90%
13040000	SR 684	CORTEZ RD	SR 789/Gulf Dr	0.000	119th St W	1.188	1.188	2	D	23,520	19,134	C	2	D	23,520	22,757	D	24,428	F	9.00%	57.20%	4.30%
13040000	SR 684	CORTEZ RD	119th St W	1.188	75th St W	3.971	2.783	4	D	38,430	28,500	C	4	D	38,430	35,500	D	36,358	D	9.00%	57.20%	4.30%
13040000	SR 684	CORTEZ RD	75th St W	3.971	37th St W	6.372	2.401	4	D	38,430	30,200	C	4	D	38,430	35,100	D	70,423	F	9.00%	56.10%	3.60%
13040000	SR 684	CORTEZ RD	37th St W	6.372	9th St W	8.011	1.639	6	D	56,805	54,879	D	6	D	56,805	63,703	F	60,070	F	9.00%	55.77%	3.88%
13040000	US 41	CORTEZ RD	9th St W	8.011	US 41 (Cortez Rd)	8.440	0.429	4	D	38,430	42,400	F	4	D	38,430	49,200	F	52,654	F	9.00%	54.80%	4.40%
13040000	US 41	44TH AVE E	US 41 (Cortez Rd)	8.440	0.064 mi. E of 1st St	8.574	0.134	1	D	10,937	9,700	D	1	D	10,937	14,300	F	11,130	F	9.00%	99.90%	5.60%
13040001	US 41	TAMIAMI TRAIL	SR 684/Cortez Rd/44th Ave E	0.000	41st Ave E	0.158	0.158	4	D	36,600	37,074	F	4	D	36,600	43,059	F	36,951	F	9.00%	54.80%	4.34%
13040002	SR 684	44TH AVE CONNECTOR	44th Ave	0.000	US 41/Tamiami Trail	0.073	0.073	2	D	23,924	13,700	C	2	D	23,924	19,600	D	12,069	C	9.00%	60.20%	4.60%
13050000	SR 64	6TH AVE E/7TH AVE	US 41B (10th St W)	0.000	10th St E	1.122	1.122	3	D	34,083	18,842	C	3	D	35,784	21,900	C	27,920	C	9.00%	99.90%	3.75%

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -DAILY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	SIS	Existing Context Class	Future Context Class	Functional Classification	Posted Speed	Area Type	Facility Type	FDOT LOS Std.	County LOS Std.	City LOS Std.	Year 2022										Deficiency Determination
																		Divided/UnDivided	One/Two Way	Left Turn Bays	Right Turn Bays	Thru Lanes	Daily					
																							Capacity	Volume	LOS			
13050000	SR 64	MANATEE AVE	17th St E	1.808	E. of 39th St E	3.742	1.934		C3R	C4	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	57,855	49,500	C			
13050000	SR 64	MANATEE AVE	E. of 39th St E	3.742	57th St E	5.188	1.446		C3C	C3C	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	6	56,805	49,500	C			
** 13050000	SR 64	SR 64	57th St E	5.188	I-75	6.360	1.172		C3C	C3C	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	6	56,805	59,000	F	Over Capacity		
13050000	SR 64	SR 64	I-75	6.360	Lena Rd/Heritage Green Way	7.389	1.029		C2	C3C	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	6	83,700	48,500	C			
** 13050000	SR 64	SR 64	Lena Rd/Heritage Green Way	7.389	Upper Manatee River Rd/Lakewood	8.804	1.415		C2	C2	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	6	83,700	51,000	C			
13050000	SR 64	SR 64	Upper Manatee River Rd/Lakewood	8.804	Rye Rd	10.577	1.773		C2	C3R	Principal Arterial-other	50	UA	H	D	D	D	D	2W	WL	WR	4	55,700	32,000	B			
13050000	SR 64	SR 64	Rye Rd	10.577	CR 675	16.835	6.258		C2	C3R	Principal Arterial-other	60	RDA	H	C	C	D	U	2W	WL	WR	2	8,200	7,119	C			
** 13050000	SR 70	SR 70	CR 675	22.134	Singletary Rd/Myakka Wauchula R	31.713	9.579	SIS	C2	C2	Principal Arterial-other	60	RDA	H	C	C	D	U	2W	WL	WR	2	8,200	8,300	D	Over Capacity		
** 13050000	SR 70	SR 70	Singletary Rd/Myakka Wauchula R	31.713	DeSoto County Line	39.483	7.770	SIS	C2	C2	Principal Arterial-other	60	RDA	H	C	C	D	U	2W	WL	OR	2	8,200	6,400	C			
13050002	SR 64	MANATEE AVE EB/6TH AVE	10th St E	0.000	17th St E	0.576	0.576		C3R	C4	Principal Arterial-other	35	UA	A	D	D	D	U	1W	WL	OR	3	33,060	19,155	C			
13050101	SR 64	MANATEE AVE	12th St E	0.189	10th St W	1.510	1.321		C4	C4	Principal Arterial-other	35	UA	A	D	D	D	U	1W	WL	WR	3	35,784	20,080	C			
13050102	SR 64	MANATEE AVE E	17th St E	0.000	12th St E	0.372	0.372		C3R	C4	Principal Arterial-other	35	UA	A	D	D	D	U	1W	WL	OR	3	33,060	20,011	C			
13060000	SR 62	SR 62	SR 43/US 301	0.000	Corbett John Rd	3.890	3.890		C2	C2	Minor Arterial	60	RDA	H	C	C	D	U	2W	0L	OR	2	8,200	4,300	B			
13060000	SR 62	SR 62	Corbett John Rd	3.890	CR 39 /County Line Rd	17.154	13.264		C2	C2	Minor Arterial	60	RDA	H	C	C	D	U	2W	WL	WR	2	8,200	4,300	B			
13060000	SR 62	SR 62	CR 39 /County Line Rd	17.154	SR 37	19.264	2.110		C2	C2	Minor Arterial	60	RDA	H	C	C	D	U	2W	WL	OR	2	8,200	5,500	C			
13060000	SR 62	SR 62	SR 37	19.264	Hardee County Line	23.240	3.976		C2	C2	Minor Arterial	60	RDA	H	C	C	D	U	2W	0L	WR	2	8,200	5,200	C			
13070000	SR 37	SR 37	SR 62	0.000	Polk County Line	5.534	5.534		C2	C2	Minor Arterial	60	RDA	H	C	C	D	U	2W	WL	WR	2	8,200	2,700	B			
** 13075000	I-75	SR 93/I-75	Sarasota County Line	0.000	SR 70	3.719	3.719	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		0A	8	163,400	140,000	D			
** 13075000	I-75	SR 93/I-75	SR 70	3.719	SR 64/Manatee Ave	7.330	3.611	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		0A	8	163,400	133,000	D			
13075000	I-75	SR 93/I-75	SR 64/Manatee Ave	7.330	19th St/US 301/SR 43	11.014	3.684	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		0A	6	122,800	120,500	D	Near Capacity		
** 13075000	I-75	SR 93/I-75	19th St/US 301/SR 43	11.014	I -275 off Ramp	14.548	3.534	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		0A	6	122,800	115,000	D	Near Capacity		
** 13075000	I-75	SR 93/I-75	I -275 off Ramp	14.548	I -275 on Ramp	15.300	0.752	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		0A	6	122,800	67,500	B			
** 13075000	I-75	SR 93/I-75	I -275 on Ramp	15.300	CR 6/Mocassin Wallow Rd	16.200	0.900	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		WA	6	142,800	95,000	C			
** 13075000	I-75	SR 93/I-75	CR 6/Mocassin Wallow Rd	16.200	Hillsborough County Line	20.571	4.371	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C	D	D	2W		0A	6	69,000	85,000	E	Over Capacity		
13080000	SR 789	GULF OF MEXICO DR	Sarasota County Line	0.000	Binnacle Pointe Dr	2.410	2.410		C3R	C4	Major Collector	45	UA	H	D		E	U	2W	WL	WR	2	23,520	9,900	C			
13080000	SR 789	GULF OF MEXICO DR	Binnacle Pointe Dr	2.410	Coquina Park Ent.	5.177	2.767		C3R	C4	Major Collector	45	UA	H	D		E	U	2W	WL	WR	2	23,520	9,900	C			
13080000	SR 789	GULF DR S	Coquina Park Ent.	5.177	SR 684 (Cortez Rd)	6.666	1.489		C4	C4	Major Collector	25	UA	A	D		D	U	2W	WL	WR	2	18,480	13,329	D			
13080000	SR 789	GULF DR N/EAST BAY DR	SR 684 (Cortez Rd)	6.666	SR 64/Manatee Ave	8.717	2.051		C4	C4	Major Collector	35	UA	A	D		D	U	2W	WL	WR	2	18,480	10,000	D			
13120001	SR 70	15 ST E	301 Blvd	0.000	15th St E	0.058	0.058		C3C	C3C	Major Collector	45	UA	A	D	D	D	U	1W	0L	OR	1	10,937	4,300	C			
13120001	SR 70	15 ST E	15th St E	0.058	SR 64/Manatee Ave	3.030	2.972		C3C	C3C	Major Collector	40	UA	A	D	D	D	U	2W	WL	WR	2	22,785	11,311	C			
** 13121000	US 301	SR683/US301/15ST BLVD	University Pkwy	0.000	63rd Ave E/Saunders Rd	2.811	2.811		LA	LA	Principal Arterial-other	55	UA	A	D	D	D	D	2W		0A	4	82,200	39,000	B			
** 13121000	US 301	SR683/US301/15ST BLVD	63rd Ave E/Saunders Rd	2.811	SR 70/Oneco Rd/53rd Ave E	4.060	1.249		LA	LA	Principal Arterial-other	55	UA	A	D	D	D	D	2W		0A	4	82,200	41,000	B			
13121000	US 301	SR683/US301/15ST BLVD	SR 70/Oneco Rd/53rd Ave E	4.060	38th Ave E	5.332	1.272		LA	LA	Principal Arterial-other	55	UA	A	D	D	D	D	2W		0A	4	82,200	36,000	B			
13121000	US 301	SR683/US301/15ST BLVD	38th Ave E	5.332	SR 70/15th St E	6.487	1.155		C3R	C3R	Principal Arterial-other	55	UA	A	D	D	D	D	2W	WL	WR	4	39,165	31,500	C			
13121000	US 301	SR683/US301/15ST BLVD	SR 70/15th St E	6.487	0.104 mi. S. of 13th Ave E	7.908	1.421		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	38,430	30,000	C			
13130000	US 41	1ST ST	41st Ave E	0.115	21st Ave W	1.258	1.143		C3R	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	57,855	36,995	C			
13130000	US 41	1ST ST/TAMIAMI TRAIL	0.104 mi. S. of 13th Ave E	1.661	SR 43/US 301	4.056	2.395		C3R	C3C	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	4	39,165	62,086	F	Over Capacity		
** 13130000	US 41	TAMIAMI TRAIL	SR 43/US 301	4.056	US 19/SR 55 SB/45th St Ct W	5.819	1.763		C3R	C3R	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	OR	4	37,300	39,195	F	Over Capacity		
13130000	US 19 NB	US 19	US 19 SB/SR 55/45th St Ct W	5.819	US 19 SB	6.500	0.681		C3R	C3C	Principal Arterial-other	55	UA	A	D	D	D	U	1W	0L	OR	2	22,380	11,000	C			

Note: LOS Spreadsheet should be used as a planning level analysis tool. A detailed analysis is necessary to validate the actual operating conditions (and for future conditions, as needed) which may vary from this worksheet.

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -DAILY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	Year 2028 (E+C)						Year 2045						Standard K Factor	Existing D Factor	Existing T <sub>24</sub> Factor
								Thru Lanes	LOS Std	Daily			Thru Lanes	LOS Std	Daily							
										Capacity	Volume	LOS			Capacity	Trend Volume	Trend LOS	Model Volume	Model LOS			
13050000	SR 64	MANATEE AVE	17th St E	1.808	E. of 39th St E	3.742	1.934	6	D	57,855	54,600	C	6	D	59,640	69,100	F	59,287	D	9.00%	56.10%	4.70%
13050000	SR 64	MANATEE AVE	E. of 39th St E	3.742	57th St E	5.188	1.446	6	D	56,805	54,600	D	6	D	56,805	69,100	F	58,079	F	9.00%	56.10%	4.70%
** 13050000	SR 64	SR 64	57th St E	5.188	I-75	6.360	1.172	6	D	56,805	66,600	F	6	D	56,805	88,100	F	54,580	D	9.00%	56.10%	5.70%
13050000	SR 64	SR 64	I-75	6.360	Lena Rd/Heritage Green Way	7.389	1.029	6	D	83,700	57,100	C	6	D	56,805	81,300	F	50,330	D	9.00%	56.10%	5.60%
** 13050000	SR 64	SR 64	Lena Rd/Heritage Green Way	7.389	Upper Manatee River Rd/Lakewood	8.804	1.415	6	D	83,700	64,100	C	6	D	83,700	101,200	F	41,682	B	9.00%	56.10%	5.20%
13050000	SR 64	SR 64	Upper Manatee River Rd/Lakewood	8.804	Rye Rd	10.577	1.773	4	D	55,700	40,600	C	4	D	39,165	65,100	F	33,730	C	9.00%	56.10%	7.60%
13050000	SR 64	SR 64	Rye Rd	10.577	CR 675	16.835	6.258	2	C	8,200	8,004	C	2	C	20,580	10,356	C	21,585	D	9.43%	63.32%	14.36%
** 13050000	SR 70	SR 70	CR 675	22.134	Singletary Rd/Myakka Wauchula R	31.713	9.579	2	C	8,200	9,300	D	2	C	8,200	12,100	D	4,485	B	9.50%	56.50%	21.40%
** 13050000	SR 70	SR 70	Singletary Rd/Myakka Wauchula R	31.713	DeSoto County Line	39.483	7.770	2	C	8,200	7,700	C	2	C	8,200	11,200	D	2,210	B	9.50%	56.50%	26.30%
13050002	SR 64	MANATEE AVE EB/6TH AV	10th St E	0.000	17th St E	0.576	0.576	3	D	33,060	20,285	C	3	D	34,080	23,585	C	29,148	D	9.00%	99.90%	3.95%
13050101	SR 64	MANATEE AVE	12th St E	0.189	10th St W	1.510	1.321	3	D	35,784	21,280	C	3	D	35,784	24,696	C	31,513	D	9.00%	99.90%	3.75%
13050102	SR 64	MANATEE AVE E	17th St E	0.000	12th St E	0.372	0.372	3	D	33,060	21,211	C	3	D	34,080	24,613	C	28,612	D	9.00%	99.90%	5.09%
13060000	SR 62	SR 62	SR 43/US 301	0.000	Corbett John Rd	3.890	3.890	2	C	8,200	5,000	C	2	C	8,200	7,100	C	8,445	D	9.50%	56.10%	23.10%
13060000	SR 62	SR 62	Corbett John Rd	3.890	CR 39 /County Line Rd	17.154	13.264	2	C	8,200	5,000	C	2	C	8,200	7,100	C	7,675	C	9.50%	56.10%	23.10%
13060000	SR 62	SR 62	CR 39 /County Line Rd	17.154	SR 37	19.264	2.110	2	C	8,200	6,900	C	2	C	8,200	10,800	D	8,928	D	9.50%	56.10%	32.90%
13060000	SR 62	SR 62	SR 37	19.264	Hardee County Line	23.240	3.976	2	C	8,200	6,100	C	2	C	8,200	8,700	D	7,557	C	9.50%	56.10%	27.80%
13070000	SR 37	SR 37	SR 62	0.000	Polk County Line	5.534	5.534	2	C	8,200	3,400	B	2	C	8,200	5,300	C	3,295	B	9.50%	56.10%	32.00%
** 13075000	I-75	SR 93/I-75	Sarasota County Line	0.000	SR 70	3.719	3.719	8	D	163,400	159,300	D	10	D	206,644	213,900	E	136,863	C	9.00%	55.80%	12.40%
** 13075000	I-75	SR 93/I-75	SR 70	3.719	SR 64/Manatee Ave	7.330	3.611	8	D	163,400	151,600	D	10	D	206,644	204,400	D	124,941	C	9.00%	55.80%	12.40%
** 13075000	I-75	SR 93/I-75	SR 64/Manatee Ave	7.330	19th St/US 301/SR 43	11.014	3.684	6	D	122,800	136,500	F	10	D	206,644	181,700	D	123,617	B	9.00%	55.80%	12.70%
** 13075000	I-75	SR 93/I-75	19th St/US 301/SR 43	11.014	I-275 off Ramp	14.548	3.534	6	D	122,800	130,700	F	10	D	206,644	175,300	D	103,625	B	9.00%	55.80%	14.70%
** 13075000	I-75	SR 93/I-75	I-275 off Ramp	14.548	I-275 on Ramp	15.300	0.752	6	D	122,800	79,000	C	10	D	206,644	111,500	B	45,190	B	9.00%	54.70%	17.70%
** 13075000	I-75	SR 93/I-75	I-275 on Ramp	15.300	CR 6/Mocassin Wallow Rd	16.200	0.900	6	D	142,800	112,800	C	10	D	206,644	163,200	C	69,397	B	9.00%	55.80%	15.50%
** 13075000	I-75	SR 93/I-75	CR 6/Mocassin Wallow Rd	16.200	Hillsborough County Line	20.571	4.371	6	C	69,000	100,400	F	10	C	119,880	144,000	E	67,781	B	10.50%	52.80%	11.10%
13080000	SR 789	GULF OF MEXICO DR	Sarasota County Line	0.000	Binnacle Pointe Dr	2.410	2.410	2	D	23,520	11,600	C	2	D	18,480	16,400	D	16,448	D	9.00%	57.20%	5.00%
13080000	SR 789	GULF OF MEXICO DR	Binnacle Pointe Dr	2.410	Coquina Park Ent.	5.177	2.767	2	D	23,520	11,600	C	2	D	18,480	16,400	D	16,181	D	9.00%	57.20%	5.00%
13080000	SR 789	GULF DR S	Coquina Park Ent.	5.177	SR 684 (Cortez Rd)	6.666	1.489	2	D	18,480	14,100	D	2	D	18,480	16,400	D	19,775	E	9.00%	54.90%	3.90%
13080000	SR 789	GULF DR N/EAST BAY DR	SR 684 (Cortez Rd)	6.666	SR 64/Manatee Ave	8.717	2.051	2	D	18,480	11,000	D	2	D	18,480	13,700	D	23,340	E	9.00%	57.20%	3.90%
13120001	SR 70	15 ST E	301 Blvd	0.000	15th St E	0.058	0.058	1	D	10,937	4,600	C	1	D	10,937	5,300	C	8,427	D	9.00%	99.90%	8.50%
13120001	SR 70	15 ST E	15th St E	0.058	SR 64/Manatee Ave	3.030	2.972	2	D	22,785	11,968	C	2	D	22,785	13,887	C	13,879	C	9.00%	56.10%	6.96%
** 13121000	US 301	SR683/US301/15ST BLVD	University Pkwy	0.000	63rd Ave E/Saunders Rd	2.811	2.811	4	D	82,200	42,100	B	4	D	82,200	50,900	C	34,365	B	9.00%	54.80%	7.30%
** 13121000	US 301	SR683/US301/15ST BLVD	63rd Ave E/Saunders Rd	2.811	SR 70/Oneco Rd/53rd Ave E	4.060	1.249	4	D	82,200	44,800	B	4	D	82,200	55,600	C	31,803	B	9.00%	54.80%	12.70%
13121000	US 301	SR683/US301/15ST BLVD	SR 70/Oneco Rd/53rd Ave E	4.060	38th Ave E	5.332	1.272	4	D	82,200	38,600	B	4	D	82,200	46,000	B	37,465	B	9.00%	54.80%	8.70%
13121000	US 301	SR683/US301/15ST BLVD	38th Ave E	5.332	SR 70/15th St E	6.487	1.155	4	D	39,165	35,200	C	4	D	39,165	45,800	F	33,165	C	9.00%	54.80%	10.40%
13121000	US 301	SR683/US301/15ST BLVD	SR 70/15th St E	6.487	0.104 mi. S. of 13th Ave E	7.908	1.421	4	D	38,430	32,200	C	4	D	38,430	38,400	D	34,931	D	9.00%	54.80%	9.10%
13130000	US 41	1ST ST	41st Ave E	0.115	21st Ave W	1.258	1.143	6	D	57,855	39,258	C	6	D	56,805	45,485	C	43,770	C	9.00%	54.80%	4.30%
13130000	US 41	1ST ST/TAMIAMI TRAIL	0.104 mi. S. of 13th Ave E	1.661	SR 43/US 301	4.056	2.395	4	D	39,165	66,710	F	4	D	38,430	79,923	F	75,435	F	9.00%	54.80%	7.00%
** 13130000	US 41	TAMIAMI TRAIL	SR 43/US 301	4.056	US 19/SR 55 SB/45th St Ct W	5.819	1.763	4	D	37,300	42,720	F	4	D	37,300	52,851	F	37,447	F	9.00%	54.80%	6.96%
13130000	US 19 NB	US 19	US 19 SB/SR 55/45th St Ct W	5.819	US 19 SB	6.500	0.681	2	D	22,380	11,900	C	2	D	21,960	14,500	C	30,722	F	9.00%	99.90%	6.90%

Note: LOS Spreadsheet should be used as a planning level analysis tool. A detailed analysis is necessary to validate the actual operating conditions (and for future conditions, as needed) which may vary from this worksheet.

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -DAILY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	SIS	Existing Context Class	Future Context Class	Functional Classification	Posted Speed	Area Type	Facility Type	FDOT LOS Std.	County LOS Std.	City LOS Std.	Year 2022										Deficiency Determination
																		Divided/UnDivided	One/Two Way	Left Turn Bays	Right Turn Bays	Thru Lanes	Daily					
																							Capacity	Volume	LOS			
13130000	US 19	US 19	US 41	6.500	Terra Ceia Rd	8.466	1.966		C3R	C3C	Principal Arterial-other	55	RDA	H	C	D		D	2W	WL	WR	4	36,015	23,500	C			
13130000	US 19 NB	US 19	Terra Ceia Rd	8.466	I-275 Ramp	9.194	0.728		LA	LA	Principal Arterial-other	55	RDA	H	C	C		U	1W		0A	2	28,800	11,500	B			
13130000	US 19	US 19	I-275 Ramp	9.194	Skyway Br. Hillsborough County Li	11.982	2.788	SIS	LA	LA	Principal Arterial-Interstate	65	RDA	F	C	C		D	2W	0L	0A	4	48,000	70,500	F	Over Capacity		
13130101	US 19 SB	1ST ST	US 19 NB/SR 55/45th St Ct W	0.000	US 41/SR 45	0.654	0.654		C3C	C3C	Principal Arterial-other	45	UA	H	D	D		U	1W	WL	0R	2	21,960	18,705	D			
13130401	US 41	SR 55/US 19	21st Ave	0.000	Railroad Crossing	0.403	0.403		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		U	1W	0L	WR	2	23,058	20,000	D			
13130403	US 41	1ST ST	Railroad Crossing	0.000	21st Ave W	0.403	0.403		C3C	C4	Principal Arterial-other	45	UA	A	D	D		U	1W	WL	0R	2	21,960	18,000	C			
13140000	SR 64	BRADENTON-ARCADIA RD	CR 675	0.000	Wauchula Rd	13.733	13.733		C2	C2	Principal Arterial-other	60	RDA	H	C	C		U	2W	WL	WR	2	8,200	5,800	C			
13140000	SR 64	BRADENTON-ARCADIA RD	Wauchula Rd	13.733	Hardee County Line	16.071	2.338		C2	C2	Principal Arterial-other	60	RDA	H	C	C		U	2W	WL	0R	2	8,200	4,200	B			
13150000	SR 64	MANATEE AVE	SR 789 /Gulf Drive	0.000	Flamingo Dr	2.882	2.882		C3C	C3R	Principal Arterial-other	45	UA	A	D	D	D	U	2W	WL	WR	2	22,785	17,600	D			
13150000	SR 64	MANATEE AVE	Flamingo Dr	2.882	75th St W	4.572	1.690		C4	C3R	Principal Arterial-other	45	UA	A	D	D	D	U	2W	WL	WR	2	18,480	18,000	D	Near Capacity		
13150000	SR 64	MANATEE AVE	75th St W	4.572	51st St W	6.074	1.502		C4	C4	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	37,905	32,000	D			
13150000	SR 64	MANATEE AVE	51st St W	6.074	SR 64/Manatee Ave	8.305	2.231		C4	C4	Principal Arterial-other	40	UA	A	D	D	D	D	2W	WL	WR	4	37,905	41,426	E	Over Capacity		
13150000	SR 64	15TH ST W/6TH AVE W	SR 64/Manatee Ave	8.305	10th St W	8.613	0.308		C5	C5	Principal Arterial-other	30	UA	A	D	D	D	U	1W	0L	WR	3	33,957	21,000	D			
13150101	SR 64	MANATEE AVE WB	10th St	0.000	15th St	0.235	0.235		C5	C5	Principal Arterial-other	30	UA	A	D	D	D	U	1W	WL	0R	2	22,980	24,500	E	Over Capacity		
13160000	SR 70	53RD AVE/ONECO RD	301 Blvd/15th St E	0.000	US 301/SR 683/15th St Blvd	1.001	1.001		C3C	C3C	Principal Arterial-other	40	UA	A	D	D		D	2W	WL	WR	6	56,805	34,000	C			
13160000	SR 70	53RD AVE/ONECO RD/SR 70	US 301/SR 683/15th St Blvd	1.001	Lockwood Ridge Rd/45th St E	2.538	1.537		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	56,805	46,000	C			
13160000	SR 70	SR 70	Lockwood Ridge Rd/45th St E	2.538	Fairway Gardens Dr/68th St E	4.388	1.850		C3C	C3C	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	6	56,805	52,788	D	Near Capacity		
13160000	SR 70	SR 70	Fairway Gardens Dr/68th St E	4.388	I-75	5.438	1.050		C3C	C3C	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	6	56,805	73,000	F	Over Capacity		
13160000	SR 70	SR 70	I-75	5.438	Lakewood Ranch Blvd	7.448	2.010	SIS	C3R	C3R	Principal Arterial-other	50	UA	A	D	C		D	2W	WL	WR	6	57,855	55,000	C	Near Capacity		
13160000	SR 70	SR 70	Lakewood Ranch Blvd	7.448	Lorraine Rd	9.464	2.016	SIS	C3R	C3R	Principal Arterial-other	50	UA	A	D	C		D	2W	WL	WR	6	57,855	34,500	C			
13160000	SR 70	SR 70	Lorraine Rd	9.464	Bournside Blvd	12.328	2.864	SIS	C3R	C3R	Principal Arterial-other	60	RDA	H	C	C		D	2W	WL	WR	2	21,609	19,800	C	Near Capacity		
13160000	SR 70	SR 70	Bournside Blvd	12.328	CR 675 (Waterbury Rd)	15.567	3.239	SIS	C3R	C3R	Principal Arterial-other	60	RDA	H	C	C		D	2W	WL	WR	2	21,609	19,800	C	Near Capacity		
13162000	SR 70	53RD AVE/ONECO RD	US41/SR 45/14th St	0.000	15th St E/301 Blvd	1.761	1.761		C3R	C3R	Principal Arterial-other	40	UA	A	D	D		U	2W	WL	WR	4	37,207	31,500	C			
13175000	I-275	SR 93/I-275	Erie Rd/CR 10/69th St	0.000	I-75	0.690	0.690	SIS Link	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C		U	1W		0A	2	41,100	17,000	B			
13175000	I-275	SR 93/I-275	I-75	0.690	E. of 36th Ave E/CR 683/Ellenton C	1.344	0.654	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C		U	1W		0A	2	41,100	17,000	B			
13175000	I-275	SR 93/I-275	E. of 36th Ave E/CR 683/Ellenton C	1.344	US 41/SR 45	2.279	0.935	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C		D	2W		W1A	4	92,200	61,500	C			
13175000	I-275	SR 93/I-275	US 41/SR 45	2.279	US 19 NB Ramp	5.233	2.954	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C		D	2W		0A	4	48,000	47,000	C	Near Capacity		
13000012	Piney Point Road	Piney Point Road	SR 45/US 41	0.000	Harlee Rd/North Dock St	0.583	0.583	SC	C3C	C3C	Major Collector	30	UA	A	D	D		U	2W	WL	WR	2	22,785	3,800	C			

Legend  
 Aggregated segment  Single count segment  OFF System SIS Facility

\*\* Future Model Volumes obtained from D1 Regional Planning Model are found to be less than Existing AADT.

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -DAILY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	Year 2028 (E+C)						Year 2045						Standard K Factor	Existing D Factor	Existing T <sub>24</sub> Factor
								Thru Lanes	LOS Std	Daily			Thru Lanes	LOS Std	Daily							
										Capacity	Volume	LOS			Capacity	Trend Volume	Trend LOS	Model Volume	Model LOS			
13130000	US 19	US 19	US 41	6.500	Terra Ceia Rd	8.466	1.966	4	C	36,015	25,200	C	4	C	32,235	30,100	C	27,462	C	9.00%	54.80%	6.40%
13130000	US 19 NB	US 19	Terra Ceia Rd	8.466	I-275 Ramp	9.194	0.728	2	C	28,800	13,000	B	2	C	28,800	17,400	B	26,425	C	9.50%	99.90%	7.00%
13130000	US 19	US 19	I-275 Ramp	9.194	Skyway Br. Hillsborough County Li	11.982	2.788	4	C	48,000	77,700	F	4	C	48,000	98,000	F	84,813	F	10.50%	54.70%	7.80%
13130101	US 19 SB	1ST ST	US 19 NB/SR 55/45th St Ct W	0.000	US 41/SR 45	0.654	0.654	2	D	21,960	20,439	D	2	D	21,960	25,490	F	26,073	F	9.00%	99.90%	7.63%
13130401	US 41	SR 55/US 19	21st Ave	0.000	Railroad Crossing	0.403	0.403	2	D	23,058	21,200	D	2	D	23,058	24,600	F	24,568	F	9.00%	99.90%	4.80%
13130403	US 41	1ST ST	Railroad Crossing	0.000	21st Ave W	0.403	0.403	2	D	21,960	19,100	D	2	D	21,660	22,100	E	22,413	E	9.00%	99.90%	5.10%
13140000	SR 64	BRADENTON-ARCADIA RD	CR 675	0.000	Wauchula Rd	13.733	13.733	2	C	8,200	6,200	C	2	C	8,200	7,200	C	4,485	B	9.50%	56.10%	20.60%
13140000	SR 64	BRADENTON-ARCADIA RD	Wauchula Rd	13.733	Hardee County Line	16.071	2.338	2	C	8,200	4,900	C	2	C	8,200	7,000	C	2,210	B	9.50%	56.10%	22.70%
13150000	SR 64	MANATEE AVE	SR 789 /Gulf Drive	0.000	Flamingo Dr	2.882	2.882	2	D	22,785	18,900	D	2	D	23,520	22,500	D	23,559	F	9.00%	57.20%	4.60%
13150000	SR 64	MANATEE AVE	Flamingo Dr	2.882	75th St W	4.572	1.690	2	D	18,480	19,100	E	2	D	23,520	22,100	D	27,052	F	9.00%	57.20%	4.70%
13150000	SR 64	MANATEE AVE	75th St W	4.572	51st St W	6.074	1.502	4	D	37,905	35,900	D	4	D	37,905	46,800	F	45,723	F	9.00%	56.10%	3.80%
13150000	SR 64	MANATEE AVE	51st St W	6.074	SR 64/Manatee Ave	8.305	2.231	4	D	37,905	43,922	F	4	D	37,905	50,975	F	51,332	F	9.00%	56.10%	3.55%
13150000	SR 64	15TH ST W/6TH AVE W	SR 64/Manatee Ave	8.305	10th St W	8.613	0.308	3	D	33,957	22,300	D	3	D	33,957	25,800	D	31,700	D	9.00%	99.90%	3.10%
13150101	SR 64	MANATEE AVE WB	10th St	0.000	15th St	0.235	0.235	2	D	22,980	26,000	F	2	D	22,980	30,100	F	32,694	F	9.00%	99.90%	4.30%
13160000	SR 70	53RD AVE/ONECO RD	301 Blvd/15th St E	0.000	US 301/SR 683/15th St Blvd	1.001	1.001	6	D	56,805	36,000	C	6	D	56,805	41,800	C	48,443	C	9.00%	56.10%	5.00%
13160000	SR 70	53RD AVE/ONECO RD/SR 70	US 301/SR 683/15th St Blvd	1.001	Lockwood Ridge Rd/45th St E	2.538	1.537	6	D	56,805	48,800	C	6	D	56,805	56,600	D	64,446	F	9.00%	56.50%	7.00%
13160000	SR 70	SR 70	Lockwood Ridge Rd/45th St E	2.538	Fairway Gardens Dr/68th St E	4.388	1.850	6	D	56,805	57,800	F	6	D	56,805	71,900	F	60,436	F	9.00%	51.00%	8.00%
13160000	SR 70	SR 70	Fairway Gardens Dr/68th St E	4.388	I-75	5.438	1.050	6	D	56,805	77,400	F	6	D	56,805	89,800	F	69,808	F	9.00%	56.50%	5.90%
13160000	SR 70	SR 70	I-75	5.438	Lakewood Ranch Blvd	7.448	2.010	6	D	57,855	60,400	F	6	D	57,855	75,700	F	69,032	F	9.00%	56.50%	5.10%
13160000	SR 70	SR 70	Lakewood Ranch Blvd	7.448	Lorraine Rd	9.464	2.016	6	D	57,855	41,300	C	6	D	57,855	60,500	F	42,431	C	9.00%	56.50%	5.80%
13160000	SR 70	SR 70	Lorraine Rd	9.464	Bournside Blvd	12.328	2.864	4	C	36,015	24,500	C	4	C	36,015	38,000	D	18,475	C	9.00%	56.50%	10.30%
13160000	SR 70	SR 70	Bournside Blvd	12.328	CR 675 (Waterbury Rd)	15.567	3.239	4	C	36,015	24,500	C	4	C	36,015	38,000	D	21,094	C	9.00%	56.50%	10.30%
13162000	SR 70	53RD AVE/ONECO RD	US41/SR 45/14th St	0.000	15th St E/301 Blvd	1.761	1.761	4	D	37,207	33,400	C	4	D	37,207	38,700	F	29,257	C	9.00%	56.10%	3.40%
13175000	I-275	SR 93/I-275	Erie Rd/CR 10/69th St	0.000	I-75	0.690	0.690	2	D	41,100	18,100	B	2	D	41,100	21,200	B	31,986	C	9.00%	99.90%	7.30%
13175000	I-275	SR 93/I-275	I-75	0.690	E. of 36th Ave E/CR 683/Ellenton C	1.344	0.654	2	D	41,100	18,100	B	2	D	41,100	21,200	B	24,483	B	9.00%	99.90%	7.30%
13175000	I-275	SR 93/I-275	E. of 36th Ave E/CR 683/Ellenton C	1.344	US 41/SR 45	2.279	0.935	4	D	92,200	69,900	C	4	D	92,200	93,700	E	43,520	B	9.00%	54.70%	9.80%
13175000	I-275	SR 93/I-275	US 41/SR 45	2.279	US 19 NB Ramp	5.233	2.954	4	C	48,000	52,300	D	4	C	48,000	67,300	F	58,388	E	9.00%	54.70%	6.50%
13000012	Piney Point Road	Piney Point Road	SR 45/US 41	0.000	Harlee Rd/North Dock St	0.583	0.583	2	D	22,785	5,300	C	2	D	22,785	9,400	C	3,728	C	9.00%	56.10%	47.20%

**Legend**

Aggregated segment
  Single count segment
  OFF System SIS Facility

\*\* Future Model Volumes obtained from D1 Regional Planning Model are found to be less than Existing AADT.



**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR TWO-WAY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	SIS	Existing Context Class	Future Context Class	Functional Classification	Posted Speed	Area Type	Facility Type	FDOT LOS Std.	County LOS Std.	City LOS Std.	Year 2022										Deficiency Determination
																		Divided/UnDivided	One/Two Way	Left Turn Bays	Right Turn Bays	Thru Lanes	Peak Hour Two-Way					
																							Capacity	Volume	LOS			
13010000	US 41	TAMIAMI TRAIL	Sarasota County Line	0.000	63rd Ave W/Bay Dr	3.007	3.007		C3C	C4	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	6	5,114	3,684	C			
13010000	US 41	14TH ST W.	63rd Ave W/Bay Dr	3.007	SR 684 /Cortez Rd	5.284	2.277		C3R	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	5,208	3,955	C			
13010000	USB 41	14TH ST W.	SR 684 /Cortez Rd	5.284	9th Ave W/MLK Jr Ave W	7.320	2.036		C4	C4	Principal Arterial-other	40	UA	A	D	D	D	U	2W	WL	WR	4	3,242	1,973	C			
13010000	USB 41	14TH ST W./8TH AVE W.	9th Ave	7.320	10th St W	7.552	0.232		C4	C4	Principal Arterial-other	35	UA	A	D	D	D	U	2W	WL	WR	4	3,242	1,044	C			
13010000	USB 41	8TH AVE W.	Riverside Dr	8.878	10th St W	9.498	0.620		C4	C4	Principal Arterial-other	30	UA	A	D	D	D	U	2W	WL	OR	4	3,088	2,799	D	Near Capacity		
13010001	USB 41	8TH AVE W./9TH ST W.	10th St W	0.000	Riverside Dr	1.379	1.379		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	3,454	3,315	D	Near Capacity		
** 13020000	SR 43/US 301	10TH ST W.	SR 45/US 41 Bus	0.000	US 41/Tamiami Trail	0.643	0.643		C4	C4	Principal Arterial-other	40	UA	A	D	D	D	D	2W	WL	OR	4	3,250	1,646	C			
13020000	US 301	SR 43/US 301	US 41/Tamiami Trail	0.643	CR 683/36th Ave	2.745	2.102		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	3,454	3,285	D	Near Capacity		
13020000	US 301	SR 43/US 301	CR 683/36th Ave	2.745	19th St/51st Ave	3.842	1.097		C3R	C3R	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	4	3,528	3,080	C			
13020000	US 301	SR 43/US 301	19th St/51st Ave	3.842	Old Tampa Rd/Erie Rd	6.310	2.468		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	4	3,454	3,585	F	Over Capacity		
** 13020000	US 301	SR 43/US 301	Old Tampa Rd/Erie Rd	6.310	Chin Rd/Palm Blvd	8.528	2.218		C3R	C3R	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	4	3,528	2,131	C			
** 13020000	US 301	SR 43/US 301	Chin Rd/Palm Blvd	8.528	Fort Hamer Rd/121st Ave E	10.054	1.526		C3R	C3R	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	4	3,528	1,458	C			
13020000	US 301	SR 43/US 301	Fort Hamer Rd	10.054	Moccasin Wallow Rd	11.646	1.592		C3R	C3R	Principal Arterial-other	45	UA	H	D	C		D	2W	WL	OR	4	3,360	1,540	C			
13020000	US 301	SR 43/US 301	Dickey Rd	11.646	Hillsborough County Line	16.066	4.420		C2	C2	Principal Arterial-other	60	RDA	H	C	C		U	2W	WL	WR	2	780	836	D	Over Capacity		
13030000	USB 41	8TH AVE W.	SR 43/10th St W	0.000	26th St W	1.309	1.309		C4	C4	Principal Arterial-other	35	UA	A	D	D	D	D	2W	WL	WR	4	3,412	2,344	D			
13030103	USB 41	8TH AVE W.	US 41 Bus/SR 45	0.000	26th St W	0.385	0.385		C3C	C3C	Principal Arterial-other	50	UA	A	D	D	D	U	1W	WL	WR	2	2,281	1,080	C			
13030000	US 41	TAMIAMI TRAIL	SR 55/US 19 NB On Ramp	2.225	0.021 mi. S of 45th St E	2.582	0.357		C3C	C3R	Principal Arterial-other	55	UA	A	D	D		U	1W	WL	OR	2	2,172	1,755	C			
13030000	US 41	TAMIAMI TRAIL	0.021 mi. S of 45th St E	2.582	I-275	5.202	2.620		C2	C3R	Principal Arterial-other	60	UA	A	D	D		D	2W	WL	WR	4	5,290	3,015	B			
13030000	US 41	TAMIAMI TRAIL	I-275	5.202	Piney Point Rd/Inland Transp St	8.542	3.340	SC	C2	C2	Principal Arterial-other	60	UA	H	D	D		D	2W	WL	WR	4	5,290	1,611	B			
13030000	US 41	TAMIAMI TRAIL	Piney Point Rd/Inland Transp St	8.542	Hillsborough County Line	9.782	1.240		C2	C2	Principal Arterial-other	60	UA	H	D	D		D	2W	WL	WR	4	5,290	1,386	B			
13030101	USB 41 -NB	SR 45/USB 41	26th St W/US 41/SR 45	0.000	SR 55/US 19	0.560	0.560		C3C	C3C	Principal Arterial-other	50	UA	A	D	D		U	1W	OL	OR	2	2,172	1,125	C			
13030102	USB 41 -SB	SR 45/US 41	SR 45/US 41/45th St E	0.000	SR 55/US 19	0.457	0.457		C4	C3C	Principal Arterial-other	55	UA	A	D	D		U	1W	OL	OR	2	2,148	1,620	D			
13040000	SR 684	CORTEZ RD	SR 789/Gulf Dr	0.000	119th St W	1.188	1.188		C3R	C3R	Principal Arterial-other	35	UA	A	D	D		U	2W	WL	WR	2	2,121	1,608	C			
13040000	SR 684	CORTEZ RD	119th St W	1.188	75th St W	3.971	2.783		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	4	3,454	2,340	C			
13040000	SR 684	CORTEZ RD	75th St W	3.971	37th St W	6.372	2.401		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	3,454	2,565	C			
13040000	SR 684	CORTEZ RD	37th St W	6.372	9th St W	8.011	1.639		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	5,114	4,662	D	Near Capacity		
13040000	US 41	CORTEZ RD	9th St W	8.011	US 41 (Cortez Rd)	8.440	0.429		C3C	C3C	Principal Arterial-other	35	UA	A	D	D		D	2W	WL	WR	4	3,454	3,600	F	Over Capacity		
13040000	US 41	44TH AVE E	US 41 (Cortez Rd)	8.440	0.064 mi. E of 1st St	8.574	0.134		C3C	C3C	Minor Arterial	35	UA	A	D	D		U	1W	OL	OR	1	1,079	729	C			
13040001	US 41	TAMIAMI TRAIL	SR 684/Cortez Rd/44th Ave E	0.000	41st Ave E	0.158	0.158		C3C	C3C	Principal Arterial-other	35	UA	A	D	D		D	2W	WL	OR	4	3,290	3,149	D	Near Capacity		
13040002	SR 684	44TH AVE CONNECTOR	44th Ave	0.000	US 41/Tamiami Trail	0.073	0.073		C3C	C3C	Minor Arterial	35	UA	A	D	D		D	2W	WL	WR	2	2,150	1,044	C			
13050000	SR 64	6TH AVE E/7TH AVE	US 41B (10th St W)	0.000	10th St E	1.122	1.122		C3C	C4	Principal Arterial-other	35	UA	A	D	D	D	U	1W	WL	WR	3	3,377	1,601	C			

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR TWO-WAY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	Year 2028 (E+C)					Year 2045					Standard K Factor	Existing D Factor	Existing T <sub>24</sub> Factor		
								Thru Lanes	LOS Std	Peak Hour Two-Way			Thru Lanes	LOS Std	Peak Hour Two-Way							
										Capacity	Volume	LOS			Capacity	Trend Volume	Trend LOS				Model Volume	Model LOS
13010000	US 41	TAMIAMI TRAIL	Sarasota County Line	0.000	63rd Ave W/Bay Dr	3.007	3.007	6	D	5,114	3,906	C	6	D	5,366	4,534	D	4,839	D	9.00%	54.80%	3.20%
13010000	US 41	14TH ST W.	63rd Ave W/Bay Dr	3.007	SR 684 /Cortez Rd	5.284	2.277	6	D	5,208	4,193	C	6	D	5,114	4,863	D	4,688	D	9.00%	54.80%	2.80%
13010000	USB 41	14TH ST W.	SR 684 /Cortez Rd	5.284	9th Ave W/MLK Jr Ave W	7.320	2.036	4	D	3,242	2,089	C	4	D	3,242	2,427	D	2,696	D	9.00%	54.80%	2.73%
13010000	USB 41	14TH ST W./8TH AVE W.	9th Ave	7.320	10th St W	7.552	0.232	4	D	3,242	1,107	C	4	D	3,242	1,287	C	1,340	C	9.00%	54.80%	2.80%
13010000	USB 41	8TH AVE W.	Riverside Dr	8.878	10th St W	9.498	0.620	4	D	3,088	2,966	D	4	D	3,088	3,439	E	3,254	E	9.00%	54.80%	4.10%
13010001	USB 41	8TH AVE W./9TH ST W.	10th St W	0.000	Riverside Dr	1.379	1.379	4	D	3,454	3,595	F	4	D	3,454	4,378	F	4,597	F	9.00%	54.80%	3.96%
** 13020000	SR 43/US 301	10TH ST W.	SR 45/US 41 Bus	0.000	US 41/Tamiami Trail	0.643	0.643	4	D	3,250	1,747	C	4	D	3,250	2,022	C	1,536	C	9.00%	54.80%	4.90%
13020000	US 301	SR 43/US 301	US 41/Tamiami Trail	0.643	CR 683/36th Ave	2.745	2.102	4	D	3,454	3,483	F	4	D	3,454	4,041	F	3,886	F	9.00%	54.80%	6.90%
13020000	US 301	SR 43/US 301	CR 683/36th Ave	2.745	19th St/51st Ave	3.842	1.097	4	D	3,528	3,420	D	4	D	3,528	4,383	F	3,614	F	9.00%	56.80%	6.50%
13020000	US 301	SR 43/US 301	19th St/51st Ave	3.842	Old Tampa Rd/Erie Rd	6.310	2.468	4	D	3,454	3,820	F	4	D	3,454	4,485	F	4,359	F	9.00%	54.80%	5.10%
** 13020000	US 301	SR 43/US 301	Old Tampa Rd/Erie Rd	6.310	Chin Rd/Palm Blvd	8.528	2.218	4	D	3,528	2,597	C	4	D	3,528	3,929	F	1,922	C	9.00%	54.80%	5.80%
** 13020000	US 301	SR 43/US 301	Chin Rd/Palm Blvd	8.528	Fort Hamer Rd/121st Ave E	10.054	1.526	4	D	3,528	1,746	C	4	D	3,528	2,565	C	1,079	C	9.00%	54.80%	9.30%
13020000	US 301	SR 43/US 301	Fort Hamer Rd	10.054	Moccasin Wallow Rd	11.646	1.592	4	D	3,360	1,904	C	4	D	3,360	2,942	C	1,644	C	9.00%	54.80%	12.88%
13020000	US 301	SR 43/US 301	Dickey Rd	11.646	Hillsborough County Line	16.066	4.420	2	C	780	1,064	D	2	C	780	1,720	E	1,399	E	9.50%	56.10%	10.70%
13030000	USB 41	8TH AVE W.	SR 43/10th St W	0.000	26th St W	1.309	1.309	4	D	3,412	2,530	D	4	D	3,412	3,054	D	3,184	D	9.00%	54.80%	4.83%
13030103	USB 41	8TH AVE W.	US 41 Bus/SR 45	0.000	26th St W	0.385	0.385	2	D	2,281	1,179	C	2	D	2,281	1,476	C	1,719	C	9.00%	99.90%	5.50%
13030000	US 41	TAMIAMI TRAIL	SR 55/US 19 NB On Ramp	2.225	0.021 mi. S of 45th St E	2.582	0.357	2	D	2,172	2,052	D	2	D	2,220	2,907	F	2,025	C	9.00%	99.90%	10.90%
13030000	US 41	TAMIAMI TRAIL	0.021 mi. S of 45th St E	2.582	I-275	5.202	2.620	4	D	5,290	3,573	C	4	D	5,290	5,157	F	3,209	C	9.00%	54.80%	9.40%
13030000	US 41	TAMIAMI TRAIL	I-275	5.202	Piney Point Rd/Inland Transp St	8.542	3.340	4	D	5,290	1,926	B	4	D	5,290	2,799	B	3,543	C	9.00%	54.80%	16.20%
13030000	US 41	TAMIAMI TRAIL	Piney Point Rd/Inland Transp St	8.542	Hillsborough County Line	9.782	1.240	4	D	5,290	1,746	B	4	D	5,290	2,772	B	2,766	B	9.00%	54.80%	12.60%
13030101	USB 41 -NB	SR 45/USB 41	26th St W/US 41/SR 45	0.000	SR 55/US 19	0.560	0.560	2	D	2,172	1,278	C	2	D	2,172	1,719	C	1,777	C	9.00%	99.90%	5.80%
13030102	USB 41 -SB	SR 45/US 41	SR 45/US 41/45th St E	0.000	SR 55/US 19	0.457	0.457	2	D	2,148	1,881	D	2	D	2,172	2,601	F	1,805	C	9.00%	99.90%	6.90%
13040000	SR 684	CORTEZ RD	SR 789/Gulf Dr	0.000	119th St W	1.188	1.188	2	D	2,121	1,722	C	2	D	2,121	2,048	D	2,199	F	9.00%	57.20%	4.30%
13040000	SR 684	CORTEZ RD	119th St W	1.188	75th St W	3.971	2.783	4	D	3,454	2,565	C	4	D	3,454	3,195	D	3,272	D	9.00%	57.20%	4.30%
13040000	SR 684	CORTEZ RD	75th St W	3.971	37th St W	6.372	2.401	4	D	3,454	2,718	C	4	D	3,454	3,159	D	6,338	F	9.00%	56.10%	3.60%
13040000	SR 684	CORTEZ RD	37th St W	6.372	9th St W	8.011	1.639	6	D	5,114	4,939	D	6	D	5,114	5,733	F	5,406	F	9.00%	55.77%	3.88%
13040000	US 41	CORTEZ RD	9th St W	8.011	US 41 (Cortez Rd)	8.440	0.429	4	D	3,454	3,816	F	4	D	3,454	4,428	F	4,739	F	9.00%	54.80%	4.40%
13040000	US 41	44TH AVE E	US 41 (Cortez Rd)	8.440	0.064 mi. E of 1st St	8.574	0.134	1	D	1,079	873	D	1	D	1,079	1,287	F	1,002	D	9.00%	99.90%	5.60%
13040001	US 41	TAMIAMI TRAIL	SR 684/Cortez Rd/44th Ave E	0.000	41st Ave E	0.158	0.158	4	D	3,290	3,337	F	4	D	3,290	3,875	F	3,326	F	9.00%	54.80%	4.34%
13040002	SR 684	44TH AVE CONNECTOR	44th Ave	0.000	US 41/Tamiami Trail	0.073	0.073	2	D	2,150	1,233	C	2	D	2,150	1,764	D	1,086	C	9.00%	60.20%	4.60%
13050000	SR 64	6TH AVE E/7TH AVE	US 41B (10th St W)	0.000	10th St E	1.122	1.122	3	D	3,377	1,696	C	3	D	3,541	1,971	C	2,513	C	9.00%	99.90%	3.75%

Note: LOS Spreadsheet should be used as a planning level analysis tool. A detailed analysis is necessary to validate the actual operating conditions (and for future conditions, as needed) which may vary from this worksheet.

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR TWO-WAY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	SIS	Existing Context Class	Future Context Class	Functional Classification	Posted Speed	Area Type	Facility Type	FDOT LOS Std.	County LOS Std.	City LOS Std.	Year 2022										Deficiency Determination
																		Divided/UnDivided	One/Two Way	Left Turn Bays	Right Turn Bays	Thru Lanes	Peak Hour Two-Way					
																							Capacity	Volume	LOS			
13050000	SR 64	MANATEE AVE	17th St E	1.808	E. of 39th St E	3.742	1.934		C3R	C4	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	5,208	4,455	C			
13050000	SR 64	MANATEE AVE	E. of 39th St E	3.742	57th St E	5.188	1.446		C3C	C3C	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	6	5,114	4,455	C			
** 13050000	SR 64	SR 64	57th St E	5.188	I-75	6.360	1.172		C3C	C3C	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	6	5,114	5,310	F	Over Capacity		
13050000	SR 64	SR 64	I-75	6.360	Lena Rd/Heritage Green Way	7.389	1.029		C2	C3C	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	6	7,950	4,365	B			
** 13050000	SR 64	SR 64	Lena Rd/Heritage Green Way	7.389	Upper Manatee River Rd/Lakewood	8.804	1.415		C2	C2	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	6	7,950	4,590	C			
13050000	SR 64	SR 64	Upper Manatee River Rd/Lakewood	8.804	Rye Rd	10.577	1.773		C2	C3R	Principal Arterial-other	50	UA	H	D	D	D	D	2W	WL	WR	4	5,290	2,880	B			
13050000	SR 64	SR 64	Rye Rd	10.577	CR 675	16.835	6.258		C2	C3R	Principal Arterial-other	60	RDA	H	C	C	D	U	2W	WL	WR	2	780	667	C			
** 13050000	SR 70	SR 70	CR 675	22.134	Singletary Rd/Myakka Wauchula R	31.713	9.579	SIS	C2	C2	Principal Arterial-other	60	RDA	H	C	C	D	U	2W	WL	WR	2	780	789	D	Over Capacity		
** 13050000	SR 70	SR 70	Singletary Rd/Myakka Wauchula R	31.713	DeSoto County Line	39.483	7.770	SIS	C2	C2	Principal Arterial-other	60	RDA	H	C	C	D	U	2W	WL	OR	2	780	608	C			
13050002	SR 64	MANATEE AVE EB/6TH AVE	10th St E	0.000	17th St E	0.576	0.576		C3R	C4	Principal Arterial-other	35	UA	A	D	D	D	U	1W	WL	OR	3	3,276	1,724	C			
13050101	SR 64	MANATEE AVE	12th St E	0.189	10th St W	1.510	1.321		C4	C4	Principal Arterial-other	35	UA	A	D	D	D	U	1W	WL	WR	3	3,541	1,807	C			
13050102	SR 64	MANATEE AVE E	17th St E	0.000	12th St E	0.372	0.372		C3R	C4	Principal Arterial-other	35	UA	A	D	D	D	U	1W	WL	OR	3	3,276	1,801	C			
13060000	SR 62	SR 62	SR 43/US 301	0.000	Corbett John Rd	3.890	3.890		C2	C2	Minor Arterial	60	RDA	H	C	C	D	U	2W	0L	OR	2	780	409	B			
13060000	SR 62	SR 62	Corbett John Rd	3.890	CR 39 /County Line Rd	17.154	13.264		C2	C2	Minor Arterial	60	RDA	H	C	C	D	U	2W	WL	WR	2	780	409	B			
13060000	SR 62	SR 62	CR 39 /County Line Rd	17.154	SR 37	19.264	2.110		C2	C2	Minor Arterial	60	RDA	H	C	C	D	U	2W	WL	OR	2	780	523	C			
13060000	SR 62	SR 62	SR 37	19.264	Hardee County Line	23.240	3.976		C2	C2	Minor Arterial	60	RDA	H	C	C	D	U	2W	0L	WR	2	780	494	C			
13070000	SR 37	SR 37	SR 62	0.000	Polk County Line	5.534	5.534		C2	C2	Minor Arterial	60	RDA	H	C	C	D	U	2W	WL	WR	2	780	257	B			
** 13075000	I-75	SR 93/I-75	Sarasota County Line	0.000	SR 70	3.719	3.719	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		0A	8	14,710	12,600	D			
** 13075000	I-75	SR 93/I-75	SR 70	3.719	SR 64/Manatee Ave	7.330	3.611	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		0A	8	14,710	11,970	D			
13075000	I-75	SR 93/I-75	SR 64/Manatee Ave	7.330	19th St/US 301/SR 43	11.014	3.684	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		0A	6	11,050	10,845	D	Near Capacity		
** 13075000	I-75	SR 93/I-75	19th St/US 301/SR 43	11.014	I -275 off Ramp	14.548	3.534	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		0A	6	11,050	10,350	D	Near Capacity		
** 13075000	I-75	SR 93/I-75	I -275 off Ramp	14.548	I -275 on Ramp	15.300	0.752	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		0A	6	11,050	6,075	B			
** 13075000	I-75	SR 93/I-75	I -275 on Ramp	15.300	CR 6/Mocassin Wallow Rd	16.200	0.900	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		WA	6	12,850	8,550	C			
** 13075000	I-75	SR 93/I-75	CR 6/Mocassin Wallow Rd	16.200	Hillsborough County Line	20.571	4.371	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C	D	D	2W		0A	6	7,250	8,925	E	Over Capacity		
13080000	SR 789	GULF OF MEXICO DR	Sarasota County Line	0.000	Binnacle Pointe Dr	2.410	2.410		C3R	C4	Major Collector	45	UA	H	D		E	U	2W	WL	WR	2	2,121	891	C			
13080000	SR 789	GULF OF MEXICO DR	Binnacle Pointe Dr	2.410	Coquina Park Ent.	5.177	2.767		C3R	C4	Major Collector	45	UA	H	D		E	U	2W	WL	WR	2	2,121	891	C			
13080000	SR 789	GULF DR S	Coquina Park Ent.	5.177	SR 684 (Cortez Rd)	6.666	1.489		C4	C4	Major Collector	25	UA	A	D		D	U	2W	WL	WR	2	1,659	1,200	D			
13080000	SR 789	GULF DR N/EAST BAY DR	SR 684 (Cortez Rd)	6.666	SR 64/Manatee Ave	8.717	2.051		C4	C4	Major Collector	35	UA	A	D		D	U	2W	WL	WR	2	1,659	900	D			
13120001	SR 70	15 ST E	301 Blvd	0.000	15th St E	0.058	0.058		C3C	C3C	Major Collector	45	UA	A	D	D	D	U	1W	0L	OR	1	1,079	387	C			
13120001	SR 70	15 ST E	15th St E	0.058	SR 64/Manatee Ave	3.030	2.972		C3C	C3C	Major Collector	40	UA	A	D	D	D	U	2W	WL	WR	2	2,048	1,018	C			
** 13121000	US 301	SR683/US301/15ST BLVD	University Pkwy	0.000	63rd Ave E/Saunders Rd	2.811	2.811		LA	LA	Principal Arterial-other	55	UA	A	D	D	D	D	2W		0A	4	7,400	3,510	B			
** 13121000	US 301	SR683/US301/15ST BLVD	63rd Ave E/Saunders Rd	2.811	SR 70/Oneco Rd/53rd Ave E	4.060	1.249		LA	LA	Principal Arterial-other	55	UA	A	D	D	D	D	2W		0A	4	7,400	3,690	B			
13121000	US 301	SR683/US301/15ST BLVD	SR 70/Oneco Rd/53rd Ave E	4.060	38th Ave E	5.332	1.272		LA	LA	Principal Arterial-other	55	UA	A	D	D	D	D	2W		0A	4	7,400	3,240	B			
13121000	US 301	SR683/US301/15ST BLVD	38th Ave E	5.332	SR 70/15th St E	6.487	1.155		C3R	C3R	Principal Arterial-other	55	UA	A	D	D	D	D	2W	WL	WR	4	3,528	2,835	C			
13121000	US 301	SR683/US301/15ST BLVD	SR 70/15th St E	6.487	0.104 mi. S. of 13th Ave E	7.908	1.421		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	3,454	2,700	C			
13130000	US 41	1ST ST	41st Ave E	0.115	21st Ave W	1.258	1.143		C3R	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	5,208	3,330	C			
13130000	US 41	1ST ST/TAMIAMI TRAIL	0.104 mi. S. of 13th Ave E	1.661	SR 43/US 301	4.056	2.395		C3R	C3C	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	4	3,528	5,588	F	Over Capacity		
** 13130000	US 41	TAMIAMI TRAIL	SR 43/US 301	4.056	US 19/SR 55 SB/45th St Ct W	5.819	1.763		C3R	C3R	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	OR	4	3,360	3,528	F	Over Capacity		
13130000	US 19 NB	US 19	US 19 SB/SR 55/45th St Ct W	5.819	US 19 SB	6.500	0.681		C3R	C3C	Principal Arterial-other	55	UA	A	D	D	D	U	1W	0L	OR	2	2,220	990	C			

Note: LOS Spreadsheet should be used as a planning level analysis tool. A detailed analysis is necessary to validate the actual operating conditions (and for future conditions, as needed) which may vary from this worksheet.

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR TWO-WAY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	Year 2028 (E+C)						Year 2045						Standard K Factor	Existing D Factor	Existing T <sub>24</sub> Factor
								Thru Lanes	LOS Std	Peak Hour Two-Way			Thru Lanes	LOS Std	Peak Hour Two-Way							
										Capacity	Volume	LOS			Capacity	Trend Volume	Trend LOS	Model Volume	Model LOS			
13050000	SR 64	MANATEE AVE	17th St E	1.808	E. of 39th St E	3.742	1.934	6	D	5,208	4,914	C	6	D	5,366	6,219	F	5,336	D	9.00%	56.10%	4.70%
13050000	SR 64	MANATEE AVE	E. of 39th St E	3.742	57th St E	5.188	1.446	6	D	5,114	4,914	D	6	D	5,114	6,219	F	5,227	F	9.00%	56.10%	4.70%
** 13050000	SR 64	SR 64	57th St E	5.188	I-75	6.360	1.172	6	D	5,114	5,994	F	6	D	5,114	7,929	F	4,912	D	9.00%	56.10%	5.70%
13050000	SR 64	SR 64	I-75	6.360	Lena Rd/Heritage Green Way	7.389	1.029	6	D	7,950	5,139	C	6	D	5,114	7,317	F	4,530	D	9.00%	56.10%	5.60%
** 13050000	SR 64	SR 64	Lena Rd/Heritage Green Way	7.389	Upper Manatee River Rd/Lakewood	8.804	1.415	6	D	7,950	5,769	C	6	D	7,950	9,108	E	3,751	B	9.00%	56.10%	5.20%
13050000	SR 64	SR 64	Upper Manatee River Rd/Lakewood	8.804	Rye Rd	10.577	1.773	4	D	5,290	3,654	C	4	D	3,528	5,859	F	3,036	C	9.00%	56.10%	7.60%
13050000	SR 64	SR 64	Rye Rd	10.577	CR 675	16.835	6.258	2	C	780	750	C	2	C	1,848	968	C	2,034	D	9.43%	63.32%	14.36%
** 13050000	SR 70	SR 70	CR 675	22.134	Singletary Rd/Myakka Wauchula R	31.713	9.579	2	C	780	884	D	2	C	780	1,150	D	426	B	9.50%	56.50%	21.40%
** 13050000	SR 70	SR 70	Singletary Rd/Myakka Wauchula R	31.713	DeSoto County Line	39.483	7.770	2	C	780	732	C	2	C	780	1,064	D	210	B	9.50%	56.50%	26.30%
13050002	SR 64	MANATEE AVE EB/6TH AV	10th St E	0.000	17th St E	0.576	0.576	3	D	3,276	1,826	C	3	D	3,372	2,123	C	2,624	C	9.00%	99.90%	3.95%
13050101	SR 64	MANATEE AVE	12th St E	0.189	10th St W	1.510	1.321	3	D	3,541	1,915	C	3	D	3,541	2,223	C	2,836	D	9.00%	99.90%	3.75%
13050102	SR 64	MANATEE AVE E	17th St E	0.000	12th St E	0.372	0.372	3	D	3,276	1,909	C	3	D	3,372	2,215	C	2,575	C	9.00%	99.90%	5.09%
13060000	SR 62	SR 62	SR 43/US 301	0.000	Corbett John Rd	3.890	3.890	2	C	780	475	C	2	C	780	675	C	802	D	9.50%	56.10%	23.10%
13060000	SR 62	SR 62	Corbett John Rd	3.890	CR 39 /County Line Rd	17.154	13.264	2	C	780	475	C	2	C	780	675	C	729	C	9.50%	56.10%	23.10%
13060000	SR 62	SR 62	CR 39 /County Line Rd	17.154	SR 37	19.264	2.110	2	C	780	656	C	2	C	780	1,026	D	848	D	9.50%	56.10%	32.90%
13060000	SR 62	SR 62	SR 37	19.264	Hardee County Line	23.240	3.976	2	C	780	580	C	2	C	780	827	D	718	C	9.50%	56.10%	27.80%
13070000	SR 37	SR 37	SR 62	0.000	Polk County Line	5.534	5.534	2	C	780	323	B	2	C	780	504	C	313	B	9.50%	56.10%	32.00%
** 13075000	I-75	SR 93/I-75	Sarasota County Line	0.000	SR 70	3.719	3.719	8	D	14,710	14,337	D	10	D	18,598	19,251	E	12,318	C	9.00%	55.80%	12.40%
** 13075000	I-75	SR 93/I-75	SR 70	3.719	SR 64/Manatee Ave	7.330	3.611	8	D	14,710	13,644	D	10	D	18,598	18,396	D	11,245	C	9.00%	55.80%	12.40%
** 13075000	I-75	SR 93/I-75	SR 64/Manatee Ave	7.330	19th St/US 301/SR 43	11.014	3.684	6	D	11,050	12,285	F	10	D	18,598	16,353	D	11,126	B	9.00%	55.80%	12.70%
** 13075000	I-75	SR 93/I-75	19th St/US 301/SR 43	11.014	I-275 off Ramp	14.548	3.534	6	D	11,050	11,763	F	10	D	18,598	15,777	D	9,326	B	9.00%	55.80%	14.70%
** 13075000	I-75	SR 93/I-75	I-275 off Ramp	14.548	I-275 on Ramp	15.300	0.752	6	D	11,050	7,110	C	10	D	18,598	10,035	B	4,067	B	9.00%	54.70%	17.70%
** 13075000	I-75	SR 93/I-75	I-275 on Ramp	15.300	CR 6/Mocassin Wallow Rd	16.200	0.900	6	D	12,850	10,152	C	10	D	18,598	14,688	C	6,246	B	9.00%	55.80%	15.50%
** 13075000	I-75	SR 93/I-75	CR 6/Mocassin Wallow Rd	16.200	Hillsborough County Line	20.571	4.371	6	C	7,250	10,542	F	10	C	12,592	15,120	E	7,117	B	10.50%	52.80%	11.10%
13080000	SR 789	GULF OF MEXICO DR	Sarasota County Line	0.000	Binnacle Pointe Dr	2.410	2.410	2	D	2,121	1,044	C	2	D	1,659	1,476	D	1,480	D	9.00%	57.20%	5.00%
13080000	SR 789	GULF OF MEXICO DR	Binnacle Pointe Dr	2.410	Coquina Park Ent.	5.177	2.767	2	D	2,121	1,044	C	2	D	1,659	1,476	D	1,456	D	9.00%	57.20%	5.00%
13080000	SR 789	GULF DR S	Coquina Park Ent.	5.177	SR 684 (Cortez Rd)	6.666	1.489	2	D	1,659	1,269	D	2	D	1,659	1,476	D	1,780	F	9.00%	54.90%	3.90%
13080000	SR 789	GULF DR N/EAST BAY DR	SR 684 (Cortez Rd)	6.666	SR 64/Manatee Ave	8.717	2.051	2	D	1,659	990	D	2	D	1,659	1,233	D	2,101	F	9.00%	57.20%	3.90%
13120001	SR 70	15 ST E	301 Blvd	0.000	15th St E	0.058	0.058	1	D	1,079	414	C	1	D	1,079	477	C	758	C	9.00%	99.90%	8.50%
13120001	SR 70	15 ST E	15th St E	0.058	SR 64/Manatee Ave	3.030	2.972	2	D	2,048	1,077	C	2	D	2,048	1,250	C	1,249	C	9.00%	56.10%	6.96%
** 13121000	US 301	SR683/US301/15ST BLVD	University Pkwy	0.000	63rd Ave E/Saunders Rd	2.811	2.811	4	D	7,400	3,789	B	4	D	7,400	4,581	C	3,093	B	9.00%	54.80%	7.30%
** 13121000	US 301	SR683/US301/15ST BLVD	63rd Ave E/Saunders Rd	2.811	SR 70/Oneco Rd/53rd Ave E	4.060	1.249	4	D	7,400	4,032	B	4	D	7,400	5,004	C	2,862	B	9.00%	54.80%	12.70%
13121000	US 301	SR683/US301/15ST BLVD	SR 70/Oneco Rd/53rd Ave E	4.060	38th Ave E	5.332	1.272	4	D	7,400	3,474	B	4	D	7,400	4,140	B	3,372	B	9.00%	54.80%	8.70%
13121000	US 301	SR683/US301/15ST BLVD	38th Ave E	5.332	SR 70/15th St E	6.487	1.155	4	D	3,528	3,168	C	4	D	3,528	4,122	F	2,985	C	9.00%	54.80%	10.40%
13121000	US 301	SR683/US301/15ST BLVD	SR 70/15th St E	6.487	0.104 mi. S. of 13th Ave E	7.908	1.421	4	D	3,454	2,898	C	4	D	3,454	3,456	F	3,144	D	9.00%	54.80%	9.10%
13130000	US 41	1ST ST	41st Ave E	0.115	21st Ave W	1.258	1.143	6	D	5,208	3,533	C	6	D	5,114	4,094	C	3,939	C	9.00%	54.80%	4.30%
13130000	US 41	1ST ST/TAMIAMI TRAIL	0.104 mi. S. of 13th Ave E	1.661	SR 43/US 301	4.056	2.395	4	D	3,528	6,004	F	4	D	3,454	7,193	F	6,789	F	9.00%	54.80%	7.00%
** 13130000	US 41	TAMIAMI TRAIL	SR 43/US 301	4.056	US 19/SR 55 SB/45th St Ct W	5.819	1.763	4	D	3,360	3,845	F	4	D	3,360	4,757	F	3,371	F	9.00%	54.80%	6.96%
13130000	US 19 NB	US 19	US 19 SB/SR 55/45th St Ct W	5.819	US 19 SB	6.500	0.681	2	D	2,220	1,071	C	2	D	2,172	1,305	C	2,765	F	9.00%	99.90%	6.90%

Note: LOS Spreadsheet should be used as a planning level analysis tool. A detailed analysis is necessary to validate the actual operating conditions (and for future conditions, as needed) which may vary from this worksheet.

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR TWO-WAY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	SIS	Existing Context Class	Future Context Class	Functional Classification	Posted Speed	Area Type	Facility Type	FDOT LOS Std.	County LOS Std.	City LOS Std.	Year 2022										Deficiency Determination
																		Divided/UnDivided	One/Two Way	Left Turn Bays	Right Turn Bays	Thru Lanes	Peak Hour Two-Way					
																							Capacity	Volume	LOS			
13130000	US 19	US 19	US 41	6.500	Terra Ceia Rd	8.466	1.966		C3R	C3C	Principal Arterial-other	55	RDA	H	C	D		D	2W	WL	WR	4	3,244	2,115	C			
13130000	US 19 NB	US 19	Terra Ceia Rd	8.466	I-275 Ramp	9.194	0.728		LA	LA	Principal Arterial-other	55	RDA	H	C	C		U	1W		0A	2	3,024	1,093	B			
13130000	US 19	US 19	I-275 Ramp	9.194	Skyway Br. Hillsborough County Li	11.982	2.788	SIS	LA	LA	Principal Arterial-Interstate	65	RDA	F	C	C		D	2W	0L	0A	4	5,040	7,403	F	Over Capacity		
13130101	US 19 SB	1ST ST	US 19 NB/SR 55/45th St Ct W	0.000	US 41/SR 45	0.654	0.654		C3C	C3C	Principal Arterial-other	45	UA	H	D	D		U	1W	WL	0R	2	2,172	1,683	C			
13130401	US 41	SR 55/US 19	21st Ave	0.000	Railroad Crossing	0.403	0.403		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		U	1W	0L	WR	2	2,281	1,800	C			
13130403	US 41	1ST ST	Railroad Crossing	0.000	21st Ave W	0.403	0.403		C3C	C4	Principal Arterial-other	45	UA	A	D	D		U	1W	WL	0R	2	2,172	1,620	C			
13140000	SR 64	BRADENTON-ARCADIA RD	CR 675	0.000	Wauchula Rd	13.733	13.733		C2	C2	Principal Arterial-other	60	RDA	H	C	C		U	2W	WL	WR	2	780	551	C			
13140000	SR 64	BRADENTON-ARCADIA RD	Wauchula Rd	13.733	Hardee County Line	16.071	2.338		C2	C2	Principal Arterial-other	60	RDA	H	C	C		U	2W	WL	0R	2	780	399	B			
13150000	SR 64	MANATEE AVE	SR 789 /Gulf Drive	0.000	Flamingo Dr	2.882	2.882		C3C	C3R	Principal Arterial-other	45	UA	A	D	D	D	U	2W	WL	WR	2	2,048	1,584	D			
13150000	SR 64	MANATEE AVE	Flamingo Dr	2.882	75th St W	4.572	1.690		C4	C3R	Principal Arterial-other	45	UA	A	D	D	D	U	2W	WL	WR	2	1,659	1,620	D	Near Capacity		
13150000	SR 64	MANATEE AVE	75th St W	4.572	51st St W	6.074	1.502		C4	C4	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	3,412	2,880	D			
13150000	SR 64	MANATEE AVE	51st St W	6.074	SR 64/Manatee Ave	8.305	2.231		C4	C4	Principal Arterial-other	40	UA	A	D	D	D	D	2W	WL	WR	4	3,412	3,728	E	Over Capacity		
13150000	SR 64	15TH ST W/6TH AVE W	SR 64/Manatee Ave	8.305	10th St W	8.613	0.308		C5	C5	Principal Arterial-other	30	UA	A	D	D	D	U	1W	0L	WR	3	3,364	1,890	D			
13150101	SR 64	MANATEE AVE WB	10th St	0.000	15th St	0.235	0.235		C5	C5	Principal Arterial-other	30	UA	A	D	D	D	U	1W	WL	0R	2	2,280	2,205	D	Near Capacity		
13160000	SR 70	53RD AVE/ONECO RD	301 Blvd/15th St E	0.000	US 301/SR 683/15th St Blvd	1.001	1.001		C3C	C3C	Principal Arterial-other	40	UA	A	D	D		D	2W	WL	WR	6	5,114	3,060	C			
13160000	SR 70	53RD AVE/ONECO RD/SR 70	US 301/SR 683/15th St Blvd	1.001	Lockwood Ridge Rd/45th St E	2.538	1.537		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	5,114	4,140	C			
13160000	SR 70	SR 70	Lockwood Ridge Rd/45th St E	2.538	Fairway Gardens Dr/68th St E	4.388	1.850		C3C	C3C	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	6	5,114	4,751	D	Near Capacity		
13160000	SR 70	SR 70	Fairway Gardens Dr/68th St E	4.388	I-75	5.438	1.050		C3C	C3C	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	6	5,114	6,570	F	Over Capacity		
13160000	SR 70	SR 70	I-75	5.438	Lakewood Ranch Blvd	7.448	2.010	SIS	C3R	C3R	Principal Arterial-other	50	UA	A	D	C		D	2W	WL	WR	6	5,208	4,950	C	Near Capacity		
13160000	SR 70	SR 70	Lakewood Ranch Blvd	7.448	Lorraine Rd	9.464	2.016	SIS	C3R	C3R	Principal Arterial-other	50	UA	A	D	C		D	2W	WL	WR	6	5,208	3,105	C			
13160000	SR 70	SR 70	Lorraine Rd	9.464	Bournside Blvd	12.328	2.864	SIS	C3R	C3R	Principal Arterial-other	60	RDA	H	C	C		D	2W	WL	WR	2	1,940	1,782	C	Near Capacity		
13160000	SR 70	SR 70	Bournside Blvd	12.328	CR 675 (Waterbury Rd)	15.567	3.239	SIS	C3R	C3R	Principal Arterial-other	60	RDA	H	C	C		D	2W	WL	WR	2	1,940	1,782	C	Near Capacity		
13162000	SR 70	53RD AVE/ONECO RD	US41/SR 45/14th St	0.000	15th St E/301 Blvd	1.761	1.761		C3R	C3R	Principal Arterial-other	40	UA	A	D	D		U	2W	WL	WR	4	3,352	2,835	C			
13175000	I-275	SR 93/I-275	Erie Rd/CR 10/69th St	0.000	I-75	0.690	0.690	SIS Link	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C		U	1W		0A	2	3,700	1,530	B			
13175000	I-275	SR 93/I-275	I-75	0.690	E. of 36th Ave E/CR 683/Ellenton C	1.344	0.654	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C		U	1W		0A	2	3,700	1,530	B			
13175000	I-275	SR 93/I-275	E. of 36th Ave E/CR 683/Ellenton C	1.344	US 41/SR 45	2.279	0.935	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C		D	2W		W1A	4	8,300	5,535	C			
13175000	I-275	SR 93/I-275	US 41/SR 45	2.279	US 19 NB Ramp	5.233	2.954	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C		D	2W		0A	4	5,040	4,230	C			
13000012	Piney Point Road	Piney Point Road	SR 45/US 41	0.000	Harlee Rd/North Dock St	0.583	0.583	SC	C3C	C3C	Major Collector	30	UA	A	D	D		U	2W	WL	WR	2	2,048	342	C			

**Legend**

Aggregated segment
  Single count segment
  OFF System SIS Facility

\*\* Future Model Volumes obtained from D1 Regional Planning Model are found to be less than Existing AADT.

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR TWO-WAY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	Year 2028 (E+C)						Year 2045						Standard K Factor	Existing D Factor	Existing T <sub>24</sub> Factor
								Thru Lanes	LOS Std	Peak Hour Two-Way			Thru Lanes	LOS Std	Peak Hour Two-Way							
										Capacity	Volume	LOS			Capacity	Trend Volume	Trend LOS	Model Volume	Model LOS			
13130000	US 19	US 19	US 41	6.500	Terra Ceia Rd	8.466	1.966	4	C	3,244	2,268	C	4	C	2,898	2,709	C	2,471	C	9.00%	54.80%	6.40%
13130000	US 19 NB	US 19	Terra Ceia Rd	8.466	I-275 Ramp	9.194	0.728	2	C	3,024	1,235	B	2	C	3,024	1,653	B	2,510	C	9.50%	99.90%	7.00%
13130000	US 19	US 19	I-275 Ramp	9.194	Skyway Br. Hillsborough County Li	11.982	2.788	4	C	5,040	8,159	F	4	C	5,040	10,290	F	8,905	F	10.50%	54.70%	7.80%
13130101	US 19 SB	1ST ST	US 19 NB/SR 55/45th St Ct W	0.000	US 41/SR 45	0.654	0.654	2	D	2,172	1,840	D	2	D	2,172	2,294	F	2,347	F	9.00%	99.90%	7.63%
13130401	US 41	SR 55/US 19	21st Ave	0.000	Railroad Crossing	0.403	0.403	2	D	2,281	1,908	C	2	D	2,281	2,214	D	2,211	D	9.00%	99.90%	4.80%
13130403	US 41	1ST ST	Railroad Crossing	0.000	21st Ave W	0.403	0.403	2	D	2,172	1,719	C	2	D	2,148	1,989	D	2,017	D	9.00%	99.90%	5.10%
13140000	SR 64	BRADENTON-ARCADIA RD	CR 675	0.000	Wauchula Rd	13.733	13.733	2	C	780	589	C	2	C	780	684	C	426	B	9.50%	56.10%	20.60%
13140000	SR 64	BRADENTON-ARCADIA RD	Wauchula Rd	13.733	Hardee County Line	16.071	2.338	2	C	780	466	C	2	C	780	665	C	210	B	9.50%	56.10%	22.70%
13150000	SR 64	MANATEE AVE	SR 789 /Gulf Drive	0.000	Flamingo Dr	2.882	2.882	2	D	2,048	1,701	D	2	D	2,121	2,025	D	2,120	D	9.00%	57.20%	4.60%
13150000	SR 64	MANATEE AVE	Flamingo Dr	2.882	75th St W	4.572	1.690	2	D	1,659	1,719	F	2	D	2,121	1,989	D	2,435	F	9.00%	57.20%	4.70%
13150000	SR 64	MANATEE AVE	75th St W	4.572	51st St W	6.074	1.502	4	D	3,412	3,231	D	4	D	3,412	4,212	F	4,115	F	9.00%	56.10%	3.80%
13150000	SR 64	MANATEE AVE	51st St W	6.074	SR 64/Manatee Ave	8.305	2.231	4	D	3,412	3,953	F	4	D	3,412	4,588	F	4,620	F	9.00%	56.10%	3.55%
13150000	SR 64	15TH ST W/6TH AVE W	SR 64/Manatee Ave	8.305	10th St W	8.613	0.308	3	D	3,364	2,007	D	3	D	3,364	2,322	D	2,853	D	9.00%	99.90%	3.10%
13150101	SR 64	MANATEE AVE WB	10th St	0.000	15th St	0.235	0.235	2	D	2,280	2,340	E	2	D	2,280	2,709	F	2,942	F	9.00%	99.90%	4.30%
13160000	SR 70	53RD AVE/ONECO RD	301 Blvd/15th St E	0.000	US 301/SR 683/15th St Blvd	1.001	1.001	6	D	5,114	3,240	C	6	D	5,114	3,762	C	4,360	C	9.00%	56.10%	5.00%
13160000	SR 70	53RD AVE/ONECO RD/SR 70	US 301/SR 683/15th St Blvd	1.001	Lockwood Ridge Rd/45th St E	2.538	1.537	6	D	5,114	4,392	C	6	D	5,114	5,094	D	5,800	F	9.00%	56.50%	7.00%
13160000	SR 70	SR 70	Lockwood Ridge Rd/45th St E	2.538	Fairway Gardens Dr/68th St E	4.388	1.850	6	D	5,114	5,202	F	6	D	5,114	6,471	F	5,439	F	9.00%	51.00%	8.00%
13160000	SR 70	SR 70	Fairway Gardens Dr/68th St E	4.388	I-75	5.438	1.050	6	D	5,114	6,966	F	6	D	5,114	8,082	F	6,283	F	9.00%	56.50%	5.90%
13160000	SR 70	SR 70	I-75	5.438	Lakewood Ranch Blvd	7.448	2.010	6	D	5,208	5,436	F	6	D	5,208	6,813	F	6,213	F	9.00%	56.50%	5.10%
13160000	SR 70	SR 70	Lakewood Ranch Blvd	7.448	Lorraine Rd	9.464	2.016	6	D	5,208	3,717	C	6	D	5,208	5,445	F	3,819	C	9.00%	56.50%	5.80%
13160000	SR 70	SR 70	Lorraine Rd	9.464	Bournside Blvd	12.328	2.864	4	C	3,244	2,205	C	4	C	3,244	3,420	D	1,663	C	9.00%	56.50%	10.30%
13160000	SR 70	SR 70	Bournside Blvd	12.328	CR 675 (Waterbury Rd)	15.567	3.239	4	C	3,244	2,205	C	4	C	3,244	3,420	D	1,898	C	9.00%	56.50%	10.30%
13162000	SR 70	53RD AVE/ONECO RD	US41/SR 45/14th St	0.000	15th St E/301 Blvd	1.761	1.761	4	D	3,352	3,006	C	4	D	3,352	3,483	F	2,633	C	9.00%	56.10%	3.40%
13175000	I-275	SR 93/I-275	Erie Rd/CR 10/69th St	0.000	I-75	0.690	0.690	2	D	3,700	1,629	B	2	D	3,700	1,908	B	2,879	C	9.00%	99.90%	7.30%
13175000	I-275	SR 93/I-275	I-75	0.690	E. of 36th Ave E/CR 683/Ellenton C	1.344	0.654	2	D	3,700	1,629	B	2	D	3,700	1,908	B	2,203	B	9.00%	99.90%	7.30%
13175000	I-275	SR 93/I-275	E. of 36th Ave E/CR 683/Ellenton C	1.344	US 41/SR 45	2.279	0.935	4	D	8,300	6,291	C	4	D	8,300	8,433	E	3,917	B	9.00%	54.70%	9.80%
13175000	I-275	SR 93/I-275	US 41/SR 45	2.279	US 19 NB Ramp	5.233	2.954	4	C	5,040	4,707	C	4	C	5,040	6,057	E	5,255	D	9.00%	54.70%	6.50%
13000012	Piney Point Road	Piney Point Road	SR 45/US 41	0.000	Harlee Rd/North Dock St	0.583	0.583	2	D	2,048	477	C	2	D	2,048	846	C	336	C	9.00%	56.10%	47.20%

**Legend**

Aggregated segment
  Single count segment
  OFF System SIS Facility

\*\* Future Model Volumes obtained from D1 Regional Planning Model are found to be less than Existing AADT.

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR PEAK DIRECTION**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	SIS	Existing Context Class	Future Context Class	Functional Classification	Posted Speed	Area Type	Facility Type	FDOT LOS Std.	County LOS Std.	City LOS Std.	Year 2022										Deficiency Determination
																		Divided/UnDivided	One/Two Way	Left Turn Bays	Right Turn Bays	Thru Lanes	Peak Hour Peak Direction					
																							Capacity	Volume	LOS			
13010000	US 41	TAMIAMI TRAIL	Sarasota County Line	0.000	63rd Ave W/Bay Dr	3.007	3.007		C3C	C4	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	6	2,814	2,019	C			
13010000	US 41	14TH ST W.	63rd Ave W/Bay Dr	3.007	SR 684 /Cortez Rd	5.284	2.277		C3R	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	2,866	2,167	C			
13010000	USB 41	14TH ST W.	SR 684 /Cortez Rd	5.284	9th Ave W/MLK Jr Ave W	7.320	2.036		C4	C4	Principal Arterial-other	40	UA	A	D	D	D	U	2W	WL	WR	4	1,786	1,081	C			
13010000	USB 41	14TH ST W./8TH AVE W.	9th Ave	7.320	10th St W	7.552	0.232		C4	C4	Principal Arterial-other	35	UA	A	D	D	D	U	2W	WL	WR	4	1,786	572	C			
13010000	USB 41	8TH AVE W.	Riverside Dr	8.878	10th St W	9.498	0.620		C4	C4	Principal Arterial-other	30	UA	A	D	D	D	U	2W	WL	OR	4	1,700	1,534	D	Near Capacity		
13010001	USB 41	8TH AVE W./9TH ST W.	10th St W	0.000	Riverside Dr	1.379	1.379		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	1,900	1,816	D	Near Capacity		
** 13020000	SR 43/US 301	10TH ST W.	SR 45/US 41 Bus	0.000	US 41/Tamiami Trail	0.643	0.643		C4	C4	Principal Arterial-other	40	UA	A	D	D	D	D	2W	WL	OR	4	1,790	902	C			
13020000	US 301	SR 43/US 301	US 41/Tamiami Trail	0.643	CR 683/36th Ave	2.745	2.102		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	1,900	1,800	D	Near Capacity		
13020000	US 301	SR 43/US 301	CR 683/36th Ave	2.745	19th St/51st Ave	3.842	1.097		C3R	C3R	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	4	1,942	1,749	C	Near Capacity		
13020000	US 301	SR 43/US 301	19th St/51st Ave	3.842	Old Tampa Rd/Erie Rd	6.310	2.468		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	4	1,900	1,964	F	Over Capacity		
** 13020000	US 301	SR 43/US 301	Old Tampa Rd/Erie Rd	6.310	Chin Rd/Palm Blvd	8.528	2.218		C3R	C3R	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	4	1,942	1,168	C			
** 13020000	US 301	SR 43/US 301	Chin Rd/Palm Blvd	8.528	Fort Hamer Rd/121st Ave E	10.054	1.526		C3R	C3R	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	4	1,942	799	C			
13020000	US 301	SR 43/US 301	Fort Hamer Rd	10.054	Moccasin Wallow Rd	11.646	1.592		C3R	C3R	Principal Arterial-other	45	UA	H	D	C		D	2W	WL	OR	4	1,850	844	C			
13020000	US 301	SR 43/US 301	Dickey Rd	11.646	Hillsborough County Line	16.066	4.420		C2	C2	Principal Arterial-other	60	RDA	H	C	C		U	2W	WL	WR	2	430	469	D	Over Capacity		
13030000	USB 41	8TH AVE W.	SR 43/10th St W	0.000	26th St W	1.309	1.309		C4	C4	Principal Arterial-other	35	UA	A	D	D	D	D	2W	WL	WR	4	1,880	1,284	D			
13030103	USB 41	8TH AVE W.	US 41 Bus/SR 45	0.000	26th St W	0.385	0.385		C3C	C3C	Principal Arterial-other	50	UA	A	D	D	D	U	1W	WL	WR	2	2,281	1,079	C			
13030000	US 41	TAMIAMI TRAIL	SR 55/US 19 NB On Ramp	2.225	0.021 mi. S of 45th St E	2.582	0.357		C3C	C3R	Principal Arterial-other	55	UA	A	D	D		U	1W	WL	OR	2	2,172	1,753	C			
13030000	US 41	TAMIAMI TRAIL	0.021 mi. S of 45th St E	2.582	I-275	5.202	2.620		C2	C3R	Principal Arterial-other	60	UA	A	D	D		D	2W	WL	WR	4	2,910	1,652	B			
13030000	US 41	TAMIAMI TRAIL	I-275	5.202	Piney Point Rd/Inland Transp St	8.542	3.340	SC	C2	C2	Principal Arterial-other	60	UA	H	D	D		D	2W	WL	WR	4	2,910	883	B			
13030000	US 41	TAMIAMI TRAIL	Piney Point Rd/Inland Transp St	8.542	Hillsborough County Line	9.782	1.240		C2	C2	Principal Arterial-other	60	UA	H	D	D		D	2W	WL	WR	4	2,910	760	B			
13030101	USB 41 -NB	SR 45/USB 41	26th St W/US 41/SR 45	0.000	SR 55/US 19	0.560	0.560		C3C	C3C	Principal Arterial-other	50	UA	A	D	D		U	1W	OL	OR	2	2,172	1,124	C			
13030102	USB 41 -SB	SR 45/US 41	SR 45/US 41/45th St E	0.000	SR 55/US 19	0.457	0.457		C4	C3C	Principal Arterial-other	55	UA	A	D	D		U	1W	OL	OR	2	2,148	1,618	D			
13040000	SR 684	CORTEZ RD	SR 789/Gulf Dr	0.000	119th St W	1.188	1.188		C3R	C3R	Principal Arterial-other	35	UA	A	D	D		U	2W	WL	WR	2	1,166	920	C			
13040000	SR 684	CORTEZ RD	119th St W	1.188	75th St W	3.971	2.783		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	4	1,900	1,338	C			
13040000	SR 684	CORTEZ RD	75th St W	3.971	37th St W	6.372	2.401		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	1,900	1,439	C			
13040000	SR 684	CORTEZ RD	37th St W	6.372	9th St W	8.011	1.639		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	2,814	2,608	D	Near Capacity		
13040000	US 41	CORTEZ RD	9th St W	8.011	US 41 (Cortez Rd)	8.440	0.429		C3C	C3C	Principal Arterial-other	35	UA	A	D	D		D	2W	WL	WR	4	1,900	1,973	F	Over Capacity		
13040000	US 41	44TH AVE E	US 41 (Cortez Rd)	8.440	0.064 mi. E of 1st St	8.574	0.134		C3C	C3C	Minor Arterial	35	UA	A	D	D		U	1W	OL	OR	1	1,079	728	C			
13040001	US 41	TAMIAMI TRAIL	SR 684/Cortez Rd/44th Ave E	0.000	41st Ave E	0.158	0.158		C3C	C3C	Principal Arterial-other	35	UA	A	D	D		D	2W	WL	OR	4	1,810	1,726	D	Near Capacity		
** 13040002	SR 684	44TH AVE CONNECTOR	44th Ave	0.000	US 41/Tamiami Trail	0.073	0.073		C3C	C3C	Minor Arterial	35	UA	A	D	D		D	2W	WL	WR	2	1,180	628	C			
13050000	SR 64	6TH AVE E/7TH AVE	US 41B (10th St W)	0.000	10th St E	1.122	1.122		C3C	C4	Principal Arterial-other	35	UA	A	D	D	D	U	1W	WL	WR	3	3,377	1,599	C			

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR PEAK DIRECTION**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	Year 2028 (E+C)						Year 2045						Standard K Factor	Existing D Factor	Existing T <sub>24</sub> Factor
								Thru Lanes	LOS Std	Peak Hour Peak Direction			Thru Lanes	LOS Std	Peak Hour Peak Direction							
										Capacity	Volume	LOS			Capacity	Trend Volume	Trend LOS	Model Volume	Model LOS			
13010000	US 41	TAMIAMI TRAIL	Sarasota County Line	0.000	63rd Ave W/Bay Dr	3.007	3.007	6	D	2,814	2,148	C	6	D	2,950	2,494	D	2,662	D	9.00%	54.80%	3.20%
13010000	US 41	14TH ST W.	63rd Ave W/Bay Dr	3.007	SR 684 /Cortez Rd	5.284	2.277	6	D	2,866	2,306	C	6	D	2,814	2,675	D	2,578	D	9.00%	54.80%	2.80%
13010000	USB 41	14TH ST W.	SR 684 /Cortez Rd	5.284	9th Ave W/MLK Jr Ave W	7.320	2.036	4	D	1,786	1,149	C	4	D	1,786	1,335	D	1,482	D	9.00%	54.80%	2.73%
13010000	USB 41	14TH ST W./8TH AVE W.	9th Ave	7.320	10th St W	7.552	0.232	4	D	1,786	609	C	4	D	1,786	708	C	737	C	9.00%	54.80%	2.80%
13010000	USB 41	8TH AVE W.	Riverside Dr	8.878	10th St W	9.498	0.620	4	D	1,700	1,631	D	4	D	1,700	1,892	E	1,789	E	9.00%	54.80%	4.10%
13010001	USB 41	8TH AVE W./9TH ST W.	10th St W	0.000	Riverside Dr	1.379	1.379	4	D	1,900	1,977	F	4	D	1,900	2,408	F	2,529	F	9.00%	54.80%	3.96%
** 13020000	SR 43/US 301	10TH ST W.	SR 45/US 41 Bus	0.000	US 41/Tamiami Trail	0.643	0.643	4	D	1,790	961	C	4	D	1,790	1,112	C	845	C	9.00%	54.80%	4.90%
13020000	US 301	SR 43/US 301	US 41/Tamiami Trail	0.643	CR 683/36th Ave	2.745	2.102	4	D	1,900	1,916	F	4	D	1,900	2,223	F	2,137	F	9.00%	54.80%	6.90%
13020000	US 301	SR 43/US 301	CR 683/36th Ave	2.745	19th St/51st Ave	3.842	1.097	4	D	1,942	1,881	D	4	D	1,942	2,411	F	1,988	F	9.00%	56.80%	6.50%
13020000	US 301	SR 43/US 301	19th St/51st Ave	3.842	Old Tampa Rd/Erie Rd	6.310	2.468	4	D	1,900	2,101	F	4	D	1,900	2,467	F	2,397	F	9.00%	54.80%	5.10%
** 13020000	US 301	SR 43/US 301	Old Tampa Rd/Erie Rd	6.310	Chin Rd/Palm Blvd	8.528	2.218	4	D	1,942	1,428	C	4	D	1,942	2,161	F	1,057	C	9.00%	54.80%	5.80%
** 13020000	US 301	SR 43/US 301	Chin Rd/Palm Blvd	8.528	Fort Hamer Rd/121st Ave E	10.054	1.526	4	D	1,942	960	C	4	D	1,942	1,411	C	594	C	9.00%	54.80%	9.30%
13020000	US 301	SR 43/US 301	Fort Hamer Rd	10.054	Moccasin Wallow Rd	11.646	1.592	4	D	1,850	1,047	C	4	D	1,850	1,618	C	904	C	9.00%	54.80%	12.88%
13020000	US 301	SR 43/US 301	Dickey Rd	11.646	Hillsborough County Line	16.066	4.420	2	C	430	585	D	2	C	430	946	E	770	E	9.50%	56.10%	10.70%
13030000	USB 41	8TH AVE W.	SR 43/10th St W	0.000	26th St W	1.309	1.309	4	D	1,880	1,392	D	4	D	1,880	1,679	D	1,751	D	9.00%	54.80%	4.83%
13030103	USB 41	8TH AVE W.	US 41 Bus/SR 45	0.000	26th St W	0.385	0.385	2	D	2,281	1,179	C	2	D	2,281	1,476	C	1,719	C	9.00%	99.90%	5.50%
13030000	US 41	TAMIAMI TRAIL	SR 55/US 19 NB On Ramp	2.225	0.021 mi. S of 45th St E	2.582	0.357	2	D	2,172	2,052	D	2	D	2,220	2,907	F	2,025	C	9.00%	99.90%	10.90%
13030000	US 41	TAMIAMI TRAIL	0.021 mi. S of 45th St E	2.582	I-275	5.202	2.620	4	D	2,910	1,965	C	4	D	1,942	2,836	F	1,765	C	9.00%	54.80%	9.40%
13030000	US 41	TAMIAMI TRAIL	I-275	5.202	Piney Point Rd/Inland Transp St	8.542	3.340	4	D	2,910	1,059	B	4	D	2,910	1,539	B	1,949	C	9.00%	54.80%	16.20%
13030000	US 41	TAMIAMI TRAIL	Piney Point Rd/Inland Transp St	8.542	Hillsborough County Line	9.782	1.240	4	D	2,910	960	B	4	D	2,910	1,525	B	1,521	B	9.00%	54.80%	12.60%
13030101	USB 41 -NB	SR 45/USB 41	26th St W/US 41/SR 45	0.000	SR 55/US 19	0.560	0.560	2	D	2,172	1,278	C	2	D	2,172	1,719	C	1,777	C	9.00%	99.90%	5.80%
13030102	USB 41 -SB	SR 45/US 41	SR 45/US 41/45th St E	0.000	SR 55/US 19	0.457	0.457	2	D	2,148	1,881	D	2	D	2,172	2,601	F	1,805	C	9.00%	99.90%	6.90%
13040000	SR 684	CORTEZ RD	SR 789/Gulf Dr	0.000	119th St W	1.188	1.188	2	D	1,166	947	C	2	D	1,166	1,127	D	1,209	F	9.00%	57.20%	4.30%
13040000	SR 684	CORTEZ RD	119th St W	1.188	75th St W	3.971	2.783	4	D	1,900	1,411	C	4	D	1,900	1,757	D	1,800	D	9.00%	57.20%	4.30%
13040000	SR 684	CORTEZ RD	75th St W	3.971	37th St W	6.372	2.401	4	D	1,900	1,495	C	4	D	1,900	1,737	D	3,486	F	9.00%	56.10%	3.60%
13040000	SR 684	CORTEZ RD	37th St W	6.372	9th St W	8.011	1.639	6	D	2,814	2,716	D	6	D	2,814	3,153	F	2,974	F	9.00%	55.77%	3.88%
13040000	US 41	CORTEZ RD	9th St W	8.011	US 41 (Cortez Rd)	8.440	0.429	4	D	1,900	2,099	F	4	D	1,900	2,435	F	2,606	F	9.00%	54.80%	4.40%
13040000	US 41	44TH AVE E	US 41 (Cortez Rd)	8.440	0.064 mi. E of 1st St	8.574	0.134	1	D	1,079	873	D	1	D	1,079	1,287	F	1,002	D	9.00%	99.90%	5.60%
13040001	US 41	TAMIAMI TRAIL	SR 684/Cortez Rd/44th Ave E	0.000	41st Ave E	0.158	0.158	4	D	1,810	1,835	F	4	D	1,810	2,131	F	1,829	F	9.00%	54.80%	4.34%
** 13040002	SR 684	44TH AVE CONNECTOR	44th Ave	0.000	US 41/Tamiami Trail	0.073	0.073	2	D	1,180	678	C	2	D	1,180	970	D	597	C	9.00%	60.20%	4.60%
13050000	SR 64	6TH AVE E/7TH AVE	US 41B (10th St W)	0.000	10th St E	1.122	1.122	3	D	3,377	1,696	C	3	D	3,541	1,971	C	2,513	C	9.00%	99.90%	3.75%

Note: LOS Spreadsheet should be used as a planning level analysis tool. A detailed analysis is necessary to validate the actual operating conditions (and for future conditions, as needed) which may vary from this worksheet.



**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR PEAK DIRECTION**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	SIS	Existing Context Class	Future Context Class	Functional Classification	Posted Speed	Area Type	Facility Type	FDOT LOS Std.	County LOS Std.	City LOS Std.	Year 2022										Deficiency Determination
																		Divided/UnDivided	One/Two Way	Left Turn Bays	Right Turn Bays	Thru Lanes	Peak Hour Peak Direction					
																							Capacity	Volume	LOS			
13050000	SR 64	MANATEE AVE	17th St E	1.808	E. of 39th St E	3.742	1.934		C3R	C4	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	2,866	2,499	C			
13050000	SR 64	MANATEE AVE	E. of 39th St E	3.742	57th St E	5.188	1.446		C3C	C3C	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	6	2,814	2,499	D			
** 13050000	SR 64	SR 64	57th St E	5.188	I-75	6.360	1.172		C3C	C3C	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	6	2,814	2,979	F	Over Capacity		
13050000	SR 64	SR 64	I-75	6.360	Lena Rd/Heritage Green Way	7.389	1.029		C2	C3C	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	6	4,370	2,449	B			
** 13050000	SR 64	SR 64	Lena Rd/Heritage Green Way	7.389	Upper Manatee River Rd/Lakewood	8.804	1.415		C2	C2	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	6	4,370	2,575	C			
13050000	SR 64	SR 64	Upper Manatee River Rd/Lakewood	8.804	Rye Rd	10.577	1.773		C2	C3R	Principal Arterial-other	50	UA	H	D	D	D	D	2W	WL	WR	4	2,910	1,616	B			
13050000	SR 64	SR 64	Rye Rd	10.577	CR 675	16.835	6.258		C2	C3R	Principal Arterial-other	60	RDA	H	C	C	D	U	2W	WL	WR	2	430	417	C	Near Capacity		
** 13050000	SR 70	SR 70	CR 675	22.134	Singletary Rd/Myakka Wauchula R	31.713	9.579	SIS	C2	C2	Principal Arterial-other	60	RDA	H	C	C	D	U	2W	WL	WR	2	430	446	D	Over Capacity		
** 13050000	SR 70	SR 70	Singletary Rd/Myakka Wauchula R	31.713	DeSoto County Line	39.483	7.770	SIS	C2	C2	Principal Arterial-other	60	RDA	H	C	C	D	U	2W	WL	OR	2	430	344	C			
13050002	SR 64	MANATEE AVE EB/6TH AVE	10th St E	0.000	17th St E	0.576	0.576		C3R	C4	Principal Arterial-other	35	UA	A	D	D	D	U	1W	WL	OR	3	3,276	1,722	C			
13050101	SR 64	MANATEE AVE	12th St E	0.189	10th St W	1.510	1.321		C4	C4	Principal Arterial-other	35	UA	A	D	D	D	U	1W	WL	WR	3	3,541	1,805	C			
13050102	SR 64	MANATEE AVE E	17th St E	0.000	12th St E	0.372	0.372		C3R	C4	Principal Arterial-other	35	UA	A	D	D	D	U	1W	WL	OR	3	3,276	1,799	C			
13060000	SR 62	SR 62	SR 43/US 301	0.000	Corbett John Rd	3.890	3.890		C2	C2	Minor Arterial	60	RDA	H	C	C	D	U	2W	0L	OR	2	430	229	B			
13060000	SR 62	SR 62	Corbett John Rd	3.890	CR 39 /County Line Rd	17.154	13.264		C2	C2	Minor Arterial	60	RDA	H	C	C	D	U	2W	WL	WR	2	430	229	B			
13060000	SR 62	SR 62	CR 39 /County Line Rd	17.154	SR 37	19.264	2.110		C2	C2	Minor Arterial	60	RDA	H	C	C	D	U	2W	WL	OR	2	430	293	C			
13060000	SR 62	SR 62	SR 37	19.264	Hardee County Line	23.240	3.976		C2	C2	Minor Arterial	60	RDA	H	C	C	D	U	2W	0L	WR	2	430	277	C			
13070000	SR 37	SR 37	SR 62	0.000	Polk County Line	5.534	5.534		C2	C2	Minor Arterial	60	RDA	H	C	C	D	U	2W	WL	WR	2	430	144	B			
** 13075000	I-75	SR 93/I-75	Sarasota County Line	0.000	SR 70	3.719	3.719	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		0A	8	8,090	7,031	D			
** 13075000	I-75	SR 93/I-75	SR 70	3.719	SR 64/Manatee Ave	7.330	3.611	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		0A	8	8,090	6,679	D			
** 13075000	I-75	SR 93/I-75	SR 64/Manatee Ave	7.330	19th St/US 301/SR 43	11.014	3.684	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		0A	6	6,080	6,052	D	Near Capacity		
** 13075000	I-75	SR 93/I-75	19th St/US 301/SR 43	11.014	I -275 off Ramp	14.548	3.534	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		0A	6	6,080	5,775	D	Near Capacity		
** 13075000	I-75	SR 93/I-75	I -275 off Ramp	14.548	I -275 on Ramp	15.300	0.752	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		0A	6	6,080	3,323	B			
** 13075000	I-75	SR 93/I-75	I -275 on Ramp	15.300	CR 6/Mocassin Wallow Rd	16.200	0.900	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C	D	D	2W		WA	6	7,080	4,771	C			
** 13075000	I-75	SR 93/I-75	CR 6/Mocassin Wallow Rd	16.200	Hillsborough County Line	20.571	4.371	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C	D	D	2W		0A	6	3,990	4,712	D	Over Capacity		
13080000	SR 789	GULF OF MEXICO DR	Sarasota County Line	0.000	Binnacle Pointe Dr	2.410	2.410		C3R	C4	Major Collector	45	UA	H	D		E	U	2W	WL	WR	2	1,166	510	C			
13080000	SR 789	GULF OF MEXICO DR	Binnacle Pointe Dr	2.410	Coquina Park Ent.	5.177	2.767		C3R	C4	Major Collector	45	UA	H	D		E	U	2W	WL	WR	2	1,166	510	C			
13080000	SR 789	GULF DR S	Coquina Park Ent.	5.177	SR 684 (Cortez Rd)	6.666	1.489		C4	C4	Major Collector	25	UA	A	D		D	U	2W	WL	WR	2	914	659	D			
13080000	SR 789	GULF DR N/EAST BAY DR	SR 684 (Cortez Rd)	6.666	SR 64/Manatee Ave	8.717	2.051		C4	C4	Major Collector	35	UA	A	D		D	U	2W	WL	WR	2	914	515	D			
13120001	SR 70	15 ST E	301 Blvd	0.000	15th St E	0.058	0.058		C3C	C3C	Major Collector	45	UA	A	D	D	D	U	1W	0L	OR	1	1,079	387	C			
13120001	SR 70	15 ST E	15th St E	0.058	SR 64/Manatee Ave	3.030	2.972		C3C	C3C	Major Collector	40	UA	A	D	D	D	U	2W	WL	WR	2	1,124	571	C			
** 13121000	US 301	SR683/US301/15ST BLVD	University Pkwy	0.000	63rd Ave E/Saunders Rd	2.811	2.811		LA	LA	Principal Arterial-other	55	UA	A	D	D	D	D	2W		0A	4	4,070	1,923	B			
** 13121000	US 301	SR683/US301/15ST BLVD	63rd Ave E/Saunders Rd	2.811	SR 70/Oneco Rd/53rd Ave E	4.060	1.249		LA	LA	Principal Arterial-other	55	UA	A	D	D	D	D	2W		0A	4	4,070	2,022	B			
13121000	US 301	SR683/US301/15ST BLVD	SR 70/Oneco Rd/53rd Ave E	4.060	38th Ave E	5.332	1.272		LA	LA	Principal Arterial-other	55	UA	A	D	D	D	D	2W		0A	4	4,070	1,776	B			
13121000	US 301	SR683/US301/15ST BLVD	38th Ave E	5.332	SR 70/15th St E	6.487	1.155		C3R	C3R	Principal Arterial-other	55	UA	A	D	D	D	D	2W	WL	WR	4	1,942	1,554	C			
13121000	US 301	SR683/US301/15ST BLVD	SR 70/15th St E	6.487	0.104 mi. S. of 13th Ave E	7.908	1.421		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	1,900	1,480	C			
13130000	US 41	1ST ST	41st Ave E	0.115	21st Ave W	1.258	1.143		C3R	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	2,866	1,825	C			
13130000	US 41	1ST ST/TAMIAMI TRAIL	0.104 mi. S. of 13th Ave E	1.661	SR 43/US 301	4.056	2.395		C3R	C3C	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	4	1,942	3,062	F	Over Capacity		
** 13130000	US 41	TAMIAMI TRAIL	SR 43/US 301	4.056	US 19/SR 55 SB/45th St Ct W	5.819	1.763		C3R	C3R	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	OR	4	1,850	1,933	F	Over Capacity		
13130000	US 19 NB	US 19	US 19 SB/SR 55/45th St Ct W	5.819	US 19 SB	6.500	0.681		C3R	C3C	Principal Arterial-other	55	UA	A	D	D	D	U	1W	0L	OR	2	2,220	989	C			

Note: LOS Spreadsheet should be used as a planning level analysis tool. A detailed analysis is necessary to validate the actual operating conditions (and for future conditions, as needed) which may vary from this worksheet.

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR PEAK DIRECTION**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	Year 2028 (E+C)						Year 2045						Standard K Factor	Existing D Factor	Existing T <sub>24</sub> Factor
								Thru Lanes	LOS Std	Peak Hour Peak Direction			Thru Lanes	LOS Std	Peak Hour Peak Direction							
										Capacity	Volume	LOS			Capacity	Trend Volume	Trend LOS	Model Volume	Model LOS			
13050000	SR 64	MANATEE AVE	17th St E	1.808	E. of 39th St E	3.742	1.934	6	D	2,866	2,703	C	6	D	2,950	3,420	F	2,935	D	9.00%	56.10%	4.70%
13050000	SR 64	MANATEE AVE	E. of 39th St E	3.742	57th St E	5.188	1.446	6	D	2,814	2,703	D	6	D	2,814	3,420	F	2,875	F	9.00%	56.10%	4.70%
** 13050000	SR 64	SR 64	57th St E	5.188	I-75	6.360	1.172	6	D	2,814	3,297	F	6	D	2,814	4,361	F	2,702	D	9.00%	56.10%	5.70%
13050000	SR 64	SR 64	I-75	6.360	Lena Rd/Heritage Green Way	7.389	1.029	6	D	4,370	2,826	C	6	D	2,814	4,024	F	2,492	D	9.00%	56.10%	5.60%
** 13050000	SR 64	SR 64	Lena Rd/Heritage Green Way	7.389	Upper Manatee River Rd/Lakewood	8.804	1.415	6	D	4,370	3,173	C	6	D	4,370	5,009	E	2,063	B	9.00%	56.10%	5.20%
13050000	SR 64	SR 64	Upper Manatee River Rd/Lakewood	8.804	Rye Rd	10.577	1.773	4	D	2,910	2,010	C	4	D	1,942	3,222	F	1,670	C	9.00%	56.10%	7.60%
13050000	SR 64	SR 64	Rye Rd	10.577	CR 675	16.835	6.258	2	C	430	412	C	2	C	1,018	532	C	1,119	D	9.43%	63.32%	14.36%
** 13050000	SR 70	SR 70	CR 675	22.134	Singletary Rd/Myakka Wauchula R	31.713	9.579	2	C	430	486	D	2	C	430	632	D	234	B	9.50%	56.50%	21.40%
** 13050000	SR 70	SR 70	Singletary Rd/Myakka Wauchula R	31.713	DeSoto County Line	39.483	7.770	2	C	430	402	C	2	C	430	585	D	115	B	9.50%	56.50%	26.30%
13050002	SR 64	MANATEE AVE EB/6TH AV	10th St E	0.000	17th St E	0.576	0.576	3	D	3,276	1,826	C	3	D	3,372	2,123	C	2,624	C	9.00%	99.90%	3.95%
13050101	SR 64	MANATEE AVE	12th St E	0.189	10th St W	1.510	1.321	3	D	3,541	1,915	C	3	D	3,541	2,223	C	2,836	D	9.00%	99.90%	3.75%
13050102	SR 64	MANATEE AVE E	17th St E	0.000	12th St E	0.372	0.372	3	D	3,276	1,909	C	3	D	3,372	2,215	C	2,575	C	9.00%	99.90%	5.09%
13060000	SR 62	SR 62	SR 43/US 301	0.000	Corbett John Rd	3.890	3.890	2	C	430	261	C	2	C	430	371	C	441	D	9.50%	56.10%	23.10%
13060000	SR 62	SR 62	Corbett John Rd	3.890	CR 39 /County Line Rd	17.154	13.264	2	C	430	261	C	2	C	430	371	C	401	C	9.50%	56.10%	23.10%
13060000	SR 62	SR 62	CR 39 /County Line Rd	17.154	SR 37	19.264	2.110	2	C	430	361	C	2	C	430	564	D	466	D	9.50%	56.10%	32.90%
13060000	SR 62	SR 62	SR 37	19.264	Hardee County Line	23.240	3.976	2	C	430	319	C	2	C	430	455	D	395	C	9.50%	56.10%	27.80%
13070000	SR 37	SR 37	SR 62	0.000	Polk County Line	5.534	5.534	2	C	430	178	B	2	C	430	277	C	172	B	9.50%	56.10%	32.00%
** 13075000	I-75	SR 93/I-75	Sarasota County Line	0.000	SR 70	3.719	3.719	8	D	8,090	7,885	D	10	D	10,231	10,588	E	6,775	C	9.00%	55.80%	12.40%
** 13075000	I-75	SR 93/I-75	SR 70	3.719	SR 64/Manatee Ave	7.330	3.611	8	D	8,090	7,504	D	10	D	10,231	10,118	D	6,185	C	9.00%	55.80%	12.40%
** 13075000	I-75	SR 93/I-75	SR 64/Manatee Ave	7.330	19th St/US 301/SR 43	11.014	3.684	6	D	6,080	6,757	F	10	D	10,231	8,994	D	6,119	B	9.00%	55.80%	12.70%
** 13075000	I-75	SR 93/I-75	19th St/US 301/SR 43	11.014	I-275 off Ramp	14.548	3.534	6	D	6,080	6,470	F	10	D	10,231	8,677	D	5,129	B	9.00%	55.80%	14.70%
** 13075000	I-75	SR 93/I-75	I-275 off Ramp	14.548	I-275 on Ramp	15.300	0.752	6	D	6,080	3,911	C	10	D	10,231	5,519	B	2,237	B	9.00%	54.70%	17.70%
** 13075000	I-75	SR 93/I-75	I-275 on Ramp	15.300	CR 6/Mocassin Wallow Rd	16.200	0.900	6	D	7,080	5,584	C	10	D	10,231	8,078	C	3,435	B	9.00%	55.80%	15.50%
** 13075000	I-75	SR 93/I-75	CR 6/Mocassin Wallow Rd	16.200	Hillsborough County Line	20.571	4.371	6	C	3,990	5,798	F	10	C	6,926	8,316	E	3,914	B	10.50%	52.80%	11.10%
13080000	SR 789	GULF OF MEXICO DR	Sarasota County Line	0.000	Binnacle Pointe Dr	2.410	2.410	2	D	1,166	574	C	2	D	914	812	D	814	D	9.00%	57.20%	5.00%
13080000	SR 789	GULF OF MEXICO DR	Binnacle Pointe Dr	2.410	Coquina Park Ent.	5.177	2.767	2	D	1,166	574	C	2	D	914	812	D	801	D	9.00%	57.20%	5.00%
13080000	SR 789	GULF DR S	Coquina Park Ent.	5.177	SR 684 (Cortez Rd)	6.666	1.489	2	D	914	698	D	2	D	914	812	D	979	F	9.00%	54.90%	3.90%
13080000	SR 789	GULF DR N/EAST BAY DR	SR 684 (Cortez Rd)	6.666	SR 64/Manatee Ave	8.717	2.051	2	D	914	545	D	2	D	914	678	D	1,155	F	9.00%	57.20%	3.90%
13120001	SR 70	15 ST E	301 Blvd	0.000	15th St E	0.058	0.058	1	D	1,079	414	C	1	D	1,079	477	C	758	C	9.00%	99.90%	8.50%
13120001	SR 70	15 ST E	15th St E	0.058	SR 64/Manatee Ave	3.030	2.972	2	D	1,124	593	C	2	D	1,124	687	C	687	C	9.00%	56.10%	6.96%
** 13121000	US 301	SR683/US301/15ST BLVD	University Pkwy	0.000	63rd Ave E/Saunders Rd	2.811	2.811	4	D	4,070	2,084	B	4	D	4,070	2,520	C	1,701	B	9.00%	54.80%	7.30%
** 13121000	US 301	SR683/US301/15ST BLVD	63rd Ave E/Saunders Rd	2.811	SR 70/Oneco Rd/53rd Ave E	4.060	1.249	4	D	4,070	2,218	B	4	D	4,070	2,752	C	1,574	B	9.00%	54.80%	12.70%
13121000	US 301	SR683/US301/15ST BLVD	SR 70/Oneco Rd/53rd Ave E	4.060	38th Ave E	5.332	1.272	4	D	4,070	1,911	B	4	D	4,070	2,277	B	1,855	B	9.00%	54.80%	8.70%
13121000	US 301	SR683/US301/15ST BLVD	38th Ave E	5.332	SR 70/15th St E	6.487	1.155	4	D	1,942	1,742	C	4	D	1,942	2,267	F	1,642	C	9.00%	54.80%	10.40%
13121000	US 301	SR683/US301/15ST BLVD	SR 70/15th St E	6.487	0.104 mi. S. of 13th Ave E	7.908	1.421	4	D	1,900	1,594	C	4	D	1,900	1,901	E	1,729	D	9.00%	54.80%	9.10%
13130000	US 41	1ST ST	41st Ave E	0.115	21st Ave W	1.258	1.143	6	D	2,866	1,943	C	6	D	2,814	2,252	C	2,166	C	9.00%	54.80%	4.30%
13130000	US 41	1ST ST/TAMIAMI TRAIL	0.104 mi. S. of 13th Ave E	1.661	SR 43/US 301	4.056	2.395	4	D	1,942	3,302	F	4	D	1,900	3,956	F	3,734	F	9.00%	54.80%	7.00%
** 13130000	US 41	TAMIAMI TRAIL	SR 43/US 301	4.056	US 19/SR 55 SB/45th St Ct W	5.819	1.763	4	D	1,850	2,115	F	4	D	1,850	2,616	F	1,853	F	9.00%	54.80%	6.96%
13130000	US 19 NB	US 19	US 19 SB/SR 55/45th St Ct W	5.819	US 19 SB	6.500	0.681	2	D	2,220	1,071	C	2	D	2,172	1,305	C	2,765	F	9.00%	99.90%	6.90%

Note: LOS Spreadsheet should be used as a planning level analysis tool. A detailed analysis is necessary to validate the actual operating conditions (and for future conditions, as needed) which may vary from this worksheet.

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR PEAK DIRECTION**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	SIS	Existing Context Class	Future Context Class	Functional Classification	Posted Speed	Area Type	Facility Type	FDOT LOS Std.	County LOS Std.	City LOS Std.	Year 2022										Deficiency Determination
																		Divided/UnDivided	One/Two Way	Left Turn Bays	Right Turn Bays	Thru Lanes	Peak Hour Peak Direction					
																							Capacity	Volume	LOS			
13130000	US 19	US 19	US 41	6.500	Terra Ceia Rd	8.466	1.966		C3R	C3C	Principal Arterial-other	55	RDA	H	C	D		D	2W	WL	WR	4	1,785	1,159	C			
13130000	US 19 NB	US 19	Terra Ceia Rd	8.466	I-275 Ramp	9.194	0.728		LA	LA	Principal Arterial-other	55	RDA	H	C	C		U	1W		0A	2	1,662	1,091	B			
13130000	US 19	US 19	I-275 Ramp	9.194	Skyway Br. Hillsborough County Li	11.982	2.788	SIS	LA	LA	Principal Arterial-Interstate	65	RDA	F	C	C		D	2W	0L	0A	4	2,770	4,049	F	Over Capacity		
13130101	US 19 SB	1ST ST	US 19 NB/SR 55/45th St Ct W	0.000	US 41/SR 45	0.654	0.654		C3C	C3C	Principal Arterial-other	45	UA	H	D	D		U	1W	WL	0R	2	2,172	1,682	C			
13130401	US 41	SR 55/US 19	21st Ave	0.000	Railroad Crossing	0.403	0.403		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		U	1W	0L	WR	2	2,281	1,798	C			
13130403	US 41	1ST ST	Railroad Crossing	0.000	21st Ave W	0.403	0.403		C3C	C4	Principal Arterial-other	45	UA	A	D	D		U	1W	WL	0R	2	2,172	1,618	C			
13140000	SR 64	BRADENTON-ARCADIA RD	CR 675	0.000	Wauchula Rd	13.733	13.733		C2	C2	Principal Arterial-other	60	RDA	H	C	C		U	2W	WL	WR	2	430	309	C			
13140000	SR 64	BRADENTON-ARCADIA RD	Wauchula Rd	13.733	Hardee County Line	16.071	2.338		C2	C2	Principal Arterial-other	60	RDA	H	C	C		U	2W	WL	0R	2	430	224	B			
13150000	SR 64	MANATEE AVE	SR 789 /Gulf Drive	0.000	Flamingo Dr	2.882	2.882		C3C	C3R	Principal Arterial-other	45	UA	A	D	D	D	U	2W	WL	WR	2	1,124	906	D			
13150000	SR 64	MANATEE AVE	Flamingo Dr	2.882	75th St W	4.572	1.690		C4	C3R	Principal Arterial-other	45	UA	A	D	D	D	U	2W	WL	WR	2	914	927	F	Over Capacity		
13150000	SR 64	MANATEE AVE	75th St W	4.572	51st St W	6.074	1.502		C4	C4	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	1,880	1,616	D			
13150000	SR 64	MANATEE AVE	51st St W	6.074	SR 64/Manatee Ave	8.305	2.231		C4	C4	Principal Arterial-other	40	UA	A	D	D	D	D	2W	WL	WR	4	1,880	2,091	E	Over Capacity		
13150000	SR 64	15TH ST W/6TH AVE W	SR 64/Manatee Ave	8.305	10th St W	8.613	0.308		C5	C5	Principal Arterial-other	30	UA	A	D	D	D	U	1W	0L	WR	3	3,364	1,888	D			
13150101	SR 64	MANATEE AVE WB	10th St	0.000	15th St	0.235	0.235		C5	C5	Principal Arterial-other	30	UA	A	D	D	D	U	1W	WL	0R	2	2,280	2,203	D	Near Capacity		
13160000	SR 70	53RD AVE/ONECO RD	301 Blvd/15th St E	0.000	US 301/SR 683/15th St Blvd	1.001	1.001		C3C	C3C	Principal Arterial-other	40	UA	A	D	D		D	2W	WL	WR	6	2,814	1,717	C			
13160000	SR 70	53RD AVE/ONECO RD/SR 70	US 301/SR 683/15th St Blvd	1.001	Lockwood Ridge Rd/45th St E	2.538	1.537		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	2,814	2,339	C			
13160000	SR 70	SR 70	Lockwood Ridge Rd/45th St E	2.538	Fairway Gardens Dr/68th St E	4.388	1.850		C3C	C3C	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	6	2,814	2,423	C			
13160000	SR 70	SR 70	Fairway Gardens Dr/68th St E	4.388	I-75	5.438	1.050		C3C	C3C	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	6	2,814	3,712	F	Over Capacity		
13160000	SR 70	SR 70	I-75	5.438	Lakewood Ranch Blvd	7.448	2.010	SIS	C3R	C3R	Principal Arterial-other	50	UA	A	D	C		D	2W	WL	WR	6	2,866	2,797	D	Near Capacity		
13160000	SR 70	SR 70	Lakewood Ranch Blvd	7.448	Lorraine Rd	9.464	2.016	SIS	C3R	C3R	Principal Arterial-other	50	UA	A	D	C		D	2W	WL	WR	6	2,866	1,754	C			
13160000	SR 70	SR 70	Lorraine Rd	9.464	Bournside Blvd	12.328	2.864	SIS	C3R	C3R	Principal Arterial-other	60	RDA	H	C	C		D	2W	WL	WR	2	1,069	1,007	C	Near Capacity		
13160000	SR 70	SR 70	Bournside Blvd	12.328	CR 675 (Waterbury Rd)	15.567	3.239	SIS	C3R	C3R	Principal Arterial-other	60	RDA	H	C	C		D	2W	WL	WR	2	1,069	1,007	C	Near Capacity		
13162000	SR 70	53RD AVE/ONECO RD	US41/SR 45/14th St	0.000	15th St E/301 Blvd	1.761	1.761		C3R	C3R	Principal Arterial-other	40	UA	A	D	D		U	2W	WL	WR	4	1,845	1,590	C			
13175000	I-275	SR 93/I-275	Erie Rd/CR 10/69th St	0.000	I-75	0.690	0.690	SIS Link	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C		U	1W		0A	2	2,035	1,528	C			
13175000	I-275	SR 93/I-275	I-75	0.690	E. of 36th Ave E/CR 683/Ellenton C	1.344	0.654	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C		U	1W		0A	2	2,035	1,528	C			
13175000	I-275	SR 93/I-275	E. of 36th Ave E/CR 683/Ellenton C	1.344	US 41/SR 45	2.279	0.935	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	C		D	2W		W1A	4	4,570	3,028	C			
13175000	I-275	SR 93/I-275	US 41/SR 45	2.279	US 19 NB Ramp	5.233	2.954	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C		D	2W		0A	4	2,770	2,314	C			
13000012	Piney Point Road	Piney Point Road	SR 45/US 41	0.000	Harlee Rd/North Dock St	0.583	0.583	SC	C3C	C3C	Major Collector	30	UA	A	D	D		U	2W	WL	WR	2	1,124	192	C			

**Legend**

Aggregated segment
  Single count segment
  OFF System SIS Facility

\*\* Future Model Volumes obtained from D1 Regional Planning Model are found to be less than Existing AADT.

**YEAR 2022 MANATEE COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR PEAK DIRECTION**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	Year 2028 (E+C)					Year 2045					Standard K Factor	Existing D Factor	Existing T <sub>24</sub> Factor		
								Thru Lanes	LOS Std	Peak Hour Peak Direction			Thru Lanes	LOS Std	Peak Hour Peak Direction							
										Capacity	Volume	LOS			Capacity	Trend Volume	Trend LOS				Model Volume	Model LOS
13130000	US 19	US 19	US 41	6.500	Terra Ceia Rd	8.466	1.966	4	C	1,785	1,247	C	4	C	1,596	1,490	C	1,359	C	9.00%	54.80%	6.40%
13130000	US 19 NB	US 19	Terra Ceia Rd	8.466	I-275 Ramp	9.194	0.728	2	C	1,662	1,235	C	2	C	1,662	1,653	C	2,510	F	9.50%	99.90%	7.00%
13130000	US 19	US 19	I-275 Ramp	9.194	Skyway Br. Hillsborough County Li	11.982	2.788	4	C	2,770	4,487	F	4	C	2,770	5,660	F	4,898	F	10.50%	54.70%	7.80%
13130101	US 19 SB	1ST ST	US 19 NB/SR 55/45th St Ct W	0.000	US 41/SR 45	0.654	0.654	2	D	2,172	1,840	D	2	D	2,172	2,294	F	2,347	F	9.00%	99.90%	7.63%
13130401	US 41	SR 55/US 19	21st Ave	0.000	Railroad Crossing	0.403	0.403	2	D	2,281	1,908	C	2	D	2,281	2,214	D	2,211	D	9.00%	99.90%	4.80%
13130403	US 41	1ST ST	Railroad Crossing	0.000	21st Ave W	0.403	0.403	2	D	2,172	1,719	C	2	D	2,148	1,989	D	2,017	D	9.00%	99.90%	5.10%
13140000	SR 64	BRADENTON-ARCADIA RD	CR 675	0.000	Wauchula Rd	13.733	13.733	2	C	430	324	C	2	C	430	376	C	234	B	9.50%	56.10%	20.60%
13140000	SR 64	BRADENTON-ARCADIA RD	Wauchula Rd	13.733	Hardee County Line	16.071	2.338	2	C	430	256	C	2	C	430	366	C	115	B	9.50%	56.10%	22.70%
13150000	SR 64	MANATEE AVE	SR 789 /Gulf Drive	0.000	Flamingo Dr	2.882	2.882	2	D	1,124	936	D	2	D	1,166	1,114	D	1,166	E	9.00%	57.20%	4.60%
13150000	SR 64	MANATEE AVE	Flamingo Dr	2.882	75th St W	4.572	1.690	2	D	914	945	F	2	D	1,166	1,094	D	1,339	F	9.00%	57.20%	4.70%
13150000	SR 64	MANATEE AVE	75th St W	4.572	51st St W	6.074	1.502	4	D	1,880	1,777	D	4	D	1,880	2,317	F	2,263	F	9.00%	56.10%	3.80%
13150000	SR 64	MANATEE AVE	51st St W	6.074	SR 64/Manatee Ave	8.305	2.231	4	D	1,880	2,174	F	4	D	1,880	2,523	F	2,541	F	9.00%	56.10%	3.55%
13150000	SR 64	15TH ST W/6TH AVE W	SR 64/Manatee Ave	8.305	10th St W	8.613	0.308	3	D	3,364	2,007	D	3	D	3,364	2,322	D	2,853	D	9.00%	99.90%	3.10%
13150101	SR 64	MANATEE AVE WB	10th St	0.000	15th St	0.235	0.235	2	D	2,280	2,340	E	2	D	2,280	2,709	F	2,942	F	9.00%	99.90%	4.30%
13160000	SR 70	53RD AVE/ONECO RD	301 Blvd/15th St E	0.000	US 301/SR 683/15th St Blvd	1.001	1.001	6	D	2,814	1,782	C	6	D	2,814	2,069	C	2,398	C	9.00%	56.10%	5.00%
13160000	SR 70	53RD AVE/ONECO RD/SR 70	US 301/SR 683/15th St Blvd	1.001	Lockwood Ridge Rd/45th St E	2.538	1.537	6	D	2,814	2,416	C	6	D	2,814	2,802	D	3,190	F	9.00%	56.50%	7.00%
13160000	SR 70	SR 70	Lockwood Ridge Rd/45th St E	2.538	Fairway Gardens Dr/68th St E	4.388	1.850	6	D	2,814	2,861	F	6	D	2,814	3,559	F	2,992	F	9.00%	51.00%	8.00%
13160000	SR 70	SR 70	Fairway Gardens Dr/68th St E	4.388	I-75	5.438	1.050	6	D	2,814	3,831	F	6	D	2,814	4,445	F	3,455	F	9.00%	56.50%	5.90%
13160000	SR 70	SR 70	I-75	5.438	Lakewood Ranch Blvd	7.448	2.010	6	D	2,866	2,990	F	6	D	2,866	3,747	F	3,417	F	9.00%	56.50%	5.10%
13160000	SR 70	SR 70	Lakewood Ranch Blvd	7.448	Lorraine Rd	9.464	2.016	6	D	2,866	2,044	C	6	D	2,866	2,995	F	2,100	C	9.00%	56.50%	5.80%
13160000	SR 70	SR 70	Lorraine Rd	9.464	Bournside Blvd	12.328	2.864	4	C	1,785	1,213	C	4	C	1,785	1,881	D	914	C	9.00%	56.50%	10.30%
13160000	SR 70	SR 70	Bournside Blvd	12.328	CR 675 (Waterbury Rd)	15.567	3.239	4	C	1,785	1,213	C	4	C	1,785	1,881	D	1,044	C	9.00%	56.50%	10.30%
13162000	SR 70	53RD AVE/ONECO RD	US41/SR 45/14th St	0.000	15th St E/301 Blvd	1.761	1.761	4	D	1,845	1,653	C	4	D	1,845	1,916	F	1,448	C	9.00%	56.10%	3.40%
13175000	I-275	SR 93/I-275	Erie Rd/CR 10/69th St	0.000	I-75	0.690	0.690	2	D	2,035	1,629	C	2	D	2,035	1,908	D	2,879	F	9.00%	99.90%	7.30%
13175000	I-275	SR 93/I-275	I-75	0.690	E. of 36th Ave E/CR 683/Ellenton C	1.344	0.654	2	D	2,035	1,629	C	2	D	2,035	1,908	D	2,203	F	9.00%	99.90%	7.30%
13175000	I-275	SR 93/I-275	E. of 36th Ave E/CR 683/Ellenton C	1.344	US 41/SR 45	2.279	0.935	4	D	4,570	3,460	C	4	D	4,570	4,638	E	2,154	B	9.00%	54.70%	9.80%
13175000	I-275	SR 93/I-275	US 41/SR 45	2.279	US 19 NB Ramp	5.233	2.954	4	C	2,770	2,589	C	4	C	2,770	3,331	E	2,890	D	9.00%	54.70%	6.50%
13000012	Piney Point Road	Piney Point Road	SR 45/US 41	0.000	Harlee Rd/North Dock St	0.583	0.583	2	D	1,124	262	C	2	D	1,124	465	C	185	C	9.00%	56.10%	47.20%

**Legend**

Aggregated segment
  Single count segment
  OFF System SIS Facility

\*\* Future Model Volumes obtained from D1 Regional Planning Model are found to be less than Existing AADT.

## Sarasota County

**YEAR 2022 SARASOTA COUNTY LEVEL OF SERVICE SPREADSHEET -DAILY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	SIS	Existing Context Class	Future Context Class	Functional Classification	Posted Speed	Area Type	Facility Type	FDOT LOS Std.	County LOS Std.	City LOS Std.	Year 2022										Deficiency Determination
																		Divided/UnDivided	One/Two Way	Left Turn Bays	Right Turn Bays	Thru Lanes	Daily					
																							Capacity	Volume	LOS			
17005000	SR 681	VENICE CONNECTOR	SR 45/US41	0.000	US 41 Ramp	0.327	0.327		C2	C2	Principal Arterial-other	45	UA	H	D	D		U	1W	0L	0R	2	33,420	5,800	B			
17005000	SR 681	VENICE CONNECTOR	US 41 Ramp	0.327	I-75 Ramp	3.158	2.831		C2	C2	Principal Arterial-other	55	UA	H	D	D		D	2W	WL	WR	4	55,700	15,400	B			
17005000	SR 681	VENICE CONNECTOR	I-75 Ramp	3.158	I-75/SR 93	4.142	0.984	SIS Link	C2	C2	Principal Arterial-other	70	RDA	H	C	C		U	1W	0L	0R	2	27,480	16,000	B			
17008000	SR 758	BEE RIDGE ROAD	SR 45/US 41	0.000	Beneva Rd	2.026	2.026		C4	C4	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	59,640	32,522	C			
17008000	SR 758	BEE RIDGE ROAD	Beneva Rd	2.026	McIntosh Rd	3.044	1.018		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	0R	6	54,100	41,000	C			
17008000	SR 758	BEE RIDGE ROAD	McIntosh Rd	3.044	Honore Ave	4.056	1.012		C3R	C3R	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	4	39,165	38,500	D	Near Capacity		
17008000	SR 758	BEE RIDGE ROAD	Honore Ave	4.056	I-75 Ramp	5.066	1.010		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	4	38,430	37,500	D	Near Capacity		
17008000	SR 758	BEE RIDGE ROAD	I-75 Ramp	5.066	R/W Line	5.432	0.366		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	0L	WR	4	38,430	29,000	C			
17010000	US 41	TAMIAMI TRAIL	Charlotte County Line	0.000	100 Ft E. of Tuscola Blvd	1.205	1.205		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	56,805	37,500	C			
17010000	US 41	TAMIAMI TRAIL	100 Ft E. of Tuscola Blvd	1.205	SR 777/River Rd	6.138	4.933		C3R	C3R	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	39,165	37,500	D	Near Capacity		
17010000	US 41	TAMIAMI TRAIL	SR 777/River Rd	6.138	Rockley Blvd	9.878	3.740		C3R	C3R	Principal Arterial-other	55	TA	A	C	C	D	D	2W	WL	WR	4	36,015	22,000	C			
17010000	US 41	TAMIAMI TRAIL	Rockley Blvd	9.878	SR 776/Park Pl	12.826	2.948		C3R	C3R	Principal Arterial-other	55	UA	A	D	D		D	2W	WL	WR	4	39,165	31,231	C			
17010000	US 41	TAMIAMI TRAIL	SR 776/Park Pl	12.826	SR 45A/Venice Bypass	14.882	2.056		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	56,805	51,500	D	Near Capacity		
17010000	USB 41	TAMIAMI TRAIL	SR 45A/Venice Bypass	14.882	CR 772/Venice Ave	17.155	2.273		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	38,430	31,000	C			
17020000	USB 41	US-41/TAMIAMI TRAIL	CR 772/Venice Ave	0.000	SR 45A/US 41	0.638	0.638		C3R	C3R	Principal Arterial-other	40	UA	A	D	D	D	D	2W	WL	WR	4	39,165	18,800	C			
17020000	US 41	US-41/TAMIAMI TRAIL	SR 45A/US 41	0.638	Laurel Rd	2.894	2.256		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	56,805	46,000	C			
17020000	US 41	US-41/TAMIAMI TRAIL	Laurel Rd	2.894	SR 681 Ramp	3.421	0.527		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	56,805	50,000	C			
17020000	US 41	TAMIAMI TRAIL	SR 681 Ramp	3.421	CR 789/Old Venice Rd	6.134	2.713		C3C	C3C	Principal Arterial-other	55	UA	A	D	D		D	2W	WL	WR	4	38,430	35,000	D	Near Capacity		
17020000	US 41	US-41/TAMIAMI TRAIL	CR 789/Old Venice Rd	6.134	CR 773/Beneva Rd	10.029	3.895		C3C	C3C	Principal Arterial-other	55	UA	A	D	D		D	2W	WL	WR	4	38,430	42,500	F	Over Capacity		
17020000	US 41	US-41/TAMIAMI TRAIL	CR 773/Beneva Rd	10.029	Buccaneer Dr	11.716	1.687		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	4	38,430	33,275	D			
**	17020000	US 41	US-41/TAMIAMI TRAIL	Buccaneer Dr	11.716	CR 72 A/Proctor Rd	14.299	2.583		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	56,805	44,549	C		
**	17020000	US 41	US-41/TAMIAMI TRAIL	CR 72 A/Proctor Rd	14.299	Webber St	15.930	1.631		C4	C4	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	59,640	48,300	D		
**	17020000	US 41	US-41/TAMIAMI TRAIL	Webber St	15.930	SR 683/US 301	17.371	1.441		C4	C4	Principal Arterial-other	40	UA	A	D	D	D	D	2W	WL	WR	6	59,640	58,000	D	Near Capacity	
17020000	US 41	MOUND ST	SR 683/US 301	17.371	SR 789/Gulf Stream Ave	18.642	1.271		C4	C4	Principal Arterial-other	35	UA	A	D	D	D	D	2W	WL	WR	4	37,905	21,015	C			
17020000	US 41	N TAMIAMI TRAIL	SR 789/Gulf Stream Ave	18.642	University Pkwy	21.879	3.237		C4	C4	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	37,905	32,289	D			
17020000	US 41	N TAMIAMI TRAIL	University Pkwy	21.879	Manatee County Line	22.39	0.511		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	5	47,618	39,000	C			
17030000	SR 789	RINGLING BLVD/ST.ARMANDS CIR	SR 45/Tamiami Trail	0.000	Madison Dr	2.368	2.368		C3R	C3R	Minor Arterial	35	UA	A	D	D	D	D	2W	WL	WR	4	39,165	33,548	C			
17030000	SR 789	JOHN-RINGLING PKWY	Madison Dr	2.368	Harborside Dr	5.987	3.619		C3R	C3R	Major Collector	45	UA	A	D	D	E	D	2W	WL	WR	2	24,696	17,752	C			
17030000	SR 789	GULF OF MEXICO DR	Harborside Dr	5.987	Manatee County Line	8.957	2.970		C3R	C3R	Major Collector	45	UA	A	D	D	E	D	2W	WL	WR	2	24,696	13,100	C			
17030201	SR 789	GULF STREAM AVE	Sunset Dr	0.000	Bird Key Dr	1.006	1.006		C3R	C3R	Minor Arterial	40	UA	A	D	D	D	D	2W	WL	0R	4	37,300	37,000	D	Near Capacity		
17030401	SR 789	JOHN RINGLING PKWY	Westway Pl	0.000	Long Boat Club Rd	0.648	0.648		C3R	C3R	Major Collector	35	UA	A	D	D	D	D	2W	WL	WR	2	24,696	19,600	C			
17040000	SR 780	FRUITVILLE RD/SR 780	School Ave	0.381	CR 773/ Beneva Rd	2.134	1.753		C4	C4	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	59,640	46,412	C			
17040000	SR 780	FRUITVILLE RD/SR 780	CR 773/ Beneva Rd	2.134	Honore Ave	4.203	2.069		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	56,805	51,500	D	Near Capacity		
17040000	SR 780	FRUITVILLE RD/SR 780	Honore Ave	4.203	State R/W Line	5.692	1.489		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	56,805	59,500	F	Over Capacity		

**YEAR 2022 SARASOTA COUNTY LEVEL OF SERVICE SPREADSHEET -DAILY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	Year 2028 (E+C)					Year 2045					Existing K Factor	Existing D Factor	Existing T <sub>24</sub> Factor			
								Thru Lanes	LOS Std	Daily			Thru Lanes	LOS Std	Daily								
										Capacity	Volume	LOS			Capacity	Trend Volume	Trend LOS				Model Volume	Model LOS	
17005000	SR 681	VENICE CONNECTOR	SR 45/US41	0.000	US 41 Ramp	0.327	0.327	2	D	33,420	6,100	B	2	D	33,420	7,100	B	11,178	B	9.00%	99.90%	9.30%	
17005000	SR 681	VENICE CONNECTOR	US 41 Ramp	0.327	I-75 Ramp	3.158	2.831	4	D	55,700	17,500	B	4	D	55,700	23,500	B	31,073	B	9.00%	52.90%	9.30%	
17005000	SR 681	VENICE CONNECTOR	I-75 Ramp	3.158	I-75/SR 93	4.142	0.984	2	C	27,480	21,600	C	2	C	27,480	37,700	E	17,252	B	9.50%	99.90%	9.30%	
17008000	SR 758	BEE RIDGE ROAD	SR 45/US 41	0.000	Beneva Rd	2.026	2.026	6	D	59,640	34,474	C	6	D	59,640	40,002	C	48,300	D	9.00%	52.90%	7.30%	
17008000	SR 758	BEE RIDGE ROAD	Beneva Rd	2.026	McIntosh Rd	3.044	1.018	6	D	54,100	43,500	C	6	D	54,100	50,400	D	53,717	D	9.00%	52.90%	7.30%	
17008000	SR 758	BEE RIDGE ROAD	McIntosh Rd	3.044	Honore Ave	4.056	1.012	4	D	39,165	40,800	F	4	D	39,165	47,400	F	49,212	F	9.00%	52.90%	7.30%	
17008000	SR 758	BEE RIDGE ROAD	Honore Ave	4.056	I-75 Ramp	5.066	1.010	4	D	38,430	39,800	F	4	D	38,430	46,100	F	57,056	F	9.00%	52.90%	4.80%	
17008000	SR 758	BEE RIDGE ROAD	I-75 Ramp	5.066	R/W Line	5.432	0.366	4	D	38,430	30,700	C	4	D	38,430	35,700	D	63,179	F	9.00%	52.90%	5.00%	
17010000	US 41	TAMIAMI TRAIL	Charlotte County Line	0.000	100 Ft E. of Tuscola Blvd	1.205	1.205	6	D	56,805	41,100	C	6	D	56,805	51,400	D	49,959	C	9.00%	52.80%	4.60%	
17010000	US 41	TAMIAMI TRAIL	100 Ft E. of Tuscola Blvd	1.205	SR 777/River Rd	6.138	4.933	4	D	39,165	41,100	F	4	D	39,165	51,400	F	51,929	F	9.00%	52.80%	4.60%	
17010000	US 41	TAMIAMI TRAIL	SR 777/River Rd	6.138	Rockley Blvd	9.878	3.740	4	C	36,015	24,000	C	4	C	36,015	29,600	C	49,299	F	9.50%	52.80%	4.60%	
17010000	US 41	TAMIAMI TRAIL	Rockley Blvd	9.878	SR 776/Park Pl	12.826	2.948	4	D	39,165	33,250	C	4	D	39,165	39,050	D	43,850	F	9.00%	52.80%	3.33%	
17010000	US 41	TAMIAMI TRAIL	SR 776/Park Pl	12.826	SR 45A/Venice Bypass	14.882	2.056	6	D	56,805	54,600	D	6	D	56,805	63,300	F	74,387	F	9.00%	52.80%	2.80%	
17010000	USB 41	TAMIAMI TRAIL	SR 45A/Venice Bypass	14.882	CR 772/Venice Ave	17.155	2.273	4	D	38,430	35,800	D	4	D	38,430	49,200	F	33,076	D	9.00%	52.80%	2.40%	
17020000	USB 41	US-41/TAMIAMI TRAIL	CR 772/Venice Ave	0.000	SR 45A/US 41	0.638	0.638	4	D	39,165	19,900	C	4	D	39,165	23,100	C	24,257	C	9.00%	52.80%	2.60%	
17020000	US 41	US-41/TAMIAMI TRAIL	SR 45A/US 41	0.638	Laurel Rd	2.894	2.256	6	D	56,805	48,800	C	6	D	56,805	56,600	D	60,156	F	9.00%	52.80%	3.50%	
17020000	US 41	US-41/TAMIAMI TRAIL	Laurel Rd	2.894	SR 681 Ramp	3.421	0.527	6	D	56,805	53,000	D	6	D	56,805	61,500	F	64,297	F	9.00%	52.80%	4.50%	
17020000	US 41	TAMIAMI TRAIL	SR 681 Ramp	3.421	CR 789/Old Venice Rd	6.134	2.713	4	D	38,430	37,100	D	4	D	38,430	43,100	F	46,151	F	9.00%	52.80%	4.90%	
17020000	US 41	US-41/TAMIAMI TRAIL	CR 789/Old Venice Rd	6.134	CR 773/Beneva Rd	10.029	3.895	4	D	38,430	45,100	F	4	D	38,430	52,300	F	50,415	F	9.00%	52.80%	3.40%	
17020000	US 41	US-41/TAMIAMI TRAIL	CR 773/Beneva Rd	10.029	Buccaneer Dr	11.716	1.687	4	D	38,430	35,300	D	4	D	38,430	40,900	F	50,206	F	9.00%	52.80%	3.20%	
**	17020000	US 41	US-41/TAMIAMI TRAIL	Buccaneer Dr	11.716	CR 72 A/Proctor Rd	14.299	2.583	6	D	56,805	47,226	C	6	D	56,805	54,836	D	44,216	C	9.00%	52.80%	3.11%
**	17020000	US 41	US-41/TAMIAMI TRAIL	CR 72 A/Proctor Rd	14.299	Webber St	15.930	1.631	6	D	59,640	51,200	D	6	D	59,640	59,421	D	45,954	C	9.00%	52.80%	2.80%
**	17020000	US 41	US-41/TAMIAMI TRAIL	Webber St	15.930	SR 683/US 301	17.371	1.441	6	D	59,640	61,500	E	6	D	59,640	71,300	F	49,239	D	9.00%	52.80%	2.80%
17020000	US 41	MOUND ST	SR 683/US 301	17.371	SR 789/Gulf Stream Ave	18.642	1.271	4	D	37,905	22,261	C	4	D	37,905	25,866	D	32,886	D	9.00%	52.80%	2.83%	
17020000	US 41	N TAMIAMI TRAIL	SR 789/Gulf Stream Ave	18.642	University Pkwy	21.879	3.237	4	D	37,905	34,252	D	4	D	37,905	39,695	E	41,710	E	9.00%	52.80%	2.99%	
17020000	US 41	N TAMIAMI TRAIL	University Pkwy	21.879	Manatee County Line	22.39	0.511	5	D	47,618	41,300	D	5	D	47,618	48,000	F	43,736	D	9.00%	52.80%	3.00%	
17030000	SR 789	RINGLING BLVD/ST.ARMANDS CIR	SR 45/Tamiami Trail	0.000	Madison Dr	2.368	2.368	4	D	39,165	36,206	D	4	D	39,165	43,688	F	41,007	F	9.00%	62.37%	4.30%	
17030000	SR 789	JOHN-RINGLING PKWY	Madison Dr	2.368	Harborside Dr	5.987	3.619	2	D	24,696	19,759	C	2	D	24,696	25,392	F	24,483	D	9.00%	52.40%	5.20%	
17030000	SR 789	GULF OF MEXICO DR	Harborside Dr	5.987	Manatee County Line	8.957	2.970	2	D	24,696	15,100	C	2	D	24,696	20,700	C	19,147	C	9.00%	52.40%	3.60%	
17030201	SR 789	GULF STREAM AVE	Sunset Dr	0.000	Bird Key Dr	1.006	1.006	4	D	37,300	39,300	F	4	D	37,300	45,800	F	43,783	F	9.00%	52.40%	3.70%	
17030401	SR 789	JOHN RINGLING PKWY	Westway Pl	0.000	Long Boat Club Rd	0.648	0.648	2	D	24,696	20,800	C	2	D	24,696	24,100	D	28,205	F	9.00%	52.40%	3.60%	
17040000	SR 780	FRUITVILLE RD/SR 780	School Ave	0.381	CR 773/ Beneva Rd	2.134	1.753	6	D	59,640	49,187	D	6	D	59,640	57,111	D	57,833	D	9.00%	52.90%	3.65%	
17040000	SR 780	FRUITVILLE RD/SR 780	CR 773/ Beneva Rd	2.134	Honore Ave	4.203	2.069	6	D	56,805	54,700	D	6	D	56,805	63,800	F	55,235	D	9.00%	52.90%	4.20%	
17040000	SR 780	FRUITVILLE RD/SR 780	Honore Ave	4.203	State R/W Line	5.692	1.489	6	D	56,805	63,100	F	6	D	56,805	73,200	F	72,788	F	9.00%	52.90%	4.20%	

Note: LOS Spreadsheet should be used as a planning level analysis tool. A detailed analysis is necessary to validate the actual operating conditions (and for future conditions, as needed) which may vary from this worksheet.

**YEAR 2022 SARASOTA COUNTY LEVEL OF SERVICE SPREADSHEET -DAILY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	SIS	Existing Context Class	Future Context Class	Functional Classification	Posted Speed	Area Type	Facility Type	FDOT LOS Std.	County LOS Std.	City LOS Std.	Year 2022										Deficiency Determination
																		Divided/UnDivided	One/Two Way	Left Turn Bays	Right Turn Bays	Thru Lanes	Daily					
																							Capacity	Volume	LOS			
17040401	SR 780	FRUITVILLE RD	Washington Blvd	0.000	School Ave	0.325	0.325		C4	C5	Principal Arterial-other	40	UA	A	D	D	D	D	2W	WL	WR	6	59,640	30,500	C			
17050000	SR 776	INDIANA AVE SOUTH	Charlotte County Line	0.000	SR 777/Dearborn St	1.108	1.108		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	4	38,430	23,500	C			
17050000	SR 776	INDIANA AVE N	SR 777/Dearborn St	1.108	Old Englewood Rd	3.432	2.324		C3R	C3R	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	4	39,165	22,500	C			
17050000	SR 776	ENGLEWOOD RD	Old Englewood Rd	3.432	Fire Station Ent.	4.474	1.042		C3R	C3R	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	4	39,165	22,500	C			
17050000	SR 776	ENGLEWOOD RD	Fire Station Ent.	4.474	Manasota Beach Rd	5.890	1.416		C3R	C3R	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	57,855	22,500	C			
17050000	SR 776	ENGLEWOOD RD	Manasota Beach Rd	5.890	Jacaranda Blvd	7.496	1.606		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	56,805	36,500	C			
17050000	SR 776	ENGLEWOOD RD	Jacaranda Blvd	7.496	SR 45/US 41	8.222	0.726		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	56,805	19,600	C			
17070000	SR 72	CLARK RD	SR 45/US 41	0.000	CR 773/ Beneva Rd	1.803	1.803		C3R	C4	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	57,855	34,435	C			
17070000	SR 72	CLARK RD	CR 773/ Beneva Rd	1.803	I-75	4.796	2.993		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	56,805	43,000	C			
17070000	SR 72	CLARK RD	I-75	4.796	Lorraine Rd	7.967	3.171		C3C	C3R	Minor Arterial	45	UA	H	D	D		D	2W	WL	WR	2	23,924	12,000	C			
17070000	SR 72	SR 72	Lorraine Rd	7.967	Myakka Valley Tr	11.769	3.802		C2	C2	Minor Arterial	55	RDA	H	C	C		D	2W	WL	WR	2	8,610	7,400	C			
17070000	SR 72	SR 72	Myakka Valley Tr	11.769	Myakka River State Park	13.502	1.733		C1	C1	Minor Arterial	55	RDA	H	C	C		U	2W	WL	OR	2	8,200	3,400	B			
17070000	SR 72	SR 72	Myakka River State Park	13.502	DeSoto County Line	30.591	17.089		C2	C2	Minor Arterial	60	RDA	H	C	C		U	2W	OL	OR	2	8,200	2,700	B			
17075000	I-75	SR 93/I-75	Charlotte County Line	0.000	Toledo Blade Blvd	8.043	8.043	SIS	LA	LA	Principal Arterial-Interstate	70	TA	F	C	C	D	D	2W		OA	6	86,600	57,500	B			
17075000	I-75	SR 93/I-75	Toledo Blade Blvd	8.043	Sumter Blvd	10.987	2.944	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	D	D	D	2W		OA	6	122,800	73,000	C			
17075000	I-75	SR 93/I-75	Sumter Blvd	10.987	River Rd	20.053	9.066	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C	D	D	2W		OA	6	69,000	72,500	D	Over Capacity		
17075000	I-75	SR 93/I-75	River Rd	20.053	Jacaranda Blvd	22.288	2.235	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C	C	D	2W		OA	6	69,000	93,500	E	Over Capacity		
17075000	I-75	SR 93/I-75	Jacaranda Blvd	22.288	Laurel Rd	24.794	2.506	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	D	D	D	2W		OA	6	122,800	99,500	D			
17075000	I-75	SR 93/I-75	Laurel Rd	24.794	SR 681/Venice Connector	29.405	4.611	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C	C	D	2W		OA	6	69,000	93,000	E	Over Capacity		
17075000	I-75	SR 93/I-75	SR 681/Venice Connector	29.405	SR 72/Clark Rd	34.357	4.952	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C		D	2W		OA	6	69,000	114,000	F	Over Capacity		
17075000	I-75	SR 93/I-75	SR 72/Clark Rd	34.357	SR 758/Bee Ridge Rd	36.424	2.067	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	D		D	2W		OA	6	122,800	120,000	D	Near Capacity		
17075000	I-75	SR 93/I-75	SR 758/Bee Ridge Rd	36.424	SR 780/Fruitville Rd	39.139	2.715	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	D		D	2W		OA	6	122,800	139,000	F	Over Capacity		
17075000	I-75	SR 93/I-75	SR 780/Fruitville Rd	39.139	Manatee County Line/University P	42.615	3.476	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	D		D	2W		WA	6	142,800	145,000	E	Over Capacity		
17080000	SR 758	MIDNIGHT PASS RD	Stickney Point Rd	0.000	CR 789A/Beach Rd	1.238	1.238		C3R	C4	Major Collector	35	UA	A	D	D		D	2W	WL	OR	2	23,520	17,800	C			
** 17080000	SR 758	MIDNIGHT PASS RD	CR 789A/Beach Rd	1.238	Higel Ave	3.051	1.813		C3R	C3R	Major Collector	40	UA	A	D	D		U	2W	OL	WR	2	18,816	7,200	C			
17080000	SR 758	OSPREY AVE/BAY RD/SIESTA DR	Higel Ave	3.051	SR 45/US 41	5.771	2.720		C3R	C4	Major Collector	40	UA	A	D	D	D	U	2W	WL	WR	2	23,520	16,234	C			
17110000	SR 72	STICKNEY POINT RD	SR 758/Midnight Pass RD	0.000	SR 45/US 41	0.980	0.980		C4	C4	Major Collector	45	UA	A	D	D		D	2W	WL	WR	4	37,905	25,000	C			
** 17120000	US 301	US-301/WASHINGTON BLVD	SR 45/US 41/Tamiami Tr	0.000	17th St	1.644	1.644		C4	C4	Principal Arterial-other	35	UA	A	D	D	D	D	2W	WL	WR	4	37,905	36,280	D	Near Capacity		
** 17120000	US 301	US-301/WASHINGTON BLVD	17th St	1.644	Manatee County Line/University P	4.184	2.540		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	56,805	44,876	C			
17180000	US 41	VENICE BYPASS	SR 45/US 41 Bus	0.106	SR 45/US 41 Bus	3.074	2.968		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	56,805	35,184	C			
17192000	Univ. Pkwy.	UNIVERSITY PKWY	Airport Entrance	0.269	US 301	1.721	1.452	SC	C4	C4	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	4	37,905	26,188	D			
17192000	Univ. Pkwy.	UNIVERSITY PKWY	US 301	1.721	E. of Lockwood Ridge Rd	3.321	1.600	SC	C3C	C3C	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	6	56,805	47,500	C			
17192000	Univ. Pkwy.	UNIVERSITY PKWY	E. of Lockwood Ridge Rd	3.321	Honore Ave	5.887	2.566	SC	C3R	C3R	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	6	57,855	59,500	F	Over Capacity		
17192000	Univ. Pkwy.	UNIVERSITY PKWY	Honore Ave	5.887	I-75 NB Ramp	6.945	1.058	SC	C3C	C3C	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	10	74,300	61,500	C			
17000064	SR 777	River Rd	US 41	0.000	N. of West Villages Pkwy	1.800	1.800		C3R	C3R	Principal Arterial-other	45	RDA	A	C	D		D	2W	WL	WR	2	21,609	23,000	D	Over Capacity		
17000064	SR 777	River Rd	N. of West Villages Pkwy	1.800	Center Rd	3.219	1.419		C3R	C3R	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	2	24,696	23,000	D	Near Capacity		
17000064	SR 777	River Rd	Center Rd	3.219	I-75	5.819	2.600		C3R	C3R	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	2	24,696	23,000	D	Near Capacity		
17000709	roe Ave Exten	Honroe Ave Extension	Laurel Road	0.000	SR 681	3.746	3.746		C3R	C3R	Major Collector	45	UA	H	D	D		D	2W	WL	WR	2	24,696	7,600	C			

**Legend**  
 Aggregated segment   
 Single count segment   
 OFF System SIS Facility

\*\* Future Model Volumes obtained from D1 Regional Planning Model are found to be less than Existing AADT.



YEAR 2022 SARASOTA COUNTY LEVEL OF SERVICE SPREADSHEET -DAILY

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	Year 2028 (E+C)					Year 2045					Existing K Factor	Existing D Factor	Existing T <sub>24</sub> Factor		
								Thru Lanes	LOS Std	Daily			Thru Lanes	LOS Std	Daily							
										Capacity	Volume	LOS			Capacity	Trend Volume	Trend LOS				Model Volume	Model LOS
1704041	SR 780	FRUITVILLE RD	Washington Blvd	0.000	School Ave	0.325	0.325	6	D	59,640	32,300	C	6	D	56,595	37,500	D	41,481	D	9.00%	52.90%	3.90%
17050000	SR 776	INDIANA AVE SOUTH	Charlotte County Line	0.000	SR 777/Dearborn St	1.108	1.108	4	D	38,430	24,900	C	4	D	38,430	28,900	C	29,991	C	9.00%	52.40%	4.40%
17050000	SR 776	INDIANA AVE N	SR 777/Dearborn St	1.108	Old Englewood Rd	3.432	2.324	4	D	39,165	23,900	C	4	D	39,165	27,700	C	33,432	C	9.00%	52.40%	4.20%
17050000	SR 776	ENGLEWOOD RD	Old Englewood Rd	3.432	Fire Station Ent.	4.474	1.042	4	D	39,165	23,900	C	6	D	57,855	27,700	C	39,811	C	9.00%	52.40%	4.20%
17050000	SR 776	ENGLEWOOD RD	Fire Station Ent.	4.474	Manasota Beach Rd	5.890	1.416	6	D	57,855	23,900	C	6	D	57,855	27,700	C	46,571	C	9.00%	52.40%	4.20%
17050000	SR 776	ENGLEWOOD RD	Manasota Beach Rd	5.890	Jacaranda Blvd	7.496	1.606	6	D	56,805	39,200	C	6	D	56,805	47,000	C	71,383	F	9.00%	52.40%	4.00%
17050000	SR 776	ENGLEWOOD RD	Jacaranda Blvd	7.496	SR 45/US 41	8.222	0.726	6	D	56,805	20,800	C	6	D	56,805	24,100	C	39,479	C	9.00%	52.40%	4.80%
17070000	SR 72	CLARK RD	SR 45/US 41	0.000	CR 773/ Beneva Rd	1.803	1.803	6	D	57,855	36,546	C	6	D	59,640	42,378	C	46,473	C	9.00%	52.90%	3.32%
17070000	SR 72	CLARK RD	CR 773/ Beneva Rd	1.803	I-75	4.796	2.993	6	D	56,805	45,600	C	6	D	56,805	52,900	D	66,019	F	9.00%	52.90%	5.00%
17070000	SR 72	CLARK RD	I-75	4.796	Lorraine Rd	7.967	3.171	2	D	23,924	13,800	C	2	D	24,696	18,900	C	26,034	F	9.00%	55.40%	8.40%
17070000	SR 72	SR 72	Lorraine Rd	7.967	Myakka Valley Tr	11.769	3.802	2	C	8,610	8,600	C	2	C	8,610	12,100	D	28,596	E	9.00%	55.40%	13.70%
17070000	SR 72	SR 72	Myakka Valley Tr	11.769	Myakka River State Park	13.502	1.733	2	C	8,200	4,300	B	2	C	8,200	6,800	C	19,758	E	9.50%	55.40%	15.00%
17070000	SR 72	SR 72	Myakka River State Park	13.502	DeSoto County Line	30.591	17.089	2	C	8,200	3,500	B	2	C	8,200	5,600	C	3,259	B	9.50%	55.40%	15.30%
17075000	I-75	SR 93/I-75	Charlotte County Line	0.000	Toledo Blade Blvd	8.043	8.043	6	C	86,600	67,200	C	6	C	86,600	94,800	D	75,710	C	9.00%	51.80%	11.80%
17075000	I-75	SR 93/I-75	Toledo Blade Blvd	8.043	Sumter Blvd	10.987	2.944	6	D	122,800	85,600	C	6	D	122,800	121,300	D	88,769	C	9.00%	53.30%	11.80%
17075000	I-75	SR 93/I-75	Sumter Blvd	10.987	River Rd	20.053	9.066	6	C	69,000	85,800	E	6	C	69,000	123,600	F	110,098	F	9.00%	53.30%	11.60%
17075000	I-75	SR 93/I-75	River Rd	20.053	Jacaranda Blvd	22.288	2.235	6	C	69,000	110,400	F	10	C	119,880	158,300	E	139,979	D	9.00%	53.30%	9.40%
17075000	I-75	SR 93/I-75	Jacaranda Blvd	22.288	Laurel Rd	24.794	2.506	6	D	122,800	114,500	D	10	D	206,644	156,800	C	160,100	C	9.00%	53.30%	9.40%
17075000	I-75	SR 93/I-75	Laurel Rd	24.794	SR 681/Venice Connector	29.405	4.611	6	C	69,000	105,600	F	10	C	119,880	141,300	D	171,031	F	10.50%	53.30%	10.50%
17075000	I-75	SR 93/I-75	SR 681/Venice Connector	29.405	SR 72/Clark Rd	34.357	4.952	6	C	69,000	128,700	F	10	C	119,880	170,300	F	181,659	F	9.00%	53.30%	9.40%
17075000	I-75	SR 93/I-75	SR 72/Clark Rd	34.357	SR 758/Bee Ridge Rd	36.424	2.067	6	D	122,800	135,700	F	10	D	206,644	180,300	D	183,449	D	9.00%	52.30%	8.00%
17075000	I-75	SR 93/I-75	SR 758/Bee Ridge Rd	36.424	SR 780/Fruitville Rd	39.139	2.715	6	D	122,800	152,400	F	10	D	206,644	190,200	D	195,454	D	9.00%	53.30%	8.10%
17075000	I-75	SR 93/I-75	SR 780/Fruitville Rd	39.139	Manatee County Line/University P	42.615	3.476	6	D	142,800	161,200	F	10	D	206,644	207,200	E	194,561	D	9.00%	53.30%	11.60%
17080000	SR 758	MIDNIGHT PASS RD	Stickney Point Rd	0.000	CR 789A/ Beach Rd	1.238	1.238	2	D	23,520	18,900	C	2	D	18,480	21,900	E	29,912	F	9.00%	52.40%	3.00%
** 17080000	SR 758	MIDNIGHT PASS RD	CR 789A/ Beach Rd	1.238	Higel Ave	3.051	1.813	2	D	18,816	7,600	C	2	D	18,816	8,900	C	6,291	C	9.00%	52.40%	4.20%
17080000	SR 758	OSPREY AVE/BAY RD/SIESTA DR	Higel Ave	3.051	SR 45/US 41	5.771	2.720	2	D	23,520	17,200	C	2	D	18,480	20,001	E	23,758	E	9.00%	52.40%	3.53%
17110000	SR 72	STICKNEY POINT RD	SR 758/Midnight Pass RD	0.000	SR 45/US 41	0.980	0.980	4	D	37,905	26,500	D	4	D	37,905	30,800	D	42,831	E	9.00%	52.40%	2.80%
17120000	US 301	US-301/WASHINGTON BLVD	SR 45/US 41/Tamiami Tr	0.000	17th St	1.644	1.644	4	D	37,905	38,649	E	4	D	37,905	45,253	F	36,666	D	9.00%	52.80%	4.56%
** 17120000	US 301	US-301/WASHINGTON BLVD	17th St	1.644	Manatee County Line/University P	4.184	2.540	6	D	56,805	47,878	C	6	D	56,805	56,305	D	41,216	C	9.00%	52.80%	5.05%
17180000	US 41	VENICE BYPASS	SR 45/US 41 Bus	0.106	SR 45/US 41 Bus	3.074	2.968	6	D	56,805	37,280	C	6	D	56,805	43,269	C	50,320	D	9.00%	52.80%	3.98%
17192000	Univ. Pkwy.	UNIVERSITY PKWY	Airport Entrance	0.269	US 301	1.721	1.452	4	D	37,905	27,963	D	4	D	37,905	33,120	D	26,283	D	9.00%	52.90%	3.29%
17192000	Univ. Pkwy.	UNIVERSITY PKWY	US 301	1.721	E. of Lockwood Ridge Rd	3.321	1.600	6	D	56,805	51,700	D	6	D	56,805	63,700	F	49,659	C	9.00%	52.90%	4.00%
17192000	Univ. Pkwy.	UNIVERSITY PKWY	E. of Lockwood Ridge Rd	3.321	Honore Ave	5.887	2.566	6	D	57,855	63,100	F	6	D	57,855	73,200	F	65,317	F	9.00%	52.90%	4.40%
17192000	Univ. Pkwy.	UNIVERSITY PKWY	Honore Ave	5.887	I-75 NB Ramp	6.945	1.058	10	D	74,300	65,200	C	10	D	74,300	75,600	C	75,389	C	9.00%	52.90%	3.70%
17000064	SR 777	River Rd	US 41	0.000	N. of West Villages Pkwy	1.800	1.800	6	C	55,545	27,800	C	6	C	55,545	41,500	C	49,210	C	9.00%	52.90%	9.90%
17000064	SR 777	River Rd	N. of West Villages Pkwy	1.800	Center Rd	3.219	1.419	6	D	57,855	27,800	C	6	D	57,855	41,500	C	51,209	C	9.00%	52.90%	9.90%
17000064	SR 777	River Rd	Center Rd	3.219	I-75	5.819	2.600	4	D	39,165	27,800	C	4	D	39,165	41,500	F	47,162	F	9.00%	52.90%	9.90%
17000709	roe Ave Exten	Honroe Ave Extension	Laurel Road	0.000	SR 681	3.746	3.746	2	D	24,696	8,100	C	2	D	24,696	9,300	C	10,453	C	9.00%	52.90%	6.30%

Legend  
 Aggregated segment   
 Single count segment   
 OFF System SIS Facility

\*\* Future Model Volumes obtained from D1 Regional Planning Model are found to be less than Existing AADT.

**YEAR 2022 SARASOTA COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR TWO-WAY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	SIS	Existing Context Class	Future Context Class	Functional Classification	Posted Speed	Area Type	Facility Type	FDOT LOS Std.	County LOS Std.	City LOS Std.	Year 2022										Deficiency Determination
																		Divided/UnDivided	One/Two Way	Left Turn Bays	Right Turn Bays	Thru Lanes	Peak Hour Two-Way					
																							Capacity	Volume	LOS			
17005000	SR 681	VENICE CONNECTOR	SR 45/US41	0.000	US 41 Ramp	0.327	0.327		C2	C2	Principal Arterial-other	45	UA	H	D	D		U	1W	0L	0R	2	3,492	522	B			
17005000	SR 681	VENICE CONNECTOR	US 41 Ramp	0.327	I-75 Ramp	3.158	2.831		C2	C2	Principal Arterial-other	55	UA	H	D	D		D	2W	WL	WR	4	5,290	1,386	B			
17005000	SR 681	VENICE CONNECTOR	I-75 Ramp	3.158	I-75/SR 93	4.142	0.984	SIS Link	C2	C2	Principal Arterial-other	70	RDA	H	C	C		U	1W	0L	0R	2	2,868	1,520	B			
17008000	SR 758	BEE RIDGE ROAD	SR 45/US 41	0.000	Beneva Rd	2.026	2.026		C4	C4	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	5,366	2,927	C			
17008000	SR 758	BEE RIDGE ROAD	Beneva Rd	2.026	McIntosh Rd	3.044	1.018		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	0R	6	4,870	3,690	C			
17008000	SR 758	BEE RIDGE ROAD	McIntosh Rd	3.044	Honore Ave	4.056	1.012		C3R	C3R	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	4	3,528	3,465	D	Near Capacity		
17008000	SR 758	BEE RIDGE ROAD	Honore Ave	4.056	I-75 Ramp	5.066	1.010		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	4	3,454	3,375	D	Near Capacity		
17008000	SR 758	BEE RIDGE ROAD	I-75 Ramp	5.066	R/W Line	5.432	0.366		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	0L	WR	4	3,454	2,610	C			
17010000	US 41	TAMIAMI TRAIL	Charlotte County Line	0.000	100 Ft E. of Tuscola Blvd	1.205	1.205		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	5,114	3,375	C			
17010000	US 41	TAMIAMI TRAIL	100 Ft E. of Tuscola Blvd	1.205	SR 777/River Rd	6.138	4.933		C3R	C3R	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	3,528	3,375	D	Near Capacity		
17010000	US 41	TAMIAMI TRAIL	SR 777/River Rd	6.138	Rockley Blvd	9.878	3.740		C3R	C3R	Principal Arterial-other	55	TA	A	C	C	D	D	2W	WL	WR	4	3,244	2,090	C			
17010000	US 41	TAMIAMI TRAIL	Rockley Blvd	9.878	SR 776/Park Pl	12.826	2.948		C3R	C3R	Principal Arterial-other	55	UA	A	D	D		D	2W	WL	WR	4	3,528	2,811	C			
17010000	US 41	TAMIAMI TRAIL	SR 776/Park Pl	12.826	SR 45A/Venice Bypass	14.882	2.056		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	5,114	4,635	D	Near Capacity		
17010000	USB 41	TAMIAMI TRAIL	SR 45A/Venice Bypass	14.882	CR 772/Venice Ave	17.155	2.273		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	3,454	2,790	C			
17020000	USB 41	US-41/TAMIAMI TRAIL	CR 772/Venice Ave	0.000	SR 45A/US 41	0.638	0.638		C3R	C3R	Principal Arterial-other	40	UA	A	D	D	D	D	2W	WL	WR	4	3,528	1,692	C			
17020000	US 41	US-41/TAMIAMI TRAIL	SR 45A/US 41	0.638	Laurel Rd	2.894	2.256		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	5,114	4,140	C			
17020000	US 41	US-41/TAMIAMI TRAIL	Laurel Rd	2.894	SR 681 Ramp	3.421	0.527		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	5,114	4,500	C			
17020000	US 41	TAMIAMI TRAIL	SR 681 Ramp	3.421	CR 789/Old Venice Rd	6.134	2.713		C3C	C3C	Principal Arterial-other	55	UA	A	D	D		D	2W	WL	WR	4	3,454	3,150	D	Near Capacity		
17020000	US 41	US-41/TAMIAMI TRAIL	CR 789/Old Venice Rd	6.134	CR 773/Beneva Rd	10.029	3.895		C3C	C3C	Principal Arterial-other	55	UA	A	D	D		D	2W	WL	WR	4	3,454	3,825	F	Over Capacity		
17020000	US 41	US-41/TAMIAMI TRAIL	CR 773/Beneva Rd	10.029	Buccaneer Dr	11.716	1.687		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	4	3,454	2,995	D			
**	17020000	US 41	US-41/TAMIAMI TRAIL	Buccaneer Dr	11.716	CR 72 A/Proctor Rd	14.299	2.583		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	5,114	4,009	C		
**	17020000	US 41	US-41/TAMIAMI TRAIL	CR 72 A/Proctor Rd	14.299	Webber St	15.930	1.631		C4	C4	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	5,366	4,347	D		
**	17020000	US 41	US-41/TAMIAMI TRAIL	Webber St	15.930	SR 683/US 301	17.371	1.441		C4	C4	Principal Arterial-other	40	UA	A	D	D	D	D	2W	WL	WR	6	5,366	5,220	D	Near Capacity	
17020000	US 41	MOUND ST	SR 683/US 301	17.371	SR 789/Gulf Stream Ave	18.642	1.271		C4	C4	Principal Arterial-other	35	UA	A	D	D	D	D	2W	WL	WR	4	3,412	1,891	C			
17020000	US 41	N TAMIAMI TRAIL	SR 789/Gulf Stream Ave	18.642	University Pkwy	21.879	3.237		C4	C4	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	3,412	2,906	D			
17020000	US 41	N TAMIAMI TRAIL	University Pkwy	21.879	Manatee County Line	22.39	0.511		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	5	4,284	3,510	C			
17030000	SR 789	RINGLING BLVD/ST.ARMANDS CIR	SR 45/Tamiami Trail	0.000	Madison Dr	2.368	2.368		C3R	C3R	Minor Arterial	35	UA	A	D	D	D	D	2W	WL	WR	4	3,528	3,019	C			
17030000	SR 789	JOHN-RINGLING PKWY	Madison Dr	2.368	Harborside Dr	5.987	3.619		C3R	C3R	Major Collector	45	UA	A	D	D	E	D	2W	WL	WR	2	2,227	1,598	C			
17030000	SR 789	GULF OF MEXICO DR	Harborside Dr	5.987	Manatee County Line	8.957	2.970		C3R	C3R	Major Collector	45	UA	A	D	D	E	D	2W	WL	WR	2	2,227	1,179	C			
17030201	SR 789	GULF STREAM AVE	Sunset Dr	0.000	Bird Key Dr	1.006	1.006		C3R	C3R	Minor Arterial	40	UA	A	D	D	D	D	2W	WL	0R	4	3,360	3,330	D	Near Capacity		
17030401	SR 789	JOHN RINGLING PKWY	Westway Pl	0.000	Long Boat Club Rd	0.648	0.648		C3R	C3R	Major Collector	35	UA	A	D	D	D	D	2W	WL	WR	2	2,227	1,764	C			
17040000	SR 780	FRUITVILLE RD/SR 780	School Ave	0.381	CR 773/ Beneva Rd	2.134	1.753		C4	C4	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	5,366	4,177	C			
17040000	SR 780	FRUITVILLE RD/SR 780	CR 773/ Beneva Rd	2.134	Honore Ave	4.203	2.069		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	5,114	4,635	D	Near Capacity		
17040000	SR 780	FRUITVILLE RD/SR 780	Honore Ave	4.203	State R/W Line	5.692	1.489		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	5,114	5,355	F	Over Capacity		

Note: LOS Spreadsheet should be used as a planning level analysis tool. A detailed analysis is necessary to validate the actual operating conditions (and for future conditions, as needed) which may vary from this worksheet.

**YEAR 2022 SARASOTA COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR TWO-WAY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	Year 2028 (E+C)					Year 2045					Existing K Factor	Existing D Factor	Existing T <sub>24</sub> Factor			
								Thru Lanes	LOS Std	Peak Hour Two-Way			Thru Lanes	LOS Std	Peak Hour Two-Way								
										Capacity	Volume	LOS			Capacity	Trend Volume	Trend LOS				Model Volume	Model LOS	
17005000	SR 681	VENICE CONNECTOR	SR 45/US41	0.000	US 41 Ramp	0.327	0.327	2	D	3,492	549	B	2	D	3,492	639	B	1,006	B	9.00%	99.90%	9.30%	
17005000	SR 681	VENICE CONNECTOR	US 41 Ramp	0.327	I-75 Ramp	3.158	2.831	4	D	5,290	1,575	B	4	D	5,290	2,115	B	2,797	B	9.00%	52.90%	9.30%	
17005000	SR 681	VENICE CONNECTOR	I-75 Ramp	3.158	I-75/SR 93	4.142	0.984	2	C	2,868	2,052	C	2	C	2,868	3,582	E	1,639	B	9.50%	99.90%	9.30%	
17008000	SR 758	BEE RIDGE ROAD	SR 45/US 41	0.000	Beneva Rd	2.026	2.026	6	D	5,366	3,103	C	6	D	5,366	3,600	C	4,347	D	9.00%	52.90%	7.30%	
17008000	SR 758	BEE RIDGE ROAD	Beneva Rd	2.026	McIntosh Rd	3.044	1.018	6	D	4,870	3,915	C	6	D	4,870	4,536	D	4,835	D	9.00%	52.90%	7.30%	
17008000	SR 758	BEE RIDGE ROAD	McIntosh Rd	3.044	Honore Ave	4.056	1.012	4	D	3,528	3,672	F	4	D	3,528	4,266	F	4,429	F	9.00%	52.90%	7.30%	
17008000	SR 758	BEE RIDGE ROAD	Honore Ave	4.056	I-75 Ramp	5.066	1.010	4	D	3,454	3,582	F	4	D	3,454	4,149	F	5,135	F	9.00%	52.90%	4.80%	
17008000	SR 758	BEE RIDGE ROAD	I-75 Ramp	5.066	R/W Line	5.432	0.366	4	D	3,454	2,763	C	4	D	3,454	3,213	D	5,686	F	9.00%	52.90%	5.00%	
17010000	US 41	TAMIAMI TRAIL	Charlotte County Line	0.000	100 Ft E. of Tuscola Blvd	1.205	1.205	6	D	5,114	3,699	C	6	D	5,114	4,626	D	4,496	C	9.00%	52.80%	4.60%	
17010000	US 41	TAMIAMI TRAIL	100 Ft E. of Tuscola Blvd	1.205	SR 777/River Rd	6.138	4.933	4	D	3,528	3,699	F	4	D	3,528	4,626	F	4,674	F	9.00%	52.80%	4.60%	
17010000	US 41	TAMIAMI TRAIL	SR 777/River Rd	6.138	Rockley Blvd	9.878	3.740	4	C	3,244	2,280	C	4	C	3,244	2,812	C	4,684	F	9.50%	52.80%	4.60%	
17010000	US 41	TAMIAMI TRAIL	Rockley Blvd	9.878	SR 776/Park Pl	12.826	2.948	4	D	3,528	2,992	C	4	D	3,528	3,514	D	3,947	F	9.00%	52.80%	3.33%	
17010000	US 41	TAMIAMI TRAIL	SR 776/Park Pl	12.826	SR 45A/Venice Bypass	14.882	2.056	6	D	5,114	4,914	D	6	D	5,114	5,697	F	6,695	F	9.00%	52.80%	2.80%	
17010000	USB 41	TAMIAMI TRAIL	SR 45A/Venice Bypass	14.882	CR 772/Venice Ave	17.155	2.273	4	D	3,454	3,222	D	4	D	3,454	4,428	F	2,977	D	9.00%	52.80%	2.40%	
17020000	USB 41	US-41/TAMIAMI TRAIL	CR 772/Venice Ave	0.000	SR 45A/US 41	0.638	0.638	4	D	3,528	1,791	C	4	D	3,528	2,079	C	2,183	C	9.00%	52.80%	2.60%	
17020000	US 41	US-41/TAMIAMI TRAIL	SR 45A/US 41	0.638	Laurel Rd	2.894	2.256	6	D	5,114	4,392	C	6	D	5,114	5,094	D	5,414	F	9.00%	52.80%	3.50%	
17020000	US 41	US-41/TAMIAMI TRAIL	Laurel Rd	2.894	SR 681 Ramp	3.421	0.527	6	D	5,114	4,770	D	6	D	5,114	5,535	F	5,787	F	9.00%	52.80%	4.50%	
17020000	US 41	TAMIAMI TRAIL	SR 681 Ramp	3.421	CR 789/Old Venice Rd	6.134	2.713	4	D	3,454	3,339	D	4	D	3,454	3,879	F	4,154	F	9.00%	52.80%	4.90%	
17020000	US 41	US-41/TAMIAMI TRAIL	CR 789/Old Venice Rd	6.134	CR 773/Beneva Rd	10.029	3.895	4	D	3,454	4,059	F	4	D	3,454	4,707	F	4,537	F	9.00%	52.80%	3.40%	
17020000	US 41	US-41/TAMIAMI TRAIL	CR 773/Beneva Rd	10.029	Buccaneer Dr	11.716	1.687	4	D	3,454	3,177	D	4	D	3,454	3,681	F	4,519	F	9.00%	52.80%	3.20%	
**	17020000	US 41	US-41/TAMIAMI TRAIL	Buccaneer Dr	11.716	CR 72 A/Proctor Rd	14.299	2.583	6	D	5,114	4,250	C	6	D	5,114	4,935	D	3,979	C	9.00%	52.80%	3.11%
**	17020000	US 41	US-41/TAMIAMI TRAIL	CR 72 A/Proctor Rd	14.299	Webber St	15.930	1.631	6	D	5,366	4,608	D	6	D	5,366	5,348	D	4,136	C	9.00%	52.80%	2.80%
**	17020000	US 41	US-41/TAMIAMI TRAIL	Webber St	15.930	SR 683/US 301	17.371	1.441	6	D	5,366	5,535	E	6	D	5,366	6,417	F	4,432	D	9.00%	52.80%	2.80%
17020000	US 41	MOUND ST	SR 683/US 301	17.371	SR 789/Gulf Stream Ave	18.642	1.271	4	D	3,412	2,003	C	4	D	3,412	2,328	D	2,960	D	9.00%	52.80%	2.83%	
17020000	US 41	N TAMIAMI TRAIL	SR 789/Gulf Stream Ave	18.642	University Pkwy	21.879	3.237	4	D	3,412	3,083	D	4	D	3,412	3,573	E	3,754	E	9.00%	52.80%	2.99%	
17020000	US 41	N TAMIAMI TRAIL	University Pkwy	21.879	Manatee County Line	22.39	0.511	5	D	4,284	3,717	D	5	D	4,284	4,320	F	3,936	D	9.00%	52.80%	3.00%	
17030000	SR 789	RINGLING BLVD/ST.ARMANDS CIR	SR 45/Tamiami Trail	0.000	Madison Dr	2.368	2.368	4	D	3,528	3,259	D	4	D	3,528	3,932	F	3,691	F	9.00%	62.37%	4.30%	
17030000	SR 789	JOHN-RINGLING PKWY	Madison Dr	2.368	Harborside Dr	5.987	3.619	2	D	2,227	1,778	C	2	D	2,227	2,285	F	2,204	D	9.00%	52.40%	5.20%	
17030000	SR 789	GULF OF MEXICO DR	Harborside Dr	5.987	Manatee County Line	8.957	2.970	2	D	2,227	1,359	C	2	D	2,227	1,863	C	1,723	C	9.00%	52.40%	3.60%	
17030201	SR 789	GULF STREAM AVE	Sunset Dr	0.000	Bird Key Dr	1.006	1.006	4	D	3,360	3,537	F	4	D	3,360	4,122	F	3,940	F	9.00%	52.40%	3.70%	
17030401	SR 789	JOHN RINGLING PKWY	Westway Pl	0.000	Long Boat Club Rd	0.648	0.648	2	D	2,227	1,872	C	2	D	2,227	2,169	D	2,538	F	9.00%	52.40%	3.60%	
17040000	SR 780	FRUITVILLE RD/SR 780	School Ave	0.381	CR 773/ Beneva Rd	2.134	1.753	6	D	5,366	4,427	D	6	D	5,366	5,140	D	5,205	D	9.00%	52.90%	3.65%	
17040000	SR 780	FRUITVILLE RD/SR 780	CR 773/ Beneva Rd	2.134	Honore Ave	4.203	2.069	6	D	5,114	4,923	D	6	D	5,114	5,742	F	4,971	D	9.00%	52.90%	4.20%	
17040000	SR 780	FRUITVILLE RD/SR 780	Honore Ave	4.203	State R/W Line	5.692	1.489	6	D	5,114	5,679	F	6	D	5,114	6,588	F	6,551	F	9.00%	52.90%	4.20%	

Note: LOS Spreadsheet should be used as a planning level analysis tool. A detailed analysis is necessary to validate the actual operating conditions (and for future conditions, as needed) which may vary from this worksheet.

YEAR 2022 SARASOTA COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR TWO-WAY

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	SIS	Existing Context Class	Future Context Class	Functional Classification	Posted Speed	Area Type	Facility Type	FDOT LOS Std.	County LOS Std.	City LOS Std.	Year 2022								Deficiency Determination
																		Divided/UnDivided	One/Two Way	Left Turn Bays	Right Turn Bays	Thru Lanes	Peak Hour Two-Way			
																							Capacity	Volume	LOS	
17040401	SR 780	FRUITVILLE RD	Washington Blvd	0.000	School Ave	0.325	0.325		C4	C5	Principal Arterial-other	40	UA	A	D	D	D	D	2W	WL	WR	6	5,366	2,745	C	
17050000	SR 776	INDIANA AVE SOUTH	Charlotte County Line	0.000	SR 777/Dearborn St	1.108	1.108		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	4	3,454	2,115	C	
17050000	SR 776	INDIANA AVE N	SR 777/Dearborn St	1.108	Old Englewood Rd	3.432	2.324		C3R	C3R	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	4	3,528	2,025	C	
17050000	SR 776	ENGLEWOOD RD	Old Englewood Rd	3.432	Fire Station Ent.	4.474	1.042		C3R	C3R	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	4	3,528	2,025	C	
17050000	SR 776	ENGLEWOOD RD	Fire Station Ent.	4.474	Manasota Beach Rd	5.890	1.416		C3R	C3R	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	5,208	2,025	C	
17050000	SR 776	ENGLEWOOD RD	Manasota Beach Rd	5.890	Jacaranda Blvd	7.496	1.606		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	5,114	3,285	C	
17050000	SR 776	ENGLEWOOD RD	Jacaranda Blvd	7.496	SR 45/US 41	8.222	0.726		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	5,114	1,764	C	
17070000	SR 72	CLARK RD	SR 45/US 41	0.000	CR 773/ Beneva Rd	1.803	1.803		C3R	C4	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	5,208	3,099	C	
17070000	SR 72	CLARK RD	CR 773/ Beneva Rd	1.803	I-75	4.796	2.993		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	5,114	3,870	C	
17070000	SR 72	CLARK RD	I-75	4.796	Lorraine Rd	7.967	3.171		C3C	C3R	Minor Arterial	45	UA	H	D	D		D	2W	WL	WR	2	2,150	1,080	C	
17070000	SR 72	SR 72	Lorraine Rd	7.967	Myakka Valley Tr	11.769	3.802		C2	C2	Minor Arterial	55	RDA	H	C	C		D	2W	WL	WR	2	819	666	C	
17070000	SR 72	SR 72	Myakka Valley Tr	11.769	Myakka River State Park	13.502	1.733		C1	C1	Minor Arterial	55	RDA	H	C	C		U	2W	WL	OR	2	780	323	B	
17070000	SR 72	SR 72	Myakka River State Park	13.502	DeSoto County Line	30.591	17.089		C2	C2	Minor Arterial	60	RDA	H	C	C		U	2W	OL	OR	2	780	257	B	
17075000	I-75	SR 93/I-75	Charlotte County Line	0.000	Toledo Blade Blvd	8.043	8.043	SIS	LA	LA	Principal Arterial-Interstate	70	TA	F	C	C	D	D	2W		OA	6	8,490	5,175	B	
17075000	I-75	SR 93/I-75	Toledo Blade Blvd	8.043	Sumter Blvd	10.987	2.944	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	D	D	D	2W		OA	6	11,050	6,570	C	
17075000	I-75	SR 93/I-75	Sumter Blvd	10.987	River Rd	20.053	9.066	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C	D	D	2W		OA	6	7,250	6,525	C	Near Capacity
17075000	I-75	SR 93/I-75	River Rd	20.053	Jacaranda Blvd	22.288	2.235	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C	C	D	2W		OA	6	7,250	8,415	D	Over Capacity
17075000	I-75	SR 93/I-75	Jacaranda Blvd	22.288	Laurel Rd	24.794	2.506	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	D	D	D	2W		OA	6	11,050	8,955	D	
17075000	I-75	SR 93/I-75	Laurel Rd	24.794	SR 681/Venice Connector	29.405	4.611	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C	C	D	2W		OA	6	7,250	9,765	E	Over Capacity
17075000	I-75	SR 93/I-75	SR 681/Venice Connector	29.405	SR 72/Clark Rd	34.357	4.952	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C		D	2W		OA	6	7,250	10,260	F	Over Capacity
17075000	I-75	SR 93/I-75	SR 72/Clark Rd	34.357	SR 758/Bee Ridge Rd	36.424	2.067	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	D		D	2W		OA	6	11,050	10,800	D	Near Capacity
17075000	I-75	SR 93/I-75	SR 758/Bee Ridge Rd	36.424	SR 780/Fruitville Rd	39.139	2.715	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	D		D	2W		OA	6	11,050	12,510	F	Over Capacity
17075000	I-75	SR 93/I-75	SR 780/Fruitville Rd	39.139	Manatee County Line/University P	42.615	3.476	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	D		D	2W		WA	6	12,850	13,050	E	Over Capacity
17080000	SR 758	MIDNIGHT PASS RD	Stickney Point Rd	0.000	CR 789A/Beach Rd	1.238	1.238		C3R	C4	Major Collector	35	UA	A	D	D		D	2W	WL	OR	2	2,121	1,602	C	
** 17080000	SR 758	MIDNIGHT PASS RD	CR 789A/Beach Rd	1.238	Higel Ave	3.051	1.813		C3R	C3R	Major Collector	40	UA	A	D	D		U	2W	OL	WR	2	1,697	648	C	
17080000	SR 758	OSPREY AVE/BAY RD/SIESTA DR	Higel Ave	3.051	SR 45/US 41	5.771	2.720		C3R	C4	Major Collector	40	UA	A	D	D	D	U	2W	WL	WR	2	2,121	1,461	C	
17110000	SR 72	STICKNEY POINT RD	SR 758/Midnight Pass RD	0.000	SR 45/US 41	0.980	0.980		C4	C4	Major Collector	45	UA	A	D	D		D	2W	WL	WR	4	3,412	2,250	C	
17120000	US 301	US-301/WASHINGTON BLVD	SR 45/US 41/Tamiami Tr	0.000	17th St	1.644	1.644		C4	C4	Principal Arterial-other	35	UA	A	D	D	D	D	2W	WL	WR	4	3,412	3,265	D	Near Capacity
** 17120000	US 301	US-301/WASHINGTON BLVD	17th St	1.644	Manatee County Line/University P	4.184	2.540		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	5,114	4,039	C	
17180000	US 41	VENICE BYPASS	SR 45/US 41 Bus	0.106	SR 45/US 41 Bus	3.074	2.968		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	5,114	3,167	C	
17192000	Univ. Pkwy.	UNIVERSITY PKWY	Airport Entrance	0.269	US 301	1.721	1.452	SC	C4	C4	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	4	3,412	2,357	D	
17192000	Univ. Pkwy.	UNIVERSITY PKWY	US 301	1.721	E. of Lockwood Ridge Rd	3.321	1.600	SC	C3C	C3C	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	6	5,114	4,275	C	
17192000	Univ. Pkwy.	UNIVERSITY PKWY	E. of Lockwood Ridge Rd	3.321	Honore Ave	5.887	2.566	SC	C3R	C3R	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	6	5,208	5,355	F	Over Capacity
17192000	Univ. Pkwy.	UNIVERSITY PKWY	Honore Ave	5.887	I-75 NB Ramp	6.945	1.058	SC	C3C	C3C	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	10	6,690	5,535	C	
17000064	SR 777	River Rd	US 41	0.000	N. of West Villages Pkwy	1.800	1.800		C3R	C3R	Principal Arterial-other	45	RDA	A	C	D		D	2W	WL	WR	2	1,940	2,070	D	Over Capacity
17000064	SR 777	River Rd	N. of West Villages Pkwy	1.800	Center Rd	3.219	1.419		C3R	C3R	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	2	2,227	2,070	D	Near Capacity
17000064	SR 777	River Rd	Center Rd	3.219	I-75	5.819	2.600		C3R	C3R	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	2	2,227	2,070	D	Near Capacity
17000709	roe Ave Exten	Honroe Ave Extension	Laurel Road	0.000	SR 681	3.746	3.746		C3R	C3R	Major Collector	45	UA	H	D	D		D	2W	WL	WR	2	2,227	684	C	

**Legend**  
 Aggregated segment   
 Single count segment   
 OFF System SIS Facility

\*\* Future Model Volumes obtained from D1 Regional Planning Model are found to be less than Existing AADT.

**YEAR 2022 SARASOTA COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR TWO-WAY**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	Year 2028 (E+C)					Year 2045					Existing K Factor	Existing D Factor	Existing T <sub>24</sub> Factor		
								Thru Lanes	LOS Std	Peak Hour Two-Way			Thru Lanes	LOS Std	Peak Hour Two-Way							
										Capacity	Volume	LOS			Capacity	Trend Volume	Trend LOS				Model Volume	Model LOS
1704041	SR 780	FRUITVILLE RD	Washington Blvd	0.000	School Ave	0.325	0.325	6	D	5,366	2,907	C	6	D	5,092	3,375	D	3,733	D	9.00%	52.90%	3.90%
17050000	SR 776	INDIANA AVE SOUTH	Charlotte County Line	0.000	SR 777/Dearborn St	1.108	1.108	4	D	3,454	2,241	C	4	D	3,454	2,601	C	2,699	C	9.00%	52.40%	4.40%
17050000	SR 776	INDIANA AVE N	SR 777/Dearborn St	1.108	Old Englewood Rd	3.432	2.324	4	D	3,528	2,151	C	4	D	3,528	2,493	C	3,009	C	9.00%	52.40%	4.20%
17050000	SR 776	ENGLEWOOD RD	Old Englewood Rd	3.432	Fire Station Ent.	4.474	1.042	4	D	3,528	2,151	C	6	D	5,208	2,493	C	3,583	C	9.00%	52.40%	4.20%
17050000	SR 776	ENGLEWOOD RD	Fire Station Ent.	4.474	Manasota Beach Rd	5.890	1.416	6	D	5,208	2,151	C	6	D	5,208	2,493	C	4,191	C	9.00%	52.40%	4.20%
17050000	SR 776	ENGLEWOOD RD	Manasota Beach Rd	5.890	Jacaranda Blvd	7.496	1.606	6	D	5,114	3,528	C	6	D	5,114	4,230	C	6,424	F	9.00%	52.40%	4.00%
17050000	SR 776	ENGLEWOOD RD	Jacaranda Blvd	7.496	SR 45/US 41	8.222	0.726	6	D	5,114	1,872	C	6	D	5,114	2,169	C	3,553	C	9.00%	52.40%	4.80%
17070000	SR 72	CLARK RD	SR 45/US 41	0.000	CR 773/ Beneva Rd	1.803	1.803	6	D	5,208	3,289	C	6	D	5,366	3,814	C	4,183	C	9.00%	52.90%	3.32%
17070000	SR 72	CLARK RD	CR 773/ Beneva Rd	1.803	I-75	4.796	2.993	6	D	5,114	4,104	C	6	D	5,114	4,761	D	5,942	F	9.00%	52.90%	5.00%
17070000	SR 72	CLARK RD	I-75	4.796	Lorraine Rd	7.967	3.171	2	D	2,150	1,242	C	2	D	2,227	1,701	C	2,343	F	9.00%	55.40%	8.40%
17070000	SR 72	SR 72	Lorraine Rd	7.967	Myakka Valley Tr	11.769	3.802	2	C	819	774	C	2	C	819	1,089	D	2,573	E	9.00%	55.40%	13.70%
17070000	SR 72	SR 72	Myakka Valley Tr	11.769	Myakka River State Park	13.502	1.733	2	C	780	409	B	2	C	780	646	C	1,877	E	9.50%	55.40%	15.00%
17070000	SR 72	SR 72	Myakka River State Park	13.502	DeSoto County Line	30.591	17.089	2	C	780	333	B	2	C	780	532	C	310	B	9.50%	55.40%	15.30%
17075000	I-75	SR 93/I-75	Charlotte County Line	0.000	Toledo Blade Blvd	8.043	8.043	6	C	8,490	6,048	B	6	C	8,490	8,532	D	6,814	C	9.00%	51.80%	11.80%
17075000	I-75	SR 93/I-75	Toledo Blade Blvd	8.043	Sumter Blvd	10.987	2.944	6	D	11,050	7,704	C	6	D	11,050	10,917	D	7,989	C	9.00%	53.30%	11.80%
17075000	I-75	SR 93/I-75	Sumter Blvd	10.987	River Rd	20.053	9.066	6	C	7,250	7,722	D	6	C	7,250	11,124	F	9,909	E	9.00%	53.30%	11.60%
17075000	I-75	SR 93/I-75	River Rd	20.053	Jacaranda Blvd	22.288	2.235	6	C	7,250	9,936	E	10	C	12,592	14,247	D	12,598	D	9.00%	53.30%	9.40%
17075000	I-75	SR 93/I-75	Jacaranda Blvd	22.288	Laurel Rd	24.794	2.506	6	D	11,050	10,305	D	10	D	18,598	14,112	C	14,409	C	9.00%	53.30%	9.40%
17075000	I-75	SR 93/I-75	Laurel Rd	24.794	SR 681/Venice Connector	29.405	4.611	6	C	7,250	11,088	F	10	C	12,592	14,837	D	17,958	F	10.50%	53.30%	10.50%
17075000	I-75	SR 93/I-75	SR 681/Venice Connector	29.405	SR 72/Clark Rd	34.357	4.952	6	C	7,250	11,583	F	10	C	12,592	15,327	E	16,349	E	9.00%	53.30%	9.40%
17075000	I-75	SR 93/I-75	SR 72/Clark Rd	34.357	SR 758/Bee Ridge Rd	36.424	2.067	6	D	11,050	12,213	F	10	D	18,598	16,227	D	16,510	D	9.00%	52.30%	8.00%
17075000	I-75	SR 93/I-75	SR 758/Bee Ridge Rd	36.424	SR 780/Fruitville Rd	39.139	2.715	6	D	11,050	13,716	F	10	D	18,598	17,118	D	17,591	D	9.00%	53.30%	8.10%
17075000	I-75	SR 93/I-75	SR 780/Fruitville Rd	39.139	Manatee County Line/University P	42.615	3.476	6	D	12,850	14,508	F	10	D	18,598	18,648	E	17,510	D	9.00%	53.30%	11.60%
17080000	SR 758	MIDNIGHT PASS RD	Stickney Point Rd	0.000	CR 789A/ Beach Rd	1.238	1.238	2	D	2,121	1,701	C	2	D	1,659	1,971	E	2,692	F	9.00%	52.40%	3.00%
** 17080000	SR 758	MIDNIGHT PASS RD	CR 789A/ Beach Rd	1.238	Higel Ave	3.051	1.813	2	D	1,697	684	C	2	D	1,697	801	C	566	C	9.00%	52.40%	4.20%
17080000	SR 758	OSPNEY AVE/BAY RD/SIESTA DR	Higel Ave	3.051	SR 45/US 41	5.771	2.720	2	D	2,121	1,548	C	2	D	1,659	1,800	F	2,139	F	9.00%	52.40%	3.53%
17110000	SR 72	STICKNEY POINT RD	SR 758/Midnight Pass RD	0.000	SR 45/US 41	0.980	0.980	4	D	3,412	2,385	D	4	D	3,412	2,772	D	3,855	F	9.00%	52.40%	2.80%
17120000	US 301	US-301/WASHINGTON BLVD	SR 45/US 41/Tamiami Tr	0.000	17th St	1.644	1.644	4	D	3,412	3,478	E	4	D	3,412	4,073	F	3,300	D	9.00%	52.80%	4.56%
** 17120000	US 301	US-301/WASHINGTON BLVD	17th St	1.644	Manatee County Line/University P	4.184	2.540	6	D	5,114	4,309	C	6	D	5,114	5,067	D	3,710	C	9.00%	52.80%	5.05%
17180000	US 41	VENICE BYPASS	SR 45/US 41 Bus	0.106	SR 45/US 41 Bus	3.074	2.968	6	D	5,114	3,355	C	6	D	5,114	3,894	C	4,529	D	9.00%	52.80%	3.98%
17192000	Univ. Pkwy.	UNIVERSITY PKWY	Airport Entrance	0.269	US 301	1.721	1.452	4	D	3,412	2,517	D	4	D	3,412	2,981	D	2,366	D	9.00%	52.90%	3.29%
17192000	Univ. Pkwy.	UNIVERSITY PKWY	US 301	1.721	E. of Lockwood Ridge Rd	3.321	1.600	6	D	5,114	4,653	D	6	D	5,114	5,733	F	4,469	C	9.00%	52.90%	4.00%
17192000	Univ. Pkwy.	UNIVERSITY PKWY	E. of Lockwood Ridge Rd	3.321	Honore Ave	5.887	2.566	6	D	5,208	5,679	F	6	D	5,208	6,588	F	5,879	F	9.00%	52.90%	4.40%
17192000	Univ. Pkwy.	UNIVERSITY PKWY	Honore Ave	5.887	I-75 NB Ramp	6.945	1.058	10	D	6,690	5,868	C	10	D	6,690	6,804	C	6,785	C	9.00%	52.90%	3.70%
17000064	SR 777	River Rd	US 41	0.000	N. of West Villages Pkwy	1.800	1.800	6	C	4,998	2,502	C	6	C	4,998	3,735	C	4,429	C	9.00%	52.90%	9.90%
17000064	SR 777	River Rd	N. of West Villages Pkwy	1.800	Center Rd	3.219	1.419	6	D	5,208	2,502	C	6	D	5,208	3,735	C	4,609	C	9.00%	52.90%	9.90%
17000064	SR 777	River Rd	Center Rd	3.219	I-75	5.819	2.600	4	D	3,528	2,502	C	4	D	3,528	3,735	F	4,245	F	9.00%	52.90%	9.90%
17000709	roe Ave Exten	Honroe Ave Extension	Laurel Road	0.000	SR 681	3.746	3.746	2	D	2,227	729	C	2	D	2,227	837	C	941	C	9.00%	52.90%	6.30%

**Legend**  
 Aggregated segment    
 Single count segment    
 OFF System SIS Facility

\*\* Future Model Volumes obtained from D1 Regional Planning Model are found to be less than Existing AADT.

**YEAR 2022 SARASOTA COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR PEAK DIRECTION**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	SIS	Existing Context Class	Future Context Class	Functional Classification	Posted Speed	Area Type	Facility Type	FDOT LOS Std.	County LOS Std.	City LOS Std.	Year 2022										Deficiency Determination
																		Divided/UnDivided	One/Two Way	Left Turn Bays	Right Turn Bays	Thru Lanes	Peak Hour Peak Direction					
																							Capacity	Volume	LOS			
17005000	SR 681	VENICE CONNECTOR	SR 45/US41	0.000	US 41 Ramp	0.327	0.327		C2	C2	Principal Arterial-other	45	UA	H	D	D		U	1W	0L	0R	2	3,492	521	B			
17005000	SR 681	VENICE CONNECTOR	US 41 Ramp	0.327	I-75 Ramp	3.158	2.831		C2	C2	Principal Arterial-other	55	UA	H	D	D		D	2W	WL	WR	4	2,910	733	B			
17005000	SR 681	VENICE CONNECTOR	I-75 Ramp	3.158	I-75/SR 93	4.142	0.984	SIS Link	C2	C2	Principal Arterial-other	70	RDA	H	C	C		U	1W	0L	0R	2	2,868	1,518	B			
17008000	SR 758	BEE RIDGE ROAD	SR 45/US 41	0.000	Beneva Rd	2.026	2.026		C4	C4	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	2,950	1,549	C			
17008000	SR 758	BEE RIDGE ROAD	Beneva Rd	2.026	McIntosh Rd	3.044	1.018		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	0R	6	2,680	1,952	C			
17008000	SR 758	BEE RIDGE ROAD	McIntosh Rd	3.044	Honore Ave	4.056	1.012		C3R	C3R	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	4	1,942	1,833	D	Near Capacity		
17008000	SR 758	BEE RIDGE ROAD	Honore Ave	4.056	I-75 Ramp	5.066	1.010		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	4	1,900	1,785	D	Near Capacity		
17008000	SR 758	BEE RIDGE ROAD	I-75 Ramp	5.066	R/W Line	5.432	0.366		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	0L	WR	4	1,900	1,381	C			
17010000	US 41	TAMIAMI TRAIL	Charlotte County Line	0.000	100 Ft E. of Tuscola Blvd	1.205	1.205		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	2,814	1,782	C			
17010000	US 41	TAMIAMI TRAIL	100 Ft E. of Tuscola Blvd	1.205	SR 777/River Rd	6.138	4.933		C3R	C3R	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	1,942	1,782	C	Near Capacity		
17010000	US 41	TAMIAMI TRAIL	SR 777/River Rd	6.138	Rockley Blvd	9.878	3.740		C3R	C3R	Principal Arterial-other	55	TA	A	C	C	D	D	2W	WL	WR	4	1,785	1,104	C			
17010000	US 41	TAMIAMI TRAIL	Rockley Blvd	9.878	SR 776/Park Pl	12.826	2.948		C3R	C3R	Principal Arterial-other	55	UA	A	D	D		D	2W	WL	WR	4	1,942	1,484	C			
17010000	US 41	TAMIAMI TRAIL	SR 776/Park Pl	12.826	SR 45A/Venice Bypass	14.882	2.056		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	2,814	2,447	C			
17010000	USB 41	TAMIAMI TRAIL	SR 45A/Venice Bypass	14.882	CR 772/Venice Ave	17.155	2.273		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	1,900	1,473	C			
17020000	USB 41	US-41/TAMIAMI TRAIL	CR 772/Venice Ave	0.000	SR 45A/US 41	0.638	0.638		C3R	C3R	Principal Arterial-other	40	UA	A	D	D	D	D	2W	WL	WR	4	1,942	893	C			
17020000	US 41	US-41/TAMIAMI TRAIL	SR 45A/US 41	0.638	Laurel Rd	2.894	2.256		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	2,814	2,186	C			
17020000	US 41	US-41/TAMIAMI TRAIL	Laurel Rd	2.894	SR 681 Ramp	3.421	0.527		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	2,814	2,376	C			
17020000	US 41	TAMIAMI TRAIL	SR 681 Ramp	3.421	CR 789/Old Venice Rd	6.134	2.713		C3C	C3C	Principal Arterial-other	55	UA	A	D	D		D	2W	WL	WR	4	1,900	1,663	D			
17020000	US 41	US-41/TAMIAMI TRAIL	CR 789/Old Venice Rd	6.134	CR 773/Beneva Rd	10.029	3.895		C3C	C3C	Principal Arterial-other	55	UA	A	D	D		D	2W	WL	WR	4	1,900	2,020	F	Over Capacity		
17020000	US 41	US-41/TAMIAMI TRAIL	CR 773/Beneva Rd	10.029	Buccaneer Dr	11.716	1.687		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	4	1,900	1,581	C			
17020000	US 41	US-41/TAMIAMI TRAIL	Buccaneer Dr	11.716	CR 72 A/Proctor Rd	14.299	2.583		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	2,814	2,117	C			
** 17020000	US 41	US-41/TAMIAMI TRAIL	CR 72 A/Proctor Rd	14.299	Webber St	15.930	1.631		C4	C4	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	2,950	2,295	C			
** 17020000	US 41	US-41/TAMIAMI TRAIL	Webber St	15.930	SR 683/US 301	17.371	1.441		C4	C4	Principal Arterial-other	40	UA	A	D	D	D	D	2W	WL	WR	6	2,950	2,756	D	Near Capacity		
17020000	US 41	MOUND ST	SR 683/US 301	17.371	SR 789/Gulf Stream Ave	18.642	1.271		C4	C4	Principal Arterial-other	35	UA	A	D	D	D	D	2W	WL	WR	4	1,880	999	C			
17020000	US 41	N TAMIAMI TRAIL	SR 789/Gulf Stream Ave	18.642	University Pkwy	21.879	3.237		C4	C4	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	4	1,880	1,534	D			
17020000	US 41	N TAMIAMI TRAIL	University Pkwy	21.879	Manatee County Line	22.39	0.511		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	5	2,357	1,853	C			
17030000	SR 789	RINGLING BLVD/ST.ARMANDS CIR	SR 45/Tamiami Trail	0.000	Madison Dr	2.368	2.368		C3R	C3R	Minor Arterial	35	UA	A	D	D	D	D	2W	WL	WR	4	1,942	1,721	C			
17030000	SR 789	JOHN-RINGLING PKWY	Madison Dr	2.368	Harborside Dr	5.987	3.619		C3R	C3R	Major Collector	45	UA	A	D	D	E	D	2W	WL	WR	2	1,224	837	C			
17030000	SR 789	GULF OF MEXICO DR	Harborside Dr	5.987	Manatee County Line	8.957	2.970		C3R	C3R	Major Collector	45	UA	A	D	D	E	D	2W	WL	WR	2	1,224	618	C			
17030201	SR 789	GULF STREAM AVE	Sunset Dr	0.000	Bird Key Dr	1.006	1.006		C3R	C3R	Minor Arterial	40	UA	A	D	D	D	D	2W	WL	0R	4	1,850	1,745	D	Near Capacity		
17030401	SR 789	JOHN RINGLING PKWY	Westway Pl	0.000	Long Boat Club Rd	0.648	0.648		C3R	C3R	Major Collector	35	UA	A	D	D	D	D	2W	WL	WR	2	1,224	924	C			
17040000	SR 780	FRUITVILLE RD/SR 780	School Ave	0.381	CR 773/ Beneva Rd	2.134	1.753		C4	C4	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	2,950	2,210	C			
17040000	SR 780	FRUITVILLE RD/SR 780	CR 773/ Beneva Rd	2.134	Honore Ave	4.203	2.069		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	2,814	2,452	C			
17040000	SR 780	FRUITVILLE RD/SR 780	Honore Ave	4.203	State R/W Line	5.692	1.489		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	2,814	2,833	F	Over Capacity		

Note: LOS Spreadsheet should be used as a planning level analysis tool. A detailed analysis is necessary to validate the actual operating conditions (and for future conditions, as needed) which may vary from this worksheet.

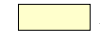


**YEAR 2022 SARASOTA COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR PEAK DIRECTION**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	Year 2028 (E+C)					Year 2045					Existing K Factor	Existing D Factor	Existing T <sub>24</sub> Factor		
								Thru Lanes	LOS Std	Peak Hour Peak Direction			Thru Lanes	LOS Std	Peak Hour Peak Direction							
										Capacity	Volume	LOS			Capacity	Trend Volume	Trend LOS				Model Volume	Model LOS
17005000	SR 681	VENICE CONNECTOR	SR 45/US41	0.000	US 41 Ramp	0.327	0.327	2	D	3,492	549	B	2	D	3,492	639	B	1,006	B	9.00%	99.90%	9.30%
17005000	SR 681	VENICE CONNECTOR	US 41 Ramp	0.327	I-75 Ramp	3.158	2.831	4	D	2,910	866	B	4	D	2,910	1,163	B	1,538	B	9.00%	52.90%	9.30%
17005000	SR 681	VENICE CONNECTOR	I-75 Ramp	3.158	I-75/SR 93	4.142	0.984	2	C	2,868	2,052	C	2	C	2,868	3,582	E	1,639	B	9.50%	99.90%	9.30%
17008000	SR 758	BEE RIDGE ROAD	SR 45/US 41	0.000	Beneva Rd	2.026	2.026	6	D	2,950	1,707	C	6	D	2,950	1,980	C	2,391	D	9.00%	52.90%	7.30%
17008000	SR 758	BEE RIDGE ROAD	Beneva Rd	2.026	McIntosh Rd	3.044	1.018	6	D	2,680	2,153	C	6	D	2,680	2,495	D	2,659	D	9.00%	52.90%	7.30%
17008000	SR 758	BEE RIDGE ROAD	McIntosh Rd	3.044	Honore Ave	4.056	1.012	4	D	1,942	2,020	F	4	D	1,942	2,346	F	2,436	F	9.00%	52.90%	7.30%
17008000	SR 758	BEE RIDGE ROAD	Honore Ave	4.056	I-75 Ramp	5.066	1.010	4	D	1,900	1,970	F	4	D	1,900	2,282	F	2,824	F	9.00%	52.90%	4.80%
17008000	SR 758	BEE RIDGE ROAD	I-75 Ramp	5.066	R/W Line	5.432	0.366	4	D	1,900	1,520	C	4	D	1,900	1,767	D	3,127	F	9.00%	52.90%	5.00%
17010000	US 41	TAMIAMI TRAIL	Charlotte County Line	0.000	100 Ft E. of Tuscola Blvd	1.205	1.205	6	D	2,814	2,034	C	6	D	2,814	2,544	D	2,473	C	9.00%	52.80%	4.60%
17010000	US 41	TAMIAMI TRAIL	100 Ft E. of Tuscola Blvd	1.205	SR 777/River Rd	6.138	4.933	4	D	1,942	2,034	F	4	D	1,942	2,544	F	2,570	F	9.00%	52.80%	4.60%
17010000	US 41	TAMIAMI TRAIL	SR 777/River Rd	6.138	Rockley Blvd	9.878	3.740	4	C	1,785	1,254	C	4	C	1,785	1,547	C	2,576	F	9.50%	52.80%	4.60%
17010000	US 41	TAMIAMI TRAIL	Rockley Blvd	9.878	SR 776/Park Pl	12.826	2.948	4	D	1,942	1,646	C	4	D	1,942	1,933	D	2,170	F	9.00%	52.80%	3.33%
17010000	US 41	TAMIAMI TRAIL	SR 776/Park Pl	12.826	SR 45A/Venice Bypass	14.882	2.056	6	D	2,814	2,703	D	6	D	2,814	3,133	F	3,682	F	9.00%	52.80%	2.80%
17010000	USB 41	TAMIAMI TRAIL	SR 45A/Venice Bypass	14.882	CR 772/Venice Ave	17.155	2.273	4	D	1,900	1,772	D	4	D	1,900	2,435	F	1,637	D	9.00%	52.80%	2.40%
17020000	USB 41	US-41/TAMIAMI TRAIL	CR 772/Venice Ave	0.000	SR 45A/US 41	0.638	0.638	4	D	1,942	985	C	4	D	1,942	1,143	C	1,200	C	9.00%	52.80%	2.60%
17020000	US 41	US-41/TAMIAMI TRAIL	SR 45A/US 41	0.638	Laurel Rd	2.894	2.256	6	D	2,814	2,416	C	6	D	2,814	2,802	D	2,978	F	9.00%	52.80%	3.50%
17020000	US 41	US-41/TAMIAMI TRAIL	Laurel Rd	2.894	SR 681 Ramp	3.421	0.527	6	D	2,814	2,624	D	6	D	2,814	3,044	F	3,183	F	9.00%	52.80%	4.50%
17020000	US 41	TAMIAMI TRAIL	SR 681 Ramp	3.421	CR 789/Old Venice Rd	6.134	2.713	4	D	1,900	1,836	D	4	D	1,900	2,133	F	2,284	F	9.00%	52.80%	4.90%
17020000	US 41	US-41/TAMIAMI TRAIL	CR 789/Old Venice Rd	6.134	CR 773/Beneva Rd	10.029	3.895	4	D	1,900	2,232	F	4	D	1,900	2,589	F	2,496	F	9.00%	52.80%	3.40%
17020000	US 41	US-41/TAMIAMI TRAIL	CR 773/Beneva Rd	10.029	Buccaneer Dr	11.716	1.687	4	D	1,900	1,747	D	4	D	1,900	2,025	F	2,485	F	9.00%	52.80%	3.20%
17020000	US 41	US-41/TAMIAMI TRAIL	Buccaneer Dr	11.716	CR 72 A/Proctor Rd	14.299	2.583	6	D	2,814	2,338	C	6	D	2,814	2,714	D	2,189	C	9.00%	52.80%	3.11%
** 17020000	US 41	US-41/TAMIAMI TRAIL	CR 72 A/Proctor Rd	14.299	Webber St	15.930	1.631	6	D	2,950	2,534	D	6	D	2,950	2,941	D	2,274	C	9.00%	52.80%	2.80%
** 17020000	US 41	US-41/TAMIAMI TRAIL	Webber St	15.930	SR 683/US 301	17.371	1.441	6	D	2,950	3,044	E	6	D	2,950	3,529	F	2,437	D	9.00%	52.80%	2.80%
17020000	US 41	MOUND ST	SR 683/US 301	17.371	SR 789/Gulf Stream Ave	18.642	1.271	4	D	1,880	1,102	C	4	D	1,880	1,281	D	1,628	D	9.00%	52.80%	2.83%
17020000	US 41	N TAMIAMI TRAIL	SR 789/Gulf Stream Ave	18.642	University Pkwy	21.879	3.237	4	D	1,880	1,695	D	4	D	1,880	1,965	E	2,065	E	9.00%	52.80%	2.99%
17020000	US 41	N TAMIAMI TRAIL	University Pkwy	21.879	Manatee County Line	22.39	0.511	5	D	2,357	2,044	D	5	D	2,357	2,376	F	2,165	D	9.00%	52.80%	3.00%
17030000	SR 789	RINGLING BLVD/ST.ARMANDS CIR	SR 45/Tamiami Trail	0.000	Madison Dr	2.368	2.368	4	D	1,942	1,792	D	4	D	1,942	2,162	F	2,030	F	9.00%	62.37%	4.30%
17030000	SR 789	JOHN-RINGLING PKWY	Madison Dr	2.368	Harborside Dr	5.987	3.619	2	D	1,224	978	C	2	D	1,224	1,257	F	1,212	D	9.00%	52.40%	5.20%
17030000	SR 789	GULF OF MEXICO DR	Harborside Dr	5.987	Manatee County Line	8.957	2.970	2	D	1,224	747	C	2	D	1,224	1,025	C	948	C	9.00%	52.40%	3.60%
17030201	SR 789	GULF STREAM AVE	Sunset Dr	0.000	Bird Key Dr	1.006	1.006	4	D	1,850	1,945	F	4	D	1,850	2,267	F	2,167	F	9.00%	52.40%	3.70%
17030401	SR 789	JOHN RINGLING PKWY	Westway Pl	0.000	Long Boat Club Rd	0.648	0.648	2	D	1,224	1,030	C	2	D	1,224	1,193	D	1,396	F	9.00%	52.40%	3.60%
17040000	SR 780	FRUITVILLE RD/SR 780	School Ave	0.381	CR 773/ Beneva Rd	2.134	1.753	6	D	2,950	2,435	D	6	D	2,950	2,827	D	2,863	D	9.00%	52.90%	3.65%
17040000	SR 780	FRUITVILLE RD/SR 780	CR 773/ Beneva Rd	2.134	Honore Ave	4.203	2.069	6	D	2,814	2,708	D	6	D	2,814	3,158	F	2,734	D	9.00%	52.90%	4.20%
17040000	SR 780	FRUITVILLE RD/SR 780	Honore Ave	4.203	State R/W Line	5.692	1.489	6	D	2,814	3,123	F	6	D	2,814	3,623	F	3,603	F	9.00%	52.90%	4.20%

Note: LOS Spreadsheet should be used as a planning level analysis tool. A detailed analysis is necessary to validate the actual operating conditions (and for future conditions, as needed) which may vary from this worksheet.

**YEAR 2022 SARASOTA COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR PEAK DIRECTION**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	SIS	Existing Context Class	Future Context Class	Functional Classification	Posted Speed	Area Type	Facility Type	FDOT LOS Std.	County LOS Std.	City LOS Std.	Year 2022										Deficiency Determination
																		Divided/UnDivided	One/Two Way	Left Turn Bays	Right Turn Bays	Thru Lanes	Peak Hour Peak Direction					
																							Capacity	Volume	LOS			
17040401	SR 780	FRUITVILLE RD	Washington Blvd	0.000	School Ave	0.325	0.325		C4	C5	Principal Arterial-other	40	UA	A	D	D	D	D	2W	WL	WR	6	2,950	1,452	C			
17050000	SR 776	INDIANA AVE SOUTH	Charlotte County Line	0.000	SR 777/Dearborn St	1.108	1.108		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	4	1,900	1,108	C			
17050000	SR 776	INDIANA AVE N	SR 777/Dearborn St	1.108	Old Englewood Rd	3.432	2.324		C3R	C3R	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	4	1,942	1,061	C			
17050000	SR 776	ENGLEWOOD RD	Old Englewood Rd	3.432	Fire Station Ent.	4.474	1.042		C3R	C3R	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	4	1,942	1,061	C			
17050000	SR 776	ENGLEWOOD RD	Fire Station Ent.	4.474	Manasota Beach Rd	5.890	1.416		C3R	C3R	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	2,866	1,061	C			
17050000	SR 776	ENGLEWOOD RD	Manasota Beach Rd	5.890	Jacaranda Blvd	7.496	1.606		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	2,814	1,721	C			
17050000	SR 776	ENGLEWOOD RD	Jacaranda Blvd	7.496	SR 45/US 41	8.222	0.726		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	2,814	924	C			
17070000	SR 72	CLARK RD	SR 45/US 41	0.000	CR 773/ Beneva Rd	1.803	1.803		C3R	C4	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	2,866	1,639	C			
17070000	SR 72	CLARK RD	CR 773/ Beneva Rd	1.803	I-75	4.796	2.993		C3C	C3C	Minor Arterial	45	UA	A	D	D		D	2W	WL	WR	6	2,814	2,047	C			
17070000	SR 72	CLARK RD	I-75	4.796	Lorraine Rd	7.967	3.171		C3C	C3R	Minor Arterial	45	UA	H	D	D		D	2W	WL	WR	2	1,180	598	C			
17070000	SR 72	SR 72	Lorraine Rd	7.967	Myakka Valley Tr	11.769	3.802		C2	C2	Minor Arterial	55	RDA	H	C	C		D	2W	WL	WR	2	452	369	C			
17070000	SR 72	SR 72	Myakka Valley Tr	11.769	Myakka River State Park	13.502	1.733		C1	C1	Minor Arterial	55	RDA	H	C	C		U	2W	WL	OR	2	430	179	B			
17070000	SR 72	SR 72	Myakka River State Park	13.502	DeSoto County Line	30.591	17.089		C2	C2	Minor Arterial	60	RDA	H	C	C		U	2W	OL	OR	2	430	142	B			
17075000	I-75	SR 93/I-75	Charlotte County Line	0.000	Toledo Blade Blvd	8.043	8.043	SIS	LA	LA	Principal Arterial-Interstate	70	TA	F	C	C	D	D	2W		OA	6	4,670	2,681	B			
17075000	I-75	SR 93/I-75	Toledo Blade Blvd	8.043	Sumter Blvd	10.987	2.944	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	D	D	D	2W		OA	6	6,080	3,502	B			
17075000	I-75	SR 93/I-75	Sumter Blvd	10.987	River Rd	20.053	9.066	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C	D	D	2W		OA	6	3,990	3,478	C			
17075000	I-75	SR 93/I-75	River Rd	20.053	Jacaranda Blvd	22.288	2.235	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C	C	D	2W		OA	6	3,990	4,485	D	Over Capacity		
17075000	I-75	SR 93/I-75	Jacaranda Blvd	22.288	Laurel Rd	24.794	2.506	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	D	D	D	2W		OA	6	6,080	4,773	C			
17075000	I-75	SR 93/I-75	Laurel Rd	24.794	SR 681/Venice Connector	29.405	4.611	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C	C	D	2W		OA	6	3,990	5,205	E	Over Capacity		
17075000	I-75	SR 93/I-75	SR 681/Venice Connector	29.405	SR 72/Clark Rd	34.357	4.952	SIS	LA	LA	Principal Arterial-Interstate	70	RDA	F	C	C		D	2W		OA	6	3,990	5,469	E	Over Capacity		
17075000	I-75	SR 93/I-75	SR 72/Clark Rd	34.357	SR 758/Bee Ridge Rd	36.424	2.067	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	D		D	2W		OA	6	6,080	5,648	D	Near Capacity		
17075000	I-75	SR 93/I-75	SR 758/Bee Ridge Rd	36.424	SR 780/Fruitville Rd	39.139	2.715	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	D		D	2W		OA	6	6,080	6,668	F	Over Capacity		
17075000	I-75	SR 93/I-75	SR 780/Fruitville Rd	39.139	Manatee County Line/University P	42.615	3.476	SIS	LA	LA	Principal Arterial-Interstate	70	UA	F	D	D		D	2W		WA	6	7,080	6,956	D	Near Capacity		
17080000	SR 758	MIDNIGHT PASS RD	Stickney Point Rd	0.000	CR 789A/Beach Rd	1.238	1.238		C3R	C4	Major Collector	35	UA	A	D	D		D	2W	WL	OR	2	1,166	839	C			
** 17080000	SR 758	MIDNIGHT PASS RD	CR 789A/Beach Rd	1.238	Higel Ave	3.051	1.813		C3R	C3R	Major Collector	40	UA	A	D	D		U	2W	OL	WR	2	932	340	C			
17080000	SR 758	OSPREY AVE/BAY RD/SIESTA DR	Higel Ave	3.051	SR 45/US 41	5.771	2.720		C3R	C4	Major Collector	40	UA	A	D	D	D	U	2W	WL	WR	2	1,166	765	C			
17110000	SR 72	STICKNEY POINT RD	SR 758/Midnight Pass RD	0.000	SR 45/US 41	0.980	0.980		C4	C4	Major Collector	45	UA	A	D	D		D	2W	WL	WR	4	1,880	1,179	C			
17120000	US 301	US-301/WASHINGTON BLVD	SR 45/US 41/Tamiami Tr	0.000	17th St	1.644	1.644		C4	C4	Principal Arterial-other	35	UA	A	D	D	D	D	2W	WL	WR	4	1,880	1,724	D	Near Capacity		
** 17120000	US 301	US-301/WASHINGTON BLVD	17th St	1.644	Manatee County Line/University P	4.184	2.540		C3C	C3C	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	6	2,814	2,133	C			
17180000	US 41	VENICE BYPASS	SR 45/US 41 Bus	0.106	SR 45/US 41 Bus	3.074	2.968		C3C	C3C	Principal Arterial-other	45	UA	A	D	D	D	D	2W	WL	WR	6	2,814	1,672	C			
17192000	Univ. Pkwy.	UNIVERSITY PKWY	Airport Entrance	0.269	US 301	1.721	1.452	SC	C4	C4	Principal Arterial-other	50	UA	A	D	D	D	D	2W	WL	WR	4	1,880	1,247	C			
17192000	Univ. Pkwy.	UNIVERSITY PKWY	US 301	1.721	E. of Lockwood Ridge Rd	3.321	1.600	SC	C3C	C3C	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	6	2,814	2,261	C			
17192000	Univ. Pkwy.	UNIVERSITY PKWY	E. of Lockwood Ridge Rd	3.321	Honore Ave	5.887	2.566	SC	C3R	C3R	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	6	2,866	2,833	D	Near Capacity		
17192000	Univ. Pkwy.	UNIVERSITY PKWY	Honore Ave	5.887	I-75 NB Ramp	6.945	1.058	SC	C3C	C3C	Principal Arterial-other	50	UA	A	D	D		D	2W	WL	WR	10	3,680	2,928	C			
17000064	SR 777	River Rd	US 41	0.000	N. of West Villages Pkwy	1.800	1.800		C3R	C3R	Principal Arterial-other	45	RDA	A	C	D		D	2W	WL	WR	2	1,069	1,095	D	Over Capacity		
17000064	SR 777	River Rd	N. of West Villages Pkwy	1.800	Center Rd	3.219	1.419		C3R	C3R	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	2	1,224	1,095	D			
17000064	SR 777	River Rd	Center Rd	3.219	I-75	5.819	2.600		C3R	C3R	Principal Arterial-other	45	UA	A	D	D		D	2W	WL	WR	2	1,224	1,095	D			
17000709	roe Ave Exten	Honroe Ave Extension	Laurel Road	0.000	SR 681	3.746	3.746		C3R	C3R	Major Collector	45	UA	H	D	D		D	2W	WL	WR	2	1,224	362	C			

**Legend**  
 Aggregated segment   
 Single count segment   
 OFF System SIS Facility

\*\* Future Model Volumes obtained from D1 Regional Planning Model are found to be less than Existing AADT.



**YEAR 2022 SARASOTA COUNTY LEVEL OF SERVICE SPREADSHEET -PEAK HOUR PEAK DIRECTION**

Section No.	State Road No.	Local Road Name	From	From M.P.	To	To M.P.	Section Length	Year 2028 (E+C)					Year 2045					Existing K Factor	Existing D Factor	Existing T <sub>24</sub> Factor		
								Thru Lanes	LOS Std	Peak Hour Peak Direction			Thru Lanes	LOS Std	Peak Hour Peak Direction							
										Capacity	Volume	LOS			Capacity	Trend Volume	Trend LOS				Model Volume	Model LOS
1704041	SR 780	FRUITVILLE RD	Washington Blvd	0.000	School Ave	0.325	0.325	6	D	2,950	1,599	C	6	D	2,804	1,856	D	2,053	D	9.00%	52.90%	3.90%
17050000	SR 776	INDIANA AVE SOUTH	Charlotte County Line	0.000	SR 777/Dearborn St	1.108	1.108	4	D	1,900	1,233	C	4	D	1,900	1,431	C	1,485	C	9.00%	52.40%	4.40%
17050000	SR 776	INDIANA AVE N	SR 777/Dearborn St	1.108	Old Englewood Rd	3.432	2.324	4	D	1,942	1,183	C	4	D	1,942	1,371	C	1,655	C	9.00%	52.40%	4.20%
17050000	SR 776	ENGLEWOOD RD	Old Englewood Rd	3.432	Fire Station Ent.	4.474	1.042	4	D	1,942	1,183	C	6	D	2,866	1,371	C	1,971	C	9.00%	52.40%	4.20%
17050000	SR 776	ENGLEWOOD RD	Fire Station Ent.	4.474	Manasota Beach Rd	5.890	1.416	6	D	2,866	1,183	C	6	D	2,866	1,371	C	2,305	C	9.00%	52.40%	4.20%
17050000	SR 776	ENGLEWOOD RD	Manasota Beach Rd	5.890	Jacaranda Blvd	7.496	1.606	6	D	2,814	1,940	C	6	D	2,814	2,327	C	3,533	F	9.00%	52.40%	4.00%
17050000	SR 776	ENGLEWOOD RD	Jacaranda Blvd	7.496	SR 45/US 41	8.222	0.726	6	D	2,814	1,030	C	6	D	2,814	1,193	C	1,954	C	9.00%	52.40%	4.80%
17070000	SR 72	CLARK RD	SR 45/US 41	0.000	CR 773/ Beneva Rd	1.803	1.803	6	D	2,866	1,809	C	6	D	2,950	2,098	C	2,301	C	9.00%	52.90%	3.32%
17070000	SR 72	CLARK RD	CR 773/ Beneva Rd	1.803	I-75	4.796	2.993	6	D	2,814	2,257	C	6	D	2,814	2,619	D	3,268	F	9.00%	52.90%	5.00%
17070000	SR 72	CLARK RD	I-75	4.796	Lorraine Rd	7.967	3.171	2	D	1,180	683	C	2	D	1,224	936	C	1,288	F	9.00%	55.40%	8.40%
17070000	SR 72	SR 72	Lorraine Rd	7.967	Myakka Valley Tr	11.769	3.802	2	C	452	426	C	2	C	452	599	D	1,416	E	9.00%	55.40%	13.70%
17070000	SR 72	SR 72	Myakka Valley Tr	11.769	Myakka River State Park	13.502	1.733	2	C	430	225	B	2	C	430	355	C	1,032	E	9.50%	55.40%	15.00%
17070000	SR 72	SR 72	Myakka River State Park	13.502	DeSoto County Line	30.591	17.089	2	C	430	183	B	2	C	430	293	C	170	B	9.50%	55.40%	15.30%
17075000	I-75	SR 93/I-75	Charlotte County Line	0.000	Toledo Blade Blvd	8.043	8.043	6	C	4,670	3,326	B	6	C	4,670	4,693	D	3,748	C	9.00%	51.80%	11.80%
17075000	I-75	SR 93/I-75	Toledo Blade Blvd	8.043	Sumter Blvd	10.987	2.944	6	D	6,080	4,237	C	6	D	6,080	6,004	D	4,394	C	9.00%	53.30%	11.80%
17075000	I-75	SR 93/I-75	Sumter Blvd	10.987	River Rd	20.053	9.066	6	C	3,990	4,247	D	6	C	3,990	6,118	F	5,450	E	9.00%	53.30%	11.60%
17075000	I-75	SR 93/I-75	River Rd	20.053	Jacaranda Blvd	22.288	2.235	6	C	3,990	5,465	E	10	C	6,926	7,836	D	6,929	D	9.00%	53.30%	9.40%
17075000	I-75	SR 93/I-75	Jacaranda Blvd	22.288	Laurel Rd	24.794	2.506	6	D	6,080	5,668	D	10	D	10,231	7,762	C	7,925	C	9.00%	53.30%	9.40%
17075000	I-75	SR 93/I-75	Laurel Rd	24.794	SR 681/Venice Connector	29.405	4.611	6	C	3,990	6,098	F	10	C	6,926	8,160	D	9,877	F	10.50%	53.30%	10.50%
17075000	I-75	SR 93/I-75	SR 681/Venice Connector	29.405	SR 72/Clark Rd	34.357	4.952	6	C	3,990	6,371	F	10	C	6,926	8,430	E	8,992	E	9.00%	53.30%	9.40%
17075000	I-75	SR 93/I-75	SR 72/Clark Rd	34.357	SR 758/Bee Ridge Rd	36.424	2.067	6	D	6,080	6,717	F	10	D	10,231	8,925	D	9,081	D	9.00%	52.30%	8.00%
17075000	I-75	SR 93/I-75	SR 758/Bee Ridge Rd	36.424	SR 780/Fruitville Rd	39.139	2.715	6	D	6,080	7,544	F	10	D	10,231	9,415	D	9,675	D	9.00%	53.30%	8.10%
17075000	I-75	SR 93/I-75	SR 780/Fruitville Rd	39.139	Manatee County Line/University P	42.615	3.476	6	D	7,080	7,979	F	10	D	10,231	10,256	E	9,631	D	9.00%	53.30%	11.60%
17080000	SR 758	MIDNIGHT PASS RD	Stickney Point Rd	0.000	CR 789A/ Beach Rd	1.238	1.238	2	D	1,166	936	C	2	D	914	1,084	E	1,481	F	9.00%	52.40%	3.00%
** 17080000	SR 758	MIDNIGHT PASS RD	CR 789A/ Beach Rd	1.238	Higel Ave	3.051	1.813	2	D	932	376	C	2	D	932	441	C	311	C	9.00%	52.40%	4.20%
17080000	SR 758	OSPREY AVE/BAY RD/SIESTA DR	Higel Ave	3.051	SR 45/US 41	5.771	2.720	2	D	1,166	851	C	2	D	914	990	F	1,176	F	9.00%	52.40%	3.53%
17110000	SR 72	STICKNEY POINT RD	SR 758/Midnight Pass RD	0.000	SR 45/US 41	0.980	0.980	4	D	1,880	1,312	D	4	D	1,880	1,525	D	2,120	E	9.00%	52.40%	2.80%
17120000	US 301	US-301/WASHINGTON BLVD	SR 45/US 41/Tamiami Tr	0.000	17th St	1.644	1.644	4	D	1,880	1,913	E	4	D	1,880	2,240	F	1,815	D	9.00%	52.80%	4.56%
** 17120000	US 301	US-301/WASHINGTON BLVD	17th St	1.644	Manatee County Line/University P	4.184	2.540	6	D	2,814	2,370	C	6	D	2,814	2,787	D	2,040	C	9.00%	52.80%	5.05%
17180000	US 41	VENICE BYPASS	SR 45/US 41 Bus	0.106	SR 45/US 41 Bus	3.074	2.968	6	D	2,814	1,845	C	6	D	2,814	2,142	C	2,491	D	9.00%	52.80%	3.98%
17192000	Univ. Pkwy.	UNIVERSITY PKWY	Airport Entrance	0.269	US 301	1.721	1.452	4	D	1,880	1,384	D	4	D	1,880	1,639	D	1,301	D	9.00%	52.90%	3.29%
17192000	Univ. Pkwy.	UNIVERSITY PKWY	US 301	1.721	E. of Lockwood Ridge Rd	3.321	1.600	6	D	2,814	2,559	D	6	D	2,814	3,153	F	2,458	C	9.00%	52.90%	4.00%
17192000	Univ. Pkwy.	UNIVERSITY PKWY	E. of Lockwood Ridge Rd	3.321	Honore Ave	5.887	2.566	6	D	2,866	3,123	F	6	D	2,866	3,623	F	3,233	F	9.00%	52.90%	4.40%
17192000	Univ. Pkwy.	UNIVERSITY PKWY	Honore Ave	5.887	I-75 NB Ramp	6.945	1.058	10	D	3,680	3,227	C	10	D	3,680	3,742	C	3,732	C	9.00%	52.90%	3.70%
17000064	SR 777	River Rd	US 41	0.000	N. of West Villages Pkwy	1.800	1.800	6	C	2,751	1,376	C	6	C	2,751	2,054	C	2,436	C	9.00%	52.90%	9.90%
17000064	SR 777	River Rd	N. of West Villages Pkwy	1.800	Center Rd	3.219	1.419	6	D	2,866	1,376	C	6	D	2,866	2,054	C	2,535	C	9.00%	52.90%	9.90%
17000064	SR 777	River Rd	Center Rd	3.219	I-75	5.819	2.600	4	D	1,942	1,376	C	4	D	1,942	2,054	F	2,335	F	9.00%	52.90%	9.90%
17000709	roe Ave Exten	Honroe Ave Extension	Laurel Road	0.000	SR 681	3.746	3.746	2	D	1,224	401	C	2	D	1,224	460	C	517	C	9.00%	52.90%	6.30%

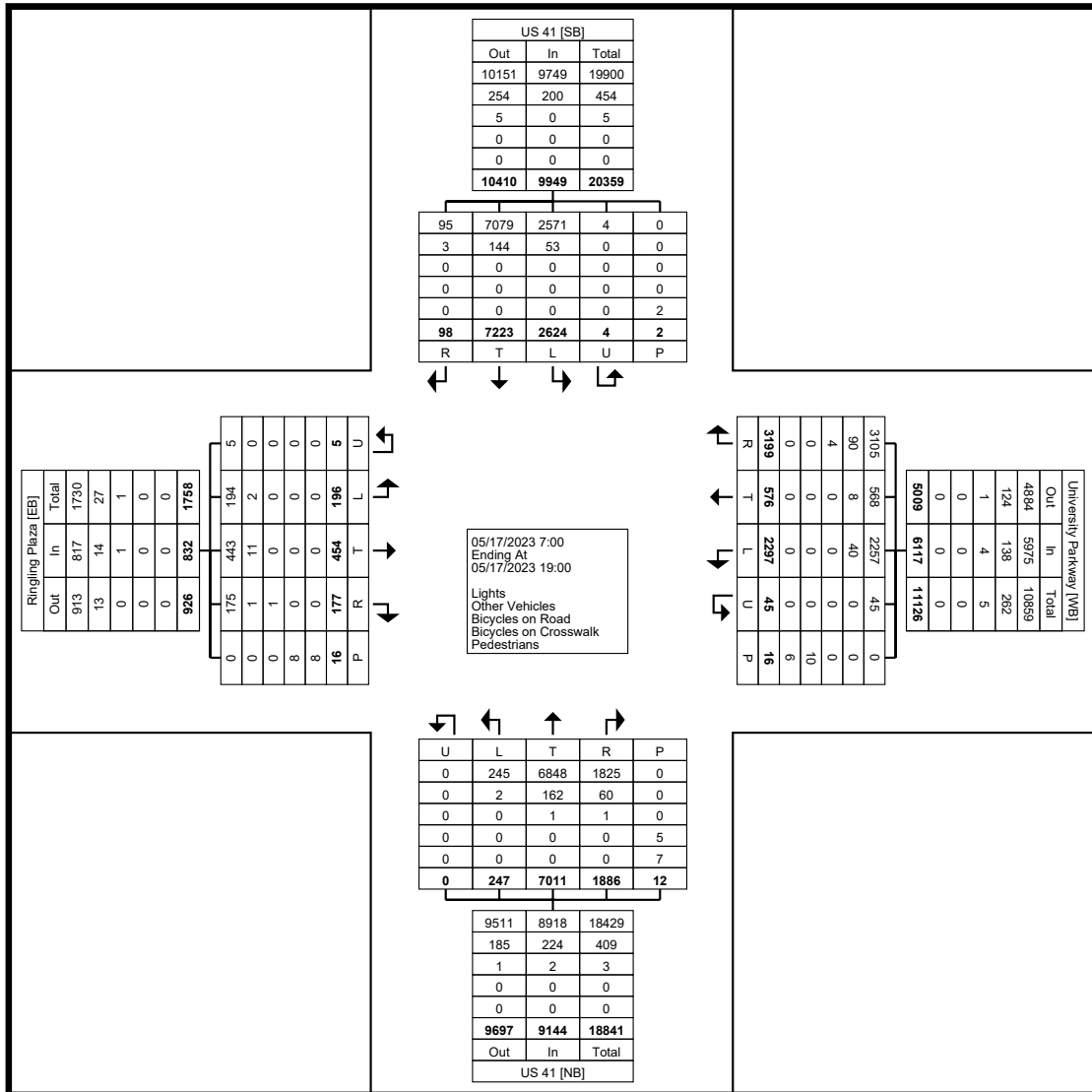
**Legend**  
 Aggregated segment   
 Single count segment   
 OFF System SIS Facility

\*\* Future Model Volumes obtained from D1 Regional Planning Model are found to be less than Existing AADT.

**Raw Counts**

### Turning Movement Data

Start Time	Ringling Plaza Eastbound						University Parkway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	2	6	1	0	9	1	55	9	63	0	128	0	2	117	27	0	146	0	109	315	3	1	427	710
7:15	1	1	4	2	0	8	1	70	7	88	0	166	0	5	132	29	0	166	0	107	437	3	0	547	887
7:30	0	4	10	0	0	14	0	85	9	134	0	228	0	2	176	40	0	218	0	129	457	3	0	589	1049
7:45	0	3	4	0	0	7	0	82	15	108	0	205	0	3	129	34	0	166	0	88	329	1	1	418	796
Hourly Total	1	10	24	3	0	38	2	292	40	393	0	727	0	12	554	130	0	696	0	433	1538	10	2	1981	3442
8:00	0	5	10	1	0	16	4	74	19	77	1	174	0	1	136	13	0	150	0	89	390	6	0	485	825
8:15	0	9	8	3	1	20	0	85	16	101	1	202	0	3	141	28	1	172	0	90	355	4	0	449	843
8:30	0	2	5	2	3	9	1	92	12	107	1	212	0	4	136	29	3	169	0	89	371	5	0	465	855
8:45	0	4	9	0	2	13	1	109	20	84	0	214	0	4	144	30	1	178	0	83	287	7	0	377	782
Hourly Total	0	20	32	6	6	58	6	360	67	369	3	802	0	12	557	100	5	669	0	351	1403	22	0	1776	3305
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	4	16	10	0	30	0	67	26	79	0	172	0	9	179	89	0	277	0	85	176	5	0	266	745
12:15	0	4	11	4	0	19	2	88	19	95	0	204	0	9	170	72	0	251	0	81	158	3	0	242	716
12:30	0	5	11	5	1	21	1	90	35	83	1	209	0	22	167	84	0	273	0	73	119	3	0	195	698
12:45	0	5	14	8	0	27	1	80	48	79	1	208	0	24	181	84	0	289	0	71	174	6	0	251	775
Hourly Total	0	18	52	27	1	97	4	325	128	336	2	793	0	64	697	329	0	1090	0	310	627	17	0	954	2934
13:00	0	6	10	6	2	22	1	65	42	81	1	189	0	19	164	72	1	255	0	84	169	5	0	258	724
13:15	0	14	15	3	0	32	2	69	24	78	0	173	0	12	203	71	0	286	0	60	165	4	0	229	720
13:30	0	3	11	3	0	17	0	52	22	75	2	149	0	5	219	68	0	292	0	87	186	1	0	274	732
13:45	0	7	12	5	0	24	0	73	20	97	0	190	0	6	183	70	0	259	0	64	183	0	0	247	720
Hourly Total	0	30	48	17	2	95	3	259	108	331	3	701	0	42	769	281	1	1092	0	295	703	10	0	1008	2896
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	1	3	21	3	0	28	2	43	6	121	1	172	0	1	266	57	0	324	0	88	154	4	0	246	770
15:15	0	8	24	10	0	42	0	59	15	99	0	173	0	7	304	84	0	395	1	85	202	3	0	291	901
15:30	0	13	60	31	1	104	0	52	10	104	0	166	0	4	282	96	1	382	1	86	234	4	0	325	977
15:45	0	10	18	6	0	34	3	70	8	96	0	177	0	4	242	76	0	322	1	100	201	2	0	304	837
Hourly Total	1	34	123	50	1	208	5	224	39	420	1	688	0	16	1094	313	1	1423	3	359	791	13	0	1166	3485
16:00	1	10	22	9	0	42	0	59	6	118	2	183	0	5	288	95	0	388	0	82	199	5	0	286	899
16:15	0	12	16	6	0	34	2	58	4	119	0	183	0	4	327	71	1	402	0	78	218	3	0	299	918
16:30	0	8	19	9	0	36	2	86	13	140	0	241	0	3	312	73	0	388	0	95	196	2	0	293	958
16:45	0	14	14	7	1	35	1	79	12	130	2	222	0	1	316	55	0	372	0	89	188	2	0	279	908
Hourly Total	1	44	71	31	1	147	5	282	35	507	4	829	0	13	1243	294	1	1550	0	344	801	12	0	1157	3683
17:00	1	9	23	14	0	47	2	52	8	118	0	180	0	3	341	65	1	409	0	82	210	0	0	292	928
17:15	0	7	22	8	2	37	3	73	12	146	0	234	0	5	344	54	0	403	0	69	224	3	0	296	970
17:30	0	5	18	6	0	29	0	75	22	119	0	216	0	11	317	83	0	411	0	82	156	1	0	239	895
17:45	0	5	12	2	1	19	6	78	29	109	1	222	0	18	293	54	1	365	0	70	172	2	0	244	850
Hourly Total	1	26	75	30	3	132	11	278	71	492	1	852	0	37	1295	256	2	1588	0	303	762	6	0	1071	3643
18:00	0	4	6	0	1	10	2	89	33	111	0	235	0	16	238	36	0	290	0	59	160	3	0	222	757
18:15	1	2	4	5	1	12	3	71	15	99	0	188	0	12	236	54	1	302	1	69	172	1	0	243	745
18:30	0	6	7	5	0	18	1	75	12	70	1	158	0	10	176	49	0	235	0	50	142	1	0	193	604
18:45	0	2	12	3	0	17	3	42	28	71	1	144	0	13	152	44	1	209	0	51	124	3	0	178	548
Hourly Total	1	14	29	13	2	57	9	277	88	351	2	725	0	51	802	183	2	1036	1	229	598	8	0	836	2654
Grand Total	5	196	454	177	16	832	45	2297	576	3199	16	6117	0	247	7011	1886	12	9144	4	2624	7223	98	2	9949	26042
Approach %	0.6	23.6	54.6	21.3	-	-	0.7	37.6	9.4	52.3	-	-	0.0	2.7	76.7	20.6	-	-	0.0	26.4	72.6	1.0	-	-	-
Total %	0.0	0.8	1.7	0.7	-	3.2	0.2	8.8	2.2	12.3	-	23.5	0.0	0.9	26.9	7.2	-	35.1	0.0	10.1	27.7	0.4	-	38.2	-
Lights	5	194	443	175	-	817	45	2257	568	3105	-	5975	0	245	6848	1825	-	8918	4	2571	7079	95	-	9749	25459
% Lights	100.0	99.0	97.6	98.9	-	98.2	100.0	98.3	98.6	97.1	-	97.7	-	99.2	97.7	96.8	-	97.5	100.0	98.0	98.0	96.9	-	98.0	97.8
Other Vehicles	0	2	11	1	-	14	0	40	8	90	-	138	0	2	162	60	-	224	0	53	144	3	-	200	576
% Other Vehicles	0.0	1.0	2.4	0.6	-	1.7	0.0	1.7	1.4	2.8	-	2.3	-	0.8	2.3	3.2	-	2.4	0.0	2.0	2.0	3.1	-	2.0	2.2
Bicycles on Road	0	0	0	1	-	1	0	0	0	4	-	4	0	0	1	1	-	2	0	0	0	0	-	0	7
% Bicycles on Road	0.0	0.0	0.0	0.6	-	0.1	0.0	0.0	0.0	0.1	-	0.1	-	0.0	0.0	0.1	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	8	-	-	-	-	-	10	-	-	-	-	-	5	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	50.0	-	-	-	-	-	62.5	-	-	-	-	-	41.7	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	8	-	-	-	-	-	6	-	-	-	-	-	7	-	-	-	-	-	2	-	-
% Pedestrians	-	-	-	-	50.0	-	-	-	-	-	37.5	-	-	-	-	-	58.3	-	-	-	-	-	100.0	-	-



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:15)

Start Time	Ringling Plaza Eastbound						University Parkway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15	1	1	4	2	0	8	1	70	7	88	0	166	0	5	132	29	0	166	0	107	437	3	0	547	887
7:30	0	4	10	0	0	14	0	85	9	134	0	228	0	2	176	40	0	218	0	129	457	3	0	589	1049
7:45	0	3	4	0	0	7	0	82	15	108	0	205	0	3	129	34	0	166	0	88	329	1	1	418	796
8:00	0	5	10	1	0	16	4	74	19	77	1	174	0	1	136	13	0	150	0	89	390	6	0	485	825
<b>Total</b>	<b>1</b>	<b>13</b>	<b>28</b>	<b>3</b>	<b>0</b>	<b>45</b>	<b>5</b>	<b>311</b>	<b>50</b>	<b>407</b>	<b>1</b>	<b>773</b>	<b>0</b>	<b>11</b>	<b>573</b>	<b>116</b>	<b>0</b>	<b>700</b>	<b>0</b>	<b>413</b>	<b>1613</b>	<b>13</b>	<b>1</b>	<b>2039</b>	<b>3557</b>
Approach %	2.2	28.9	62.2	6.7	-	-	0.6	40.2	6.5	52.7	-	-	0.0	1.6	81.9	16.6	-	-	0.0	20.3	79.1	0.6	-	-	-
Total %	0.0	0.4	0.8	0.1	-	1.3	0.1	8.7	1.4	11.4	-	21.7	0.0	0.3	16.1	3.3	-	19.7	0.0	11.6	45.3	0.4	-	57.3	-
PHF	0.250	0.650	0.700	0.375	-	0.703	0.313	0.915	0.658	0.759	-	0.848	0.000	0.550	0.814	0.725	-	0.803	0.000	0.800	0.882	0.542	-	0.865	0.848
Lights	1	12	26	3	-	42	5	304	47	390	-	746	0	10	556	108	-	674	0	410	1586	12	-	2008	3470
% Lights	100.0	92.3	92.9	100.0	-	93.3	100.0	97.7	94.0	95.8	-	96.5	-	90.9	97.0	93.1	-	96.3	-	99.3	98.3	92.3	-	98.5	97.6
Other Vehicles	0	1	2	0	-	3	0	7	3	17	-	27	0	1	17	8	-	26	0	3	27	1	-	31	87
% Other Vehicles	0.0	7.7	7.1	0.0	-	6.7	0.0	2.3	6.0	4.2	-	3.5	-	9.1	3.0	6.9	-	3.7	-	0.7	1.7	7.7	-	1.5	2.4
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



### Turning Movement Peak Hour Data (12:45)

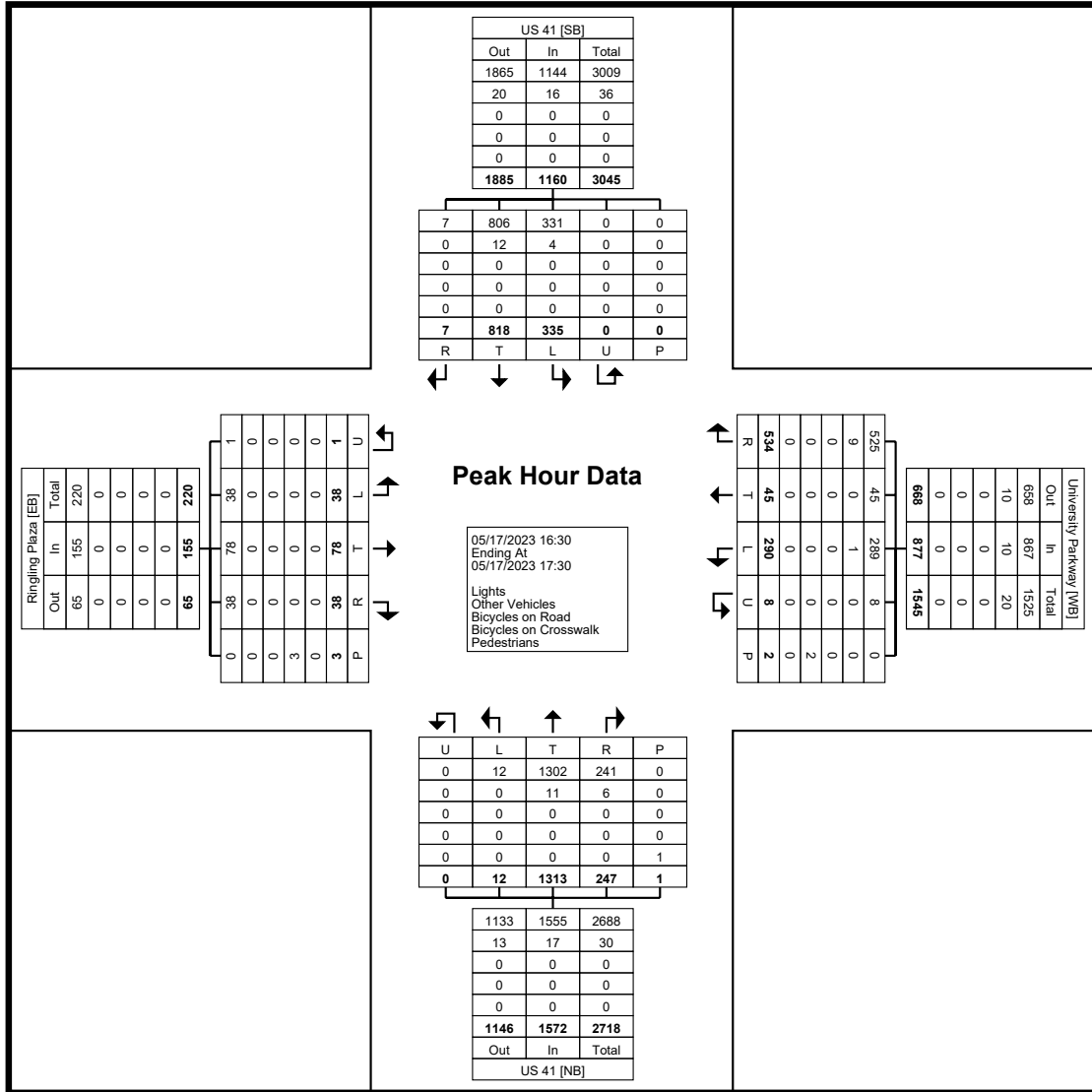
Start Time	Ringling Plaza Eastbound						University Parkway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:45	0	5	14	8	0	27	1	80	48	79	1	208	0	24	181	84	0	289	0	71	174	6	0	251	775
13:00	0	6	10	6	2	22	1	65	42	81	1	189	0	19	164	72	1	255	0	84	169	5	0	258	724
13:15	0	14	15	3	0	32	2	69	24	78	0	173	0	12	203	71	0	286	0	60	165	4	0	229	720
13:30	0	3	11	3	0	17	0	52	22	75	2	149	0	5	219	68	0	292	0	87	186	1	0	274	732
<b>Total</b>	<b>0</b>	<b>28</b>	<b>50</b>	<b>20</b>	<b>2</b>	<b>98</b>	<b>4</b>	<b>266</b>	<b>136</b>	<b>313</b>	<b>4</b>	<b>719</b>	<b>0</b>	<b>60</b>	<b>767</b>	<b>295</b>	<b>1</b>	<b>1122</b>	<b>0</b>	<b>302</b>	<b>694</b>	<b>16</b>	<b>0</b>	<b>1012</b>	<b>2951</b>
Approach %	0.0	28.6	51.0	20.4	-	-	0.6	37.0	18.9	43.5	-	-	0.0	5.3	68.4	26.3	-	-	0.0	29.8	68.6	1.6	-	-	-
Total %	0.0	0.9	1.7	0.7	-	3.3	0.1	9.0	4.6	10.6	-	24.4	0.0	2.0	26.0	10.0	-	38.0	0.0	10.2	23.5	0.5	-	34.3	-
PHF	0.000	0.500	0.833	0.625	-	0.766	0.500	0.831	0.708	0.966	-	0.864	0.000	0.625	0.876	0.878	-	0.961	0.000	0.868	0.933	0.667	-	0.923	0.952
Lights	0	28	48	19	-	95	4	255	134	306	-	699	0	59	727	285	-	1071	0	293	672	16	-	981	2846
% Lights	-	100.0	96.0	95.0	-	96.9	100.0	95.9	98.5	97.8	-	97.2	-	98.3	94.8	96.6	-	95.5	-	97.0	96.8	100.0	-	96.9	96.4
Other Vehicles	0	0	2	1	-	3	0	11	2	7	-	20	0	1	40	10	-	51	0	9	22	0	-	31	105
% Other Vehicles	-	0.0	4.0	5.0	-	3.1	0.0	4.1	1.5	2.2	-	2.8	-	1.7	5.2	3.4	-	4.5	-	3.0	3.2	0.0	-	3.1	3.6
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	2	-	-	-	-	-	2	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	100.0	-	-	-	-	-	50.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	0.0	-	-	-	-	-	50.0	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-



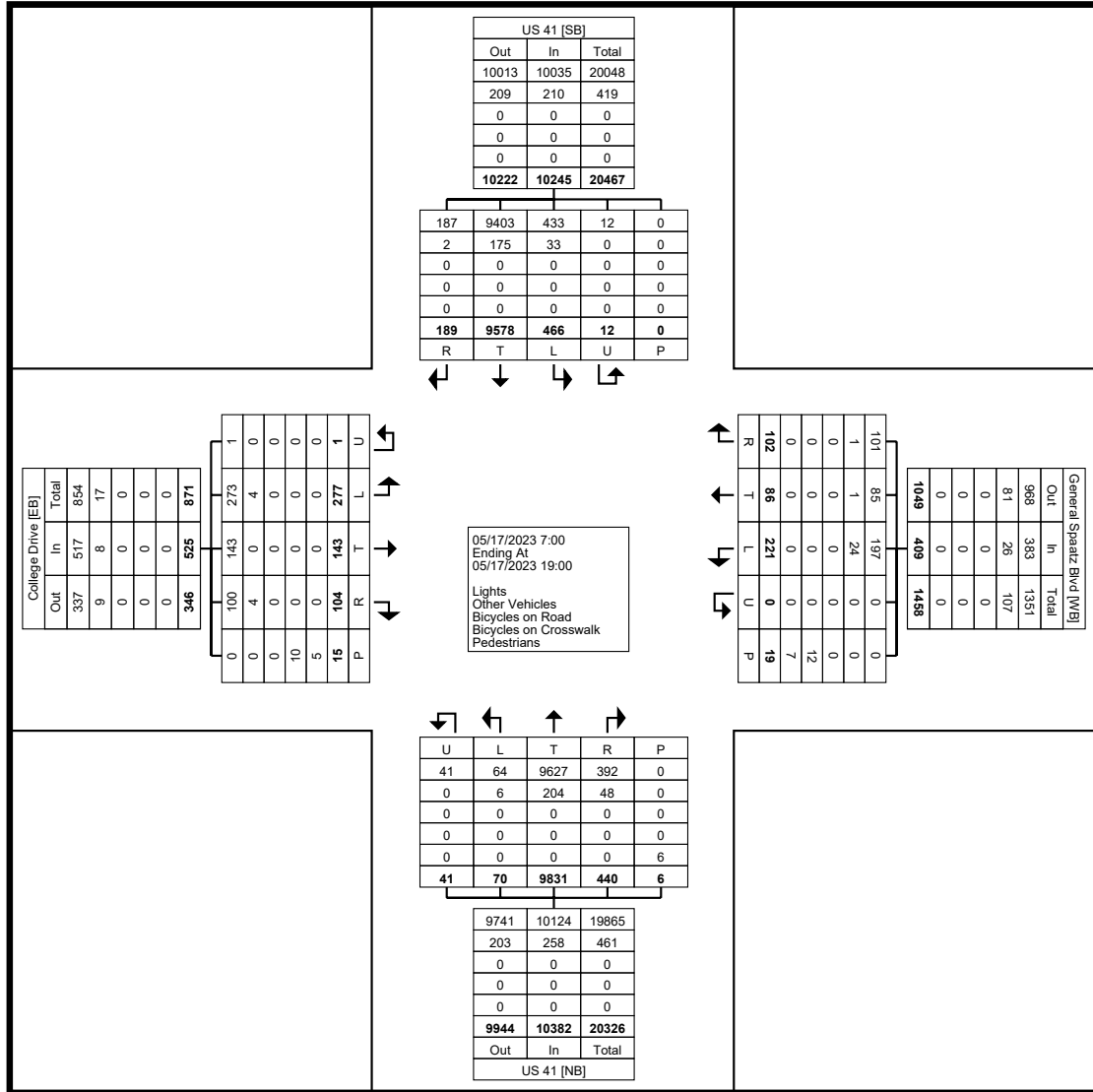


### Turning Movement Peak Hour Data (16:30)

Start Time	Ringling Plaza Eastbound						University Parkway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:30	0	8	19	9	0	36	2	86	13	140	0	241	0	3	312	73	0	388	0	95	196	2	0	293	958
16:45	0	14	14	7	1	35	1	79	12	130	2	222	0	1	316	55	0	372	0	89	188	2	0	279	908
17:00	1	9	23	14	0	47	2	52	8	118	0	180	0	3	341	65	1	409	0	82	210	0	0	292	928
17:15	0	7	22	8	2	37	3	73	12	146	0	234	0	5	344	54	0	403	0	69	224	3	0	296	970
<b>Total</b>	<b>1</b>	<b>38</b>	<b>78</b>	<b>38</b>	<b>3</b>	<b>155</b>	<b>8</b>	<b>290</b>	<b>45</b>	<b>534</b>	<b>2</b>	<b>877</b>	<b>0</b>	<b>12</b>	<b>1313</b>	<b>247</b>	<b>1</b>	<b>1572</b>	<b>0</b>	<b>335</b>	<b>818</b>	<b>7</b>	<b>0</b>	<b>1160</b>	<b>3764</b>
Approach %	0.6	24.5	50.3	24.5	-	-	0.9	33.1	5.1	60.9	-	-	0.0	0.8	83.5	15.7	-	-	0.0	28.9	70.5	0.6	-	-	-
Total %	0.0	1.0	2.1	1.0	-	4.1	0.2	7.7	1.2	14.2	-	23.3	0.0	0.3	34.9	6.6	-	41.8	0.0	8.9	21.7	0.2	-	30.8	-
PHF	0.250	0.679	0.848	0.679	-	0.824	0.667	0.843	0.865	0.914	-	0.910	0.000	0.600	0.954	0.846	-	0.961	0.000	0.882	0.913	0.583	-	0.980	0.970
Lights	1	38	78	38	-	155	8	289	45	525	-	867	0	12	1302	241	-	1555	0	331	806	7	-	1144	3721
% Lights	100.0	100.0	100.0	100.0	-	100.0	100.0	99.7	100.0	98.3	-	98.9	-	100.0	99.2	97.6	-	98.9	-	98.8	98.5	100.0	-	98.6	98.9
Other Vehicles	0	0	0	0	-	0	0	1	0	9	-	10	0	0	11	6	-	17	0	4	12	0	-	16	43
% Other Vehicles	0.0	0.0	0.0	0.0	-	0.0	0.0	0.3	0.0	1.7	-	1.1	-	0.0	0.8	2.4	-	1.1	-	1.2	1.5	0.0	-	1.4	1.1
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	3	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-







Turning Movement Data Plot

US 41 at College Drive/General Spaaatz Blvd  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 47\_US 41 at College Drive/General Spaaatz Blvd  
Site Code: 47  
Start Date: 05/17/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:15)

Start Time	College Drive Eastbound						General Spaaatz Blvd Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15	0	7	1	2	0	10	0	1	3	0	1	4	0	0	206	14	0	220	0	11	562	5	0	578	812
7:30	0	6	2	5	0	13	0	6	1	1	0	8	0	1	299	13	0	313	0	10	554	5	0	569	903
7:45	0	5	0	1	1	6	0	5	2	1	2	8	1	1	227	8	1	237	0	13	469	10	0	492	743
8:00	0	7	2	1	0	10	0	5	1	2	2	8	0	5	212	9	0	226	0	18	474	16	0	508	752
<b>Total</b>	0	25	5	9	1	39	0	17	7	4	5	28	1	7	944	44	1	996	0	52	2059	36	0	2147	3210
Approach %	0.0	64.1	12.8	23.1	-	-	0.0	60.7	25.0	14.3	-	-	0.1	0.7	94.8	4.4	-	-	0.0	2.4	95.9	1.7	-	-	-
Total %	0.0	0.8	0.2	0.3	-	1.2	0.0	0.5	0.2	0.1	-	0.9	0.0	0.2	29.4	1.4	-	31.0	0.0	1.6	64.1	1.1	-	66.9	-
PHF	0.000	0.893	0.625	0.450	-	0.750	0.000	0.708	0.583	0.500	-	0.875	0.250	0.350	0.789	0.786	-	0.796	0.000	0.722	0.916	0.563	-	0.929	0.889
Lights	0	24	5	9	-	38	0	13	7	4	-	24	1	5	918	37	-	961	0	47	2020	36	-	2103	3126
% Lights	-	96.0	100.0	100.0	-	97.4	-	76.5	100.0	100.0	-	85.7	100.0	71.4	97.2	84.1	-	96.5	-	90.4	98.1	100.0	-	98.0	97.4
Other Vehicles	0	1	0	0	-	1	0	4	0	0	-	4	0	2	26	7	-	35	0	5	39	0	-	44	84
% Other Vehicles	-	4.0	0.0	0.0	-	2.6	-	23.5	0.0	0.0	-	14.3	0.0	28.6	2.8	15.9	-	3.5	-	9.6	1.9	0.0	-	2.0	2.6
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	0.0	-	-	-	-	-	40.0	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	3	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	100.0	-	-	-	-	-	60.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-



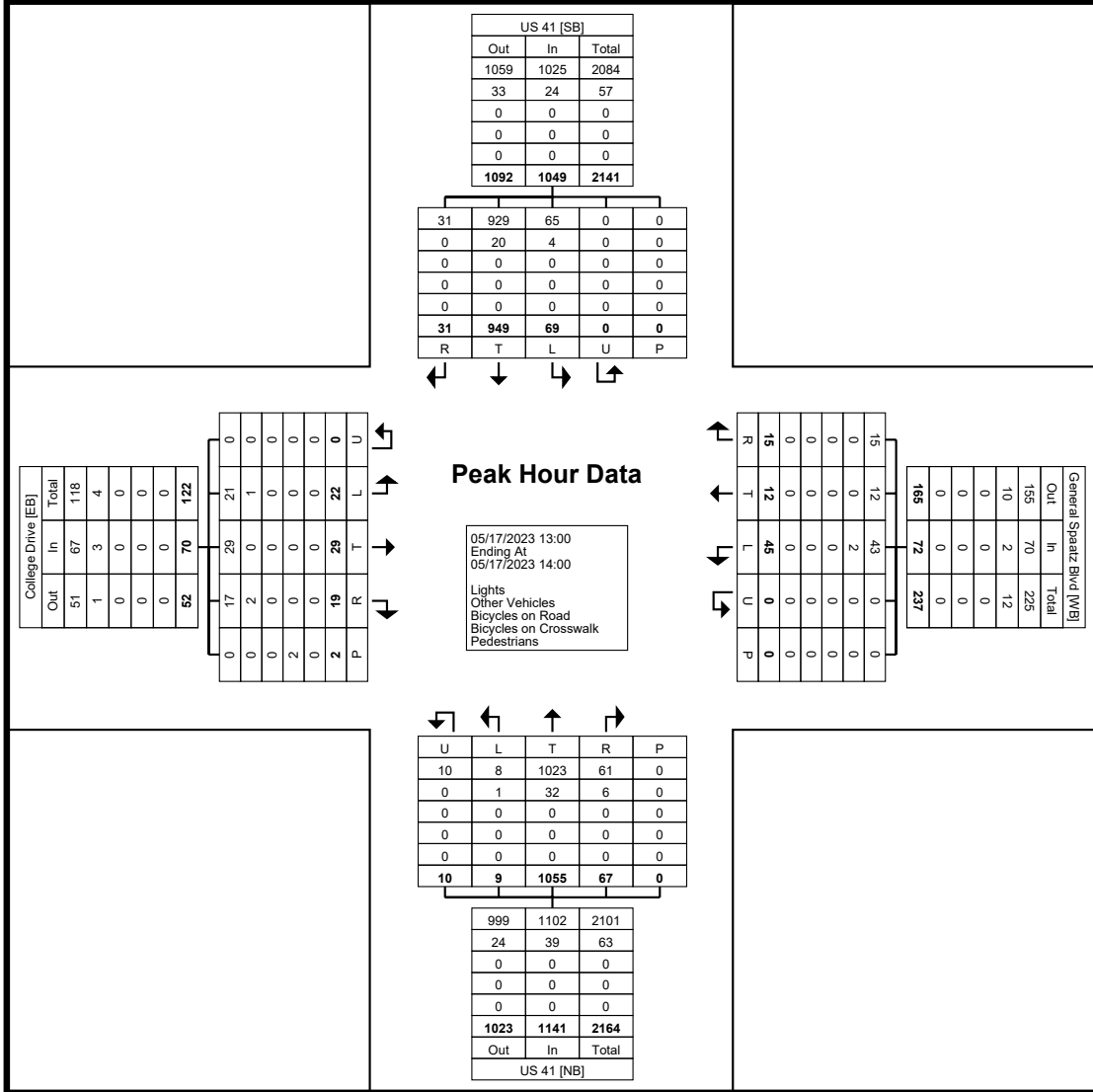
US 41 at College Drive/General Spaaatz Blvd  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 47\_US 41 at College Drive/General Spaaatz Blvd  
Site Code: 47  
Start Date: 05/17/2023  
Page No: 5

### Turning Movement Peak Hour Data (13:00)

Start Time	College Drive Eastbound						General Spaaatz Blvd Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
13:00	0	5	1	4	2	10	0	7	4	3	0	14	5	2	251	16	0	274	0	19	248	10	0	277	575
13:15	0	7	5	7	0	19	0	13	4	5	0	22	2	1	265	17	0	285	0	18	221	8	0	247	573
13:30	0	5	5	6	0	16	0	18	2	5	0	25	1	5	265	15	0	286	0	21	256	6	0	283	610
13:45	0	5	18	2	0	25	0	7	2	2	0	11	2	1	274	19	0	296	0	11	224	7	0	242	574
<b>Total</b>	<b>0</b>	<b>22</b>	<b>29</b>	<b>19</b>	<b>2</b>	<b>70</b>	<b>0</b>	<b>45</b>	<b>12</b>	<b>15</b>	<b>0</b>	<b>72</b>	<b>10</b>	<b>9</b>	<b>1055</b>	<b>67</b>	<b>0</b>	<b>1141</b>	<b>0</b>	<b>69</b>	<b>949</b>	<b>31</b>	<b>0</b>	<b>1049</b>	<b>2332</b>
Approach %	0.0	31.4	41.4	27.1	-	-	0.0	62.5	16.7	20.8	-	-	0.9	0.8	92.5	5.9	-	-	0.0	6.6	90.5	3.0	-	-	-
Total %	0.0	0.9	1.2	0.8	-	3.0	0.0	1.9	0.5	0.6	-	3.1	0.4	0.4	45.2	2.9	-	48.9	0.0	3.0	40.7	1.3	-	45.0	-
PHF	0.000	0.786	0.403	0.679	-	0.700	0.000	0.625	0.750	0.750	-	0.720	0.500	0.450	0.963	0.882	-	0.964	0.000	0.821	0.927	0.775	-	0.927	0.956
Lights	0	21	29	17	-	67	0	43	12	15	-	70	10	8	1023	61	-	1102	0	65	929	31	-	1025	2264
% Lights	-	95.5	100.0	89.5	-	95.7	-	95.6	100.0	100.0	-	97.2	100.0	88.9	97.0	91.0	-	96.6	-	94.2	97.9	100.0	-	97.7	97.1
Other Vehicles	0	1	0	2	-	3	0	2	0	0	-	2	0	1	32	6	-	39	0	4	20	0	-	24	68
% Other Vehicles	-	4.5	0.0	10.5	-	4.3	-	4.4	0.0	0.0	-	2.8	0.0	11.1	3.0	9.0	-	3.4	-	5.8	2.1	0.0	-	2.3	2.9
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-



Turning Movement Peak Hour Data Plot (13:00)



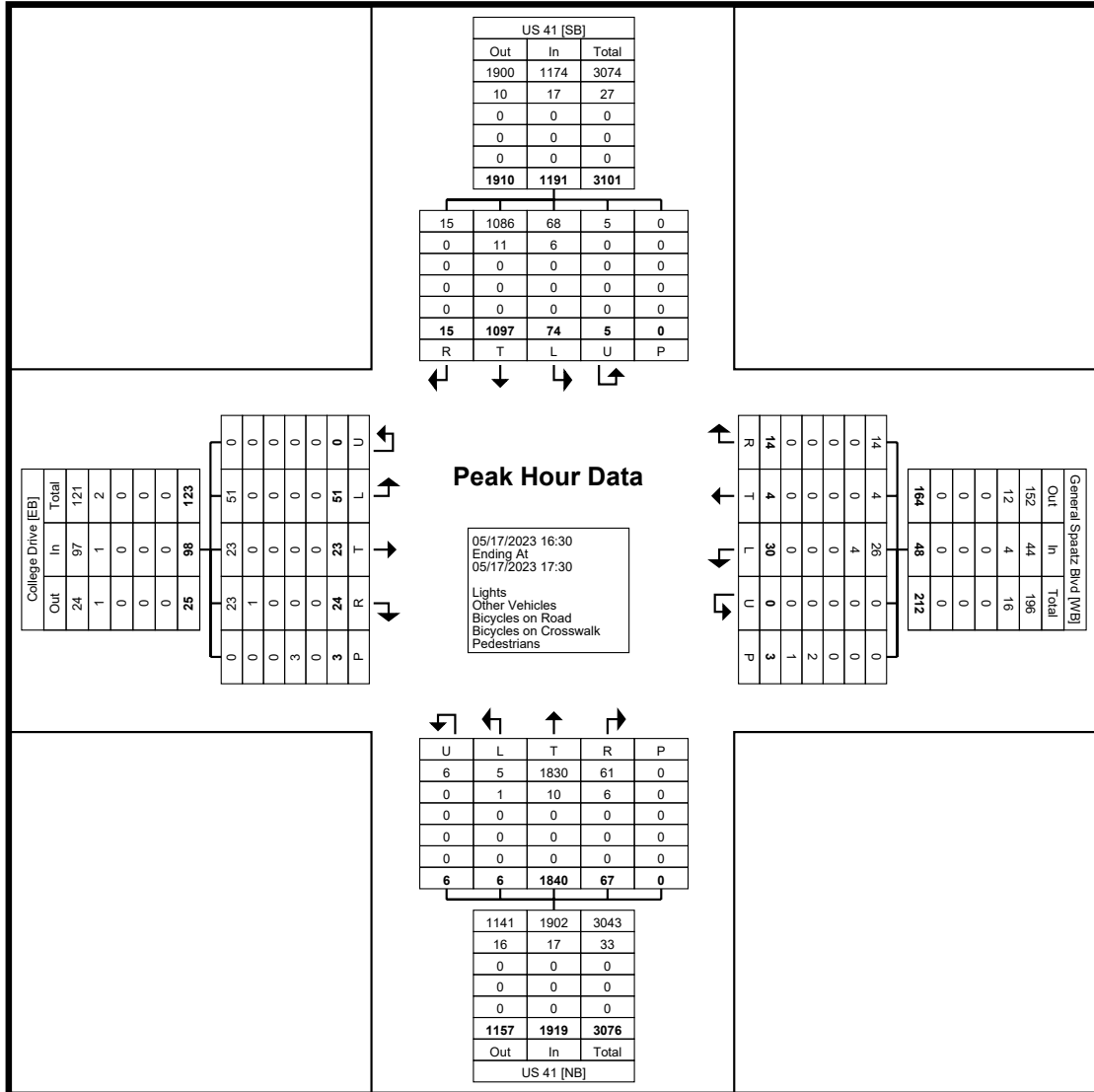
US 41 at College Drive/General Spaaatz Blvd  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 47\_US 41 at College Drive/General Spaaatz Blvd  
Site Code: 47  
Start Date: 05/17/2023  
Page No: 7

### Turning Movement Peak Hour Data (16:30)

Start Time	College Drive Eastbound						General Spaaatz Blvd Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:30	0	13	4	2	0	19	0	7	0	3	1	10	1	1	473	21	0	496	2	16	279	3	0	300	825
16:45	0	10	7	8	1	25	0	9	2	5	0	16	3	2	410	22	0	437	2	16	267	6	0	291	769
17:00	0	19	7	11	0	37	0	9	1	4	2	14	0	1	423	12	0	436	0	22	262	4	0	288	775
17:15	0	9	5	3	2	17	0	5	1	2	0	8	2	2	534	12	0	550	1	20	289	2	0	312	887
<b>Total</b>	<b>0</b>	<b>51</b>	<b>23</b>	<b>24</b>	<b>3</b>	<b>98</b>	<b>0</b>	<b>30</b>	<b>4</b>	<b>14</b>	<b>3</b>	<b>48</b>	<b>6</b>	<b>6</b>	<b>1840</b>	<b>67</b>	<b>0</b>	<b>1919</b>	<b>5</b>	<b>74</b>	<b>1097</b>	<b>15</b>	<b>0</b>	<b>1191</b>	<b>3256</b>
Approach %	0.0	52.0	23.5	24.5	-	-	0.0	62.5	8.3	29.2	-	-	0.3	0.3	95.9	3.5	-	-	0.4	6.2	92.1	1.3	-	-	-
Total %	0.0	1.6	0.7	0.7	-	3.0	0.0	0.9	0.1	0.4	-	1.5	0.2	0.2	56.5	2.1	-	58.9	0.2	2.3	33.7	0.5	-	36.6	-
PHF	0.000	0.671	0.821	0.545	-	0.662	0.000	0.833	0.500	0.700	-	0.750	0.500	0.750	0.861	0.761	-	0.872	0.625	0.841	0.949	0.625	-	0.954	0.918
Lights	0	51	23	23	-	97	0	26	4	14	-	44	6	5	1830	61	-	1902	5	68	1086	15	-	1174	3217
% Lights	-	100.0	100.0	95.8	-	99.0	-	86.7	100.0	100.0	-	91.7	100.0	83.3	99.5	91.0	-	99.1	100.0	91.9	99.0	100.0	-	98.6	98.8
Other Vehicles	0	0	0	1	-	1	0	4	0	0	-	4	0	1	10	6	-	17	0	6	11	0	-	17	39
% Other Vehicles	-	0.0	0.0	4.2	-	1.0	-	13.3	0.0	0.0	-	8.3	0.0	16.7	0.5	9.0	-	0.9	0.0	8.1	1.0	0.0	-	1.4	1.2
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	3	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	100.0	-	-	-	-	-	66.7	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	0.0	-	-	-	-	-	33.3	-	-	-	-	-	-	-	-	-	-	-	-	-	-



Turning Movement Peak Hour Data Plot (16:30)

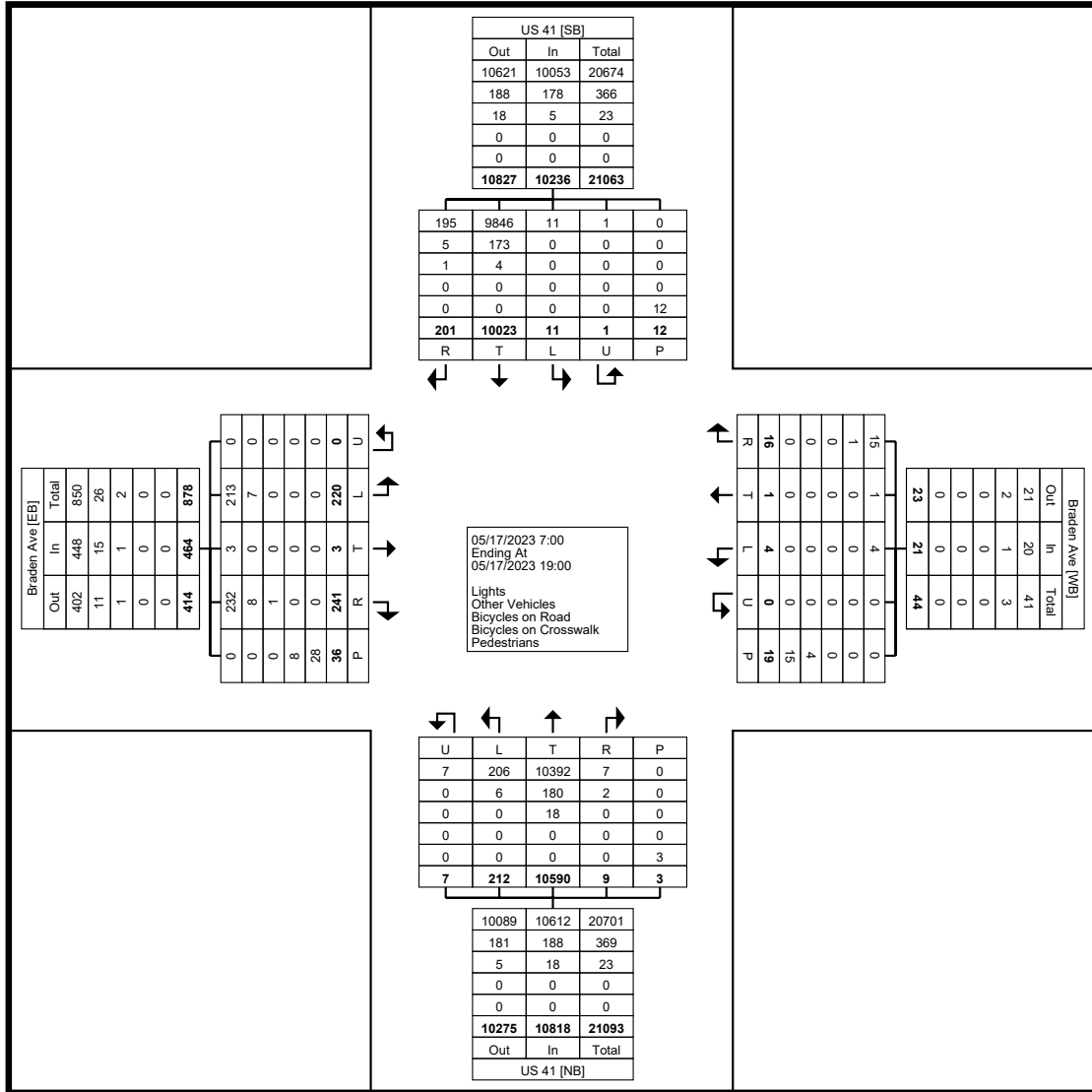
US 41 at Braden Ave  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 46\_US 41 at  
Braden Ave  
Site Code: 46  
Start Date: 05/17/2023  
Page No: 1

### Turning Movement Data

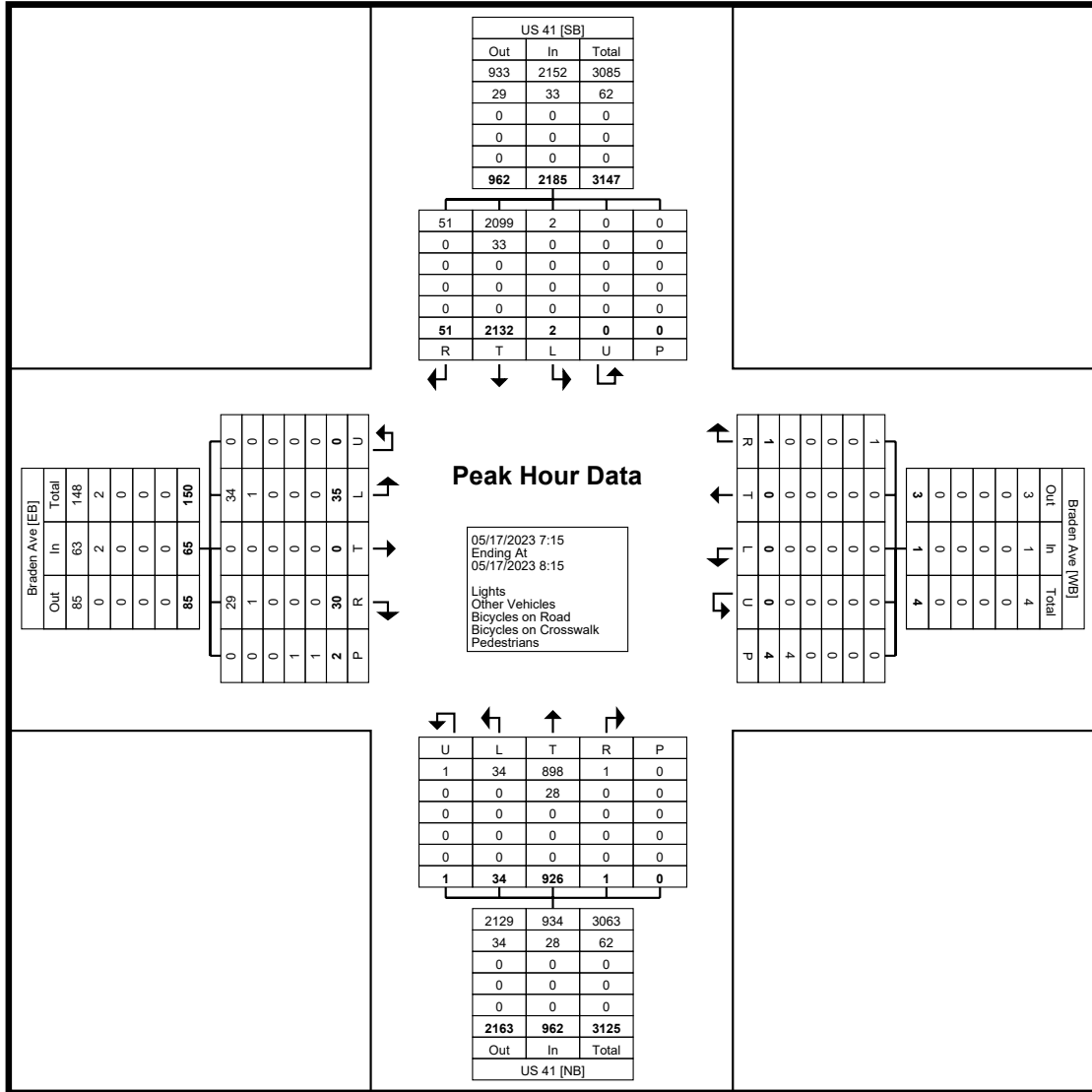
Start Time	Braden Ave Eastbound						Braden Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	1	3	4	1	8	0	0	0	0	1	0	0	2	189	0	0	191	0	0	428	4	1	432	631
7:15	0	7	0	3	1	10	0	0	0	0	0	0	0	8	207	1	0	216	0	0	567	14	0	581	807
7:30	0	5	0	6	0	11	0	0	0	0	1	0	0	6	280	0	0	286	0	2	574	15	0	591	888
7:45	0	14	0	13	0	27	0	0	0	1	2	1	1	12	232	0	0	245	0	0	510	14	0	524	797
Hourly Total	0	27	3	26	2	56	0	0	0	1	4	1	1	28	908	1	0	938	0	2	2079	47	1	2128	3123
8:00	0	9	0	8	1	17	0	0	0	0	1	0	0	8	207	0	0	215	0	0	481	8	0	489	721
8:15	0	11	0	7	1	18	0	0	0	0	0	0	0	13	234	0	0	247	0	1	478	14	0	493	758
8:30	0	4	0	14	1	18	0	0	1	0	1	1	0	16	213	0	0	229	0	0	449	12	1	461	709
8:45	0	4	0	9	0	13	0	0	0	0	0	0	1	6	224	0	0	231	0	1	395	14	0	410	654
Hourly Total	0	28	0	38	3	66	0	0	1	0	2	1	1	43	878	0	0	922	0	2	1803	48	1	1853	2842
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	7	0	9	3	16	0	1	0	0	0	1	0	5	272	1	0	278	0	0	289	6	1	295	590
12:15	0	7	0	8	1	15	0	0	0	2	0	2	0	5	277	1	0	283	0	0	252	8	0	260	560
12:30	0	5	0	6	3	11	0	1	0	0	1	1	0	5	288	0	0	293	0	0	247	2	0	249	554
12:45	0	7	0	6	1	13	0	0	0	0	0	0	0	4	259	0	0	263	0	0	257	4	0	261	537
Hourly Total	0	26	0	29	8	55	0	2	0	2	1	4	0	19	1096	2	0	1117	0	0	1045	20	1	1065	2241
13:00	0	4	0	6	1	10	0	0	0	0	3	0	0	7	270	0	0	277	0	0	272	5	2	277	564
13:15	0	4	0	8	1	12	0	0	0	1	0	1	1	4	304	1	0	310	0	0	257	5	0	262	585
13:30	0	8	0	3	1	11	0	0	0	1	0	1	0	10	289	0	0	299	0	2	278	4	0	284	595
13:45	0	8	0	9	0	17	0	0	0	0	0	0	0	7	275	0	0	282	0	0	246	5	0	251	550
Hourly Total	0	24	0	26	3	50	0	0	0	2	3	2	1	28	1138	1	0	1168	0	2	1053	19	2	1074	2294
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	11	0	13	0	24	0	0	0	1	0	1	0	8	403	1	3	412	0	0	220	6	0	226	663
15:15	0	9	0	5	1	14	0	0	0	0	0	0	0	4	448	1	0	453	0	2	274	5	0	281	748
15:30	0	5	0	8	1	13	0	1	0	2	0	3	0	9	421	0	0	430	0	1	302	4	0	307	753
15:45	0	7	0	7	1	14	0	0	0	0	0	0	0	3	371	1	0	375	0	0	272	3	0	275	664
Hourly Total	0	32	0	33	3	65	0	1	0	3	0	4	0	24	1643	3	3	1670	0	3	1068	18	0	1089	2828
16:00	0	9	0	11	1	20	0	1	0	0	1	1	1	3	389	0	0	393	0	0	278	1	0	279	693
16:15	0	7	0	6	0	13	0	0	0	0	0	0	0	9	452	0	0	461	0	0	304	4	0	308	782
16:30	0	9	0	15	1	24	0	0	0	1	2	1	0	8	490	0	0	498	0	0	278	11	2	289	812
16:45	0	9	0	10	1	19	0	0	0	0	1	0	1	6	468	0	0	475	0	0	284	4	1	288	782
Hourly Total	0	34	0	42	3	76	0	1	0	1	4	2	2	26	1799	0	0	1827	0	0	1144	20	3	1164	3069
17:00	0	19	0	8	1	27	0	0	0	1	1	1	0	7	463	0	0	470	0	0	256	4	0	260	758
17:15	0	5	0	10	2	15	0	0	0	0	1	0	0	11	549	0	0	560	0	0	289	5	3	294	869
17:30	0	11	0	7	2	18	0	0	0	0	0	0	0	4	490	0	0	494	0	0	236	5	1	241	753
17:45	0	3	0	5	3	8	0	0	0	0	0	0	0	4	438	0	0	442	0	0	239	4	0	243	693
Hourly Total	0	38	0	30	8	68	0	0	0	1	2	1	0	26	1940	0	0	1966	0	0	1020	18	4	1038	3073
18:00	0	6	0	4	1	10	0	0	0	1	0	1	0	3	366	0	0	369	0	1	221	2	0	224	604
18:15	0	2	0	4	2	6	0	0	0	3	1	3	1	7	321	1	0	330	1	0	225	4	0	230	569
18:30	0	0	0	2	1	2	0	0	0	1	0	1	1	2	273	0	0	276	0	1	183	1	0	185	464
18:45	0	3	0	7	2	10	0	0	0	1	2	1	0	6	228	1	0	235	0	0	182	4	0	186	432
Hourly Total	0	11	0	17	6	28	0	0	0	6	3	6	2	18	1188	2	0	1210	1	2	811	11	0	825	2069
Grand Total	0	220	3	241	36	464	0	4	1	16	19	21	7	212	10590	9	3	10818	1	11	10023	201	12	10236	21539
Approach %	0.0	47.4	0.6	51.9	-	-	0.0	19.0	4.8	76.2	-	-	0.1	2.0	97.9	0.1	-	-	0.0	0.1	97.9	2.0	-	-	-
Total %	0.0	1.0	0.0	1.1	-	2.2	0.0	0.0	0.0	0.1	-	0.1	0.0	1.0	49.2	0.0	-	50.2	0.0	0.1	46.5	0.9	-	47.5	-
Lights	0	213	3	232	-	448	0	4	1	15	-	20	7	206	10392	7	-	10612	1	11	9846	195	-	10053	21133
% Lights	-	96.8	100.0	96.3	-	96.6	-	100.0	100.0	93.8	-	95.2	100.0	97.2	98.1	77.8	-	98.1	100.0	100.0	98.2	97.0	-	98.2	98.1
Other Vehicles	0	7	0	8	-	15	0	0	0	1	-	1	0	6	180	2	-	188	0	0	173	5	-	178	382
% Other Vehicles	-	3.2	0.0	3.3	-	3.2	-	0.0	0.0	6.3	-	4.8	0.0	2.8	1.7	22.2	-	1.7	0.0	0.0	1.7	2.5	-	1.7	1.8
Bicycles on Road	0	0	0	1	-	1	0	0	0	0	-	0	0	0	18	0	-	18	0	0	4	1	-	5	24
% Bicycles on Road	-	0.0	0.0	0.4	-	0.2	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.2	0.0	-	0.2	0.0	0.0	0.5	-	0.0	0.1	0.1
Bicycles on Crosswalk	-	-	-	-	8	-	-	-	-	-	4	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	22.2	-	-	-	-	-	21.1	-	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	28	-	-	-	-	-	15	-	-	-	-	-	3	-	-	-	-	-	12	-	-
% Pedestrians	-	-	-	-	77.8	-	-	-	-	-	78.9	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:15)

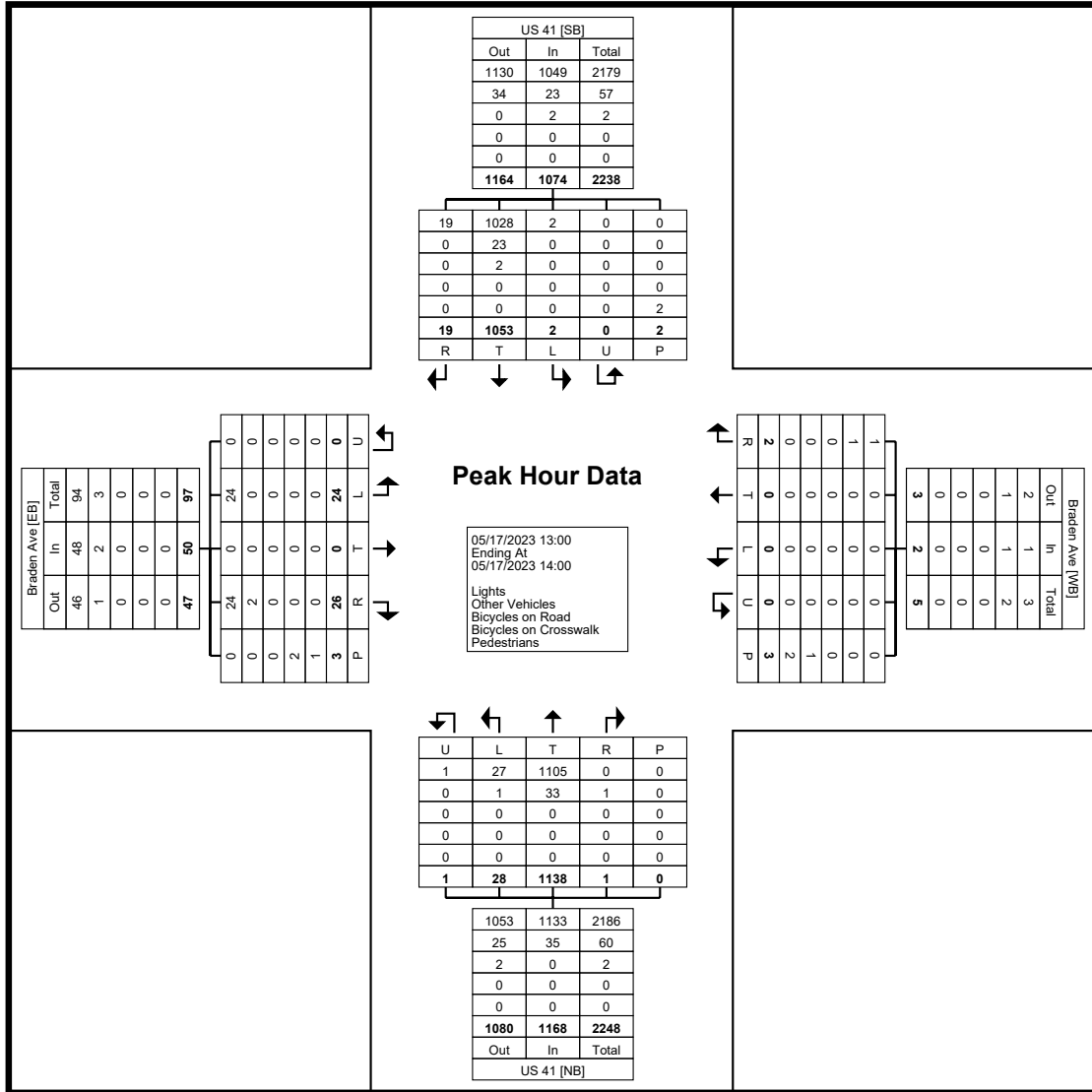
Start Time	Braden Ave Eastbound						Braden Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15	0	7	0	3	1	10	0	0	0	0	0	0	0	8	207	1	0	216	0	0	567	14	0	581	807
7:30	0	5	0	6	0	11	0	0	0	0	1	0	0	6	280	0	0	286	0	2	574	15	0	591	888
7:45	0	14	0	13	0	27	0	0	0	1	2	1	1	12	232	0	0	245	0	0	510	14	0	524	797
8:00	0	9	0	8	1	17	0	0	0	0	1	0	0	8	207	0	0	215	0	0	481	8	0	489	721
<b>Total</b>	<b>0</b>	<b>35</b>	<b>0</b>	<b>30</b>	<b>2</b>	<b>65</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>4</b>	<b>1</b>	<b>1</b>	<b>34</b>	<b>926</b>	<b>1</b>	<b>0</b>	<b>962</b>	<b>0</b>	<b>2</b>	<b>2132</b>	<b>51</b>	<b>0</b>	<b>2185</b>	<b>3213</b>
Approach %	0.0	53.8	0.0	46.2	-	-	0.0	0.0	0.0	100.0	-	-	0.1	3.5	96.3	0.1	-	-	0.0	0.1	97.6	2.3	-	-	-
Total %	0.0	1.1	0.0	0.9	-	2.0	0.0	0.0	0.0	0.0	-	0.0	0.0	1.1	28.8	0.0	-	29.9	0.0	0.1	66.4	1.6	-	68.0	-
PHF	0.000	0.625	0.000	0.577	-	0.602	0.000	0.000	0.000	0.250	-	0.250	0.250	0.708	0.827	0.250	-	0.841	0.000	0.250	0.929	0.850	-	0.924	0.905
Lights	0	34	0	29	-	63	0	0	0	1	-	1	1	34	898	1	-	934	0	2	2099	51	-	2152	3150
% Lights	-	97.1	-	96.7	-	96.9	-	-	-	100.0	-	100.0	100.0	100.0	97.0	100.0	-	97.1	-	100.0	98.5	100.0	-	98.5	98.0
Other Vehicles	0	1	0	1	-	2	0	0	0	0	-	0	0	0	28	0	-	28	0	0	33	0	-	33	63
% Other Vehicles	-	2.9	-	3.3	-	3.1	-	-	-	0.0	-	0.0	0.0	0.0	3.0	0.0	-	2.9	-	0.0	1.5	0.0	-	1.5	2.0
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	-	-	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	50.0	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	4	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	50.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-



Turning Movement Peak Hour Data Plot (7:15)

### Turning Movement Peak Hour Data (13:00)

Start Time	Braden Ave Eastbound						Braden Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
13:00	0	4	0	6	1	10	0	0	0	0	3	0	0	7	270	0	0	277	0	0	272	5	2	277	564
13:15	0	4	0	8	1	12	0	0	0	1	0	1	1	4	304	1	0	310	0	0	257	5	0	262	585
13:30	0	8	0	3	1	11	0	0	0	1	0	1	0	10	289	0	0	299	0	2	278	4	0	284	595
13:45	0	8	0	9	0	17	0	0	0	0	0	0	0	7	275	0	0	282	0	0	246	5	0	251	550
<b>Total</b>	<b>0</b>	<b>24</b>	<b>0</b>	<b>26</b>	<b>3</b>	<b>50</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2</b>	<b>3</b>	<b>2</b>	<b>1</b>	<b>28</b>	<b>1138</b>	<b>1</b>	<b>0</b>	<b>1168</b>	<b>0</b>	<b>2</b>	<b>1053</b>	<b>19</b>	<b>2</b>	<b>1074</b>	<b>2294</b>
Approach %	0.0	48.0	0.0	52.0	-	-	0.0	0.0	0.0	100.0	-	-	0.1	2.4	97.4	0.1	-	-	0.0	0.2	98.0	1.8	-	-	-
Total %	0.0	1.0	0.0	1.1	-	2.2	0.0	0.0	0.0	0.1	-	0.1	0.0	1.2	49.6	0.0	-	50.9	0.0	0.1	45.9	0.8	-	46.8	-
PHF	0.000	0.750	0.000	0.722	-	0.735	0.000	0.000	0.000	0.500	-	0.500	0.250	0.700	0.936	0.250	-	0.942	0.000	0.250	0.947	0.950	-	0.945	0.964
Lights	0	24	0	24	-	48	0	0	0	1	-	1	1	27	1105	0	-	1133	0	2	1028	19	-	1049	2231
% Lights	-	100.0	-	92.3	-	96.0	-	-	-	50.0	-	50.0	100.0	96.4	97.1	0.0	-	97.0	-	100.0	97.6	100.0	-	97.7	97.3
Other Vehicles	0	0	0	2	-	2	0	0	0	1	-	1	0	1	33	1	-	35	0	0	23	0	-	23	61
% Other Vehicles	-	0.0	-	7.7	-	4.0	-	-	-	50.0	-	50.0	0.0	3.6	2.9	100.0	-	3.0	-	0.0	2.2	0.0	-	2.1	2.7
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	2	0	-	2	2
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	-	-	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.2	0.0	-	0.2	0.1
Bicycles on Crosswalk	-	-	-	-	2	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	66.7	-	-	-	-	-	33.3	-	-	-	-	-	-	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	2	-	-
% Pedestrians	-	-	-	-	33.3	-	-	-	-	-	66.7	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



Turning Movement Peak Hour Data Plot (13:00)



### Turning Movement Peak Hour Data (16:30)

Start Time	Braden Ave Eastbound						Braden Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:30	0	9	0	15	1	24	0	0	0	1	2	1	0	8	490	0	0	498	0	0	278	11	2	289	812
16:45	0	9	0	10	1	19	0	0	0	0	1	0	1	6	468	0	0	475	0	0	284	4	1	288	782
17:00	0	19	0	8	1	27	0	0	0	1	1	1	0	7	463	0	0	470	0	0	256	4	0	260	758
17:15	0	5	0	10	2	15	0	0	0	0	1	0	0	11	549	0	0	560	0	0	289	5	3	294	869
<b>Total</b>	<b>0</b>	<b>42</b>	<b>0</b>	<b>43</b>	<b>5</b>	<b>85</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2</b>	<b>5</b>	<b>2</b>	<b>1</b>	<b>32</b>	<b>1970</b>	<b>0</b>	<b>0</b>	<b>2003</b>	<b>0</b>	<b>0</b>	<b>1107</b>	<b>24</b>	<b>6</b>	<b>1131</b>	<b>3221</b>
Approach %	0.0	49.4	0.0	50.6	-	-	0.0	0.0	0.0	100.0	-	-	0.0	1.6	98.4	0.0	-	-	0.0	0.0	97.9	2.1	-	-	-
Total %	0.0	1.3	0.0	1.3	-	2.6	0.0	0.0	0.0	0.1	-	0.1	0.0	1.0	61.2	0.0	-	62.2	0.0	0.0	34.4	0.7	-	35.1	-
PHF	0.000	0.553	0.000	0.717	-	0.787	0.000	0.000	0.000	0.500	-	0.500	0.250	0.727	0.897	0.000	-	0.894	0.000	0.000	0.958	0.545	-	0.962	0.927
Lights	0	42	0	43	-	85	0	0	0	2	-	2	1	32	1957	0	-	1990	0	0	1100	24	-	1124	3201
% Lights	-	100.0	-	100.0	-	100.0	-	-	-	100.0	-	100.0	100.0	100.0	99.3	-	-	99.4	-	-	99.4	100.0	-	99.4	99.4
Other Vehicles	0	0	0	0	-	0	0	0	0	0	-	0	0	0	13	0	-	13	0	0	7	0	-	7	20
% Other Vehicles	-	0.0	-	0.0	-	0.0	-	-	-	0.0	-	0.0	0.0	0.0	0.7	-	-	0.6	-	-	0.6	0.0	-	0.6	0.6
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	-	-	0.0	-	0.0	0.0	0.0	0.0	-	-	0.0	-	-	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	0.0	-	-	-	-	-	40.0	-	-	-	-	-	-	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	5	-	-	-	-	-	3	-	-	-	-	-	0	-	-	-	-	-	6	-	-
% Pedestrians	-	-	-	-	100.0	-	-	-	-	-	60.0	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



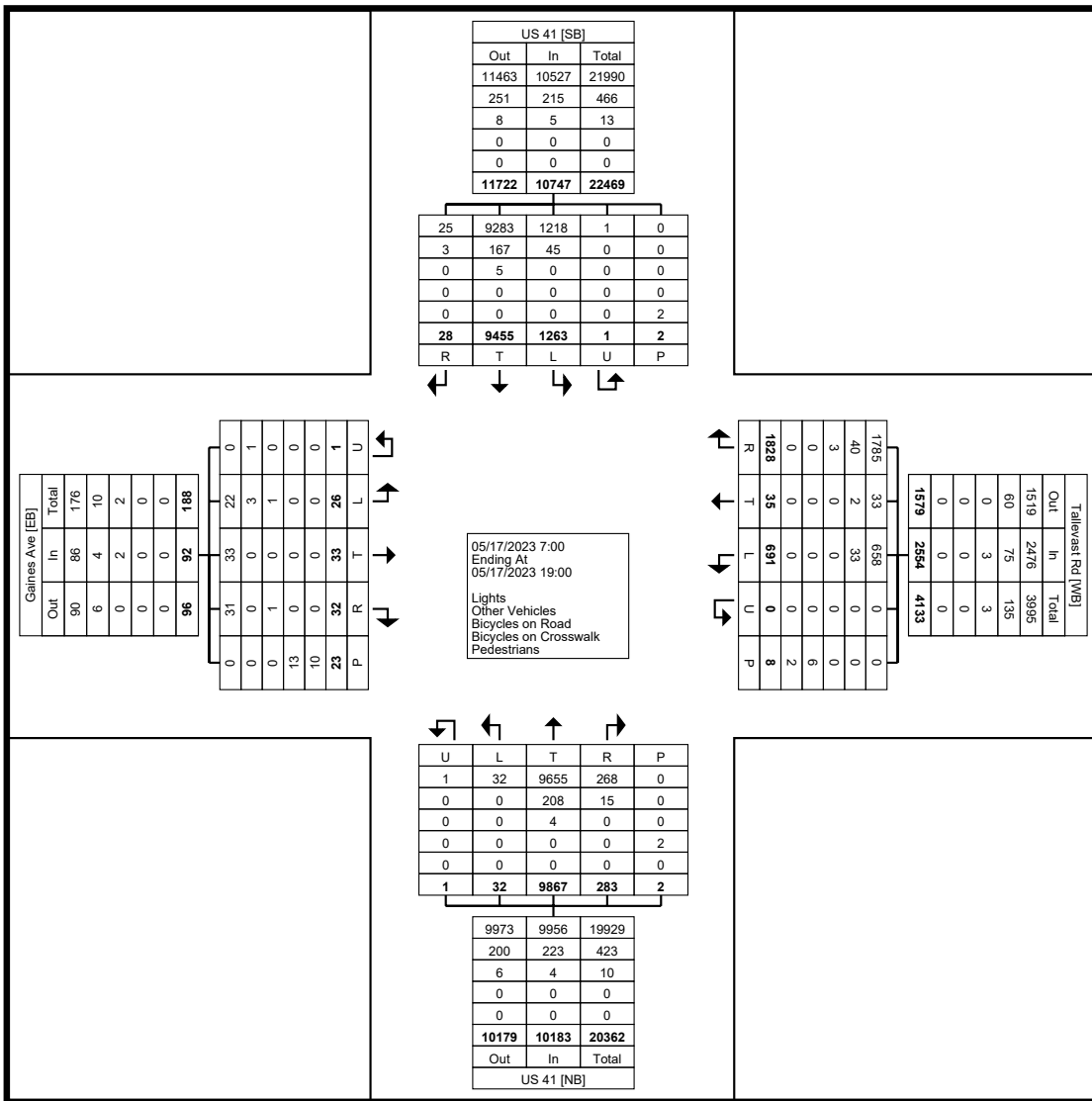
US 41 at Tallevast Rd/Gaines Ave  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 45 US 41 at Tallevast Rd/Gaines Ave  
Site Code: 45  
Start Date: 05/17/2023  
Page No: 1

### Turning Movement Data

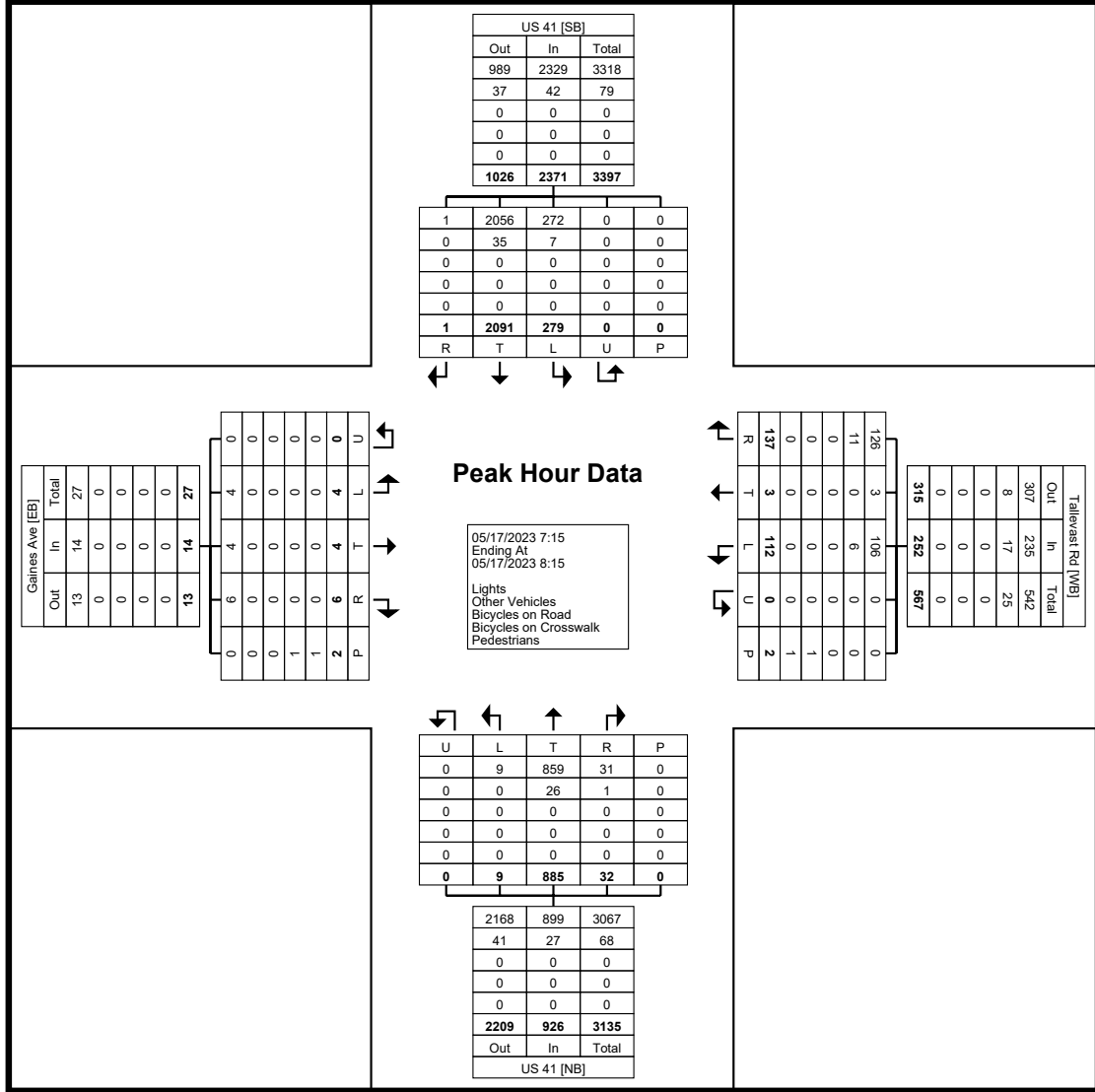
Start Time	Gaines Ave Eastbound						Tallevast Rd Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	1	1	1	2	3	0	15	1	34	0	50	0	0	148	5	0	153	0	70	476	0	0	546	752
7:15	0	0	3	1	0	4	0	27	1	29	0	57	0	3	201	4	0	208	0	83	548	0	0	631	900
7:30	0	3	0	1	0	4	0	31	2	30	1	63	0	2	272	5	0	279	0	78	563	0	0	641	987
7:45	0	0	1	2	0	3	0	33	0	43	0	76	0	2	229	11	0	242	0	78	522	0	0	600	921
Hourly Total	0	4	5	5	2	14	0	106	4	136	1	246	0	7	850	25	0	882	0	309	2109	0	0	2418	3560
8:00	0	1	0	2	2	3	0	21	0	35	1	56	0	2	183	12	0	197	0	40	458	1	0	499	755
8:15	0	0	2	3	0	5	0	24	0	49	0	73	0	1	213	11	0	225	0	67	426	0	0	493	796
8:30	0	1	1	0	1	2	0	24	2	37	1	63	0	0	207	6	0	213	0	32	455	0	0	487	765
8:45	0	0	3	2	1	5	0	28	0	39	0	67	0	0	178	9	0	187	0	54	359	1	0	414	673
Hourly Total	0	2	6	7	4	15	0	97	2	160	2	259	0	3	781	38	0	822	0	193	1698	2	0	1893	2989
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	1	1	0	2	0	29	0	50	0	79	0	1	241	11	0	253	0	26	240	3	0	269	603
12:15	0	1	0	1	0	2	0	27	0	44	0	71	0	0	239	14	0	253	0	33	243	2	0	278	604
12:30	0	3	1	1	0	5	0	18	0	41	0	59	0	1	249	16	0	266	0	48	222	0	0	270	600
12:45	0	1	2	1	0	4	0	24	0	43	0	67	0	2	260	10	0	272	0	44	238	0	0	282	625
Hourly Total	0	5	4	4	0	13	0	98	0	178	0	276	0	4	989	51	0	1044	0	151	943	5	0	1099	2432
13:00	0	0	0	0	0	0	0	21	1	41	0	63	0	0	228	13	0	241	1	28	246	1	0	276	580
13:15	0	2	1	1	0	4	0	17	4	58	0	79	0	1	253	12	0	266	0	43	222	2	0	267	616
13:30	0	1	2	3	0	6	0	26	1	44	0	71	0	3	292	8	0	303	0	36	233	3	0	272	652
13:45	0	0	4	0	0	4	0	21	2	45	0	68	0	0	233	13	0	246	0	35	232	1	0	268	586
Hourly Total	0	3	7	4	0	14	0	85	8	188	0	281	0	4	1006	46	0	1056	1	142	933	7	0	1083	2434
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	1	2	0	0	3	0	12	1	90	0	103	0	1	395	8	0	404	0	33	227	0	0	260	770
15:15	1	1	1	0	2	3	0	15	3	57	0	75	0	2	392	10	1	404	0	34	237	1	0	272	754
15:30	0	2	1	2	1	5	0	19	0	80	0	99	0	0	378	5	0	383	0	39	282	0	2	321	808
15:45	0	1	0	0	0	1	0	24	0	71	0	95	0	2	364	7	0	373	0	39	251	2	0	292	761
Hourly Total	1	5	4	2	3	12	0	70	4	298	0	372	0	5	1529	30	1	1564	0	145	997	3	2	1145	3093
16:00	0	0	1	1	1	2	0	30	3	84	0	117	0	0	365	6	1	371	0	29	235	2	0	266	756
16:15	0	1	0	1	0	2	0	18	0	81	1	99	0	0	414	1	0	415	0	40	306	1	0	347	863
16:30	0	0	0	0	0	0	0	28	2	108	0	138	0	1	502	8	0	511	0	28	245	0	0	273	922
16:45	0	0	1	1	1	2	0	18	1	76	0	95	0	1	493	5	0	499	0	29	246	1	0	276	872
Hourly Total	0	1	2	3	2	6	0	94	6	349	1	449	0	2	1774	20	1	1796	0	126	1032	4	0	1162	3413
17:00	0	0	4	1	2	5	0	21	1	101	1	123	1	1	431	17	0	450	0	28	261	0	0	289	867
17:15	0	3	0	1	3	4	0	24	1	84	1	109	0	0	516	7	0	523	0	22	254	2	0	278	914
17:30	0	1	0	1	1	2	0	25	5	90	0	120	0	0	421	11	0	432	0	40	239	1	0	280	834
17:45	0	0	1	1	0	2	0	11	2	61	1	74	0	1	416	11	0	428	0	27	237	2	0	266	770
Hourly Total	0	4	5	4	6	13	0	81	9	336	3	426	1	2	1784	46	0	1833	0	117	991	5	0	1113	3385
18:00	0	0	0	2	0	2	0	20	1	43	1	64	0	3	372	9	0	384	0	25	188	0	0	213	663
18:15	0	1	0	1	3	2	0	16	1	70	0	87	0	0	302	4	0	306	0	21	207	0	0	228	623
18:30	0	0	0	0	1	0	0	13	0	33	0	46	0	2	265	6	0	273	0	18	170	2	0	190	509
18:45	0	1	0	0	2	1	0	11	0	37	0	48	0	0	215	8	0	223	0	16	187	0	0	203	475
Hourly Total	0	2	0	3	6	5	0	60	2	183	1	245	0	5	1154	27	0	1186	0	80	752	2	0	834	2270
Grand Total	1	26	33	32	23	92	0	691	35	1828	8	2554	1	32	9867	283	2	10183	1	1263	9455	28	2	10747	23576
Approach %	1.1	28.3	35.9	34.8	-	-	0.0	27.1	1.4	71.6	-	-	0.0	0.3	96.9	2.8	-	-	0.0	11.8	88.0	0.3	-	-	-
Total %	0.0	0.1	0.1	0.1	-	0.4	0.0	2.9	0.1	7.8	-	10.8	0.0	0.1	41.9	1.2	-	43.2	0.0	5.4	40.1	0.1	-	45.6	-
Lights	0	22	33	31	-	86	0	658	33	1785	-	2476	1	32	9655	268	-	9956	1	1218	9283	25	-	10527	23045
% Lights	0.0	84.6	100.0	96.9	-	93.5	-	95.2	94.3	97.6	-	96.9	100.0	100.0	97.9	94.7	-	97.8	100.0	96.4	98.2	89.3	-	98.0	97.7
Other Vehicles	1	3	0	0	-	4	0	33	2	40	-	75	0	0	208	15	-	223	0	45	167	3	-	215	517
% Other Vehicles	100.0	11.5	0.0	0.0	-	4.3	-	4.8	5.7	2.2	-	2.9	0.0	0.0	2.1	5.3	-	2.2	0.0	3.6	1.8	10.7	-	2.0	2.2
Bicycles on Road	0	1	0	1	-	2	0	0	0	3	-	3	0	0	4	0	-	4	0	0	5	0	-	5	14
% Bicycles on Road	0.0	3.8	0.0	3.1	-	2.2	-	0.0	0.0	0.2	-	0.1	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.1	0.0	-	0.0	0.1
Bicycles on Crosswalk	-	-	-	-	13	-	-	-	-	-	6	-	-	-	-	-	2	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	56.5	-	-	-	-	-	75.0	-	-	-	-	-	100.0	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	10	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	2	-	-
% Pedestrians	-	-	-	-	43.5	-	-	-	-	-	25.0	-	-	-	-	-	0.0	-	-	-	-	-	100.0	-	-



Turning Movement Data Plot

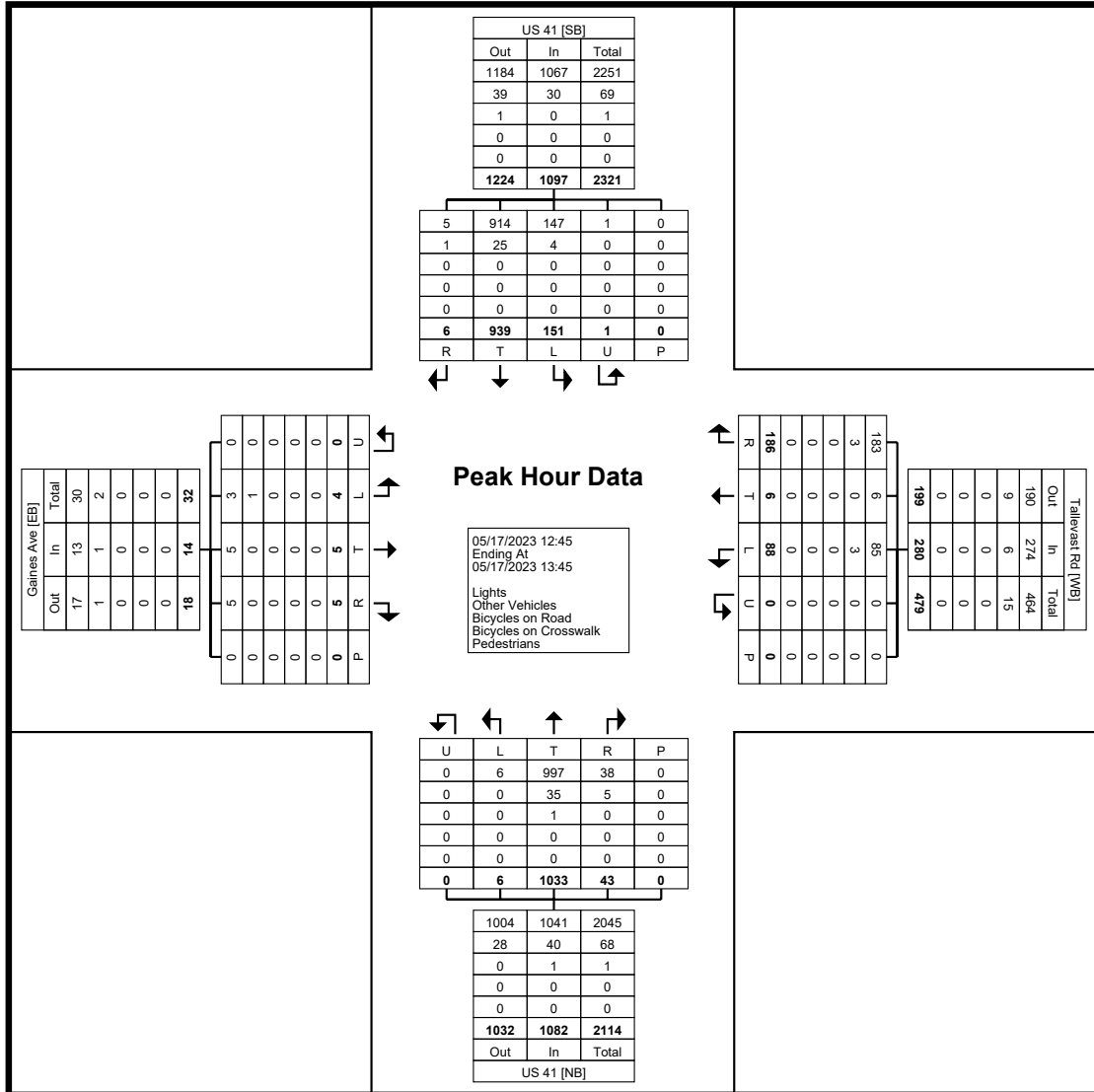
### Turning Movement Peak Hour Data (7:15)

Start Time	Gaines Ave Eastbound						Tallevast Rd Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15	0	0	3	1	0	4	0	27	1	29	0	57	0	3	201	4	0	208	0	83	548	0	0	631	900
7:30	0	3	0	1	0	4	0	31	2	30	1	63	0	2	272	5	0	279	0	78	563	0	0	641	987
7:45	0	0	1	2	0	3	0	33	0	43	0	76	0	2	229	11	0	242	0	78	522	0	0	600	921
8:00	0	1	0	2	2	3	0	21	0	35	1	56	0	2	183	12	0	197	0	40	458	1	0	499	755
<b>Total</b>	<b>0</b>	<b>4</b>	<b>4</b>	<b>6</b>	<b>2</b>	<b>14</b>	<b>0</b>	<b>112</b>	<b>3</b>	<b>137</b>	<b>2</b>	<b>252</b>	<b>0</b>	<b>9</b>	<b>885</b>	<b>32</b>	<b>0</b>	<b>926</b>	<b>0</b>	<b>279</b>	<b>2091</b>	<b>1</b>	<b>0</b>	<b>2371</b>	<b>3563</b>
Approach %	0.0	28.6	28.6	42.9	-	-	0.0	44.4	1.2	54.4	-	-	0.0	1.0	95.6	3.5	-	-	0.0	11.8	88.2	0.0	-	-	-
Total %	0.0	0.1	0.1	0.2	-	0.4	0.0	3.1	0.1	3.8	-	7.1	0.0	0.3	24.8	0.9	-	26.0	0.0	7.8	58.7	0.0	-	66.5	-
PHF	0.000	0.333	0.333	0.750	-	0.875	0.000	0.848	0.375	0.797	-	0.829	0.000	0.750	0.813	0.667	-	0.830	0.000	0.840	0.929	0.250	-	0.925	0.902
Lights	0	4	4	6	-	14	0	106	3	126	-	235	0	9	859	31	-	899	0	272	2056	1	-	2329	3477
% Lights	-	100.0	100.0	100.0	-	100.0	-	94.6	100.0	92.0	-	93.3	-	100.0	97.1	96.9	-	97.1	-	97.5	98.3	100.0	-	98.2	97.6
Other Vehicles	0	0	0	0	-	0	0	6	0	11	-	17	0	0	26	1	-	27	0	7	35	0	-	42	86
% Other Vehicles	-	0.0	0.0	0.0	-	0.0	-	5.4	0.0	8.0	-	6.7	-	0.0	2.9	3.1	-	2.9	-	2.5	1.7	0.0	-	1.8	2.4
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	1	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	50.0	-	-	-	-	-	50.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	50.0	-	-	-	-	-	50.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-



### Turning Movement Peak Hour Data (12:45)

Start Time	Gaines Ave Eastbound						Tallevast Rd Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:45	0	1	2	1	0	4	0	24	0	43	0	67	0	2	260	10	0	272	0	44	238	0	0	282	625
13:00	0	0	0	0	0	0	0	21	1	41	0	63	0	0	228	13	0	241	1	28	246	1	0	276	580
13:15	0	2	1	1	0	4	0	17	4	58	0	79	0	1	253	12	0	266	0	43	222	2	0	267	616
13:30	0	1	2	3	0	6	0	26	1	44	0	71	0	3	292	8	0	303	0	36	233	3	0	272	652
<b>Total</b>	<b>0</b>	<b>4</b>	<b>5</b>	<b>5</b>	<b>0</b>	<b>14</b>	<b>0</b>	<b>88</b>	<b>6</b>	<b>186</b>	<b>0</b>	<b>280</b>	<b>0</b>	<b>6</b>	<b>1033</b>	<b>43</b>	<b>0</b>	<b>1082</b>	<b>1</b>	<b>151</b>	<b>939</b>	<b>6</b>	<b>0</b>	<b>1097</b>	<b>2473</b>
Approach %	0.0	28.6	35.7	35.7	-	-	0.0	31.4	2.1	66.4	-	-	0.0	0.6	95.5	4.0	-	-	0.1	13.8	85.6	0.5	-	-	-
Total %	0.0	0.2	0.2	0.2	-	0.6	0.0	3.6	0.2	7.5	-	11.3	0.0	0.2	41.8	1.7	-	43.8	0.0	6.1	38.0	0.2	-	44.4	-
PHF	0.000	0.500	0.625	0.417	-	0.583	0.000	0.846	0.375	0.802	-	0.886	0.000	0.500	0.884	0.827	-	0.893	0.250	0.858	0.954	0.500	-	0.973	0.948
Lights	0	3	5	5	-	13	0	85	6	183	-	274	0	6	997	38	-	1041	1	147	914	5	-	1067	2395
% Lights	-	75.0	100.0	100.0	-	92.9	-	96.6	100.0	98.4	-	97.9	-	100.0	96.5	88.4	-	96.2	100.0	97.4	97.3	83.3	-	97.3	96.8
Other Vehicles	0	1	0	0	-	1	0	3	0	3	-	6	0	0	35	5	-	40	0	4	25	1	-	30	77
% Other Vehicles	-	25.0	0.0	0.0	-	7.1	-	3.4	0.0	1.6	-	2.1	-	0.0	3.4	11.6	-	3.7	0.0	2.6	2.7	16.7	-	2.7	3.1
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	1	0	-	1	0	0	0	0	-	0	1
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.1	0.0	-	0.1	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

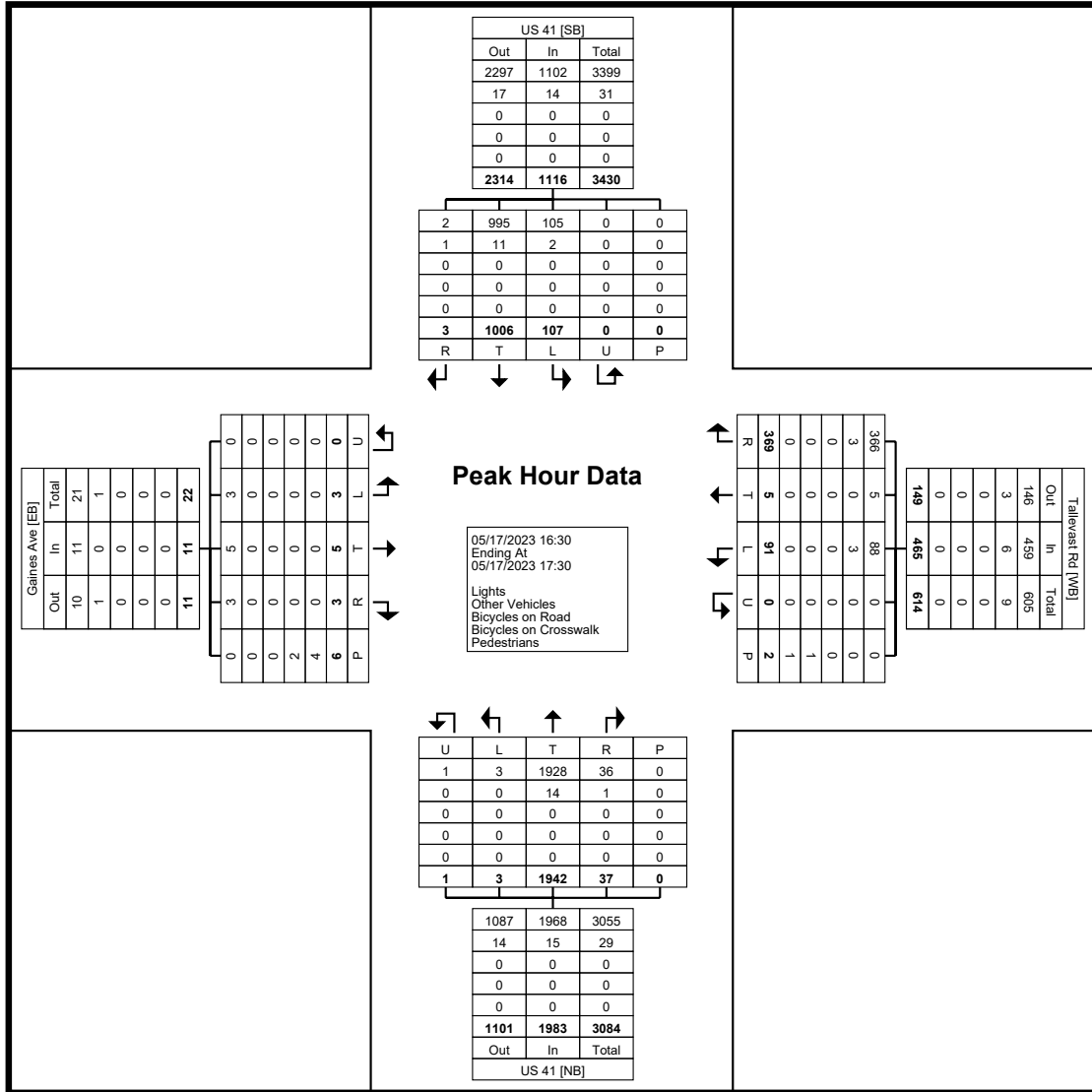


Turning Movement Peak Hour Data Plot (12:45)



### Turning Movement Peak Hour Data (16:30)

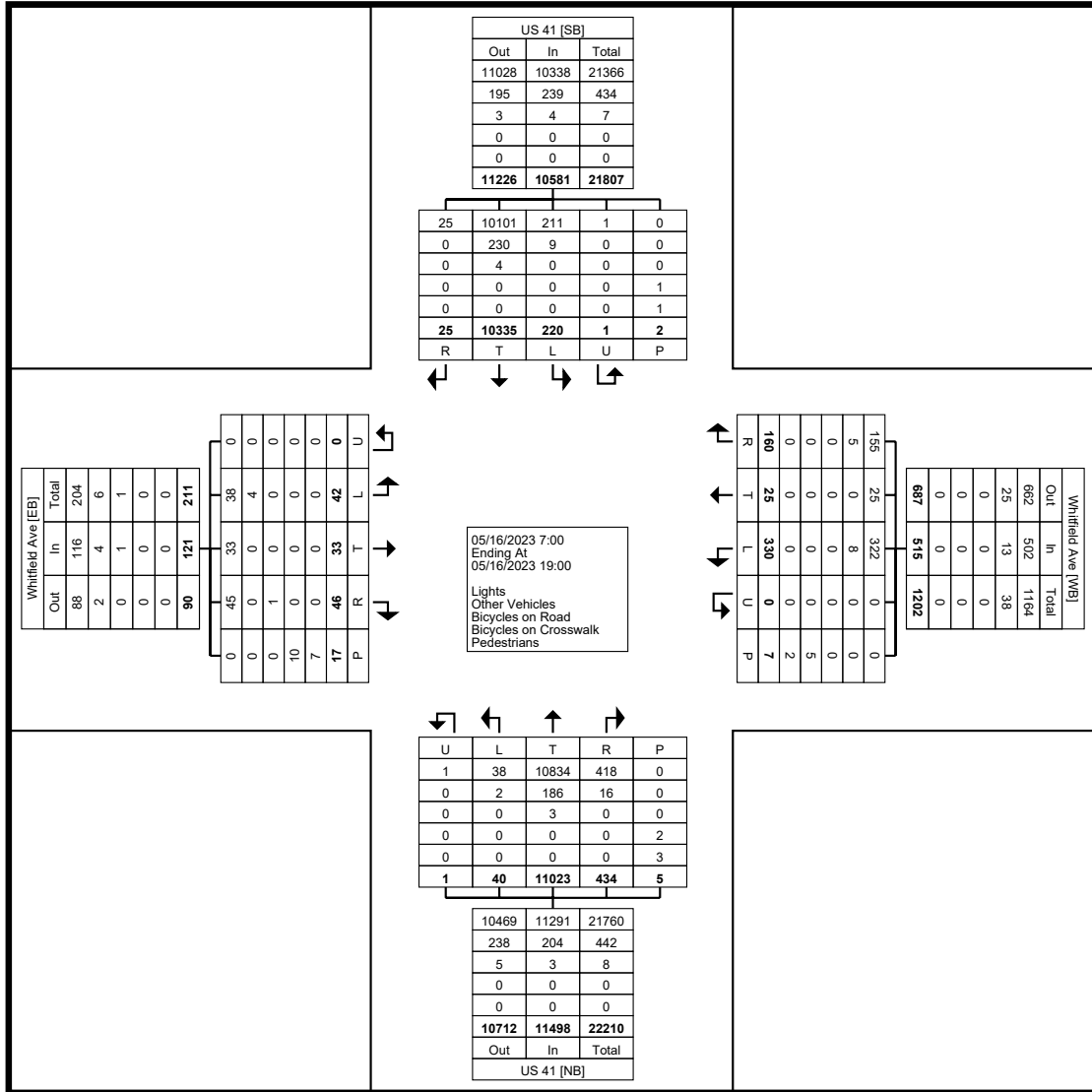
Start Time	Gaines Ave Eastbound						Tallevast Rd Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:30	0	0	0	0	0	0	0	28	2	108	0	138	0	1	502	8	0	511	0	28	245	0	0	273	922
16:45	0	0	1	1	1	2	0	18	1	76	0	95	0	1	493	5	0	499	0	29	246	1	0	276	872
17:00	0	0	4	1	2	5	0	21	1	101	1	123	1	1	431	17	0	450	0	28	261	0	0	289	867
17:15	0	3	0	1	3	4	0	24	1	84	1	109	0	0	516	7	0	523	0	22	254	2	0	278	914
<b>Total</b>	<b>0</b>	<b>3</b>	<b>5</b>	<b>3</b>	<b>6</b>	<b>11</b>	<b>0</b>	<b>91</b>	<b>5</b>	<b>369</b>	<b>2</b>	<b>465</b>	<b>1</b>	<b>3</b>	<b>1942</b>	<b>37</b>	<b>0</b>	<b>1983</b>	<b>0</b>	<b>107</b>	<b>1006</b>	<b>3</b>	<b>0</b>	<b>1116</b>	<b>3575</b>
Approach %	0.0	27.3	45.5	27.3	-	-	0.0	19.6	1.1	79.4	-	-	0.1	0.2	97.9	1.9	-	-	0.0	9.6	90.1	0.3	-	-	-
Total %	0.0	0.1	0.1	0.1	-	0.3	0.0	2.5	0.1	10.3	-	13.0	0.0	0.1	54.3	1.0	-	55.5	0.0	3.0	28.1	0.1	-	31.2	-
PHF	0.000	0.250	0.313	0.750	-	0.550	0.000	0.813	0.625	0.854	-	0.842	0.250	0.750	0.941	0.544	-	0.948	0.000	0.922	0.964	0.375	-	0.965	0.969
Lights	0	3	5	3	-	11	0	88	5	366	-	459	1	3	1928	36	-	1968	0	105	995	2	-	1102	3540
% Lights	-	100.0	100.0	100.0	-	100.0	-	96.7	100.0	99.2	-	98.7	100.0	100.0	99.3	97.3	-	99.2	-	98.1	98.9	66.7	-	98.7	99.0
Other Vehicles	0	0	0	0	-	0	0	3	0	3	-	6	0	0	14	1	-	15	0	2	11	1	-	14	35
% Other Vehicles	-	0.0	0.0	0.0	-	0.0	-	3.3	0.0	0.8	-	1.3	0.0	0.0	0.7	2.7	-	0.8	-	1.9	1.1	33.3	-	1.3	1.0
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	2	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	33.3	-	-	-	-	-	50.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	4	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	66.7	-	-	-	-	-	50.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-



Turning Movement Peak Hour Data Plot (16:30)

### Turning Movement Data

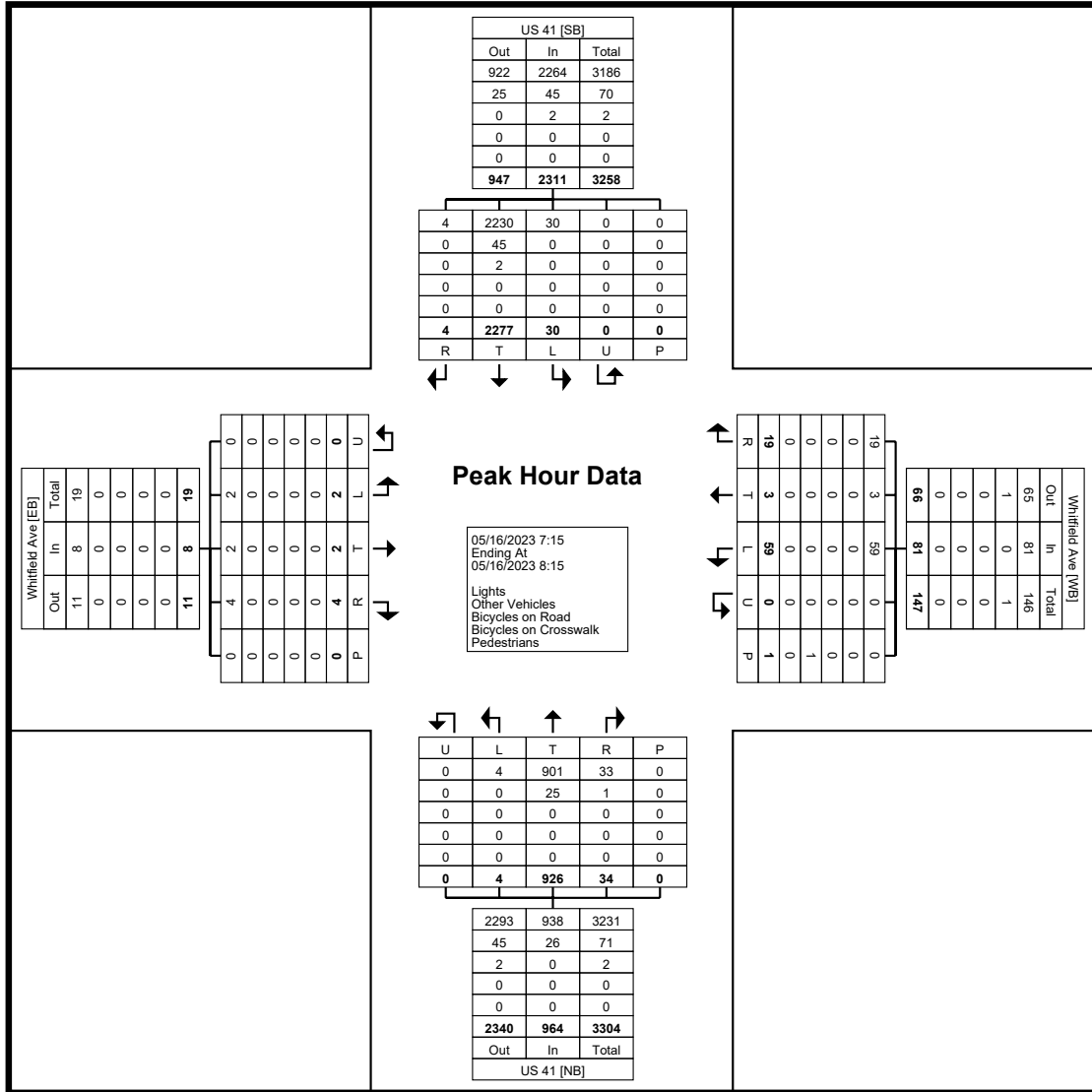
Start Time	Whitfield Ave Eastbound						Whitfield Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	3	0	1	2	4	0	9	0	0	0	9	0	0	190	7	0	197	0	4	467	0	0	471	681
7:15	0	1	0	1	0	2	0	15	0	7	0	22	0	0	253	11	0	264	0	2	618	1	0	621	909
7:30	0	0	2	0	0	2	0	16	1	4	0	21	0	1	242	7	0	250	0	7	573	1	0	581	854
7:45	0	1	0	1	0	2	0	16	2	2	1	20	0	2	238	8	0	248	0	5	591	0	0	596	866
Hourly Total	0	5	2	3	2	10	0	56	3	13	1	72	0	3	923	33	0	959	0	18	2249	2	0	2269	3310
8:00	0	0	0	2	0	2	0	12	0	6	0	18	0	1	193	8	0	202	0	16	495	2	0	513	735
8:15	0	0	1	2	0	3	0	25	1	2	0	28	0	0	219	6	1	225	0	4	500	0	0	504	760
8:30	0	4	0	1	0	5	0	20	0	6	1	26	0	2	234	10	1	246	0	12	479	0	0	491	768
8:45	0	3	1	1	0	5	0	16	0	7	1	23	0	1	192	8	0	201	0	8	406	0	0	414	643
Hourly Total	0	7	2	6	0	15	0	73	1	21	2	95	0	4	838	32	2	874	0	40	1880	2	0	1922	2906
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	2	1	0	3	0	5	0	0	0	5	0	2	316	16	0	334	0	4	248	2	0	254	596
12:15	0	1	1	4	2	6	0	8	1	3	0	12	0	2	246	10	0	258	0	6	277	1	0	284	560
12:30	0	2	2	3	0	7	0	9	0	1	0	10	0	1	281	14	0	296	0	9	258	1	0	268	581
12:45	0	2	1	4	1	7	0	6	2	2	0	10	0	3	235	12	0	250	0	8	279	0	0	287	554
Hourly Total	0	5	6	12	3	23	0	28	3	6	0	37	0	8	1078	52	0	1138	0	27	1062	4	0	1093	2291
13:00	0	0	0	3	0	3	0	14	2	4	0	20	0	0	252	11	0	263	0	7	236	0	0	243	529
13:15	0	1	0	2	1	3	0	9	0	0	0	9	0	1	234	14	0	249	0	9	266	1	0	276	537
13:30	0	1	2	1	0	4	0	6	2	8	0	16	0	1	320	11	0	332	0	3	268	0	0	271	623
13:45	0	2	2	0	0	4	0	12	0	5	1	17	0	2	294	8	0	304	0	9	246	2	0	257	582
Hourly Total	0	4	4	6	1	14	0	41	4	17	1	62	0	4	1100	44	0	1148	0	28	1016	3	0	1047	2271
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	1	1	4	0	6	0	7	0	5	0	12	1	2	423	13	0	439	0	10	258	1	0	269	726
15:15	0	2	1	0	0	3	0	10	2	3	2	15	0	1	442	16	0	459	0	7	275	1	0	283	760
15:30	0	3	2	0	1	5	0	11	2	6	0	19	0	2	402	14	0	418	0	8	291	2	1	301	743
15:45	0	1	0	3	0	4	0	9	0	7	0	16	0	4	504	23	0	531	0	7	283	1	0	291	842
Hourly Total	0	7	4	7	1	18	0	37	4	21	2	62	1	9	1771	66	0	1847	0	32	1107	5	1	1144	3071
16:00	0	1	0	2	0	3	0	4	5	11	0	20	0	1	469	16	0	486	0	10	282	1	0	293	802
16:15	0	0	5	1	1	6	0	3	0	14	0	17	0	1	529	22	0	552	0	4	274	1	0	279	854
16:30	0	3	2	0	0	5	0	9	0	12	0	21	0	0	553	17	1	570	0	4	241	0	0	245	841
16:45	0	2	1	1	1	4	0	11	2	8	0	21	0	1	466	28	0	495	1	8	286	0	0	295	815
Hourly Total	0	6	8	4	2	18	0	27	7	45	0	79	0	3	2017	83	1	2103	1	26	1083	2	0	1112	3312
17:00	0	3	2	0	0	5	0	5	1	5	0	11	0	1	484	20	0	505	0	10	271	0	1	281	802
17:15	0	0	1	2	0	3	0	12	0	8	0	20	0	0	427	16	1	443	0	10	305	0	0	315	781
17:30	0	0	1	3	2	4	0	10	0	10	0	20	0	2	685	22	0	709	0	8	262	0	0	270	1003
17:45	0	1	0	0	2	1	0	6	0	2	0	8	0	0	434	15	1	449	0	1	240	1	0	242	700
Hourly Total	0	4	4	5	4	13	0	33	1	25	0	59	0	3	2030	73	2	2106	0	29	1078	1	1	1108	3286
18:00	0	3	1	1	1	5	0	11	0	2	0	13	0	1	407	19	0	427	0	4	251	0	0	255	700
18:15	0	0	1	1	1	2	0	6	0	5	0	11	0	1	290	11	0	302	0	8	228	0	0	236	551
18:30	0	0	1	0	2	1	0	8	2	1	0	11	0	3	301	11	0	315	0	3	196	2	0	201	528
18:45	0	1	0	1	0	2	0	10	0	4	1	14	0	1	268	10	0	279	0	5	185	4	0	194	489
Hourly Total	0	4	3	3	4	10	0	35	2	12	1	49	0	6	1266	51	0	1323	0	20	860	6	0	886	2268
Grand Total	0	42	33	46	17	121	0	330	25	160	7	515	1	40	11023	434	5	11498	1	220	10335	25	2	10581	22715
Approach %	0.0	34.7	27.3	38.0	-	-	0.0	64.1	4.9	31.1	-	-	0.0	0.3	95.9	3.8	-	-	0.0	2.1	97.7	0.2	-	-	-
Total %	0.0	0.2	0.1	0.2	-	0.5	0.0	1.5	0.1	0.7	-	2.3	0.0	0.2	48.5	1.9	-	50.6	0.0	1.0	45.5	0.1	-	46.6	-
Lights	0	38	33	45	-	116	0	322	25	155	-	502	1	38	10834	418	-	11291	1	211	10101	25	-	10338	22247
% Lights	-	90.5	100.0	97.8	-	95.9	-	97.6	100.0	96.9	-	97.5	100.0	95.0	98.3	96.3	-	98.2	100.0	95.9	97.7	100.0	-	97.7	97.9
Other Vehicles	0	4	0	0	-	4	0	8	0	5	-	13	0	2	186	16	-	204	0	9	230	0	-	239	460
% Other Vehicles	-	9.5	0.0	0.0	-	3.3	-	2.4	0.0	3.1	-	2.5	0.0	5.0	1.7	3.7	-	1.8	0.0	4.1	2.2	0.0	-	2.3	2.0
Bicycles on Road	0	0	0	1	-	1	0	0	0	0	-	0	0	0	3	0	-	3	0	0	4	0	-	4	8
% Bicycles on Road	-	0.0	0.0	2.2	-	0.8	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	10	-	-	-	-	-	5	-	-	-	-	-	2	-	-	-	-	-	1	-	-
% Bicycles on Crosswalk	-	-	-	-	58.8	-	-	-	-	-	71.4	-	-	-	-	-	40.0	-	-	-	-	-	50.0	-	-
Pedestrians	-	-	-	-	7	-	-	-	-	-	2	-	-	-	-	-	3	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	41.2	-	-	-	-	-	28.6	-	-	-	-	-	60.0	-	-	-	-	-	50.0	-	-



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:15)

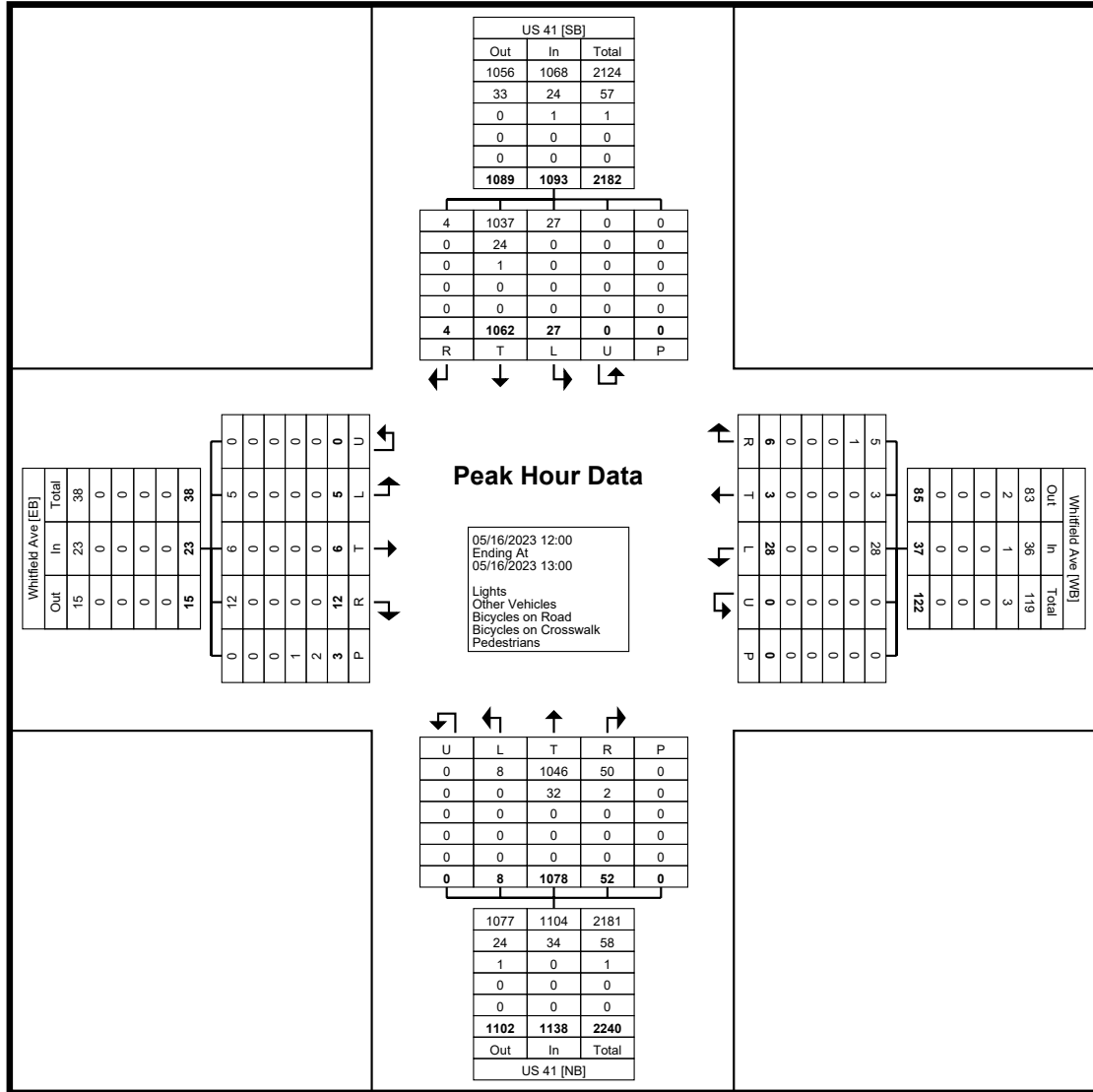
Start Time	Whitfield Ave Eastbound						Whitfield Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15	0	1	0	1	0	2	0	15	0	7	0	22	0	0	253	11	0	264	0	2	618	1	0	621	909
7:30	0	0	2	0	0	2	0	16	1	4	0	21	0	1	242	7	0	250	0	7	573	1	0	581	854
7:45	0	1	0	1	0	2	0	16	2	2	1	20	0	2	238	8	0	248	0	5	591	0	0	596	866
8:00	0	0	0	2	0	2	0	12	0	6	0	18	0	1	193	8	0	202	0	16	495	2	0	513	735
<b>Total</b>	<b>0</b>	<b>2</b>	<b>2</b>	<b>4</b>	<b>0</b>	<b>8</b>	<b>0</b>	<b>59</b>	<b>3</b>	<b>19</b>	<b>1</b>	<b>81</b>	<b>0</b>	<b>4</b>	<b>926</b>	<b>34</b>	<b>0</b>	<b>964</b>	<b>0</b>	<b>30</b>	<b>2277</b>	<b>4</b>	<b>0</b>	<b>2311</b>	<b>3364</b>
Approach %	0.0	25.0	25.0	50.0	-	-	0.0	72.8	3.7	23.5	-	-	0.0	0.4	96.1	3.5	-	-	0.0	1.3	98.5	0.2	-	-	-
Total %	0.0	0.1	0.1	0.1	-	0.2	0.0	1.8	0.1	0.6	-	2.4	0.0	0.1	27.5	1.0	-	28.7	0.0	0.9	67.7	0.1	-	68.7	-
PHF	0.000	0.500	0.250	0.500	-	1.000	0.000	0.922	0.375	0.679	-	0.920	0.000	0.500	0.915	0.773	-	0.913	0.000	0.469	0.921	0.500	-	0.930	0.925
Lights	0	2	2	4	-	8	0	59	3	19	-	81	0	4	901	33	-	938	0	30	2230	4	-	2264	3291
% Lights	-	100.0	100.0	100.0	-	100.0	-	100.0	100.0	100.0	-	100.0	-	100.0	97.3	97.1	-	97.3	-	100.0	97.9	100.0	-	98.0	97.8
Other Vehicles	0	0	0	0	-	0	0	0	0	0	-	0	0	0	25	1	-	26	0	0	45	0	-	45	71
% Other Vehicles	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	2.7	2.9	-	2.7	-	0.0	2.0	0.0	-	1.9	2.1
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	2	0	-	2	2
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.1	0.0	-	0.1	0.1
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-



Turning Movement Peak Hour Data Plot (7:15)

### Turning Movement Peak Hour Data (12:00)

Start Time	Whitfield Ave Eastbound						Whitfield Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	0	2	1	0	3	0	5	0	0	0	5	0	2	316	16	0	334	0	4	248	2	0	254	596
12:15	0	1	1	4	2	6	0	8	1	3	0	12	0	2	246	10	0	258	0	6	277	1	0	284	560
12:30	0	2	2	3	0	7	0	9	0	1	0	10	0	1	281	14	0	296	0	9	258	1	0	268	581
12:45	0	2	1	4	1	7	0	6	2	2	0	10	0	3	235	12	0	250	0	8	279	0	0	287	554
<b>Total</b>	<b>0</b>	<b>5</b>	<b>6</b>	<b>12</b>	<b>3</b>	<b>23</b>	<b>0</b>	<b>28</b>	<b>3</b>	<b>6</b>	<b>0</b>	<b>37</b>	<b>0</b>	<b>8</b>	<b>1078</b>	<b>52</b>	<b>0</b>	<b>1138</b>	<b>0</b>	<b>27</b>	<b>1062</b>	<b>4</b>	<b>0</b>	<b>1093</b>	<b>2291</b>
Approach %	0.0	21.7	26.1	52.2	-	-	0.0	75.7	8.1	16.2	-	-	0.0	0.7	94.7	4.6	-	-	0.0	2.5	97.2	0.4	-	-	-
Total %	0.0	0.2	0.3	0.5	-	1.0	0.0	1.2	0.1	0.3	-	1.6	0.0	0.3	47.1	2.3	-	49.7	0.0	1.2	46.4	0.2	-	47.7	-
PHF	0.000	0.625	0.750	0.750	-	0.821	0.000	0.778	0.375	0.500	-	0.771	0.000	0.667	0.853	0.813	-	0.852	0.000	0.750	0.952	0.500	-	0.952	0.961
Lights	0	5	6	12	-	23	0	28	3	5	-	36	0	8	1046	50	-	1104	0	27	1037	4	-	1068	2231
% Lights	-	100.0	100.0	100.0	-	100.0	-	100.0	100.0	83.3	-	97.3	-	100.0	97.0	96.2	-	97.0	-	100.0	97.6	100.0	-	97.7	97.4
Other Vehicles	0	0	0	0	-	0	0	0	0	1	-	1	0	0	32	2	-	34	0	0	24	0	-	24	59
% Other Vehicles	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	16.7	-	2.7	-	0.0	3.0	3.8	-	3.0	-	0.0	2.3	0.0	-	2.2	2.6
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	1	0	-	1	1
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.1	0.0	-	0.1	0.0
Bicycles on Crosswalk	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	33.3	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	66.7	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

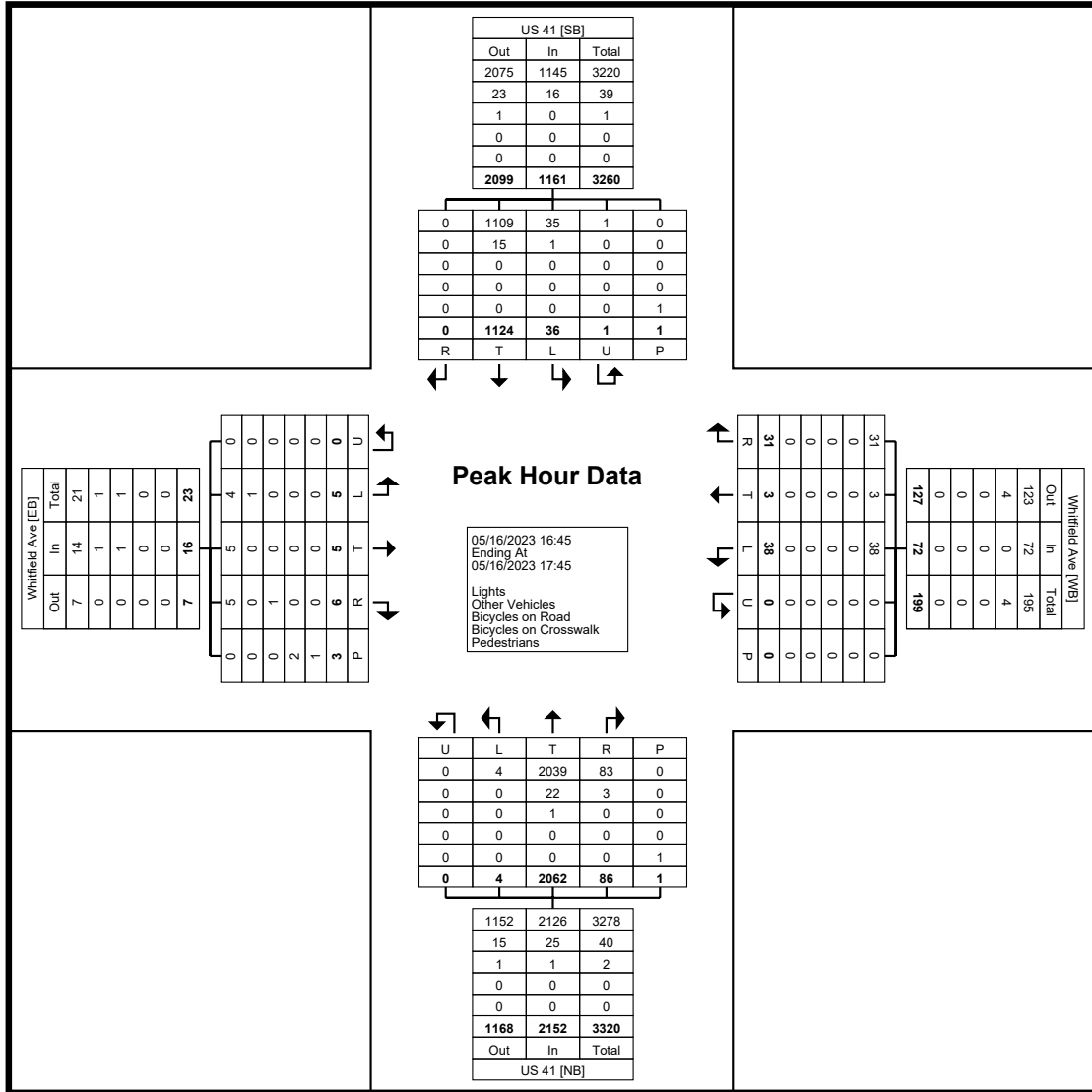


Turning Movement Peak Hour Data Plot (12:00)



### Turning Movement Peak Hour Data (16:45)

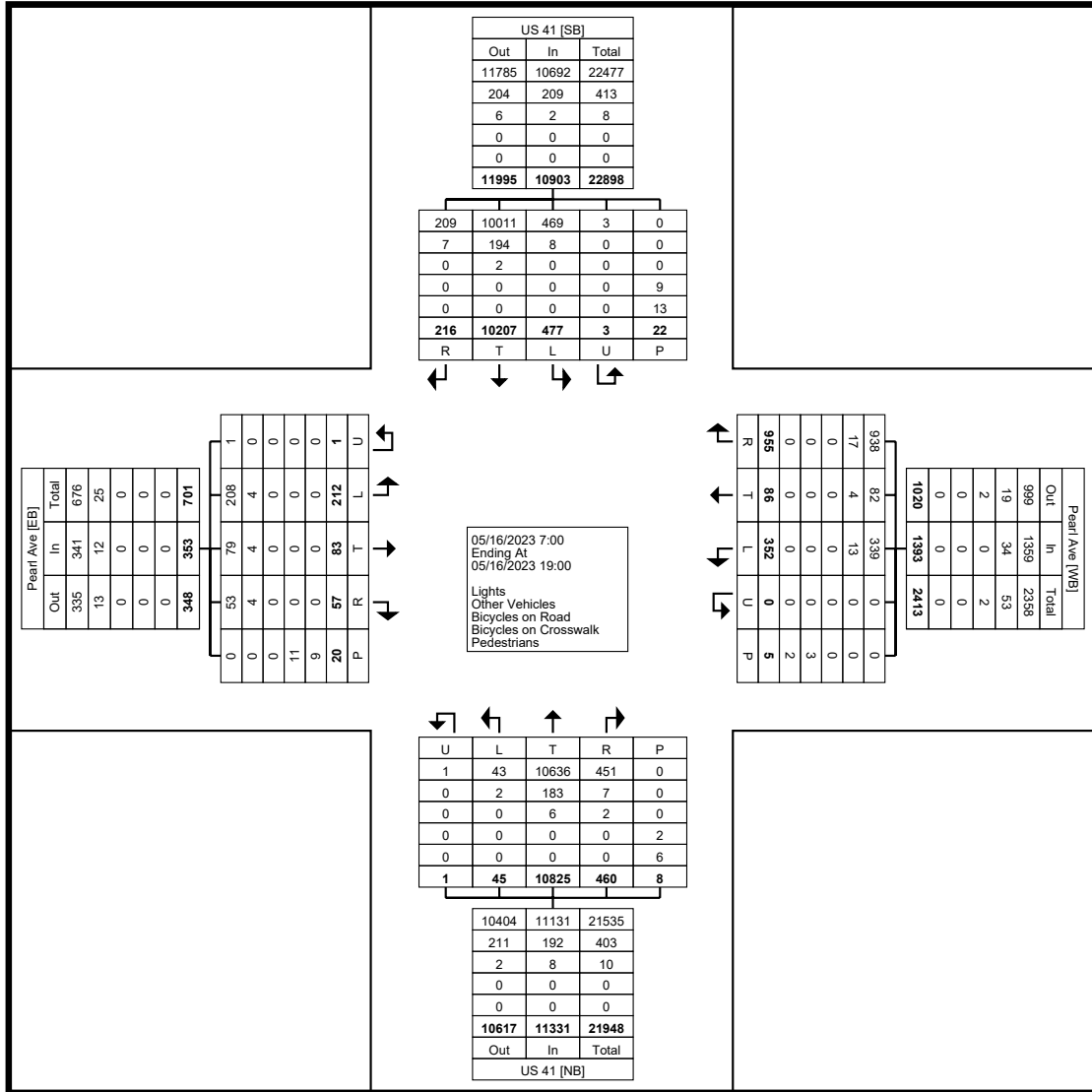
Start Time	Whitfield Ave Eastbound						Whitfield Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:45	0	2	1	1	1	4	0	11	2	8	0	21	0	1	466	28	0	495	1	8	286	0	0	295	815
17:00	0	3	2	0	0	5	0	5	1	5	0	11	0	1	484	20	0	505	0	10	271	0	1	281	802
17:15	0	0	1	2	0	3	0	12	0	8	0	20	0	0	427	16	1	443	0	10	305	0	0	315	781
17:30	0	0	1	3	2	4	0	10	0	10	0	20	0	2	685	22	0	709	0	8	262	0	0	270	1003
<b>Total</b>	<b>0</b>	<b>5</b>	<b>5</b>	<b>6</b>	<b>3</b>	<b>16</b>	<b>0</b>	<b>38</b>	<b>3</b>	<b>31</b>	<b>0</b>	<b>72</b>	<b>0</b>	<b>4</b>	<b>2062</b>	<b>86</b>	<b>1</b>	<b>2152</b>	<b>1</b>	<b>36</b>	<b>1124</b>	<b>0</b>	<b>1</b>	<b>1161</b>	<b>3401</b>
Approach %	0.0	31.3	31.3	37.5	-	-	0.0	52.8	4.2	43.1	-	-	0.0	0.2	95.8	4.0	-	-	0.1	3.1	96.8	0.0	-	-	-
Total %	0.0	0.1	0.1	0.2	-	0.5	0.0	1.1	0.1	0.9	-	2.1	0.0	0.1	60.6	2.5	-	63.3	0.0	1.1	33.0	0.0	-	34.1	-
PHF	0.000	0.417	0.625	0.500	-	0.800	0.000	0.792	0.375	0.775	-	0.857	0.000	0.500	0.753	0.768	-	0.759	0.250	0.900	0.921	0.000	-	0.921	0.848
Lights	0	4	5	5	-	14	0	38	3	31	-	72	0	4	2039	83	-	2126	1	35	1109	0	-	1145	3357
% Lights	-	80.0	100.0	83.3	-	87.5	-	100.0	100.0	100.0	-	100.0	-	100.0	98.9	96.5	-	98.8	100.0	97.2	98.7	-	-	98.6	98.7
Other Vehicles	0	1	0	0	-	1	0	0	0	0	-	0	0	0	22	3	-	25	0	1	15	0	-	16	42
% Other Vehicles	-	20.0	0.0	0.0	-	6.3	-	0.0	0.0	0.0	-	0.0	-	0.0	1.1	3.5	-	1.2	0.0	2.8	1.3	-	-	1.4	1.2
Bicycles on Road	0	0	0	1	-	1	0	0	0	0	-	0	0	0	1	0	-	1	0	0	0	0	-	0	2
% Bicycles on Road	-	0.0	0.0	16.7	-	6.3	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	-	-	0.0	0.1
Bicycles on Crosswalk	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	66.7	-	-	-	-	-	-	-	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	33.3	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-



Turning Movement Peak Hour Data Plot (16:45)

### Turning Movement Data

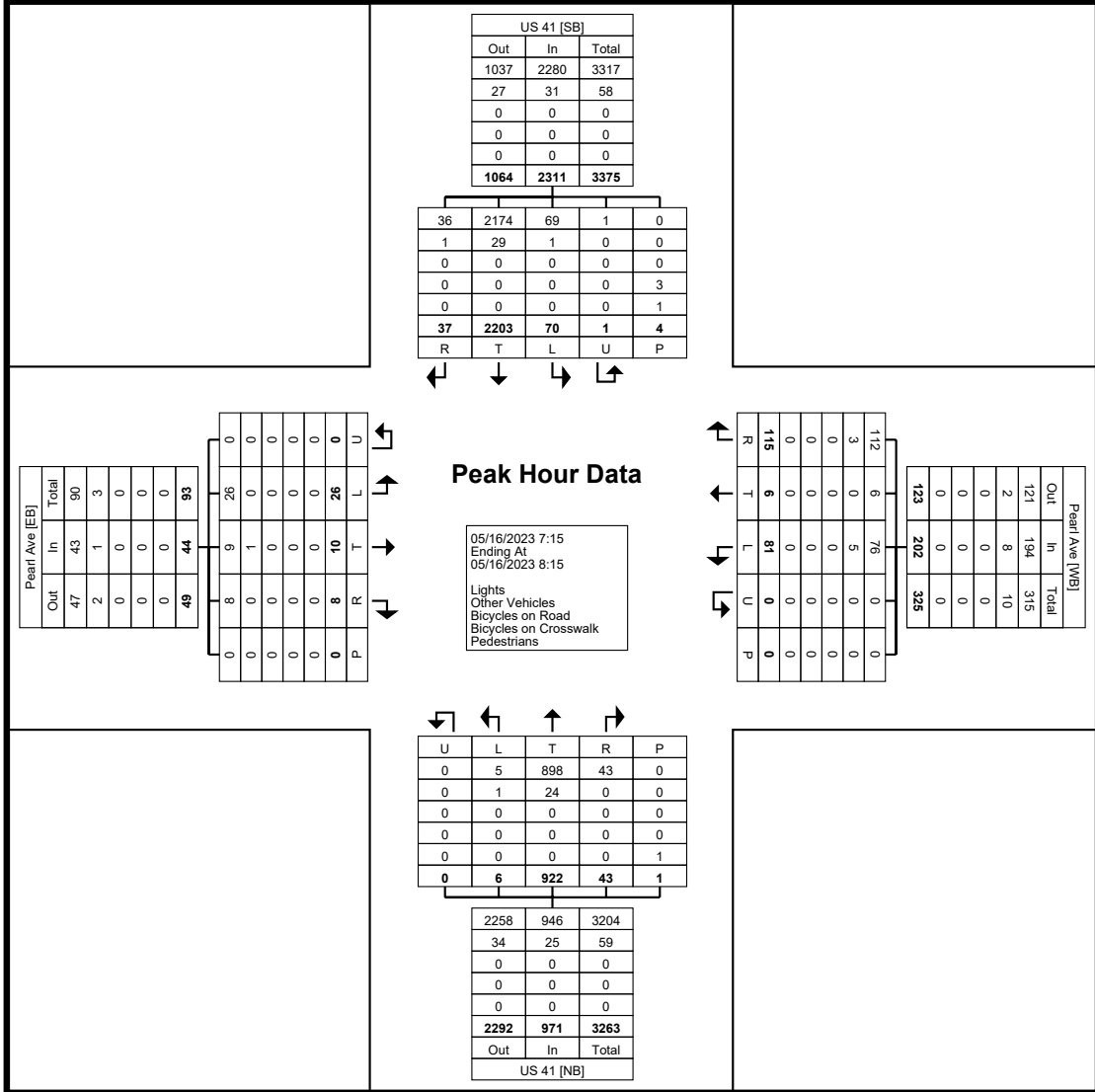
Start Time	Pearl Ave Eastbound						Pearl Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	5	5	1	1	11	0	17	5	23	0	45	0	0	178	9	1	187	0	24	516	10	1	550	793
7:15	0	9	3	3	0	15	0	25	1	29	0	55	0	0	247	14	0	261	0	17	538	12	1	567	898
7:30	0	7	1	0	0	8	0	19	2	32	0	53	0	3	254	8	0	265	0	14	600	11	1	625	951
7:45	0	6	3	3	0	12	0	20	2	30	0	52	0	2	226	13	1	241	1	23	520	5	1	549	854
Hourly Total	0	27	12	7	1	46	0	81	10	114	0	205	0	5	905	44	2	954	1	78	2174	38	4	2291	3496
8:00	0	4	3	2	0	9	0	17	1	24	0	42	0	1	195	8	0	204	0	16	545	9	1	570	825
8:15	0	5	2	1	0	8	0	20	3	30	0	53	0	0	212	11	0	223	0	20	456	2	0	478	762
8:30	0	7	0	1	0	8	0	13	2	22	0	37	0	0	237	12	0	249	0	13	473	4	0	490	784
8:45	0	5	1	2	0	8	0	16	3	28	1	47	0	1	196	5	0	202	0	9	418	10	1	437	694
Hourly Total	0	21	6	6	0	33	0	66	9	104	1	179	0	2	840	36	0	878	0	58	1892	25	2	1975	3065
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	6	3	2	1	11	0	8	8	25	0	41	0	2	290	20	0	312	0	15	244	4	0	263	627
12:15	0	7	1	3	0	11	0	8	4	28	0	40	0	2	236	10	0	248	0	10	266	5	0	281	580
12:30	0	5	3	3	1	11	0	3	4	19	0	26	0	3	277	18	0	298	0	21	262	14	0	297	632
12:45	0	5	4	3	1	12	0	11	1	23	0	35	0	1	218	12	0	231	0	19	258	2	0	279	557
Hourly Total	0	23	11	11	3	45	0	30	17	95	0	142	0	8	1021	60	0	1089	0	65	1030	25	0	1120	2396
13:00	0	9	3	0	0	12	0	2	5	18	0	25	0	0	246	17	2	263	0	7	259	7	1	273	573
13:15	0	9	3	2	1	14	0	9	3	27	0	39	0	0	214	17	0	231	0	16	251	6	0	273	557
13:30	0	9	2	3	0	14	0	11	2	30	0	43	0	1	303	14	0	318	0	15	255	8	0	278	653
13:45	0	11	2	0	0	13	0	18	1	15	0	34	0	1	283	20	0	304	0	18	248	3	0	269	620
Hourly Total	0	38	10	5	1	53	0	40	11	90	0	141	0	2	1046	68	2	1116	0	56	1013	24	1	1093	2403
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	1	12	2	6	0	21	0	9	3	37	1	49	0	2	411	14	0	427	0	21	281	6	1	308	805
15:15	0	7	3	2	0	12	0	9	4	36	0	49	0	0	422	17	0	439	0	20	269	13	0	302	802
15:30	0	8	2	2	2	12	0	6	1	48	0	55	0	2	398	14	0	414	0	16	295	4	1	315	796
15:45	0	14	3	0	0	17	0	10	3	38	0	51	0	2	464	16	0	482	0	12	286	9	1	307	857
Hourly Total	1	41	10	10	2	62	0	34	11	159	1	204	0	6	1695	61	0	1762	0	69	1131	32	3	1232	3260
16:00	0	7	3	2	0	12	0	11	2	51	0	64	0	4	494	22	0	520	0	8	248	5	1	261	857
16:15	0	4	3	2	1	9	0	13	3	30	0	46	0	1	511	15	0	527	0	15	281	9	2	305	887
16:30	0	9	3	1	0	13	0	6	3	38	0	47	0	3	506	25	0	534	0	11	256	8	0	275	869
16:45	0	6	1	3	1	10	0	6	0	43	0	49	0	3	527	22	0	552	1	12	257	5	0	275	886
Hourly Total	0	26	10	8	2	44	0	36	8	162	0	206	0	11	2038	84	0	2133	1	46	1042	27	3	1116	3499
17:00	0	3	4	2	0	9	0	11	4	52	0	67	0	2	436	14	0	452	0	11	301	4	3	316	844
17:15	0	5	2	1	2	8	0	12	2	34	0	48	0	0	422	14	4	436	0	15	283	5	1	303	795
17:30	0	7	1	0	1	8	0	5	0	40	0	45	0	3	684	14	0	701	1	16	260	1	0	278	1032
17:45	0	5	3	2	1	10	0	6	2	30	1	38	0	1	420	14	0	435	0	20	258	11	0	289	772
Hourly Total	0	20	10	5	4	35	0	34	8	156	1	198	0	6	1962	56	4	2024	1	62	1102	21	4	1186	3443
18:00	0	4	5	3	3	12	0	11	1	25	0	37	0	3	423	16	0	442	0	10	213	6	1	229	720
18:15	0	5	3	1	0	9	0	5	6	19	0	30	1	0	337	12	0	350	0	7	220	9	1	236	625
18:30	0	7	2	0	3	9	0	7	1	16	0	24	0	0	294	11	0	305	0	12	201	4	2	217	555
18:45	0	0	4	1	1	5	0	8	4	15	2	27	0	2	264	12	0	278	0	14	189	5	1	208	518
Hourly Total	0	16	14	5	7	35	0	31	12	75	2	118	1	5	1318	51	0	1375	0	43	823	24	5	890	2418
Grand Total	1	212	83	57	20	353	0	352	86	955	5	1393	1	45	10825	460	8	11331	3	477	10207	216	22	10903	23980
Approach %	0.3	60.1	23.5	16.1	-	-	0.0	25.3	6.2	68.6	-	-	0.0	0.4	95.5	4.1	-	-	0.0	4.4	93.6	2.0	-	-	-
Total %	0.0	0.9	0.3	0.2	-	1.5	0.0	1.5	0.4	4.0	-	5.8	0.0	0.2	45.1	1.9	-	47.3	0.0	2.0	42.6	0.9	-	45.5	-
Lights	1	208	79	53	-	341	0	339	82	938	-	1359	1	43	10636	451	-	11131	3	469	10011	209	-	10692	23523
% Lights	100.0	98.1	95.2	93.0	-	96.6	-	96.3	95.3	98.2	-	97.6	100.0	95.6	98.3	98.0	-	98.2	100.0	98.3	98.1	96.8	-	98.1	98.1
Other Vehicles	0	4	4	4	-	12	0	13	4	17	-	34	0	2	183	7	-	192	0	8	194	7	-	209	447
% Other Vehicles	0.0	1.9	4.8	7.0	-	3.4	-	3.7	4.7	1.8	-	2.4	0.0	4.4	1.7	1.5	-	1.7	0.0	1.7	1.9	3.2	-	1.9	1.9
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	6	2	-	8	0	0	2	0	-	2	10
% Bicycles on Road	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.1	0.4	-	0.1	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	11	-	-	-	-	3	-	-	-	-	-	-	2	-	-	-	-	-	9	-	-
% Bicycles on Crosswalk	-	-	-	-	55.0	-	-	-	-	60.0	-	-	-	-	-	-	25.0	-	-	-	-	-	40.9	-	-
Pedestrians	-	-	-	-	9	-	-	-	-	2	-	-	-	-	-	-	6	-	-	-	-	-	13	-	-
% Pedestrians	-	-	-	-	45.0	-	-	-	-	40.0	-	-	-	-	-	-	75.0	-	-	-	-	-	59.1	-	-



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:15)

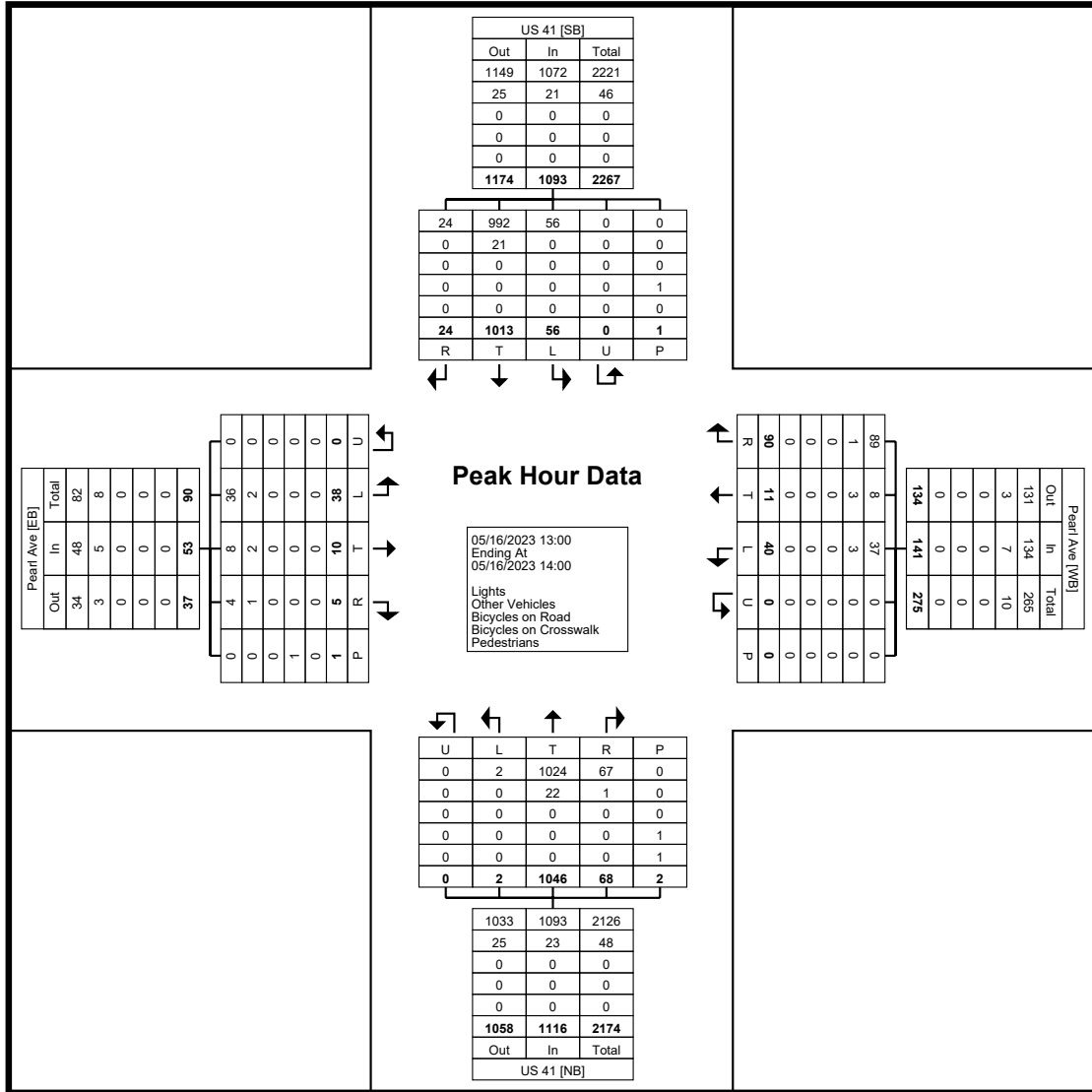
Start Time	Pearl Ave Eastbound						Pearl Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15	0	9	3	3	0	15	0	25	1	29	0	55	0	0	247	14	0	261	0	17	538	12	1	567	898
7:30	0	7	1	0	0	8	0	19	2	32	0	53	0	3	254	8	0	265	0	14	600	11	1	625	951
7:45	0	6	3	3	0	12	0	20	2	30	0	52	0	2	226	13	1	241	1	23	520	5	1	549	854
8:00	0	4	3	2	0	9	0	17	1	24	0	42	0	1	195	8	0	204	0	16	545	9	1	570	825
<b>Total</b>	<b>0</b>	<b>26</b>	<b>10</b>	<b>8</b>	<b>0</b>	<b>44</b>	<b>0</b>	<b>81</b>	<b>6</b>	<b>115</b>	<b>0</b>	<b>202</b>	<b>0</b>	<b>6</b>	<b>922</b>	<b>43</b>	<b>1</b>	<b>971</b>	<b>1</b>	<b>70</b>	<b>2203</b>	<b>37</b>	<b>4</b>	<b>2311</b>	<b>3528</b>
Approach %	0.0	59.1	22.7	18.2	-	-	0.0	40.1	3.0	56.9	-	-	0.0	0.6	95.0	4.4	-	-	0.0	3.0	95.3	1.6	-	-	-
Total %	0.0	0.7	0.3	0.2	-	1.2	0.0	2.3	0.2	3.3	-	5.7	0.0	0.2	26.1	1.2	-	27.5	0.0	2.0	62.4	1.0	-	65.5	-
PHF	0.000	0.722	0.833	0.667	-	0.733	0.000	0.810	0.750	0.898	-	0.918	0.000	0.500	0.907	0.768	-	0.916	0.250	0.761	0.918	0.771	-	0.924	0.927
Lights	0	26	9	8	-	43	0	76	6	112	-	194	0	5	898	43	-	946	1	69	2174	36	-	2280	3463
% Lights	-	100.0	90.0	100.0	-	97.7	-	93.8	100.0	97.4	-	96.0	-	83.3	97.4	100.0	-	97.4	100.0	98.6	98.7	97.3	-	98.7	98.2
Other Vehicles	0	0	1	0	-	1	0	5	0	3	-	8	0	1	24	0	-	25	0	1	29	1	-	31	65
% Other Vehicles	-	0.0	10.0	0.0	-	2.3	-	6.2	0.0	2.6	-	4.0	-	16.7	2.6	0.0	-	2.6	0.0	1.4	1.3	2.7	-	1.3	1.8
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	3	-	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.0	-	-	-	-	-	75.0	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-	-	-	-	25.0	-	-



Turning Movement Peak Hour Data Plot (7:15)

### Turning Movement Peak Hour Data (13:00)

Start Time	Pearl Ave Eastbound						Pearl Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
13:00	0	9	3	0	0	12	0	2	5	18	0	25	0	0	246	17	2	263	0	7	259	7	1	273	573
13:15	0	9	3	2	1	14	0	9	3	27	0	39	0	0	214	17	0	231	0	16	251	6	0	273	557
13:30	0	9	2	3	0	14	0	11	2	30	0	43	0	1	303	14	0	318	0	15	255	8	0	278	653
13:45	0	11	2	0	0	13	0	18	1	15	0	34	0	1	283	20	0	304	0	18	248	3	0	269	620
<b>Total</b>	<b>0</b>	<b>38</b>	<b>10</b>	<b>5</b>	<b>1</b>	<b>53</b>	<b>0</b>	<b>40</b>	<b>11</b>	<b>90</b>	<b>0</b>	<b>141</b>	<b>0</b>	<b>2</b>	<b>1046</b>	<b>68</b>	<b>2</b>	<b>1116</b>	<b>0</b>	<b>56</b>	<b>1013</b>	<b>24</b>	<b>1</b>	<b>1093</b>	<b>2403</b>
Approach %	0.0	71.7	18.9	9.4	-	-	0.0	28.4	7.8	63.8	-	-	0.0	0.2	93.7	6.1	-	-	0.0	5.1	92.7	2.2	-	-	-
Total %	0.0	1.6	0.4	0.2	-	2.2	0.0	1.7	0.5	3.7	-	5.9	0.0	0.1	43.5	2.8	-	46.4	0.0	2.3	42.2	1.0	-	45.5	-
PHF	0.000	0.864	0.833	0.417	-	0.946	0.000	0.556	0.550	0.750	-	0.820	0.000	0.500	0.863	0.850	-	0.877	0.000	0.778	0.978	0.750	-	0.983	0.920
Lights	0	36	8	4	-	48	0	37	8	89	-	134	0	2	1024	67	-	1093	0	56	992	24	-	1072	2347
% Lights	-	94.7	80.0	80.0	-	90.6	-	92.5	72.7	98.9	-	95.0	-	100.0	97.9	98.5	-	97.9	-	100.0	97.9	100.0	-	98.1	97.7
Other Vehicles	0	2	2	1	-	5	0	3	3	1	-	7	0	0	22	1	-	23	0	0	21	0	-	21	56
% Other Vehicles	-	5.3	20.0	20.0	-	9.4	-	7.5	27.3	1.1	-	5.0	-	0.0	2.1	1.5	-	2.1	-	0.0	2.1	0.0	-	1.9	2.3
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	1	-	-
% Bicycles on Crosswalk	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	50.0	-	-	-	-	-	100.0	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	50.0	-	-	-	-	-	0.0	-	-

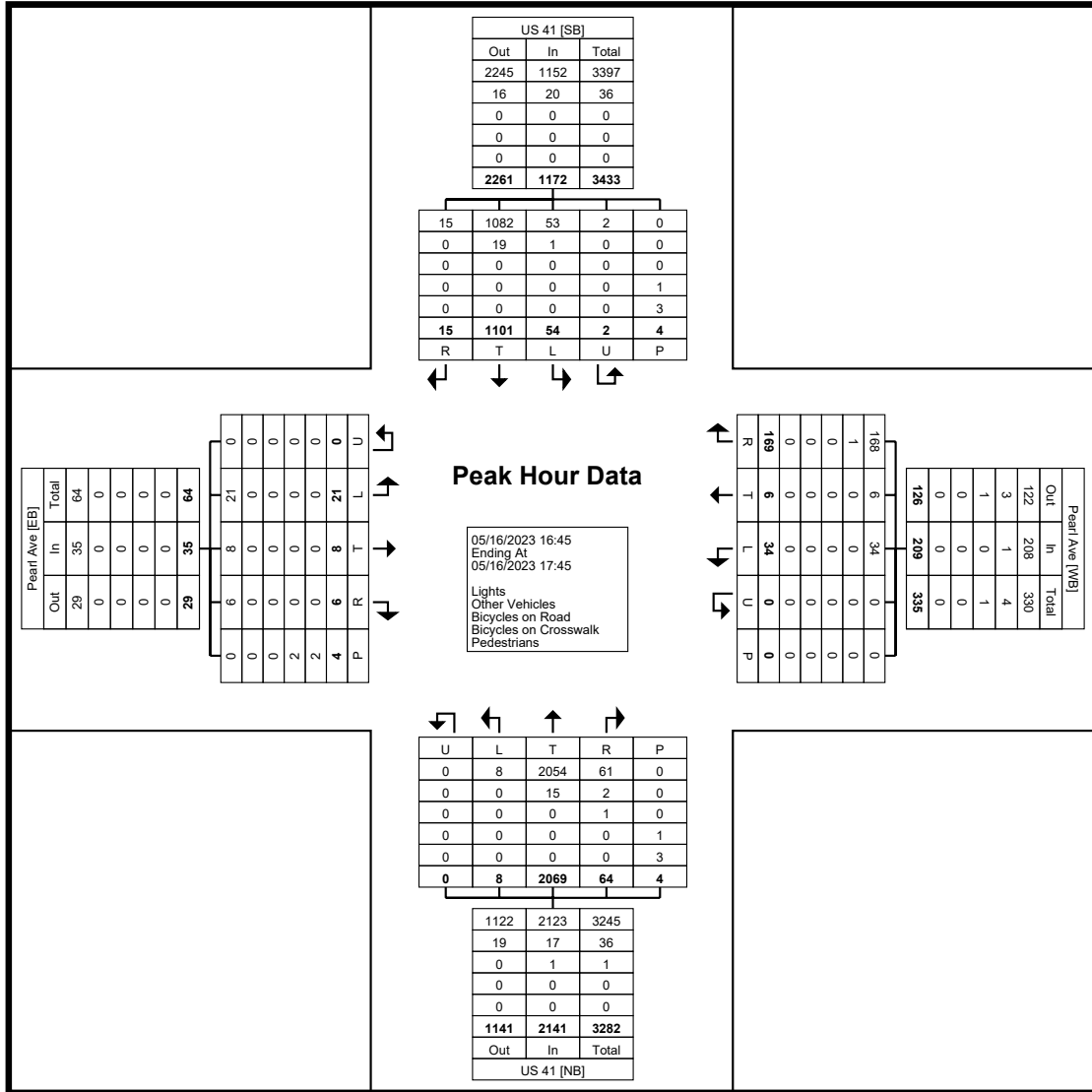


Turning Movement Peak Hour Data Plot (13:00)



### Turning Movement Peak Hour Data (16:45)

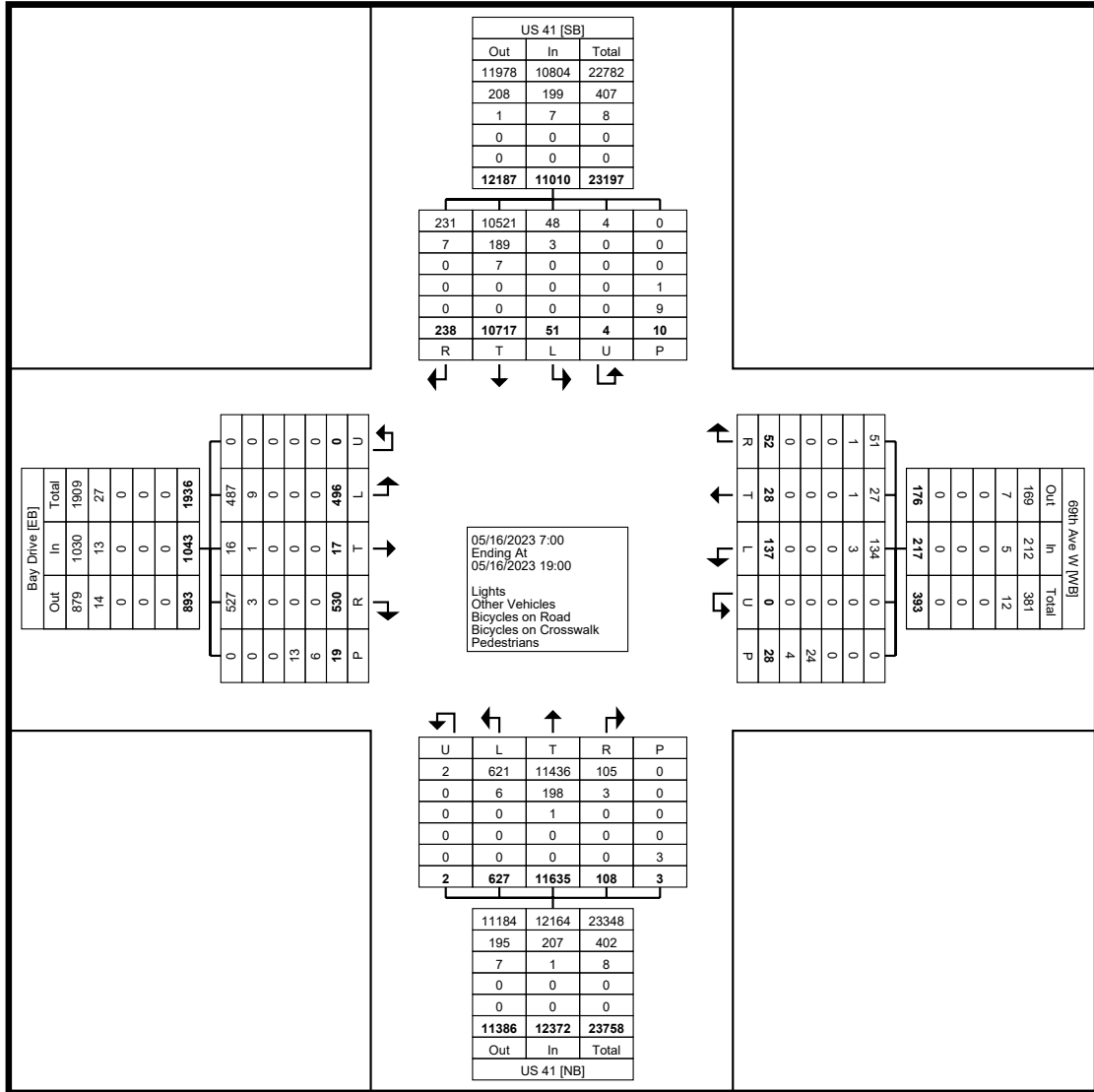
Start Time	Pearl Ave Eastbound						Pearl Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:45	0	6	1	3	1	10	0	6	0	43	0	49	0	3	527	22	0	552	1	12	257	5	0	275	886
17:00	0	3	4	2	0	9	0	11	4	52	0	67	0	2	436	14	0	452	0	11	301	4	3	316	844
17:15	0	5	2	1	2	8	0	12	2	34	0	48	0	0	422	14	4	436	0	15	283	5	1	303	795
17:30	0	7	1	0	1	8	0	5	0	40	0	45	0	3	684	14	0	701	1	16	260	1	0	278	1032
<b>Total</b>	<b>0</b>	<b>21</b>	<b>8</b>	<b>6</b>	<b>4</b>	<b>35</b>	<b>0</b>	<b>34</b>	<b>6</b>	<b>169</b>	<b>0</b>	<b>209</b>	<b>0</b>	<b>8</b>	<b>2069</b>	<b>64</b>	<b>4</b>	<b>2141</b>	<b>2</b>	<b>54</b>	<b>1101</b>	<b>15</b>	<b>4</b>	<b>1172</b>	<b>3557</b>
Approach %	0.0	60.0	22.9	17.1	-	-	0.0	16.3	2.9	80.9	-	-	0.0	0.4	96.6	3.0	-	-	0.2	4.6	93.9	1.3	-	-	-
Total %	0.0	0.6	0.2	0.2	-	1.0	0.0	1.0	0.2	4.8	-	5.9	0.0	0.2	58.2	1.8	-	60.2	0.1	1.5	31.0	0.4	-	32.9	-
PHF	0.000	0.750	0.500	0.500	-	0.875	0.000	0.708	0.375	0.813	-	0.780	0.000	0.667	0.756	0.727	-	0.764	0.500	0.844	0.914	0.750	-	0.927	0.862
Lights	0	21	8	6	-	35	0	34	6	168	-	208	0	8	2054	61	-	2123	2	53	1082	15	-	1152	3518
% Lights	-	100.0	100.0	100.0	-	100.0	-	100.0	100.0	99.4	-	99.5	-	100.0	99.3	95.3	-	99.2	100.0	98.1	98.3	100.0	-	98.3	98.9
Other Vehicles	0	0	0	0	-	0	0	0	0	1	-	1	0	0	15	2	-	17	0	1	19	0	-	20	38
% Other Vehicles	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.6	-	0.5	-	0.0	0.7	3.1	-	0.8	0.0	1.9	1.7	0.0	-	1.7	1.1
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	1	-	1	0	0	0	0	-	0	1
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	1.6	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	1	-	-
% Bicycles on Crosswalk	-	-	-	-	50.0	-	-	-	-	-	-	-	-	-	-	-	25.0	-	-	-	-	-	25.0	-	-
Pedestrians	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	3	-	-	-	-	-	3	-	-
% Pedestrians	-	-	-	-	50.0	-	-	-	-	-	-	-	-	-	-	-	75.0	-	-	-	-	-	75.0	-	-



Turning Movement Peak Hour Data Plot (16:45)

### Turning Movement Data

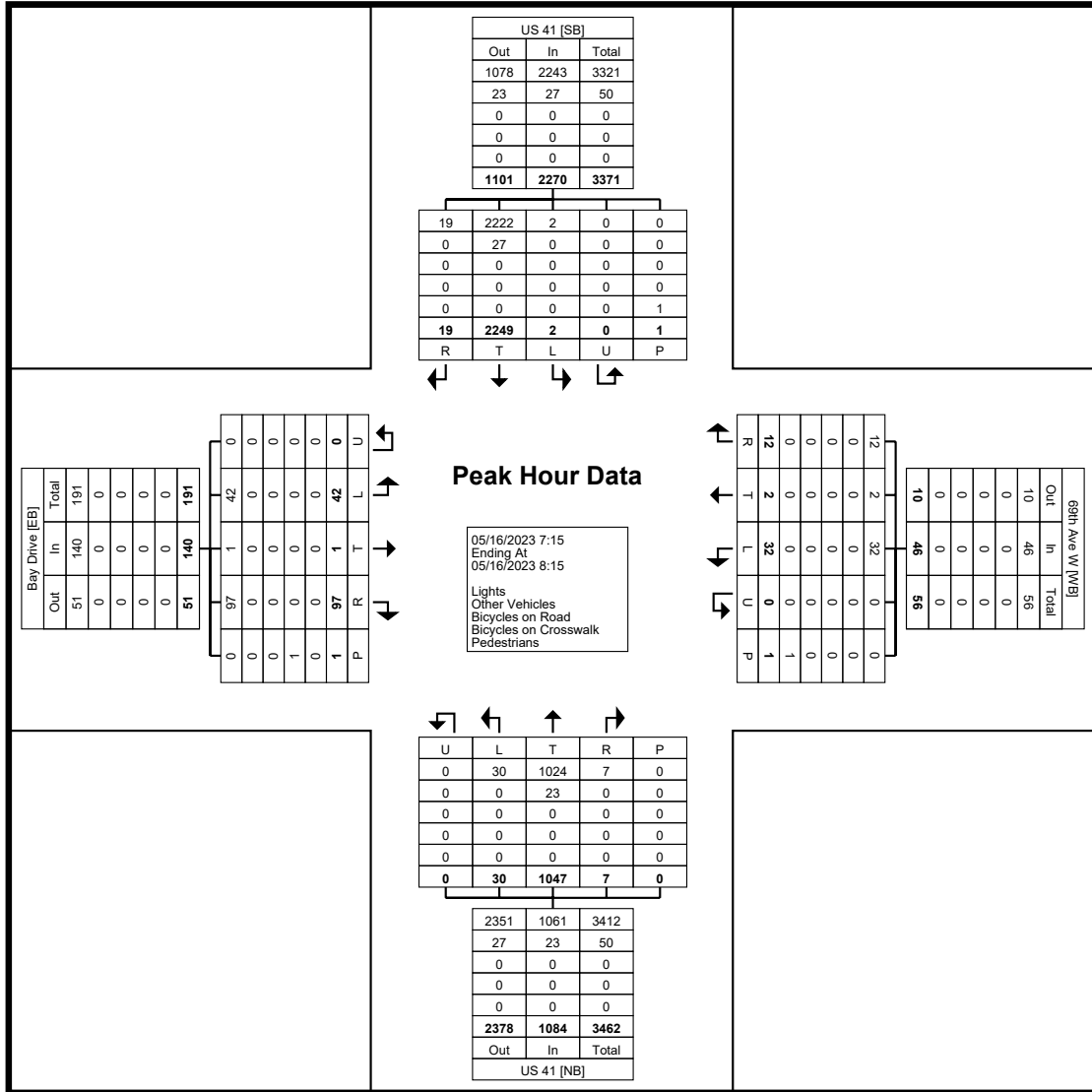
Start Time	Bay Drive Eastbound						69th Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	4	1	16	0	21	0	5	0	2	0	7	0	3	198	0	0	201	0	0	519	2	0	521	750
7:15	0	9	0	26	0	35	0	3	0	4	0	7	0	6	265	1	0	272	0	0	587	2	0	589	903
7:30	0	17	0	29	0	46	0	13	0	1	0	14	0	3	300	1	0	304	0	0	584	4	0	588	952
7:45	0	8	1	23	0	32	0	8	0	2	1	10	0	13	240	3	0	256	0	2	573	7	1	582	880
Hourly Total	0	38	2	94	0	134	0	29	0	9	1	38	0	25	1003	5	0	1033	0	2	2263	15	1	2280	3485
8:00	0	8	0	19	1	27	0	8	2	5	0	15	0	8	242	2	0	252	0	0	505	6	0	511	805
8:15	0	7	0	20	1	27	0	9	2	1	0	12	0	4	255	5	0	264	0	4	497	7	0	508	811
8:30	0	6	1	27	1	34	0	4	1	1	2	6	0	6	256	2	0	264	0	2	459	5	1	466	770
8:45	0	4	0	23	0	27	0	5	0	1	3	6	0	12	226	2	0	240	1	2	430	5	0	438	711
Hourly Total	0	25	1	89	3	115	0	26	5	8	5	39	0	30	979	11	0	1020	1	8	1891	23	1	1923	3097
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	24	0	16	1	40	0	4	2	1	1	7	0	28	312	4	0	344	0	1	248	5	0	254	645
12:15	0	24	0	11	1	35	0	5	0	1	0	6	0	22	259	3	0	284	0	2	283	13	0	298	623
12:30	0	22	1	15	1	38	0	1	0	1	2	2	0	12	295	0	0	307	0	4	288	10	0	302	649
12:45	0	14	2	10	1	26	0	3	0	1	0	4	0	14	245	0	0	259	0	1	280	12	0	293	582
Hourly Total	0	84	3	52	4	139	0	13	2	4	3	19	0	76	1111	7	0	1194	0	8	1099	40	0	1147	2499
13:00	0	24	2	13	1	39	0	5	1	0	0	6	0	19	272	2	0	293	0	1	272	7	1	280	618
13:15	0	17	0	11	0	28	0	3	0	1	1	4	0	10	237	1	0	248	0	1	272	6	0	279	559
13:30	0	24	1	7	1	32	0	5	2	2	1	9	0	23	299	7	1	329	0	4	268	9	0	281	651
13:45	0	9	0	17	1	26	0	4	1	1	0	6	0	13	269	5	0	287	0	0	272	14	0	286	605
Hourly Total	0	74	3	48	3	125	0	17	4	4	2	25	0	65	1077	15	1	1157	0	6	1084	36	1	1126	2433
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	11	0	16	0	27	0	4	0	2	3	6	1	20	437	2	0	460	0	1	299	8	0	308	801
15:15	0	18	0	22	0	40	0	4	1	2	3	7	0	18	482	2	0	502	0	3	289	9	0	301	850
15:30	0	16	0	16	2	32	0	3	0	4	1	7	0	23	451	2	1	476	0	2	317	11	0	330	845
15:45	0	17	0	27	1	44	0	4	1	0	3	5	0	25	526	3	0	554	1	2	300	11	0	314	917
Hourly Total	0	62	0	81	3	143	0	15	2	8	10	25	1	86	1896	9	1	1992	1	8	1205	39	0	1253	3413
16:00	0	21	2	13	0	36	0	3	1	0	1	4	0	28	526	5	0	559	0	1	268	7	1	276	875
16:15	0	25	0	17	0	42	0	5	2	1	0	8	0	32	537	6	0	575	0	1	285	16	1	302	927
16:30	0	24	1	12	0	37	0	3	2	0	0	5	0	37	543	8	0	588	0	3	273	8	0	284	914
16:45	0	23	0	13	0	36	0	4	1	1	1	6	0	35	561	6	0	602	0	1	251	9	0	261	905
Hourly Total	0	93	3	55	0	151	0	15	6	2	2	23	0	132	2167	25	0	2324	0	6	1077	40	2	1123	3621
17:00	0	15	1	12	0	28	0	3	1	6	2	10	0	42	454	6	0	502	0	0	315	9	0	324	864
17:15	0	17	0	13	0	30	0	4	3	3	1	10	0	22	470	5	0	497	0	3	306	6	0	315	852
17:30	0	22	1	13	1	36	0	2	1	2	0	5	0	45	715	6	0	766	0	2	285	11	3	298	1105
17:45	0	20	1	26	1	47	0	2	2	2	0	6	0	26	450	7	1	483	1	2	271	5	1	279	815
Hourly Total	0	74	3	64	2	141	0	11	7	13	3	31	0	135	2089	24	1	2248	1	7	1177	31	4	1216	3636
18:00	0	14	0	14	1	28	0	4	1	0	0	5	0	20	429	1	0	450	0	1	232	2	1	235	718
18:15	0	5	0	11	1	16	0	4	0	1	0	5	0	29	317	5	0	351	0	1	250	4	0	255	627
18:30	0	21	1	12	0	34	0	2	1	1	0	4	0	17	290	2	0	309	0	3	230	7	0	240	587
18:45	0	6	1	10	2	17	0	1	0	2	2	3	1	12	277	4	0	294	1	1	209	1	0	212	526
Hourly Total	0	46	2	47	4	95	0	11	2	4	2	17	1	78	1313	12	0	1404	1	6	921	14	1	942	2458
Grand Total	0	496	17	530	19	1043	0	137	28	52	28	217	2	627	11635	108	3	12372	4	51	10717	238	10	11010	24642
Approach %	0.0	47.6	1.6	50.8	-	-	0.0	63.1	12.9	24.0	-	-	0.0	5.1	94.0	0.9	-	-	0.0	0.5	97.3	2.2	-	-	-
Total %	0.0	2.0	0.1	2.2	-	4.2	0.0	0.6	0.1	0.2	-	0.9	0.0	2.5	47.2	0.4	-	50.2	0.0	0.2	43.5	1.0	-	44.7	-
Lights	0	487	16	527	-	1030	0	134	27	51	-	212	2	621	11436	105	-	12164	4	48	10521	231	-	10804	24210
% Lights	-	98.2	94.1	99.4	-	98.8	-	97.8	96.4	98.1	-	97.7	100.0	99.0	98.3	97.2	-	98.3	100.0	94.1	98.2	97.1	-	98.1	98.2
Other Vehicles	0	9	1	3	-	13	0	3	1	1	-	5	0	6	198	3	-	207	0	3	189	7	-	199	424
% Other Vehicles	-	1.8	5.9	0.6	-	1.2	-	2.2	3.6	1.9	-	2.3	0.0	1.0	1.7	2.8	-	1.7	0.0	5.9	1.8	2.9	-	1.8	1.7
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	1	0	-	1	0	0	7	0	-	7	8
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.1	0.0	-	0.1	0.0
Bicycles on Crosswalk	-	-	-	-	13	-	-	-	-	-	24	-	-	-	-	-	0	-	-	-	-	-	1	-	-
% Bicycles on Crosswalk	-	-	-	-	68.4	-	-	-	-	-	85.7	-	-	-	-	-	0.0	-	-	-	-	-	10.0	-	-
Pedestrians	-	-	-	-	6	-	-	-	-	-	4	-	-	-	-	-	3	-	-	-	-	-	9	-	-
% Pedestrians	-	-	-	-	31.6	-	-	-	-	-	14.3	-	-	-	-	-	100.0	-	-	-	-	-	90.0	-	-



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:15)

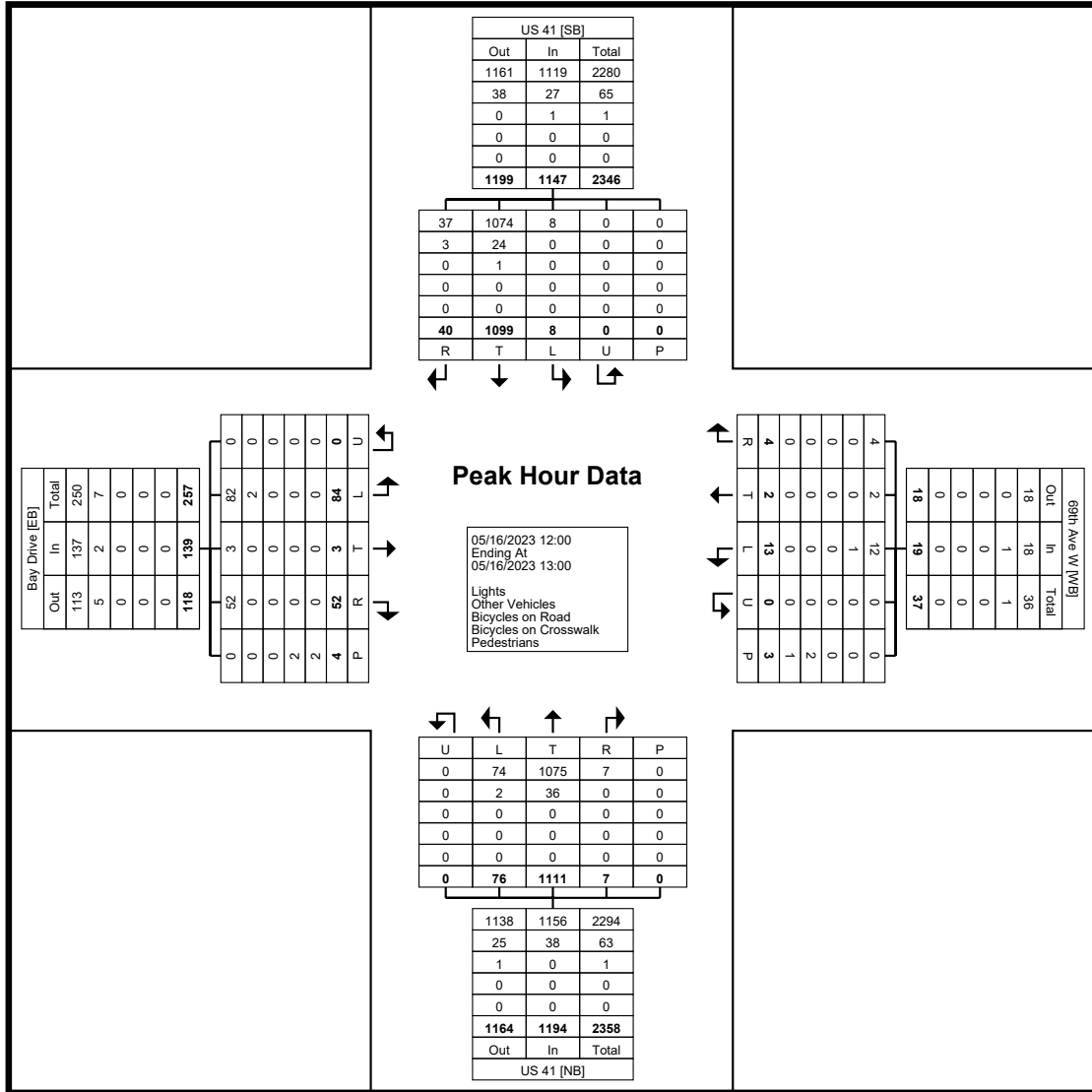
Start Time	Bay Drive Eastbound						69th Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15	0	9	0	26	0	35	0	3	0	4	0	7	0	6	265	1	0	272	0	0	587	2	0	589	903
7:30	0	17	0	29	0	46	0	13	0	1	0	14	0	3	300	1	0	304	0	0	584	4	0	588	952
7:45	0	8	1	23	0	32	0	8	0	2	1	10	0	13	240	3	0	256	0	2	573	7	1	582	880
8:00	0	8	0	19	1	27	0	8	2	5	0	15	0	8	242	2	0	252	0	0	505	6	0	511	805
<b>Total</b>	0	42	1	97	1	140	0	32	2	12	1	46	0	30	1047	7	0	1084	0	2	2249	19	1	2270	3540
Approach %	0.0	30.0	0.7	69.3	-	-	0.0	69.6	4.3	26.1	-	-	0.0	2.8	96.6	0.6	-	-	0.0	0.1	99.1	0.8	-	-	-
Total %	0.0	1.2	0.0	2.7	-	4.0	0.0	0.9	0.1	0.3	-	1.3	0.0	0.8	29.6	0.2	-	30.6	0.0	0.1	63.5	0.5	-	64.1	-
PHF	0.000	0.618	0.250	0.836	-	0.761	0.000	0.615	0.250	0.600	-	0.767	0.000	0.577	0.873	0.583	-	0.891	0.000	0.250	0.958	0.679	-	0.963	0.930
Lights	0	42	1	97	-	140	0	32	2	12	-	46	0	30	1024	7	-	1061	0	2	2222	19	-	2243	3490
% Lights	-	100.0	100.0	100.0	-	100.0	-	100.0	100.0	100.0	-	100.0	-	100.0	97.8	100.0	-	97.9	-	100.0	98.8	100.0	-	98.8	98.6
Other Vehicles	0	0	0	0	-	0	0	0	0	0	-	0	0	0	23	0	-	23	0	0	27	0	-	27	50
% Other Vehicles	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	2.2	0.0	-	2.1	-	0.0	1.2	0.0	-	1.2	1.4
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	100.0	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	0.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



Turning Movement Peak Hour Data Plot (7:15)

### Turning Movement Peak Hour Data (12:00)

Start Time	Bay Drive Eastbound						69th Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	24	0	16	1	40	0	4	2	1	1	7	0	28	312	4	0	344	0	1	248	5	0	254	645
12:15	0	24	0	11	1	35	0	5	0	1	0	6	0	22	259	3	0	284	0	2	283	13	0	298	623
12:30	0	22	1	15	1	38	0	1	0	1	2	2	0	12	295	0	0	307	0	4	288	10	0	302	649
12:45	0	14	2	10	1	26	0	3	0	1	0	4	0	14	245	0	0	259	0	1	280	12	0	293	582
<b>Total</b>	<b>0</b>	<b>84</b>	<b>3</b>	<b>52</b>	<b>4</b>	<b>139</b>	<b>0</b>	<b>13</b>	<b>2</b>	<b>4</b>	<b>3</b>	<b>19</b>	<b>0</b>	<b>76</b>	<b>1111</b>	<b>7</b>	<b>0</b>	<b>1194</b>	<b>0</b>	<b>8</b>	<b>1099</b>	<b>40</b>	<b>0</b>	<b>1147</b>	<b>2499</b>
Approach %	0.0	60.4	2.2	37.4	-	-	0.0	68.4	10.5	21.1	-	-	0.0	6.4	93.0	0.6	-	-	0.0	0.7	95.8	3.5	-	-	-
Total %	0.0	3.4	0.1	2.1	-	5.6	0.0	0.5	0.1	0.2	-	0.8	0.0	3.0	44.5	0.3	-	47.8	0.0	0.3	44.0	1.6	-	45.9	-
PHF	0.000	0.875	0.375	0.813	-	0.869	0.000	0.650	0.250	1.000	-	0.679	0.000	0.679	0.890	0.438	-	0.868	0.000	0.500	0.954	0.769	-	0.950	0.963
Lights	0	82	3	52	-	137	0	12	2	4	-	18	0	74	1075	7	-	1156	0	8	1074	37	-	1119	2430
% Lights	-	97.6	100.0	100.0	-	98.6	-	92.3	100.0	100.0	-	94.7	-	97.4	96.8	100.0	-	96.8	-	100.0	97.7	92.5	-	97.6	97.2
Other Vehicles	0	2	0	0	-	2	0	1	0	0	-	1	0	2	36	0	-	38	0	0	24	3	-	27	68
% Other Vehicles	-	2.4	0.0	0.0	-	1.4	-	7.7	0.0	0.0	-	5.3	-	2.6	3.2	0.0	-	3.2	-	0.0	2.2	7.5	-	2.4	2.7
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	1	0	-	1	1
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.1	0.0	-	0.1	0.0
Bicycles on Crosswalk	-	-	-	-	2	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	50.0	-	-	-	-	-	66.7	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	2	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	50.0	-	-	-	-	-	33.3	-	-	-	-	-	-	-	-	-	-	-	-	-	-

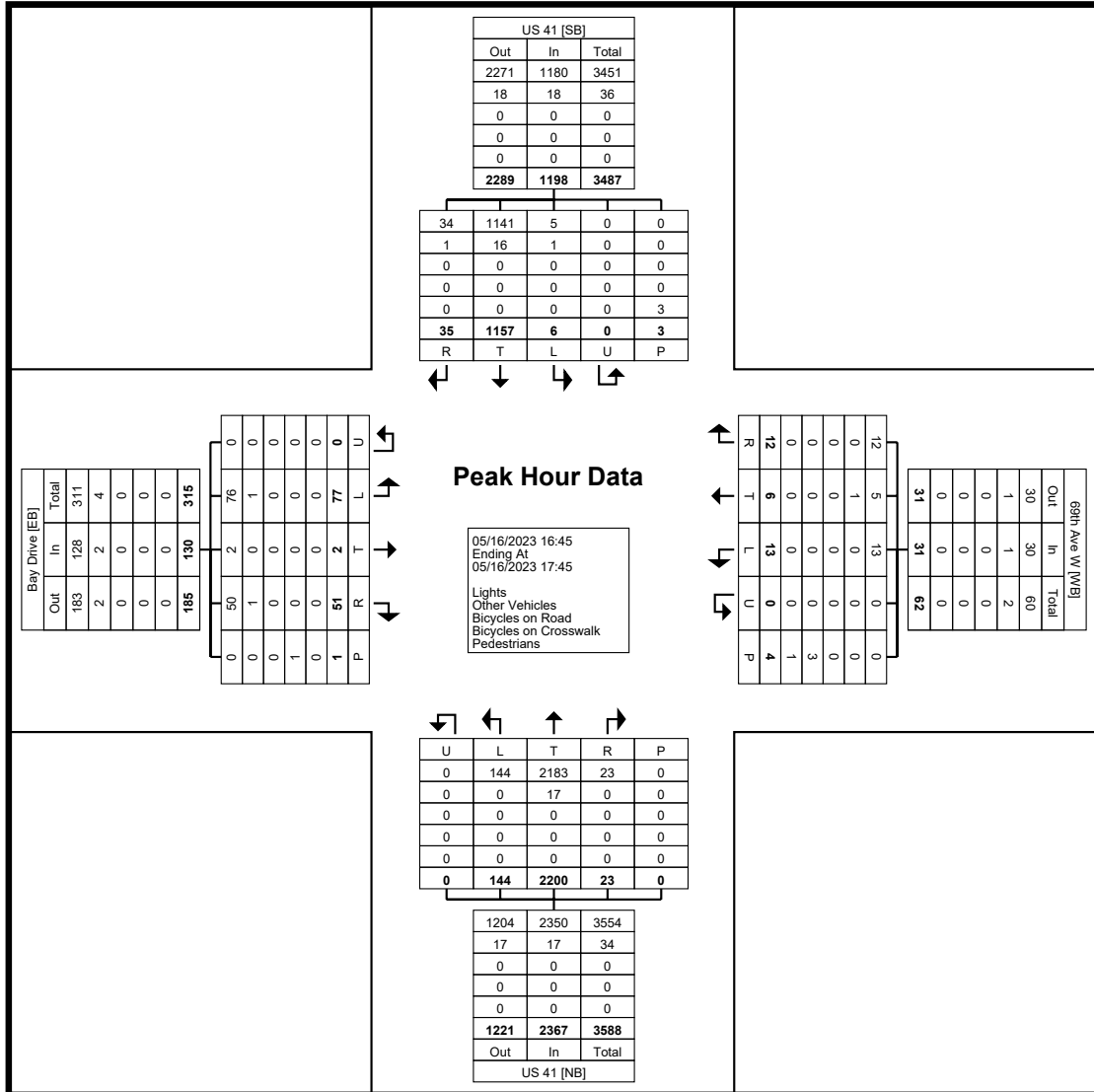


Turning Movement Peak Hour Data Plot (12:00)



### Turning Movement Peak Hour Data (16:45)

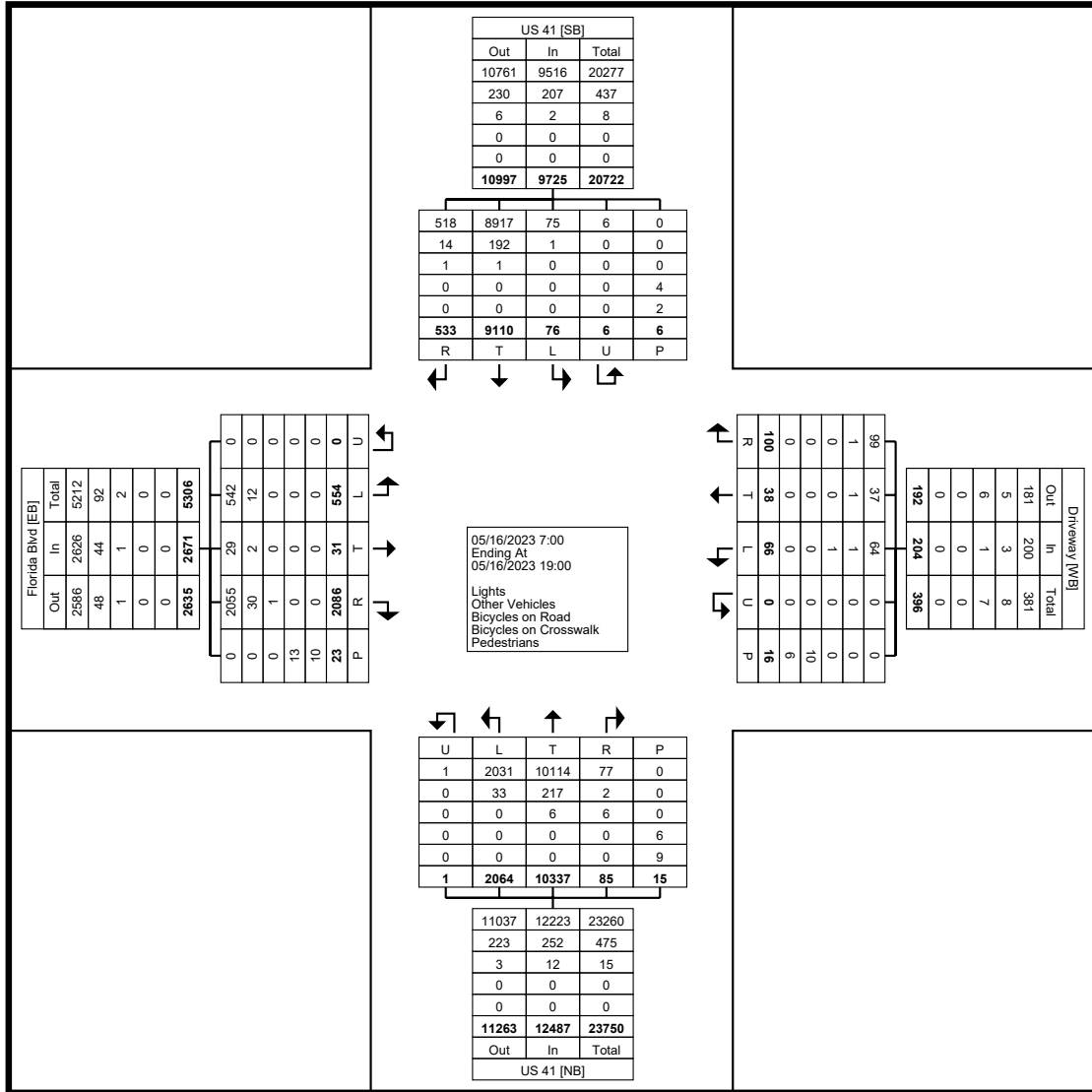
Start Time	Bay Drive Eastbound						69th Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:45	0	23	0	13	0	36	0	4	1	1	1	6	0	35	561	6	0	602	0	1	251	9	0	261	905
17:00	0	15	1	12	0	28	0	3	1	6	2	10	0	42	454	6	0	502	0	0	315	9	0	324	864
17:15	0	17	0	13	0	30	0	4	3	3	1	10	0	22	470	5	0	497	0	3	306	6	0	315	852
17:30	0	22	1	13	1	36	0	2	1	2	0	5	0	45	715	6	0	766	0	2	285	11	3	298	1105
<b>Total</b>	<b>0</b>	<b>77</b>	<b>2</b>	<b>51</b>	<b>1</b>	<b>130</b>	<b>0</b>	<b>13</b>	<b>6</b>	<b>12</b>	<b>4</b>	<b>31</b>	<b>0</b>	<b>144</b>	<b>2200</b>	<b>23</b>	<b>0</b>	<b>2367</b>	<b>0</b>	<b>6</b>	<b>1157</b>	<b>35</b>	<b>3</b>	<b>1198</b>	<b>3726</b>
Approach %	0.0	59.2	1.5	39.2	-	-	0.0	41.9	19.4	38.7	-	-	0.0	6.1	92.9	1.0	-	-	0.0	0.5	96.6	2.9	-	-	-
Total %	0.0	2.1	0.1	1.4	-	3.5	0.0	0.3	0.2	0.3	-	0.8	0.0	3.9	59.0	0.6	-	63.5	0.0	0.2	31.1	0.9	-	32.2	-
PHF	0.000	0.837	0.500	0.981	-	0.903	0.000	0.813	0.500	0.500	-	0.775	0.000	0.800	0.769	0.958	-	0.773	0.000	0.500	0.918	0.795	-	0.924	0.843
Lights	0	76	2	50	-	128	0	13	5	12	-	30	0	144	2183	23	-	2350	0	5	1141	34	-	1180	3688
% Lights	-	98.7	100.0	98.0	-	98.5	-	100.0	83.3	100.0	-	96.8	-	100.0	99.2	100.0	-	99.3	-	83.3	98.6	97.1	-	98.5	99.0
Other Vehicles	0	1	0	1	-	2	0	0	1	0	-	1	0	0	17	0	-	17	0	1	16	1	-	18	38
% Other Vehicles	-	1.3	0.0	2.0	-	1.5	-	0.0	16.7	0.0	-	3.2	-	0.0	0.8	0.0	-	0.7	-	16.7	1.4	2.9	-	1.5	1.0
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	1	-	-	-	-	-	3	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	100.0	-	-	-	-	-	75.0	-	-	-	-	-	-	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	3	-	-
% Pedestrians	-	-	-	-	0.0	-	-	-	-	-	25.0	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



Turning Movement Peak Hour Data Plot (16:45)

### Turning Movement Data

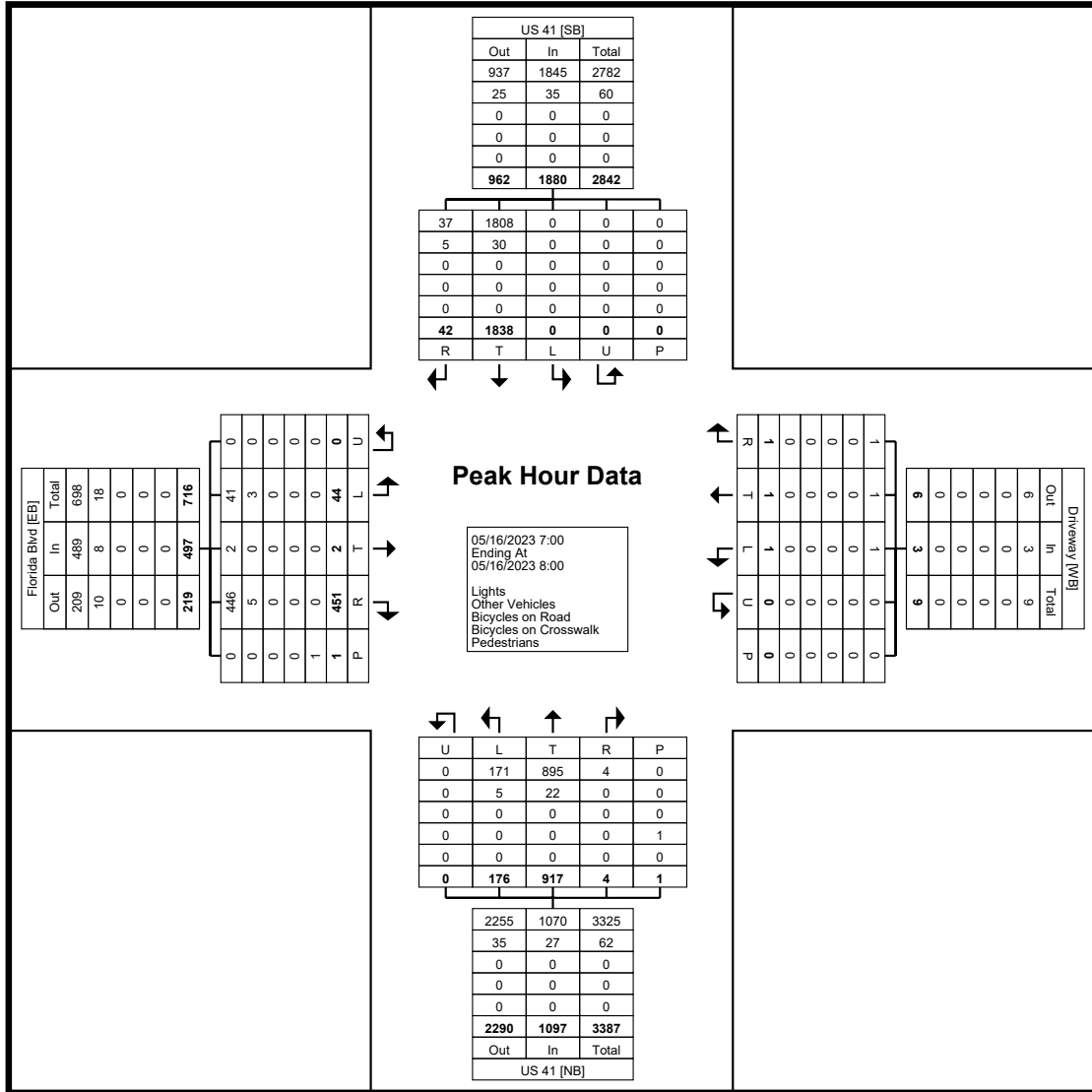
Start Time	Florida Blvd Eastbound						Driveway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	13	0	100	0	113	0	0	0	0	0	0	0	29	189	0	0	218	0	0	470	11	0	481	812
7:15	0	9	0	109	0	118	0	0	0	0	0	0	0	50	239	0	0	289	0	0	480	9	0	489	896
7:30	0	18	1	126	1	145	0	0	1	0	0	1	0	52	260	1	0	313	0	0	445	14	0	459	918
7:45	0	4	1	116	0	121	0	1	0	1	0	2	0	45	229	3	1	277	0	0	443	8	0	451	851
Hourly Total	0	44	2	451	1	497	0	1	1	1	0	3	0	176	917	4	1	1097	0	0	1838	42	0	1880	3477
8:00	0	20	0	122	0	142	0	1	1	0	1	2	0	49	201	1	0	251	0	1	406	9	0	416	811
8:15	0	8	0	88	0	96	0	0	0	0	0	0	0	48	211	0	0	259	0	1	428	13	0	442	797
8:30	0	15	1	76	0	92	0	1	1	1	2	3	0	53	217	0	0	270	0	1	374	12	0	387	752
8:45	0	18	2	75	0	95	0	1	0	0	0	1	0	32	211	3	0	246	0	1	340	10	0	351	693
Hourly Total	0	61	3	361	0	425	0	3	2	1	3	6	0	182	840	4	0	1026	0	4	1548	44	0	1596	3053
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	24	1	54	1	79	0	1	0	5	1	6	0	60	277	5	0	342	1	5	210	12	2	228	655
12:15	0	28	1	52	0	81	0	5	3	3	0	11	0	46	241	2	3	289	0	1	263	20	0	284	665
12:30	0	24	0	50	2	74	0	3	1	4	0	8	0	53	251	5	0	309	0	3	243	20	0	266	657
12:45	0	18	0	47	1	65	0	3	0	0	0	3	0	40	254	2	1	296	0	1	264	16	1	281	645
Hourly Total	0	94	2	203	4	299	0	12	4	12	1	28	0	199	1023	14	4	1236	1	10	980	68	3	1059	2622
13:00	0	18	0	45	1	63	0	2	1	4	0	7	0	56	248	1	2	305	1	4	225	19	0	249	624
13:15	0	13	0	46	0	59	0	0	1	4	0	5	0	41	237	2	0	280	0	1	246	17	0	264	608
13:30	0	9	1	54	2	64	0	1	2	3	2	6	0	51	308	3	1	362	0	4	252	19	0	275	707
13:45	0	24	2	40	0	66	0	1	2	4	0	7	0	58	265	4	0	327	0	6	249	26	0	281	681
Hourly Total	0	64	3	185	3	252	0	4	6	15	2	25	0	206	1058	10	3	1274	1	15	972	81	0	1069	2620
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	12	0	65	0	77	0	2	2	4	0	8	0	71	407	4	0	482	0	3	249	14	0	266	833
15:15	0	27	2	57	1	86	0	5	2	2	3	9	0	71	388	4	0	463	0	3	238	19	0	260	818
15:30	0	23	3	72	1	98	0	5	1	1	0	7	0	77	390	5	2	472	0	4	287	16	0	307	884
15:45	0	17	0	60	1	77	0	1	2	6	0	9	0	101	463	5	1	569	0	1	233	19	0	253	908
Hourly Total	0	79	5	254	3	338	0	13	7	13	3	33	0	320	1648	18	3	1986	0	11	1007	68	0	1086	3443
16:00	0	26	4	58	0	88	0	4	0	5	0	9	0	83	426	2	0	511	0	5	245	24	0	274	882
16:15	0	14	1	65	2	80	0	6	1	4	2	11	0	97	535	1	0	633	0	2	228	20	1	250	974
16:30	0	23	3	57	1	83	0	0	3	5	0	8	0	88	465	3	0	556	2	0	252	26	0	280	927
16:45	0	12	1	65	1	78	0	1	1	2	1	4	0	103	455	3	0	561	1	5	248	19	0	273	916
Hourly Total	0	75	9	245	4	329	0	11	5	16	3	32	0	371	1881	9	0	2261	3	12	973	89	1	1077	3699
17:00	0	30	0	49	0	79	0	2	5	3	0	10	0	80	434	0	2	514	0	2	277	19	0	298	901
17:15	0	17	0	71	1	88	0	2	2	5	0	9	0	93	361	2	0	456	0	3	255	24	0	282	835
17:30	0	19	0	58	0	77	0	1	2	9	1	12	0	136	582	4	0	722	0	4	252	22	1	278	1089
17:45	0	15	5	57	1	77	0	3	0	6	1	9	0	83	442	2	0	527	1	2	214	16	0	233	846
Hourly Total	0	81	5	235	2	321	0	8	9	23	2	40	0	392	1819	8	2	2219	1	11	998	81	1	1091	3671
18:00	0	18	0	50	3	68	0	6	1	6	0	13	0	71	341	5	1	417	0	5	225	19	0	249	747
18:15	0	8	1	28	1	37	0	4	1	8	0	13	0	53	292	3	0	348	0	1	211	18	0	230	628
18:30	0	16	0	46	1	62	0	1	1	4	0	6	1	52	271	4	1	328	0	3	175	13	0	191	587
18:45	0	14	1	28	1	43	0	3	1	1	2	5	0	42	247	6	0	295	0	4	183	10	1	197	540
Hourly Total	0	56	2	152	6	210	0	14	4	19	2	37	1	218	1151	18	2	1388	0	13	794	60	1	867	2502
Grand Total	0	554	31	2086	23	2671	0	66	38	100	16	204	1	2064	10337	85	15	12487	6	76	9110	533	6	9725	25087
Approach %	0.0	20.7	1.2	78.1	-	-	0.0	32.4	18.6	49.0	-	-	0.0	16.5	82.8	0.7	-	-	0.1	0.8	93.7	5.5	-	-	-
Total %	0.0	2.2	0.1	8.3	-	10.6	0.0	0.3	0.2	0.4	-	0.8	0.0	8.2	41.2	0.3	-	49.8	0.0	0.3	36.3	2.1	-	38.8	-
Lights	0	542	29	2055	-	2626	0	64	37	99	-	200	1	2031	10114	77	-	12223	6	75	8917	518	-	9516	24565
% Lights	-	97.8	93.5	98.5	-	98.3	-	97.0	97.4	99.0	-	98.0	100.0	98.4	97.8	90.6	-	97.9	100.0	98.7	97.9	97.2	-	97.9	97.9
Other Vehicles	0	12	2	30	-	44	0	1	1	1	-	3	0	33	217	2	-	252	0	1	192	14	-	207	506
% Other Vehicles	-	2.2	6.5	1.4	-	1.6	-	1.5	2.6	1.0	-	1.5	0.0	1.6	2.1	2.4	-	2.0	0.0	1.3	2.1	2.6	-	2.1	2.0
Bicycles on Road	0	0	0	1	-	1	0	1	0	0	-	1	0	0	6	6	-	12	0	0	1	1	-	2	16
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	1.5	0.0	0.0	-	0.5	0.0	0.0	0.1	7.1	-	0.1	0.0	0.0	0.0	0.2	-	0.0	0.1
Bicycles on Crosswalk	-	-	-	-	13	-	-	-	-	-	10	-	-	-	-	-	6	-	-	-	-	-	4	-	-
% Bicycles on Crosswalk	-	-	-	-	56.5	-	-	-	-	-	62.5	-	-	-	-	-	40.0	-	-	-	-	-	66.7	-	-
Pedestrians	-	-	-	-	10	-	-	-	-	-	6	-	-	-	-	-	9	-	-	-	-	-	2	-	-
% Pedestrians	-	-	-	-	43.5	-	-	-	-	-	37.5	-	-	-	-	-	60.0	-	-	-	-	-	33.3	-	-



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:00)

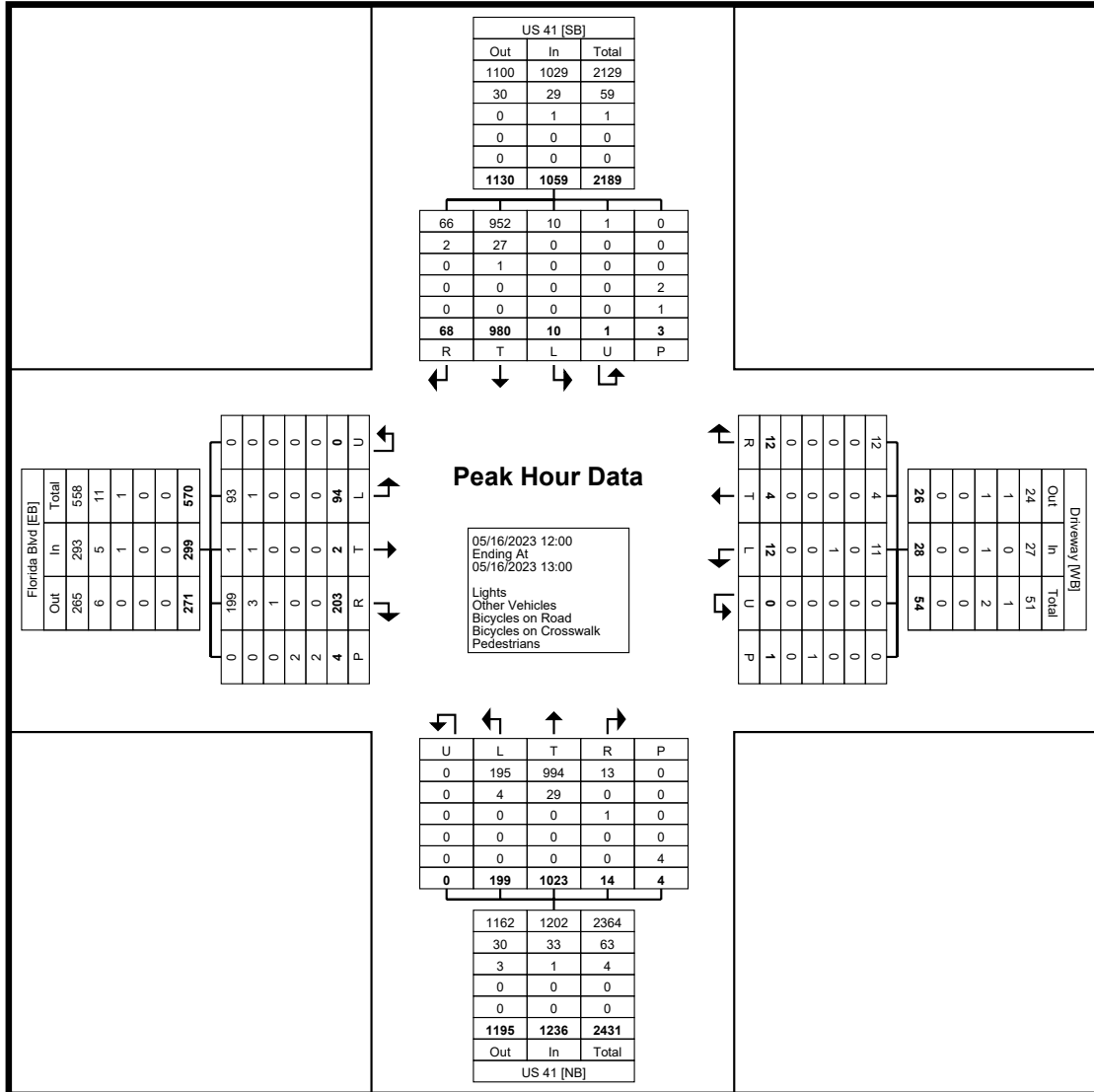
Start Time	Florida Blvd Eastbound						Driveway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	13	0	100	0	113	0	0	0	0	0	0	0	29	189	0	0	218	0	0	470	11	0	481	812
7:15	0	9	0	109	0	118	0	0	0	0	0	0	0	50	239	0	0	289	0	0	480	9	0	489	896
7:30	0	18	1	126	1	145	0	0	1	0	0	1	0	52	260	1	0	313	0	0	445	14	0	459	918
7:45	0	4	1	116	0	121	0	1	0	1	0	2	0	45	229	3	1	277	0	0	443	8	0	451	851
<b>Total</b>	<b>0</b>	<b>44</b>	<b>2</b>	<b>451</b>	<b>1</b>	<b>497</b>	<b>0</b>	<b>1</b>	<b>1</b>	<b>1</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>176</b>	<b>917</b>	<b>4</b>	<b>1</b>	<b>1097</b>	<b>0</b>	<b>0</b>	<b>1838</b>	<b>42</b>	<b>0</b>	<b>1880</b>	<b>3477</b>
Approach %	0.0	8.9	0.4	90.7	-	-	0.0	33.3	33.3	33.3	-	-	0.0	16.0	83.6	0.4	-	-	0.0	0.0	97.8	2.2	-	-	-
Total %	0.0	1.3	0.1	13.0	-	14.3	0.0	0.0	0.0	0.0	-	0.1	0.0	5.1	26.4	0.1	-	31.6	0.0	0.0	52.9	1.2	-	54.1	-
PHF	0.000	0.611	0.500	0.895	-	0.857	0.000	0.250	0.250	0.250	-	0.375	0.000	0.846	0.882	0.333	-	0.876	0.000	0.000	0.957	0.750	-	0.961	0.947
Lights	0	41	2	446	-	489	0	1	1	1	-	3	0	171	895	4	-	1070	0	0	1808	37	-	1845	3407
% Lights	-	93.2	100.0	98.9	-	98.4	-	100.0	100.0	100.0	-	100.0	-	97.2	97.6	100.0	-	97.5	-	-	98.4	88.1	-	98.1	98.0
Other Vehicles	0	3	0	5	-	8	0	0	0	0	-	0	0	5	22	0	-	27	0	0	30	5	-	35	70
% Other Vehicles	-	6.8	0.0	1.1	-	1.6	-	0.0	0.0	0.0	-	0.0	-	2.8	2.4	0.0	-	2.5	-	-	1.6	11.9	-	1.9	2.0
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	-	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-



Turning Movement Peak Hour Data Plot (7:00)

### Turning Movement Peak Hour Data (12:00)

Start Time	Florida Blvd Eastbound						Driveway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	24	1	54	1	79	0	1	0	5	1	6	0	60	277	5	0	342	1	5	210	12	2	228	655
12:15	0	28	1	52	0	81	0	5	3	3	0	11	0	46	241	2	3	289	0	1	263	20	0	284	665
12:30	0	24	0	50	2	74	0	3	1	4	0	8	0	53	251	5	0	309	0	3	243	20	0	266	657
12:45	0	18	0	47	1	65	0	3	0	0	0	3	0	40	254	2	1	296	0	1	264	16	1	281	645
<b>Total</b>	0	94	2	203	4	299	0	12	4	12	1	28	0	199	1023	14	4	1236	1	10	980	68	3	1059	2622
Approach %	0.0	31.4	0.7	67.9	-	-	0.0	42.9	14.3	42.9	-	-	0.0	16.1	82.8	1.1	-	-	0.1	0.9	92.5	6.4	-	-	-
Total %	0.0	3.6	0.1	7.7	-	11.4	0.0	0.5	0.2	0.5	-	1.1	0.0	7.6	39.0	0.5	-	47.1	0.0	0.4	37.4	2.6	-	40.4	-
PHF	0.000	0.839	0.500	0.940	-	0.923	0.000	0.600	0.333	0.600	-	0.636	0.000	0.829	0.923	0.700	-	0.904	0.250	0.500	0.928	0.850	-	0.932	0.986
Lights	0	93	1	199	-	293	0	11	4	12	-	27	0	195	994	13	-	1202	1	10	952	66	-	1029	2551
% Lights	-	98.9	50.0	98.0	-	98.0	-	91.7	100.0	100.0	-	96.4	-	98.0	97.2	92.9	-	97.2	100.0	100.0	97.1	97.1	-	97.2	97.3
Other Vehicles	0	1	1	3	-	5	0	0	0	0	-	0	0	4	29	0	-	33	0	0	27	2	-	29	67
% Other Vehicles	-	1.1	50.0	1.5	-	1.7	-	0.0	0.0	0.0	-	0.0	-	2.0	2.8	0.0	-	2.7	0.0	0.0	2.8	2.9	-	2.7	2.6
Bicycles on Road	0	0	0	1	-	1	0	1	0	0	-	1	0	0	0	1	-	1	0	0	1	0	-	1	4
% Bicycles on Road	-	0.0	0.0	0.5	-	0.3	-	8.3	0.0	0.0	-	3.6	-	0.0	0.0	7.1	-	0.1	0.0	0.0	0.1	0.0	-	0.1	0.2
Bicycles on Crosswalk	-	-	-	-	2	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	2	-	-
% Bicycles on Crosswalk	-	-	-	-	50.0	-	-	-	-	-	100.0	-	-	-	-	-	0.0	-	-	-	-	-	66.7	-	-
Pedestrians	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	4	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	50.0	-	-	-	-	-	0.0	-	-	-	-	-	100.0	-	-	-	-	-	33.3	-	-

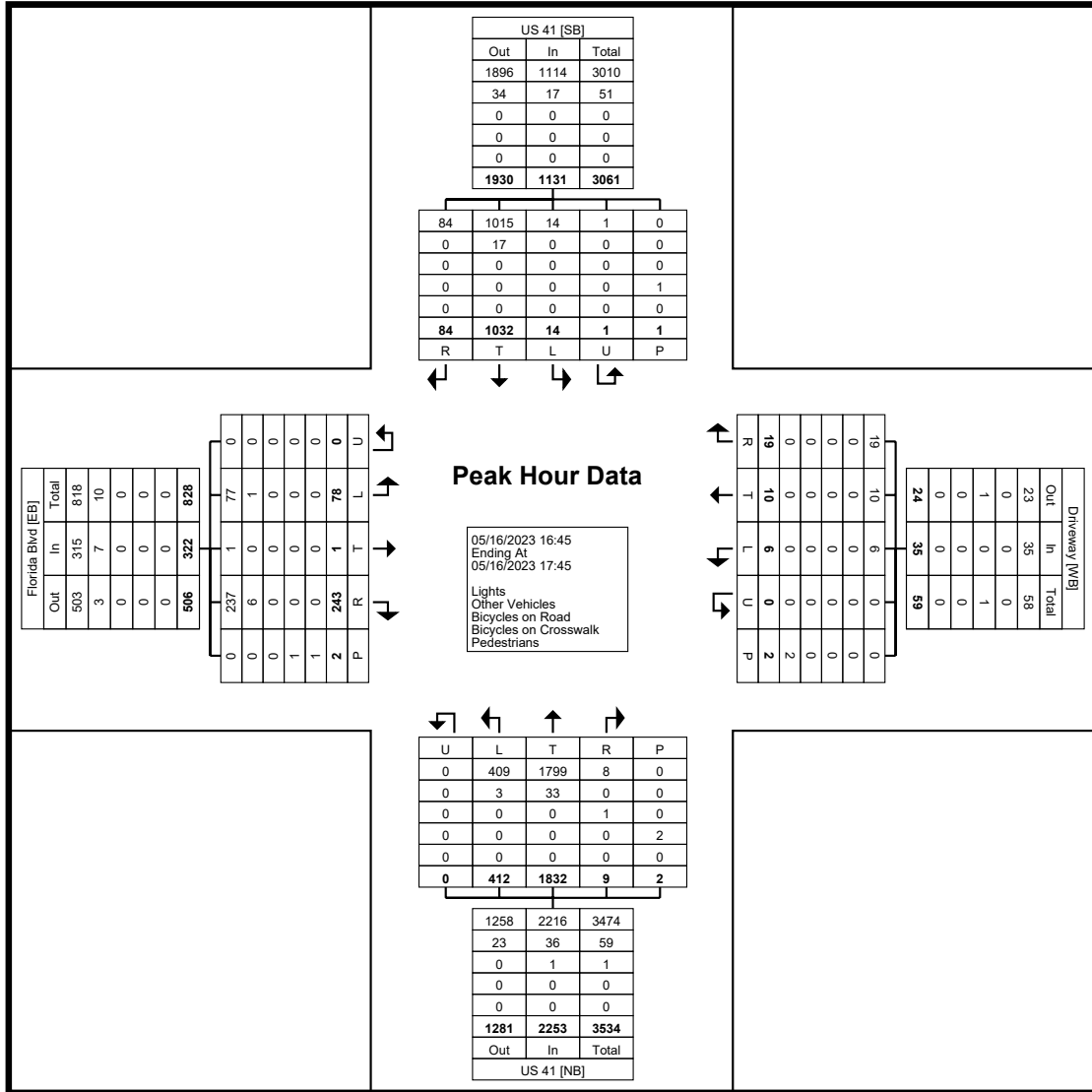


Turning Movement Peak Hour Data Plot (12:00)



### Turning Movement Peak Hour Data (16:45)

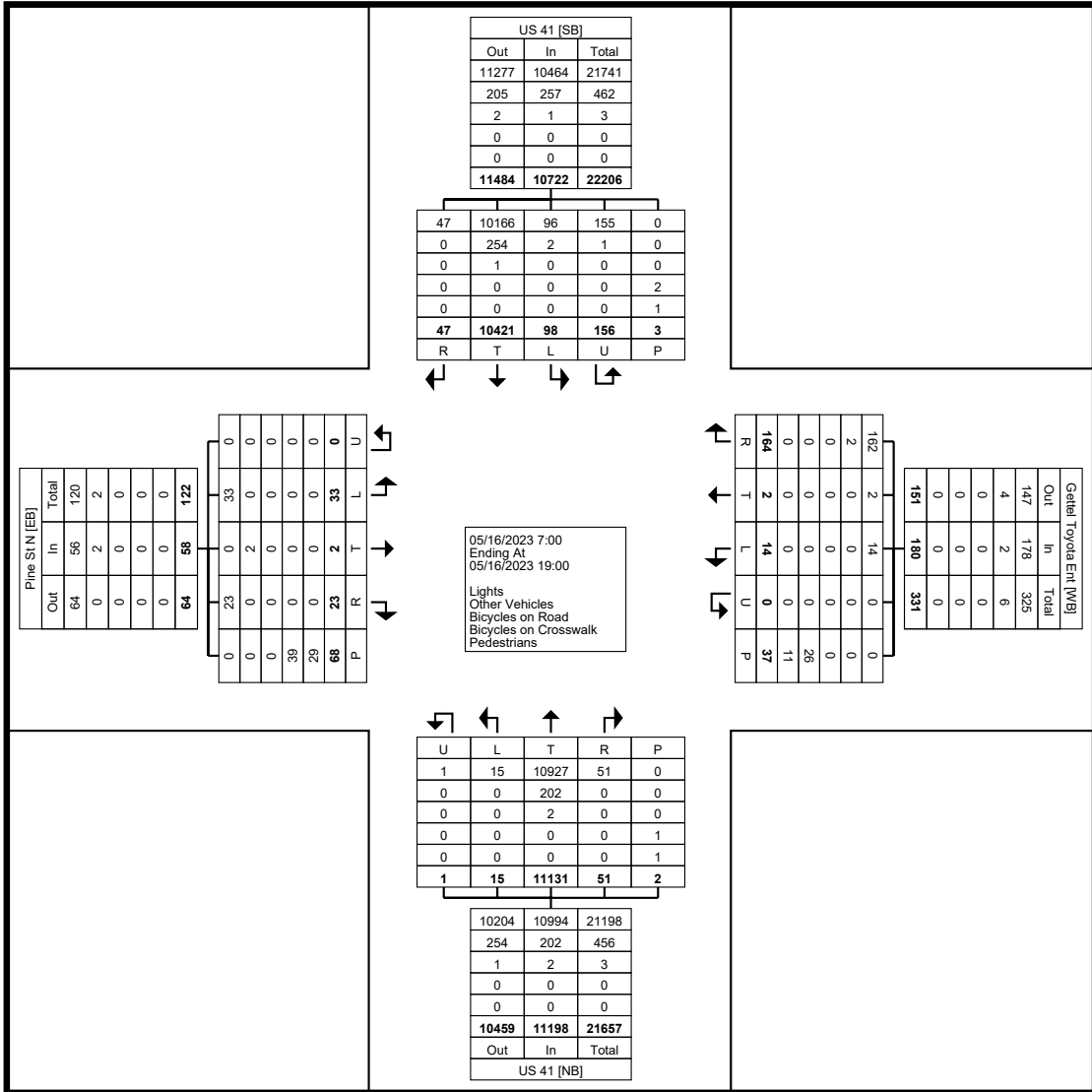
Start Time	Florida Blvd Eastbound						Driveway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:45	0	12	1	65	1	78	0	1	1	2	1	4	0	103	455	3	0	561	1	5	248	19	0	273	916
17:00	0	30	0	49	0	79	0	2	5	3	0	10	0	80	434	0	2	514	0	2	277	19	0	298	901
17:15	0	17	0	71	1	88	0	2	2	5	0	9	0	93	361	2	0	456	0	3	255	24	0	282	835
17:30	0	19	0	58	0	77	0	1	2	9	1	12	0	136	582	4	0	722	0	4	252	22	1	278	1089
<b>Total</b>	<b>0</b>	<b>78</b>	<b>1</b>	<b>243</b>	<b>2</b>	<b>322</b>	<b>0</b>	<b>6</b>	<b>10</b>	<b>19</b>	<b>2</b>	<b>35</b>	<b>0</b>	<b>412</b>	<b>1832</b>	<b>9</b>	<b>2</b>	<b>2253</b>	<b>1</b>	<b>14</b>	<b>1032</b>	<b>84</b>	<b>1</b>	<b>1131</b>	<b>3741</b>
Approach %	0.0	24.2	0.3	75.5	-	-	0.0	17.1	28.6	54.3	-	-	0.0	18.3	81.3	0.4	-	-	0.1	1.2	91.2	7.4	-	-	-
Total %	0.0	2.1	0.0	6.5	-	8.6	0.0	0.2	0.3	0.5	-	0.9	0.0	11.0	49.0	0.2	-	60.2	0.0	0.4	27.6	2.2	-	30.2	-
PHF	0.000	0.650	0.250	0.856	-	0.915	0.000	0.750	0.500	0.528	-	0.729	0.000	0.757	0.787	0.563	-	0.780	0.250	0.700	0.931	0.875	-	0.949	0.859
Lights	0	77	1	237	-	315	0	6	10	19	-	35	0	409	1799	8	-	2216	1	14	1015	84	-	1114	3680
% Lights	-	98.7	100.0	97.5	-	97.8	-	100.0	100.0	100.0	-	100.0	-	99.3	98.2	88.9	-	98.4	100.0	100.0	98.4	100.0	-	98.5	98.4
Other Vehicles	0	1	0	6	-	7	0	0	0	0	-	0	0	3	33	0	-	36	0	0	17	0	-	17	60
% Other Vehicles	-	1.3	0.0	2.5	-	2.2	-	0.0	0.0	0.0	-	0.0	-	0.7	1.8	0.0	-	1.6	0.0	0.0	1.6	0.0	-	1.5	1.6
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	1	-	1	0	0	0	0	-	0	1
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	11.1	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	2	-	-	-	-	-	1	-	-
% Bicycles on Crosswalk	-	-	-	-	50.0	-	-	-	-	-	0.0	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	50.0	-	-	-	-	-	100.0	-	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-



Turning Movement Peak Hour Data Plot (16:45)

### Turning Movement Data

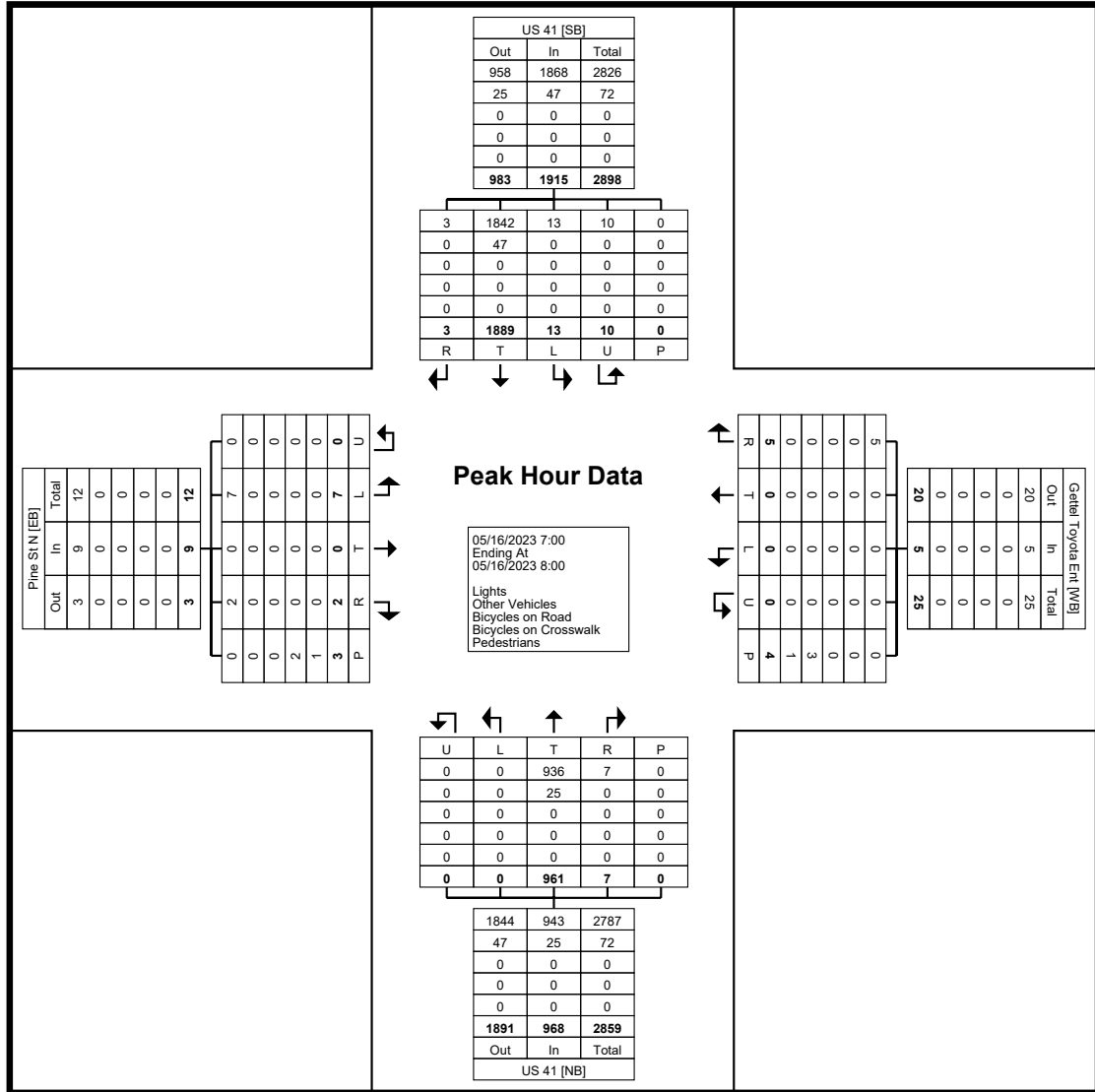
Start Time	Pine St N Eastbound						Gettel Toyota Ent Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	0	0	0	0	0	0	0	0	0	2	0	0	0	202	3	0	205	2	5	480	1	0	488	693
7:15	0	1	0	1	2	2	0	0	0	2	1	2	0	0	247	1	0	248	3	3	475	1	0	482	734
7:30	0	5	0	0	0	5	0	0	0	2	1	2	0	0	259	2	0	261	3	1	503	0	0	507	775
7:45	0	1	0	1	1	2	0	0	0	1	0	1	0	0	253	1	0	254	2	4	431	1	0	438	695
Hourly Total	0	7	0	2	3	9	0	0	0	5	4	5	0	0	961	7	0	968	10	13	1889	3	0	1915	2897
8:00	0	1	0	1	1	2	0	0	0	0	1	0	0	0	218	5	0	223	1	2	449	0	0	452	677
8:15	0	1	0	0	1	1	0	0	0	6	0	6	0	1	211	5	0	217	3	7	404	2	0	416	640
8:30	0	2	2	1	0	5	0	0	0	4	1	4	0	0	238	8	0	246	6	5	451	2	0	464	719
8:45	0	1	0	1	3	2	0	0	0	4	5	4	0	1	227	0	0	228	6	4	361	1	1	372	606
Hourly Total	0	5	2	3	5	10	0	0	0	14	7	14	0	2	894	18	0	914	16	18	1665	5	1	1704	2642
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	1	0	1	3	2	0	1	0	6	1	7	1	0	301	4	0	306	5	1	275	1	0	282	597
12:15	0	4	0	0	1	4	0	1	0	7	1	8	0	0	308	0	0	308	6	4	287	3	0	300	620
12:30	0	1	0	0	3	1	0	1	0	7	3	8	0	1	278	3	0	282	8	5	310	2	0	325	616
12:45	0	1	0	1	1	2	0	1	1	6	0	8	0	1	281	0	0	282	9	4	301	0	0	314	606
Hourly Total	0	7	0	2	8	9	0	4	1	26	5	31	1	2	1168	7	0	1178	28	14	1173	6	0	1221	2439
13:00	0	1	0	0	2	1	0	1	0	4	1	5	0	0	299	1	0	300	8	5	280	1	0	294	600
13:15	0	1	0	1	2	2	0	3	0	7	0	10	0	0	269	1	0	270	8	2	304	1	0	315	597
13:30	0	0	0	0	2	0	0	1	0	3	0	4	0	1	329	2	0	332	6	6	257	1	0	270	606
13:45	0	1	0	0	0	1	0	0	0	12	1	12	0	0	299	0	0	299	6	5	323	1	0	335	647
Hourly Total	0	3	0	1	6	4	0	5	0	26	2	31	0	1	1196	4	0	1201	28	18	1164	4	0	1214	2450
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	0	0	1	4	1	0	0	0	5	2	5	0	1	427	3	0	431	8	3	296	1	0	308	745
15:15	0	1	0	1	2	2	0	0	0	12	0	12	0	1	406	1	0	408	4	5	308	1	0	318	740
15:30	0	1	0	1	2	2	0	0	0	6	2	6	0	1	410	1	0	412	6	2	295	2	0	305	725
15:45	0	1	0	0	3	1	0	0	0	3	2	3	0	0	463	0	0	463	5	4	294	3	0	306	773
Hourly Total	0	3	0	3	11	6	0	0	0	26	6	26	0	3	1706	5	0	1714	23	14	1193	7	0	1237	2983
16:00	0	0	0	1	1	1	0	0	0	6	3	6	0	0	469	2	0	471	5	1	299	2	0	307	785
16:15	0	0	0	1	2	1	0	0	0	9	1	9	0	0	519	4	0	523	3	3	269	3	0	278	811
16:30	0	0	0	1	3	1	0	0	0	5	2	5	0	0	477	1	0	478	2	3	333	3	0	341	825
16:45	0	0	0	2	4	2	0	0	0	7	0	7	0	1	496	1	0	498	1	6	257	1	1	265	772
Hourly Total	0	0	0	5	10	5	0	0	0	27	6	27	0	1	1961	8	0	1970	11	13	1158	9	1	1191	3193
17:00	0	0	0	1	1	1	0	3	0	10	1	13	0	0	477	0	1	477	1	0	318	1	0	320	811
17:15	0	1	0	2	2	3	0	2	0	9	0	11	0	1	404	0	0	405	3	1	325	3	0	332	751
17:30	0	2	0	2	2	4	0	0	1	2	0	3	0	1	564	0	0	565	7	1	266	2	0	276	848
17:45	0	0	0	1	4	1	0	0	0	4	3	4	0	0	518	0	0	518	3	2	296	3	0	304	827
Hourly Total	0	3	0	6	9	9	0	5	1	25	4	31	0	2	1963	0	1	1965	14	4	1205	9	0	1232	3237
18:00	0	0	0	0	6	0	0	0	0	2	1	2	0	2	393	0	0	395	8	2	256	2	0	268	665
18:15	0	5	0	1	5	6	0	0	0	6	0	6	0	1	324	0	1	325	6	1	263	2	0	272	609
18:30	0	0	0	0	2	0	0	0	0	4	1	4	0	1	301	2	0	304	9	1	232	0	1	242	550
18:45	0	0	0	0	3	0	0	0	0	3	1	3	0	0	264	0	0	264	3	0	223	0	0	226	493
Hourly Total	0	5	0	1	16	6	0	0	0	15	3	15	0	4	1282	2	1	1288	26	4	974	4	1	1008	2317
Grand Total	0	33	2	23	68	58	0	14	2	164	37	180	1	15	11131	51	2	11198	156	98	10421	47	3	10722	22158
Approach %	0.0	56.9	3.4	39.7	-	-	0.0	7.8	1.1	91.1	-	-	0.0	0.1	99.4	0.5	-	-	1.5	0.9	97.2	0.4	-	-	-
Total %	0.0	0.1	0.0	0.1	-	0.3	0.0	0.1	0.0	0.7	-	0.8	0.0	0.1	50.2	0.2	-	50.5	0.7	0.4	47.0	0.2	-	48.4	-
Lights	0	33	0	23	-	56	0	14	2	162	-	178	1	15	10927	51	-	10994	155	96	10166	47	-	10464	21692
% Lights	-	100.0	0.0	100.0	-	96.6	-	100.0	100.0	98.8	-	98.9	100.0	100.0	98.2	100.0	-	98.2	99.4	98.0	97.6	100.0	-	97.6	97.9
Other Vehicles	0	0	2	0	-	2	0	0	0	2	-	2	0	0	202	0	-	202	1	2	254	0	-	257	463
% Other Vehicles	-	0.0	100.0	0.0	-	3.4	-	0.0	0.0	1.2	-	1.1	0.0	0.0	1.8	0.0	-	1.8	0.6	2.0	2.4	0.0	-	2.4	2.1
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	2	0	-	2	0	0	1	0	-	1	3
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	39	-	-	-	-	26	-	-	-	-	-	-	1	-	-	-	-	-	2	-	-
% Bicycles on Crosswalk	-	-	-	-	57.4	-	-	-	-	70.3	-	-	-	-	-	-	50.0	-	-	-	-	-	66.7	-	-
Pedestrians	-	-	-	-	29	-	-	-	-	11	-	-	-	-	-	-	1	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	42.6	-	-	-	-	29.7	-	-	-	-	-	-	50.0	-	-	-	-	-	33.3	-	-



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:00)

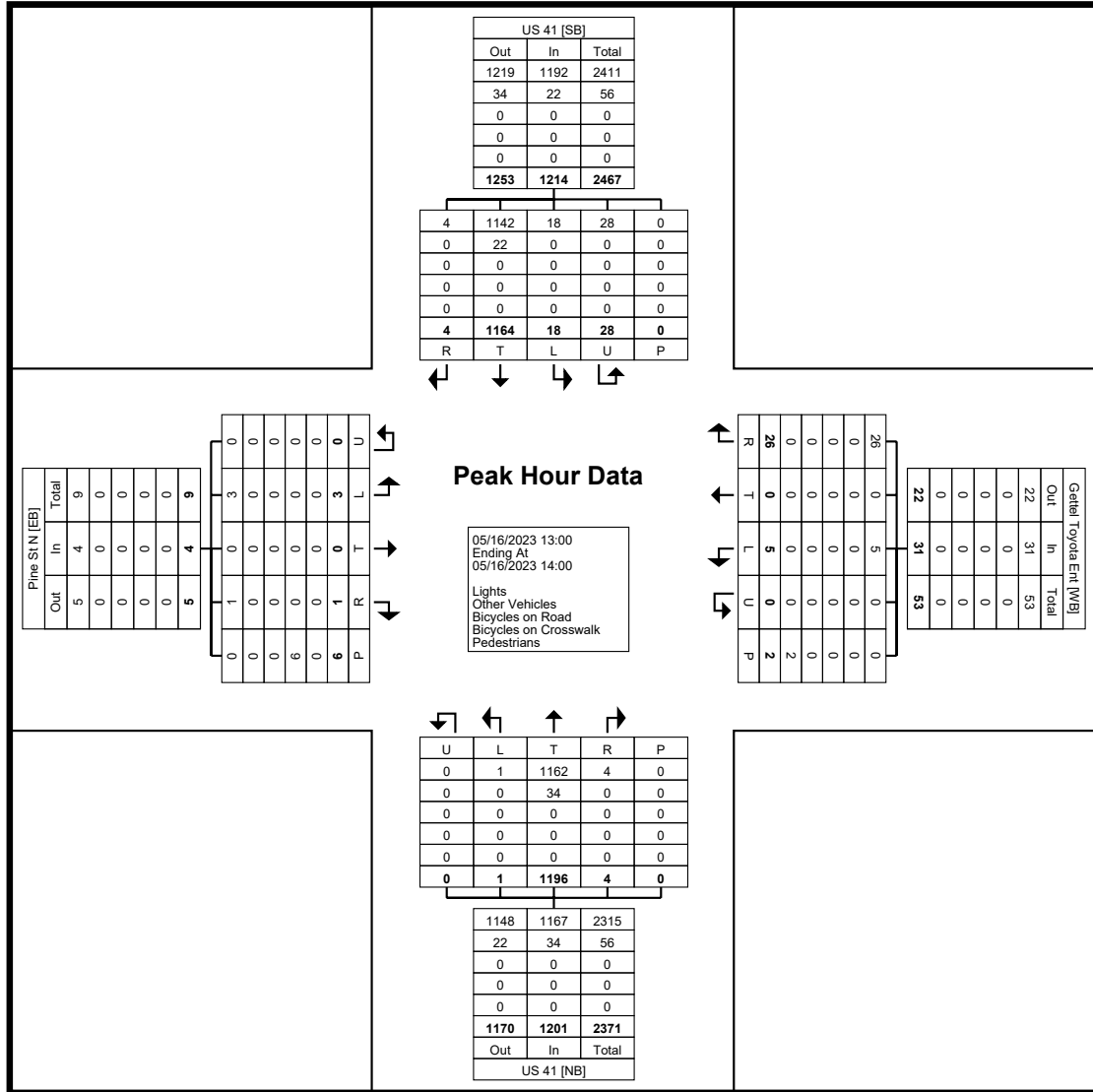
Start Time	Pine St N Eastbound						Gettel Toyota Ent Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	0	0	0	0	0	0	0	0	0	2	0	0	0	202	3	0	205	2	5	480	1	0	488	693
7:15	0	1	0	1	2	2	0	0	0	2	1	2	0	0	247	1	0	248	3	3	475	1	0	482	734
7:30	0	5	0	0	0	5	0	0	0	2	1	2	0	0	259	2	0	261	3	1	503	0	0	507	775
7:45	0	1	0	1	1	2	0	0	0	1	0	1	0	0	253	1	0	254	2	4	431	1	0	438	695
<b>Total</b>	<b>0</b>	<b>7</b>	<b>0</b>	<b>2</b>	<b>3</b>	<b>9</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>5</b>	<b>4</b>	<b>5</b>	<b>0</b>	<b>0</b>	<b>961</b>	<b>7</b>	<b>0</b>	<b>968</b>	<b>10</b>	<b>13</b>	<b>1889</b>	<b>3</b>	<b>0</b>	<b>1915</b>	<b>2897</b>
Approach %	0.0	77.8	0.0	22.2	-	-	0.0	0.0	0.0	100.0	-	-	0.0	0.0	99.3	0.7	-	-	0.5	0.7	98.6	0.2	-	-	-
Total %	0.0	0.2	0.0	0.1	-	0.3	0.0	0.0	0.0	0.2	-	0.2	0.0	0.0	33.2	0.2	-	33.4	0.3	0.4	65.2	0.1	-	66.1	-
PHF	0.000	0.350	0.000	0.500	-	0.450	0.000	0.000	0.000	0.625	-	0.625	0.000	0.000	0.928	0.583	-	0.927	0.833	0.650	0.939	0.750	-	0.944	0.935
Lights	0	7	0	2	-	9	0	0	0	5	-	5	0	0	936	7	-	943	10	13	1842	3	-	1868	2825
% Lights	-	100.0	-	100.0	-	100.0	-	-	-	100.0	-	100.0	-	-	97.4	100.0	-	97.4	100.0	100.0	97.5	100.0	-	97.5	97.5
Other Vehicles	0	0	0	0	-	0	0	0	0	0	-	0	0	0	25	0	-	25	0	0	47	0	-	47	72
% Other Vehicles	-	0.0	-	0.0	-	0.0	-	-	-	0.0	-	0.0	-	-	2.6	0.0	-	2.6	0.0	0.0	2.5	0.0	-	2.5	2.5
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	-	-	0.0	-	0.0	-	-	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	2	-	-	-	-	-	3	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	66.7	-	-	-	-	-	75.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	33.3	-	-	-	-	-	25.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-



Turning Movement Peak Hour Data Plot (7:00)

### Turning Movement Peak Hour Data (13:00)

Start Time	Pine St N Eastbound						Gettel Toyota Ent Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
13:00	0	1	0	0	2	1	0	1	0	4	1	5	0	0	299	1	0	300	8	5	280	1	0	294	600
13:15	0	1	0	1	2	2	0	3	0	7	0	10	0	0	269	1	0	270	8	2	304	1	0	315	597
13:30	0	0	0	0	2	0	0	1	0	3	0	4	0	1	329	2	0	332	6	6	257	1	0	270	606
13:45	0	1	0	0	0	1	0	0	0	12	1	12	0	0	299	0	0	299	6	5	323	1	0	335	647
<b>Total</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>1</b>	<b>6</b>	<b>4</b>	<b>0</b>	<b>5</b>	<b>0</b>	<b>26</b>	<b>2</b>	<b>31</b>	<b>0</b>	<b>1</b>	<b>1196</b>	<b>4</b>	<b>0</b>	<b>1201</b>	<b>28</b>	<b>18</b>	<b>1164</b>	<b>4</b>	<b>0</b>	<b>1214</b>	<b>2450</b>
Approach %	0.0	75.0	0.0	25.0	-	-	0.0	16.1	0.0	83.9	-	-	0.0	0.1	99.6	0.3	-	-	2.3	1.5	95.9	0.3	-	-	-
Total %	0.0	0.1	0.0	0.0	-	0.2	0.0	0.2	0.0	1.1	-	1.3	0.0	0.0	48.8	0.2	-	49.0	1.1	0.7	47.5	0.2	-	49.6	-
PHF	0.000	0.750	0.000	0.250	-	0.500	0.000	0.417	0.000	0.542	-	0.646	0.000	0.250	0.909	0.500	-	0.904	0.875	0.750	0.901	1.000	-	0.906	0.947
Lights	0	3	0	1	-	4	0	5	0	26	-	31	0	1	1162	4	-	1167	28	18	1142	4	-	1192	2394
% Lights	-	100.0	-	100.0	-	100.0	-	100.0	-	100.0	-	100.0	-	100.0	97.2	100.0	-	97.2	100.0	100.0	98.1	100.0	-	98.2	97.7
Other Vehicles	0	0	0	0	-	0	0	0	0	0	-	0	0	0	34	0	-	34	0	0	22	0	-	22	56
% Other Vehicles	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	2.8	0.0	-	2.8	0.0	0.0	1.9	0.0	-	1.8	2.3
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	6	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	100.0	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	0.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-



Turning Movement Peak Hour Data Plot (13:00)



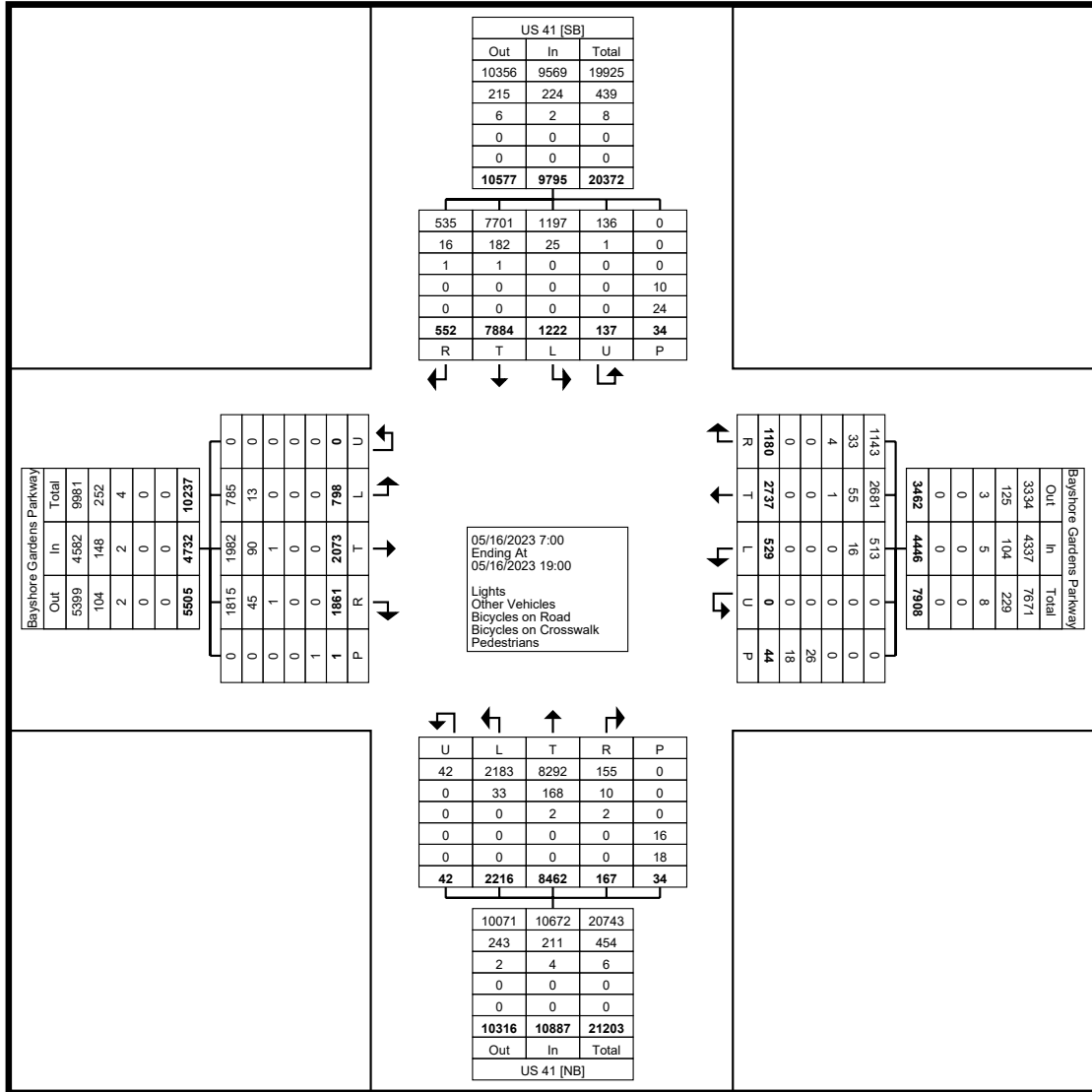
### Turning Movement Peak Hour Data (17:00)

Start Time	Pine St N Eastbound						Gettel Toyota Ent Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
17:00	0	0	0	1	1	1	0	3	0	10	1	13	0	0	477	0	1	477	1	0	318	1	0	320	811
17:15	0	1	0	2	2	3	0	2	0	9	0	11	0	1	404	0	0	405	3	1	325	3	0	332	751
17:30	0	2	0	2	2	4	0	0	1	2	0	3	0	1	564	0	0	565	7	1	266	2	0	276	848
17:45	0	0	0	1	4	1	0	0	0	4	3	4	0	0	518	0	0	518	3	2	296	3	0	304	827
<b>Total</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>6</b>	<b>9</b>	<b>9</b>	<b>0</b>	<b>5</b>	<b>1</b>	<b>25</b>	<b>4</b>	<b>31</b>	<b>0</b>	<b>2</b>	<b>1963</b>	<b>0</b>	<b>1</b>	<b>1965</b>	<b>14</b>	<b>4</b>	<b>1205</b>	<b>9</b>	<b>0</b>	<b>1232</b>	<b>3237</b>
Approach %	0.0	33.3	0.0	66.7	-	-	0.0	16.1	3.2	80.6	-	-	0.0	0.1	99.9	0.0	-	-	1.1	0.3	97.8	0.7	-	-	-
Total %	0.0	0.1	0.0	0.2	-	0.3	0.0	0.2	0.0	0.8	-	1.0	0.0	0.1	60.6	0.0	-	60.7	0.4	0.1	37.2	0.3	-	38.1	-
PHF	0.000	0.375	0.000	0.750	-	0.563	0.000	0.417	0.250	0.625	-	0.596	0.000	0.500	0.870	0.000	-	0.869	0.500	0.500	0.927	0.750	-	0.928	0.954
Lights	0	3	0	6	-	9	0	5	1	25	-	31	0	2	1929	0	-	1931	14	3	1182	9	-	1208	3179
% Lights	-	100.0	-	100.0	-	100.0	-	100.0	100.0	100.0	-	100.0	-	100.0	98.3	-	-	98.3	100.0	75.0	98.1	100.0	-	98.1	98.2
Other Vehicles	0	0	0	0	-	0	0	0	0	0	-	0	0	0	33	0	-	33	0	1	23	0	-	24	57
% Other Vehicles	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	1.7	-	-	1.7	0.0	25.0	1.9	0.0	-	1.9	1.8
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	1	0	-	1	0	0	0	0	-	0	1
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.1	-	-	0.1	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	5	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	55.6	-	-	-	-	-	50.0	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	4	-	-	-	-	-	2	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	44.4	-	-	-	-	-	50.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-



### Turning Movement Data

Start Time	Bayshore Gardens Parkway Eastbound						Bayshore Gardens Parkway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	17	54	67	0	138	0	13	59	31	2	103	3	38	177	3	1	221	2	41	350	10	1	403	865
7:15	0	28	76	69	0	173	0	18	53	26	1	97	1	35	206	1	1	243	1	37	416	9	2	463	976
7:30	0	26	99	61	0	186	0	14	56	25	1	95	1	50	260	0	0	311	0	39	377	8	1	424	1016
7:45	0	32	80	69	0	181	0	10	57	39	1	106	0	50	193	2	3	245	1	35	358	13	0	407	939
Hourly Total	0	103	309	266	0	678	0	55	225	121	5	401	5	173	836	6	5	1020	4	152	1501	40	4	1697	3796
8:00	0	24	60	69	0	153	0	17	64	40	0	121	0	37	172	1	1	210	2	36	393	8	1	439	923
8:15	0	23	44	74	0	141	0	11	72	40	1	123	0	43	201	3	0	247	4	36	321	10	0	371	882
8:30	0	22	67	63	0	152	0	21	75	40	2	136	0	47	169	2	1	218	3	32	345	6	4	386	892
8:45	0	21	36	68	0	125	0	16	56	34	4	106	2	49	175	6	0	232	1	35	288	12	3	336	799
Hourly Total	0	90	207	274	0	571	0	65	267	154	7	486	2	176	717	12	2	907	10	139	1347	36	8	1532	3496
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	21	76	58	0	155	0	22	69	34	1	125	2	54	243	4	0	303	3	35	188	17	1	243	826
12:15	0	35	52	61	0	148	0	15	65	30	1	110	1	61	212	6	0	280	8	43	214	9	0	274	812
12:30	0	18	46	52	0	116	0	11	62	39	3	112	4	56	232	4	1	296	7	45	242	13	0	307	831
12:45	0	26	58	49	0	133	0	16	54	28	1	98	1	66	201	4	2	272	5	43	245	23	1	316	819
Hourly Total	0	100	232	220	0	552	0	64	250	131	6	445	8	237	888	18	3	1151	23	166	889	62	2	1140	3288
13:00	0	29	50	52	1	131	0	11	69	44	0	124	2	67	228	7	1	304	4	42	207	22	4	275	834
13:15	0	15	72	67	0	154	0	22	61	36	0	119	2	50	204	12	1	268	8	33	234	17	0	292	833
13:30	0	25	57	46	0	128	0	15	70	41	1	126	0	54	266	5	0	325	5	47	206	15	2	273	852
13:45	0	22	62	55	0	139	0	13	52	30	0	95	0	65	245	4	1	314	2	30	234	18	2	284	832
Hourly Total	0	91	241	220	1	552	0	61	252	151	1	464	4	236	943	28	3	1211	19	152	881	72	8	1124	3351
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	30	76	60	0	166	0	17	76	34	3	127	0	79	294	10	1	383	5	45	232	16	0	298	974
15:15	0	25	61	47	0	133	0	14	104	33	0	151	1	90	355	7	1	453	7	42	214	25	0	288	1025
15:30	0	38	91	69	0	198	0	22	127	55	5	204	3	55	320	5	2	383	4	37	225	18	0	284	1069
15:45	0	28	81	69	0	178	0	18	120	39	1	177	1	104	357	4	0	466	5	42	210	20	1	277	1098
Hourly Total	0	121	309	245	0	675	0	71	427	161	9	659	5	328	1326	26	4	1685	21	166	881	79	1	1147	4166
16:00	0	28	55	52	0	135	0	14	143	46	2	203	2	96	343	8	0	449	6	50	191	26	0	273	1060
16:15	0	24	85	50	0	159	0	20	104	53	2	177	2	94	341	7	0	444	5	35	211	23	0	274	1054
16:30	0	21	72	54	0	147	0	21	95	42	1	158	2	109	354	2	2	467	5	42	243	20	0	310	1082
16:45	0	20	70	59	0	149	0	17	137	47	3	201	0	102	352	6	1	460	6	37	170	20	1	233	1043
Hourly Total	0	93	282	215	0	590	0	72	479	188	8	739	6	401	1390	23	3	1820	22	164	815	89	1	1090	4239
17:00	0	27	75	65	0	167	0	25	126	35	1	186	1	91	305	6	0	403	4	40	221	26	3	291	1047
17:15	0	21	59	67	0	147	0	21	162	40	0	223	3	106	332	5	1	446	4	36	208	22	1	270	1086
17:30	0	27	105	56	0	188	0	17	118	35	0	170	0	96	380	6	0	482	4	33	200	20	0	257	1097
17:45	0	23	61	61	0	145	0	21	96	35	1	152	1	111	354	7	5	473	5	37	208	15	0	265	1035
Hourly Total	0	98	300	249	0	647	0	84	502	145	2	731	5	404	1371	24	6	1804	17	146	837	83	4	1083	4265
18:00	0	28	55	51	0	134	0	9	111	35	1	155	2	93	320	6	1	421	3	26	167	23	0	219	929
18:15	0	29	47	48	0	124	0	20	76	37	3	133	0	64	236	13	3	313	3	36	216	22	2	277	847
18:30	0	24	34	40	0	98	0	16	71	26	0	113	2	55	236	7	3	300	7	39	182	22	2	250	761
18:45	0	21	57	33	0	111	0	12	77	31	2	120	3	49	199	4	1	255	8	36	168	24	2	236	722
Hourly Total	0	102	193	172	0	467	0	57	335	129	6	521	7	261	991	30	8	1289	21	137	733	91	6	982	3259
Grand Total	0	798	2073	1861	1	4732	0	529	2737	1180	44	4446	42	2216	8462	167	34	10887	137	1222	7884	552	34	9795	29860
Approach %	0.0	16.9	43.8	39.3	-	-	0.0	11.9	61.6	26.5	-	-	0.4	20.4	77.7	1.5	-	-	1.4	12.5	80.5	5.6	-	-	-
Total %	0.0	2.7	6.9	6.2	-	15.8	0.0	1.8	9.2	4.0	-	14.9	0.1	7.4	28.3	0.6	-	36.5	0.5	4.1	26.4	1.8	-	32.8	-
Lights	0	785	1982	1815	-	4582	0	513	2681	1143	-	4337	42	2183	8292	155	-	10672	136	1197	7701	535	-	9569	29160
% Lights	-	98.4	95.6	97.5	-	96.8	-	97.0	98.0	96.9	-	97.5	100.0	98.5	98.0	92.8	-	98.0	99.3	98.0	97.7	96.9	-	97.7	97.7
Other Vehicles	0	13	90	45	-	148	0	16	55	33	-	104	0	33	168	10	-	211	1	25	182	16	-	224	687
% Other Vehicles	-	1.6	4.3	2.4	-	3.1	-	3.0	2.0	2.8	-	2.3	0.0	1.5	2.0	6.0	-	1.9	0.7	2.0	2.3	2.9	-	2.3	2.3
Bicycles on Road	0	0	1	1	-	2	0	0	1	4	-	5	0	0	2	2	-	4	0	0	1	1	-	2	13
% Bicycles on Road	-	0.0	0.0	0.1	-	0.0	-	0.0	0.0	0.3	-	0.1	0.0	0.0	0.0	1.2	-	0.0	0.0	0.0	0.0	0.2	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	26	-	-	-	-	-	16	-	-	-	-	-	10	-	-
% Bicycles on Crosswalk	-	-	-	-	0.0	-	-	-	-	-	59.1	-	-	-	-	-	47.1	-	-	-	-	-	29.4	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	18	-	-	-	-	-	18	-	-	-	-	-	24	-	-
% Pedestrians	-	-	-	-	100.0	-	-	-	-	-	40.9	-	-	-	-	-	52.9	-	-	-	-	-	70.6	-	-



Turning Movement Data Plot

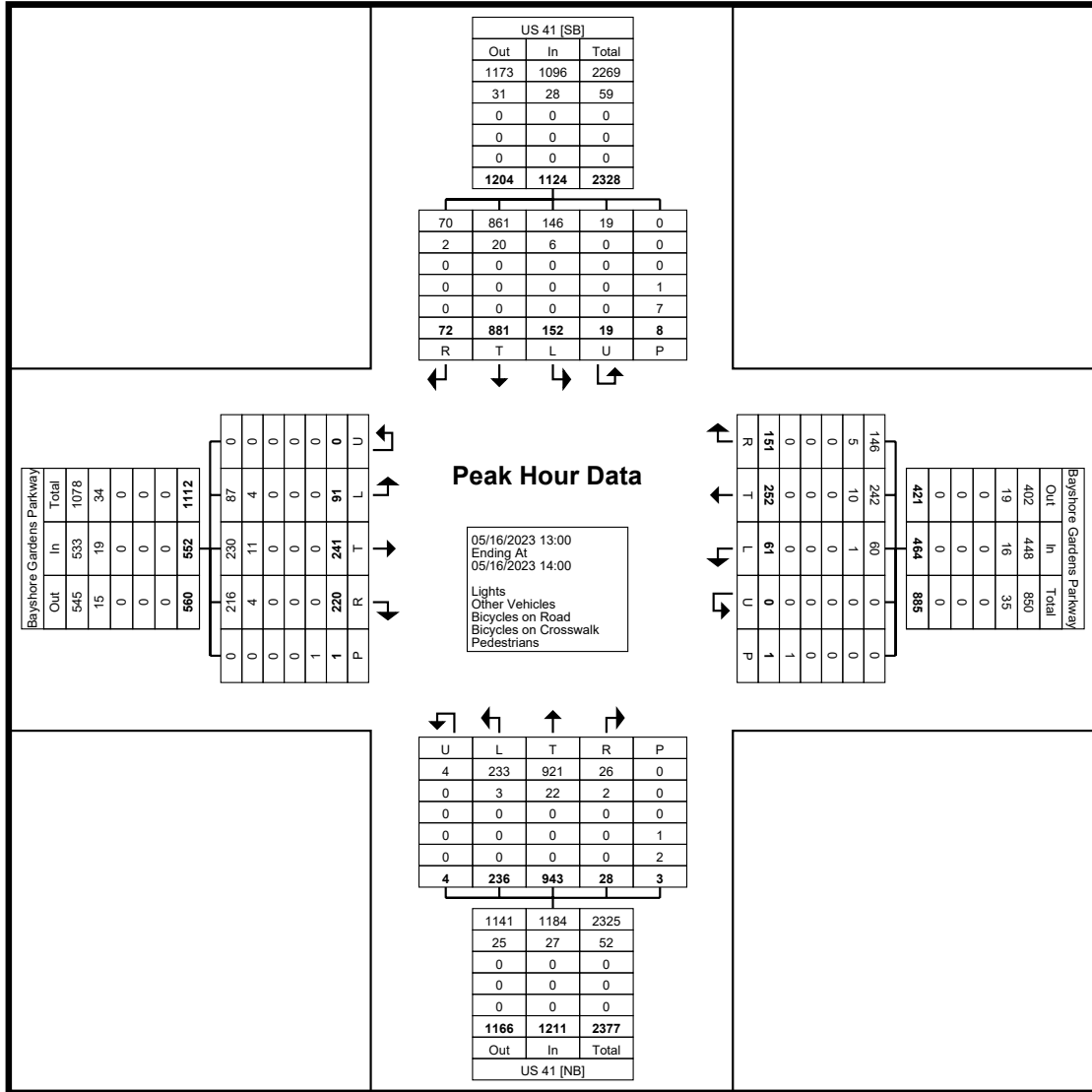
### Turning Movement Peak Hour Data (7:15)

Start Time	Bayshore Gardens Parkway Eastbound						Bayshore Gardens Parkway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15	0	28	76	69	0	173	0	18	53	26	1	97	1	35	206	1	1	243	1	37	416	9	2	463	976
7:30	0	26	99	61	0	186	0	14	56	25	1	95	1	50	260	0	0	311	0	39	377	8	1	424	1016
7:45	0	32	80	69	0	181	0	10	57	39	1	106	0	50	193	2	3	245	1	35	358	13	0	407	939
8:00	0	24	60	69	0	153	0	17	64	40	0	121	0	37	172	1	1	210	2	36	393	8	1	439	923
<b>Total</b>	0	110	315	268	0	693	0	59	230	130	3	419	2	172	831	4	5	1009	4	147	1544	38	4	1733	3854
Approach %	0.0	15.9	45.5	38.7	-	-	0.0	14.1	54.9	31.0	-	-	0.2	17.0	82.4	0.4	-	-	0.2	8.5	89.1	2.2	-	-	-
Total %	0.0	2.9	8.2	7.0	-	18.0	0.0	1.5	6.0	3.4	-	10.9	0.1	4.5	21.6	0.1	-	26.2	0.1	3.8	40.1	1.0	-	45.0	-
PHF	0.000	0.859	0.795	0.971	-	0.931	0.000	0.819	0.898	0.813	-	0.866	0.500	0.860	0.799	0.500	-	0.811	0.500	0.942	0.928	0.731	-	0.936	0.948
Lights	0	109	302	265	-	676	0	55	223	123	-	401	2	168	809	1	-	980	4	143	1514	35	-	1696	3753
% Lights	-	99.1	95.9	98.9	-	97.5	-	93.2	97.0	94.6	-	95.7	100.0	97.7	97.4	25.0	-	97.1	100.0	97.3	98.1	92.1	-	97.9	97.4
Other Vehicles	0	1	12	3	-	16	0	4	7	7	-	18	0	4	22	2	-	28	0	4	30	3	-	37	99
% Other Vehicles	-	0.9	3.8	1.1	-	2.3	-	6.8	3.0	5.4	-	4.3	0.0	2.3	2.6	50.0	-	2.8	0.0	2.7	1.9	7.9	-	2.1	2.6
Bicycles on Road	0	0	1	0	-	1	0	0	0	0	-	0	0	0	0	1	-	1	0	0	0	0	-	0	2
% Bicycles on Road	-	0.0	0.3	0.0	-	0.1	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	25.0	-	0.1	0.0	0.0	0.0	0.0	-	0.0	0.1
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	2	-	-	-	-	-	3	-	-	-	-	-	1	-	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	66.7	-	-	-	-	-	60.0	-	-	-	-	-	25.0	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	2	-	-	-	-	-	3	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	33.3	-	-	-	-	-	40.0	-	-	-	-	-	75.0	-	-



### Turning Movement Peak Hour Data (13:00)

Start Time	Bayshore Gardens Parkway Eastbound						Bayshore Gardens Parkway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
13:00	0	29	50	52	1	131	0	11	69	44	0	124	2	67	228	7	1	304	4	42	207	22	4	275	834
13:15	0	15	72	67	0	154	0	22	61	36	0	119	2	50	204	12	1	268	8	33	234	17	0	292	833
13:30	0	25	57	46	0	128	0	15	70	41	1	126	0	54	266	5	0	325	5	47	206	15	2	273	852
13:45	0	22	62	55	0	139	0	13	52	30	0	95	0	65	245	4	1	314	2	30	234	18	2	284	832
<b>Total</b>	<b>0</b>	<b>91</b>	<b>241</b>	<b>220</b>	<b>1</b>	<b>552</b>	<b>0</b>	<b>61</b>	<b>252</b>	<b>151</b>	<b>1</b>	<b>464</b>	<b>4</b>	<b>236</b>	<b>943</b>	<b>28</b>	<b>3</b>	<b>1211</b>	<b>19</b>	<b>152</b>	<b>881</b>	<b>72</b>	<b>8</b>	<b>1124</b>	<b>3351</b>
Approach %	0.0	16.5	43.7	39.9	-	-	0.0	13.1	54.3	32.5	-	-	0.3	19.5	77.9	2.3	-	-	1.7	13.5	78.4	6.4	-	-	-
Total %	0.0	2.7	7.2	6.6	-	16.5	0.0	1.8	7.5	4.5	-	13.8	0.1	7.0	28.1	0.8	-	36.1	0.6	4.5	26.3	2.1	-	33.5	-
PHF	0.000	0.784	0.837	0.821	-	0.896	0.000	0.693	0.900	0.858	-	0.921	0.500	0.881	0.886	0.583	-	0.932	0.594	0.809	0.941	0.818	-	0.962	0.983
Lights	0	87	230	216	-	533	0	60	242	146	-	448	4	233	921	26	-	1184	19	146	861	70	-	1096	3261
% Lights	-	95.6	95.4	98.2	-	96.6	-	98.4	96.0	96.7	-	96.6	100.0	98.7	97.7	92.9	-	97.8	100.0	96.1	97.7	97.2	-	97.5	97.3
Other Vehicles	0	4	11	4	-	19	0	1	10	5	-	16	0	3	22	2	-	27	0	6	20	2	-	28	90
% Other Vehicles	-	4.4	4.6	1.8	-	3.4	-	1.6	4.0	3.3	-	3.4	0.0	1.3	2.3	7.1	-	2.2	0.0	3.9	2.3	2.8	-	2.5	2.7
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	1	-	-
% Bicycles on Crosswalk	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-	-	-	-	33.3	-	-	-	-	-	12.5	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	1	-	-	-	-	-	2	-	-	-	-	-	7	-	-
% Pedestrians	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-	-	-	-	66.7	-	-	-	-	-	87.5	-	-



Turning Movement Peak Hour Data Plot (13:00)



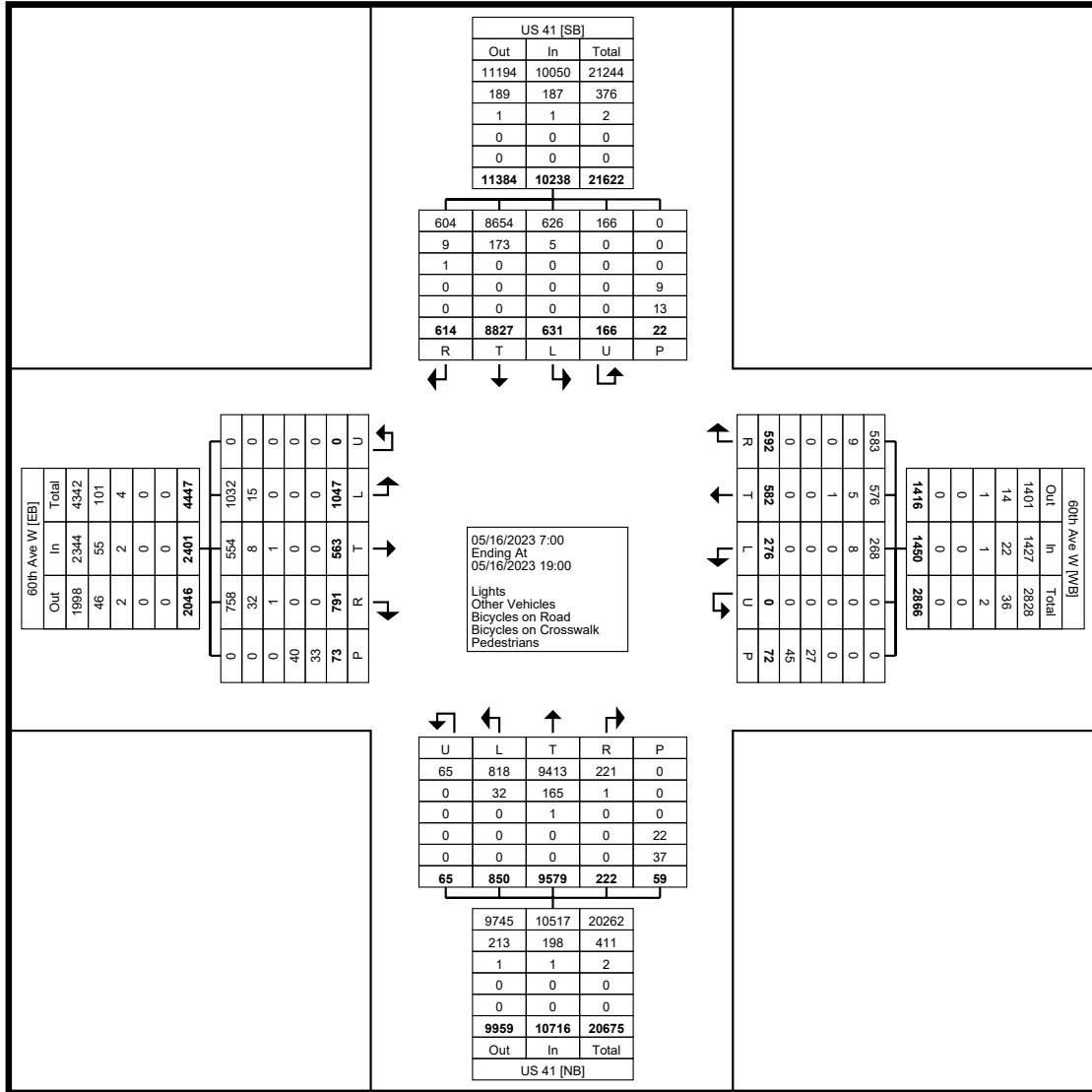
### Turning Movement Peak Hour Data (15:45)

Start Time	Bayshore Gardens Parkway Eastbound						Bayshore Gardens Parkway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
15:45	0	28	81	69	0	178	0	18	120	39	1	177	1	104	357	4	0	466	5	42	210	20	1	277	1098
16:00	0	28	55	52	0	135	0	14	143	46	2	203	2	96	343	8	0	449	6	50	191	26	0	273	1060
16:15	0	24	85	50	0	159	0	20	104	53	2	177	2	94	341	7	0	444	5	35	211	23	0	274	1054
16:30	0	21	72	54	0	147	0	21	95	42	1	158	2	109	354	2	2	467	5	42	243	20	0	310	1082
<b>Total</b>	<b>0</b>	<b>101</b>	<b>293</b>	<b>225</b>	<b>0</b>	<b>619</b>	<b>0</b>	<b>73</b>	<b>462</b>	<b>180</b>	<b>6</b>	<b>715</b>	<b>7</b>	<b>403</b>	<b>1395</b>	<b>21</b>	<b>2</b>	<b>1826</b>	<b>21</b>	<b>169</b>	<b>855</b>	<b>89</b>	<b>1</b>	<b>1134</b>	<b>4294</b>
Approach %	0.0	16.3	47.3	36.3	-	-	0.0	10.2	64.6	25.2	-	-	0.4	22.1	76.4	1.2	-	-	1.9	14.9	75.4	7.8	-	-	-
Total %	0.0	2.4	6.8	5.2	-	14.4	0.0	1.7	10.8	4.2	-	16.7	0.2	9.4	32.5	0.5	-	42.5	0.5	3.9	19.9	2.1	-	26.4	-
PHF	0.000	0.902	0.862	0.815	-	0.869	0.000	0.869	0.808	0.849	-	0.881	0.875	0.924	0.977	0.656	-	0.978	0.875	0.845	0.880	0.856	-	0.915	0.978
Lights	0	99	276	214	-	589	0	71	460	175	-	706	7	401	1359	19	-	1786	21	165	837	85	-	1108	4189
% Lights	-	98.0	94.2	95.1	-	95.2	-	97.3	99.6	97.2	-	98.7	100.0	99.5	97.4	90.5	-	97.8	100.0	97.6	97.9	95.5	-	97.7	97.6
Other Vehicles	0	2	17	11	-	30	0	2	2	4	-	8	0	2	35	1	-	38	0	4	18	3	-	25	101
% Other Vehicles	-	2.0	5.8	4.9	-	4.8	-	2.7	0.4	2.2	-	1.1	0.0	0.5	2.5	4.8	-	2.1	0.0	2.4	2.1	3.4	-	2.2	2.4
Bicycles on Road	0	0	0	0	-	0	0	0	0	1	-	1	0	0	1	1	-	2	0	0	0	1	-	1	4
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.6	-	0.1	0.0	0.0	0.1	4.8	-	0.1	0.0	0.0	0.0	1.1	-	0.1	0.1
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	6	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	100.0	-	-	-	-	-	50.0	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	0.0	-	-	-	-	-	50.0	-	-	-	-	-	100.0	-	-



### Turning Movement Data

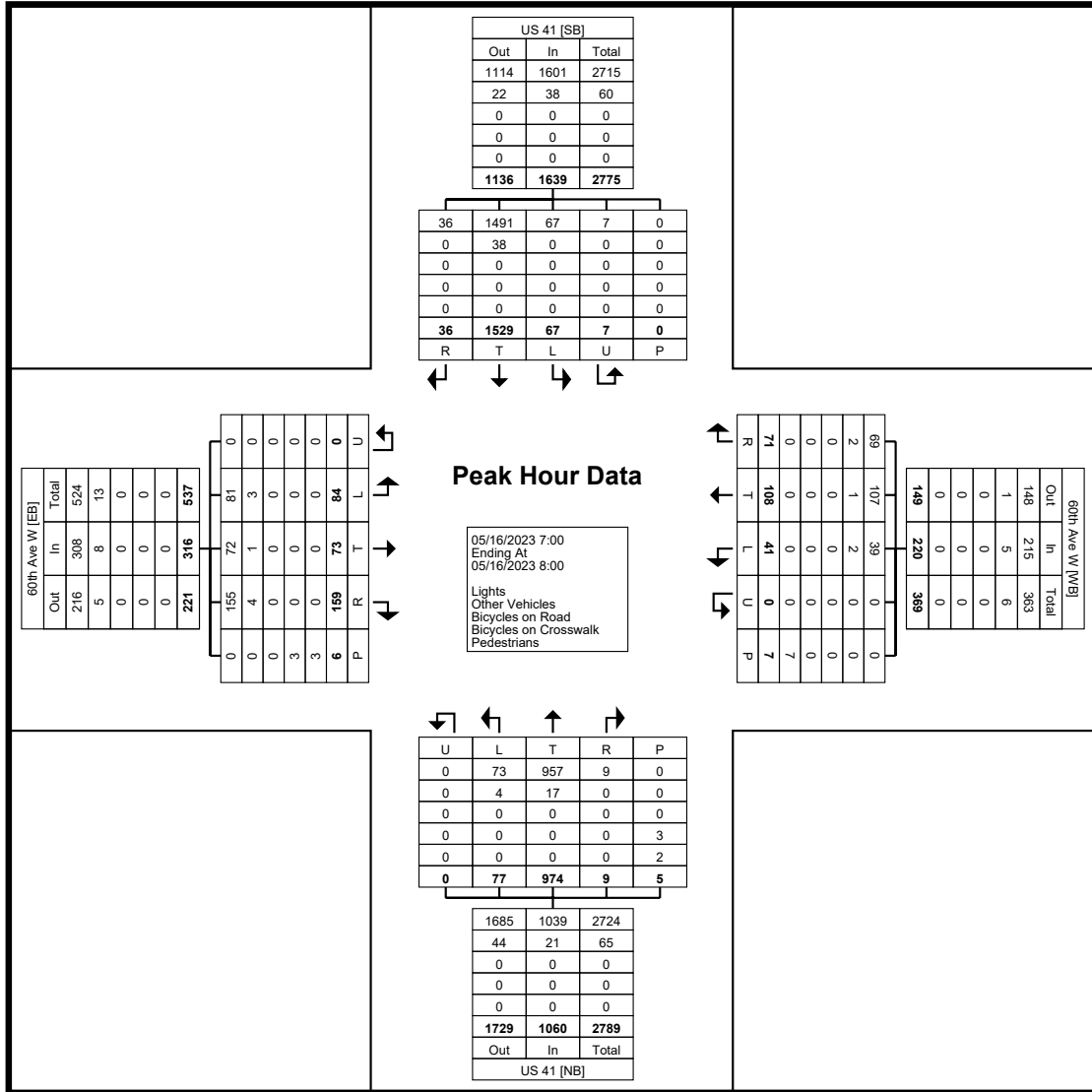
Start Time	60th Ave W Eastbound						60th Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	20	8	35	3	63	0	14	37	18	3	69	0	13	202	2	1	217	2	22	384	7	0	415	764
7:15	0	17	18	36	2	71	0	15	34	20	0	69	0	18	260	2	1	280	1	18	431	9	0	459	879
7:30	0	23	33	49	0	105	0	6	20	18	2	44	0	24	247	2	2	273	0	11	339	9	0	359	781
7:45	0	24	14	39	1	77	0	6	17	15	2	38	0	22	265	3	1	290	4	16	375	11	0	406	811
Hourly Total	0	84	73	159	6	316	0	41	108	71	7	220	0	77	974	9	5	1060	7	67	1529	36	0	1639	3235
8:00	0	22	9	34	0	65	0	7	15	18	3	40	0	12	236	3	1	251	1	12	383	11	1	407	763
8:15	0	19	9	21	2	49	0	5	23	20	2	48	2	14	212	9	2	237	0	14	361	15	0	390	724
8:30	0	16	14	26	2	56	0	9	11	19	2	39	0	13	216	2	2	231	3	8	323	14	1	348	674
8:45	0	21	13	22	1	56	0	8	23	16	7	47	3	16	203	3	2	225	3	14	317	11	1	345	673
Hourly Total	0	78	45	103	5	226	0	29	72	73	14	174	5	55	867	17	7	944	7	48	1384	51	3	1490	2834
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	53	15	11	4	79	0	10	14	16	1	40	3	28	269	6	0	306	8	10	220	18	1	256	681
12:15	0	41	15	22	1	78	0	7	10	13	4	30	3	23	238	3	0	267	7	13	294	28	1	342	717
12:30	0	50	17	27	2	94	0	4	11	20	3	35	3	22	259	5	4	289	8	17	252	22	2	299	717
12:45	0	41	11	17	2	69	0	13	13	10	1	36	4	21	234	5	3	264	2	16	283	29	0	330	699
Hourly Total	0	185	58	77	9	320	0	34	48	59	9	141	13	94	1000	19	7	1126	25	56	1049	97	4	1227	2814
13:00	0	41	15	14	3	70	0	12	12	12	5	36	3	13	277	4	0	297	2	20	253	13	0	288	691
13:15	0	24	11	18	1	53	0	7	8	11	0	26	3	19	244	6	0	272	8	19	251	24	0	302	653
13:30	0	29	12	16	3	57	0	6	9	14	2	29	2	28	278	2	6	310	7	17	263	19	0	306	702
13:45	0	39	10	30	4	79	0	4	17	10	0	31	3	20	290	5	0	318	6	12	235	19	0	272	700
Hourly Total	0	133	48	78	11	259	0	29	46	47	7	122	11	80	1089	17	6	1197	23	68	1002	75	0	1168	2746
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	35	18	30	1	83	0	4	9	12	5	25	1	30	341	9	4	381	8	17	256	16	1	297	786
15:15	0	39	12	33	3	84	0	6	16	19	2	41	0	32	331	9	4	372	9	18	248	25	1	300	797
15:30	0	33	15	34	1	82	0	11	21	12	4	44	1	27	402	13	1	443	4	20	254	30	1	308	877
15:45	0	41	28	18	0	87	0	7	22	11	0	40	1	34	381	7	4	423	3	20	255	19	1	297	847
Hourly Total	0	148	73	115	5	336	0	28	68	54	11	150	3	123	1455	38	13	1619	24	75	1013	90	4	1202	3307
16:00	0	40	20	24	3	84	0	10	26	16	0	52	2	32	384	11	2	429	5	20	256	23	3	304	869
16:15	0	26	19	21	0	66	0	10	23	15	2	48	4	45	416	12	0	477	8	23	249	24	0	304	895
16:30	0	33	29	29	1	91	0	12	18	17	1	47	5	43	362	5	0	415	11	21	258	24	2	314	867
16:45	0	38	24	19	6	81	0	11	23	16	5	50	1	38	405	12	3	456	6	27	247	21	0	301	888
Hourly Total	0	137	92	93	10	322	0	43	90	64	8	197	12	158	1567	40	5	1777	30	91	1010	92	5	1223	3519
17:00	0	40	33	26	2	99	0	8	17	20	0	45	2	42	379	11	2	434	3	29	253	33	0	318	896
17:15	0	42	28	26	2	96	0	15	26	25	2	66	2	46	349	9	2	406	8	26	240	21	2	295	863
17:30	0	38	24	23	1	85	0	9	13	29	0	51	4	39	386	8	5	437	3	17	235	26	0	281	854
17:45	0	33	16	13	3	62	0	12	23	30	3	65	3	52	388	12	0	455	5	33	250	18	2	306	888
Hourly Total	0	153	101	88	8	342	0	44	79	104	5	227	11	179	1502	40	9	1732	19	105	978	98	4	1200	3501
18:00	0	36	21	14	8	71	0	9	18	26	3	53	1	26	336	9	3	372	4	28	225	17	0	274	770
18:15	0	36	11	23	5	70	0	6	19	26	2	51	3	22	293	12	2	330	10	32	222	18	1	282	733
18:30	0	30	20	20	3	70	0	8	18	38	1	64	3	17	237	16	2	273	7	34	203	18	0	262	669
18:45	0	27	21	21	3	69	0	5	16	30	5	51	3	19	259	5	0	286	10	27	212	22	1	271	677
Hourly Total	0	129	73	78	19	280	0	28	71	120	11	219	10	84	1125	42	7	1261	31	121	862	75	2	1089	2849
Grand Total	0	1047	563	791	73	2401	0	276	582	592	72	1450	65	850	9579	222	59	10716	166	631	8827	614	22	10238	24805
Approach %	0.0	43.6	23.4	32.9	-	-	0.0	19.0	40.1	40.8	-	-	0.6	7.9	89.4	2.1	-	-	1.6	6.2	86.2	6.0	-	-	-
Total %	0.0	4.2	2.3	3.2	-	9.7	0.0	1.1	2.3	2.4	-	5.8	0.3	3.4	38.6	0.9	-	43.2	0.7	2.5	35.6	2.5	-	41.3	-
Lights	0	1032	554	758	-	2344	0	268	576	583	-	1427	65	818	9413	221	-	10517	166	626	8654	604	-	10050	24338
% Lights	-	98.6	98.4	95.8	-	97.6	-	97.1	99.0	98.5	-	98.4	100.0	96.2	98.3	99.5	-	98.1	100.0	99.2	98.0	98.4	-	98.2	98.1
Other Vehicles	0	15	8	32	-	55	0	8	5	9	-	22	0	32	165	1	-	198	0	5	173	9	-	187	462
% Other Vehicles	-	1.4	1.4	4.0	-	2.3	-	2.9	0.9	1.5	-	1.5	0.0	3.8	1.7	0.5	-	1.8	0.0	0.8	2.0	1.5	-	1.8	1.9
Bicycles on Road	0	0	1	1	-	2	0	0	1	0	-	1	0	0	1	0	-	1	0	0	0	1	-	1	5
% Bicycles on Road	-	0.0	0.2	0.1	-	0.1	-	0.0	0.2	0.0	-	0.1	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.2	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	40	-	-	-	-	-	27	-	-	-	-	-	22	-	-	-	-	-	9	-	-
% Bicycles on Crosswalk	-	-	-	-	54.8	-	-	-	-	-	37.5	-	-	-	-	-	37.3	-	-	-	-	-	40.9	-	-
Pedestrians	-	-	-	-	33	-	-	-	-	-	45	-	-	-	-	-	37	-	-	-	-	-	13	-	-
% Pedestrians	-	-	-	-	45.2	-	-	-	-	-	62.5	-	-	-	-	-	62.7	-	-	-	-	-	59.1	-	-



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:00)

Start Time	60th Ave W Eastbound						60th Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	20	8	35	3	63	0	14	37	18	3	69	0	13	202	2	1	217	2	22	384	7	0	415	764
7:15	0	17	18	36	2	71	0	15	34	20	0	69	0	18	260	2	1	280	1	18	431	9	0	459	879
7:30	0	23	33	49	0	105	0	6	20	18	2	44	0	24	247	2	2	273	0	11	339	9	0	359	781
7:45	0	24	14	39	1	77	0	6	17	15	2	38	0	22	265	3	1	290	4	16	375	11	0	406	811
<b>Total</b>	<b>0</b>	<b>84</b>	<b>73</b>	<b>159</b>	<b>6</b>	<b>316</b>	<b>0</b>	<b>41</b>	<b>108</b>	<b>71</b>	<b>7</b>	<b>220</b>	<b>0</b>	<b>77</b>	<b>974</b>	<b>9</b>	<b>5</b>	<b>1060</b>	<b>7</b>	<b>67</b>	<b>1529</b>	<b>36</b>	<b>0</b>	<b>1639</b>	<b>3235</b>
Approach %	0.0	26.6	23.1	50.3	-	-	0.0	18.6	49.1	32.3	-	-	0.0	7.3	91.9	0.8	-	-	0.4	4.1	93.3	2.2	-	-	-
Total %	0.0	2.6	2.3	4.9	-	9.8	0.0	1.3	3.3	2.2	-	6.8	0.0	2.4	30.1	0.3	-	32.8	0.2	2.1	47.3	1.1	-	50.7	-
PHF	0.000	0.875	0.553	0.811	-	0.752	0.000	0.683	0.730	0.888	-	0.797	0.000	0.802	0.919	0.750	-	0.914	0.438	0.761	0.887	0.818	-	0.893	0.920
Lights	0	81	72	155	-	308	0	39	107	69	-	215	0	73	957	9	-	1039	7	67	1491	36	-	1601	3163
% Lights	-	96.4	98.6	97.5	-	97.5	-	95.1	99.1	97.2	-	97.7	-	94.8	98.3	100.0	-	98.0	100.0	100.0	97.5	100.0	-	97.7	97.8
Other Vehicles	0	3	1	4	-	8	0	2	1	2	-	5	0	4	17	0	-	21	0	0	38	0	-	38	72
% Other Vehicles	-	3.6	1.4	2.5	-	2.5	-	4.9	0.9	2.8	-	2.3	-	5.2	1.7	0.0	-	2.0	0.0	0.0	2.5	0.0	-	2.3	2.2
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	3	-	-	-	-	-	0	-	-	-	-	-	3	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	50.0	-	-	-	-	-	0.0	-	-	-	-	-	60.0	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	3	-	-	-	-	-	7	-	-	-	-	-	2	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	50.0	-	-	-	-	-	100.0	-	-	-	-	-	40.0	-	-	-	-	-	-	-	-



Turning Movement Peak Hour Data Plot (7:00)

US 41 at 60th Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 38\_US 41 at 60th  
Ave W  
Site Code: 38  
Start Date: 05/16/2023  
Page No: 5

### Turning Movement Peak Hour Data (12:15)

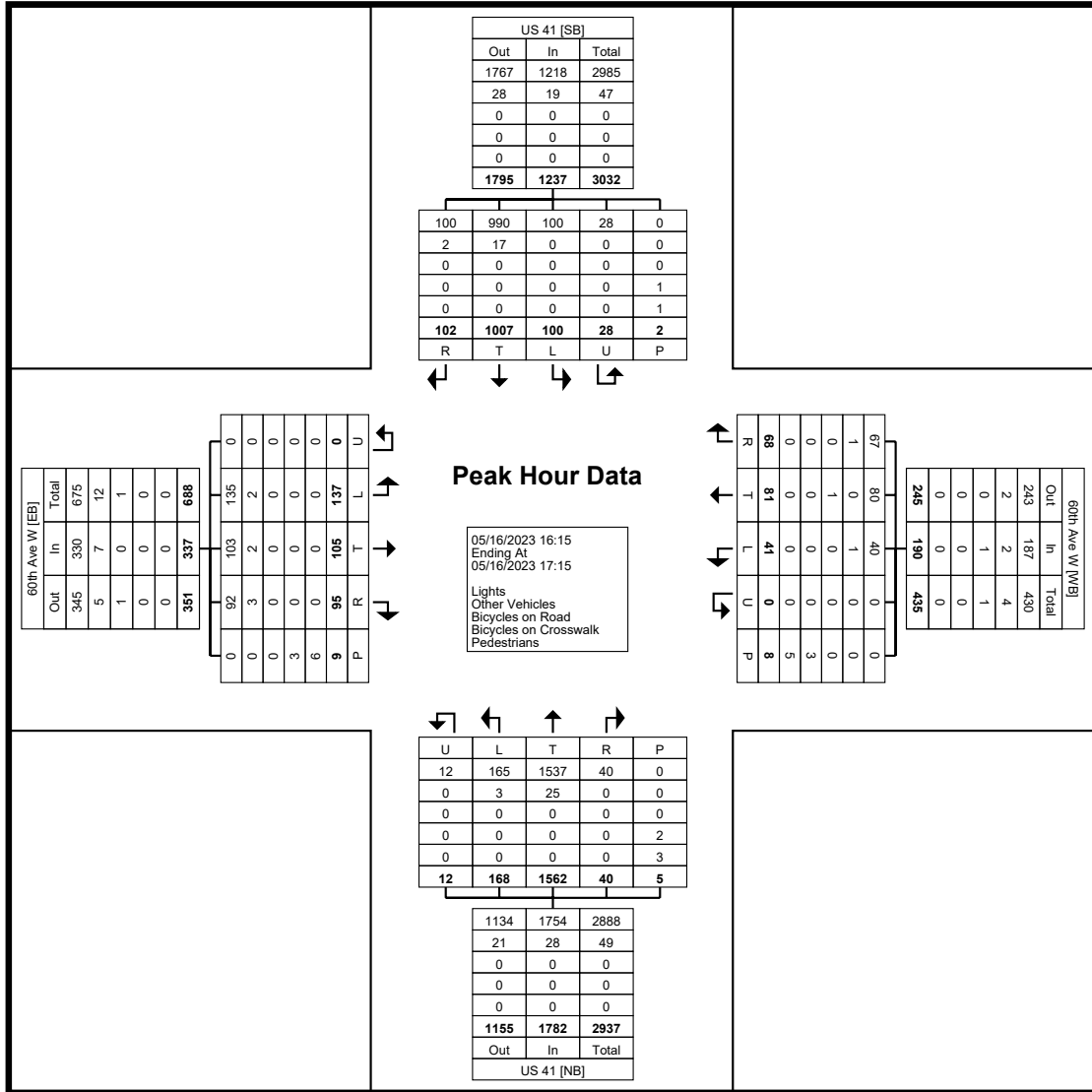
Start Time	60th Ave W Eastbound						60th Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:15	0	41	15	22	1	78	0	7	10	13	4	30	3	23	238	3	0	267	7	13	294	28	1	342	717
12:30	0	50	17	27	2	94	0	4	11	20	3	35	3	22	259	5	4	289	8	17	252	22	2	299	717
12:45	0	41	11	17	2	69	0	13	13	10	1	36	4	21	234	5	3	264	2	16	283	29	0	330	699
13:00	0	41	15	14	3	70	0	12	12	12	5	36	3	13	277	4	0	297	2	20	253	13	0	288	691
<b>Total</b>	<b>0</b>	<b>173</b>	<b>58</b>	<b>80</b>	<b>8</b>	<b>311</b>	<b>0</b>	<b>36</b>	<b>46</b>	<b>55</b>	<b>13</b>	<b>137</b>	<b>13</b>	<b>79</b>	<b>1008</b>	<b>17</b>	<b>7</b>	<b>1117</b>	<b>19</b>	<b>66</b>	<b>1082</b>	<b>92</b>	<b>3</b>	<b>1259</b>	<b>2824</b>
Approach %	0.0	55.6	18.6	25.7	-	-	0.0	26.3	33.6	40.1	-	-	1.2	7.1	90.2	1.5	-	-	1.5	5.2	85.9	7.3	-	-	-
Total %	0.0	6.1	2.1	2.8	-	11.0	0.0	1.3	1.6	1.9	-	4.9	0.5	2.8	35.7	0.6	-	39.6	0.7	2.3	38.3	3.3	-	44.6	-
PHF	0.000	0.865	0.853	0.741	-	0.827	0.000	0.692	0.885	0.688	-	0.951	0.813	0.859	0.910	0.850	-	0.940	0.594	0.825	0.920	0.793	-	0.920	0.985
Lights	0	171	57	74	-	302	0	36	46	55	-	137	13	76	985	17	-	1091	19	65	1062	91	-	1237	2767
% Lights	-	98.8	98.3	92.5	-	97.1	-	100.0	100.0	100.0	-	100.0	100.0	96.2	97.7	100.0	-	97.7	100.0	98.5	98.2	98.9	-	98.3	98.0
Other Vehicles	0	2	1	6	-	9	0	0	0	0	-	0	0	3	23	0	-	26	0	1	20	1	-	22	57
% Other Vehicles	-	1.2	1.7	7.5	-	2.9	-	0.0	0.0	0.0	-	0.0	0.0	3.8	2.3	0.0	-	2.3	0.0	1.5	1.8	1.1	-	1.7	2.0
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	5	-	-	-	-	-	4	-	-	-	-	-	0	-	-	-	-	-	1	-	-
% Bicycles on Crosswalk	-	-	-	-	62.5	-	-	-	-	-	30.8	-	-	-	-	-	0.0	-	-	-	-	-	33.3	-	-
Pedestrians	-	-	-	-	3	-	-	-	-	-	9	-	-	-	-	-	7	-	-	-	-	-	2	-	-
% Pedestrians	-	-	-	-	37.5	-	-	-	-	-	69.2	-	-	-	-	-	100.0	-	-	-	-	-	66.7	-	-





### Turning Movement Peak Hour Data (16:15)

Start Time	60th Ave W Eastbound						60th Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:15	0	26	19	21	0	66	0	10	23	15	2	48	4	45	416	12	0	477	8	23	249	24	0	304	895
16:30	0	33	29	29	1	91	0	12	18	17	1	47	5	43	362	5	0	415	11	21	258	24	2	314	867
16:45	0	38	24	19	6	81	0	11	23	16	5	50	1	38	405	12	3	456	6	27	247	21	0	301	888
17:00	0	40	33	26	2	99	0	8	17	20	0	45	2	42	379	11	2	434	3	29	253	33	0	318	896
<b>Total</b>	0	137	105	95	9	337	0	41	81	68	8	190	12	168	1562	40	5	1782	28	100	1007	102	2	1237	3546
Approach %	0.0	40.7	31.2	28.2	-	-	0.0	21.6	42.6	35.8	-	-	0.7	9.4	87.7	2.2	-	-	2.3	8.1	81.4	8.2	-	-	-
Total %	0.0	3.9	3.0	2.7	-	9.5	0.0	1.2	2.3	1.9	-	5.4	0.3	4.7	44.0	1.1	-	50.3	0.8	2.8	28.4	2.9	-	34.9	-
PHF	0.000	0.856	0.795	0.819	-	0.851	0.000	0.854	0.880	0.850	-	0.950	0.600	0.933	0.939	0.833	-	0.934	0.636	0.862	0.976	0.773	-	0.972	0.989
Lights	0	135	103	92	-	330	0	40	80	67	-	187	12	165	1537	40	-	1754	28	100	990	100	-	1218	3489
% Lights	-	98.5	98.1	96.8	-	97.9	-	97.6	98.8	98.5	-	98.4	100.0	98.2	98.4	100.0	-	98.4	100.0	100.0	98.3	98.0	-	98.5	98.4
Other Vehicles	0	2	2	3	-	7	0	1	0	1	-	2	0	3	25	0	-	28	0	0	17	2	-	19	56
% Other Vehicles	-	1.5	1.9	3.2	-	2.1	-	2.4	0.0	1.5	-	1.1	0.0	1.8	1.6	0.0	-	1.6	0.0	0.0	1.7	2.0	-	1.5	1.6
Bicycles on Road	0	0	0	0	-	0	0	0	1	0	-	1	0	0	0	0	-	0	0	0	0	0	-	0	1
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	1.2	0.0	-	0.5	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	3	-	-	-	-	-	3	-	-	-	-	-	2	-	-	-	-	-	1	-	-
% Bicycles on Crosswalk	-	-	-	-	33.3	-	-	-	-	-	37.5	-	-	-	-	-	40.0	-	-	-	-	-	50.0	-	-
Pedestrians	-	-	-	-	6	-	-	-	-	-	5	-	-	-	-	-	3	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	66.7	-	-	-	-	-	62.5	-	-	-	-	-	60.0	-	-	-	-	-	50.0	-	-



Turning Movement Peak Hour Data Plot (16:15)

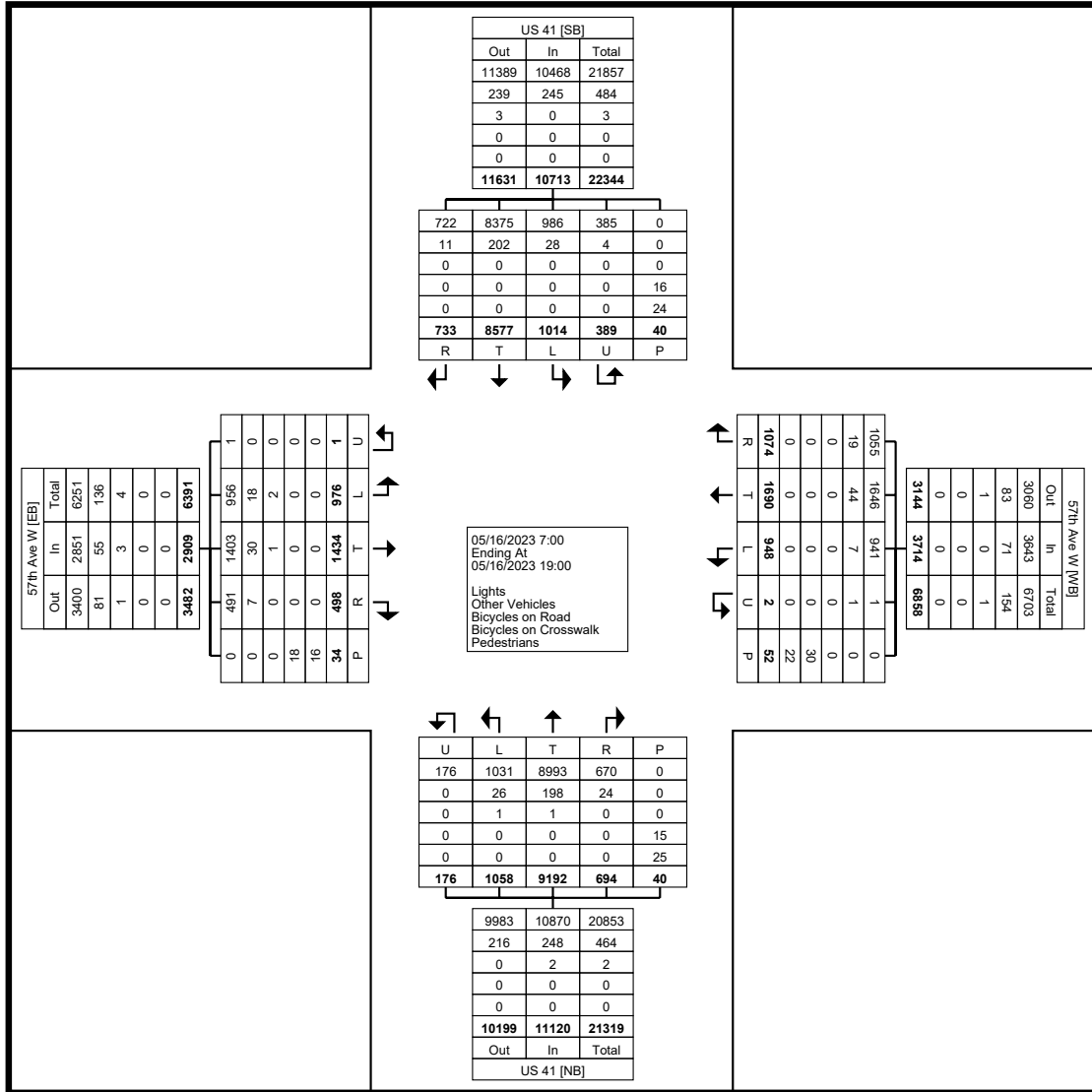
US 41 at 57th Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 37\_US 41 at 57th  
Ave W  
Site Code: 37  
Start Date: 05/16/2023  
Page No: 1

### Turning Movement Data

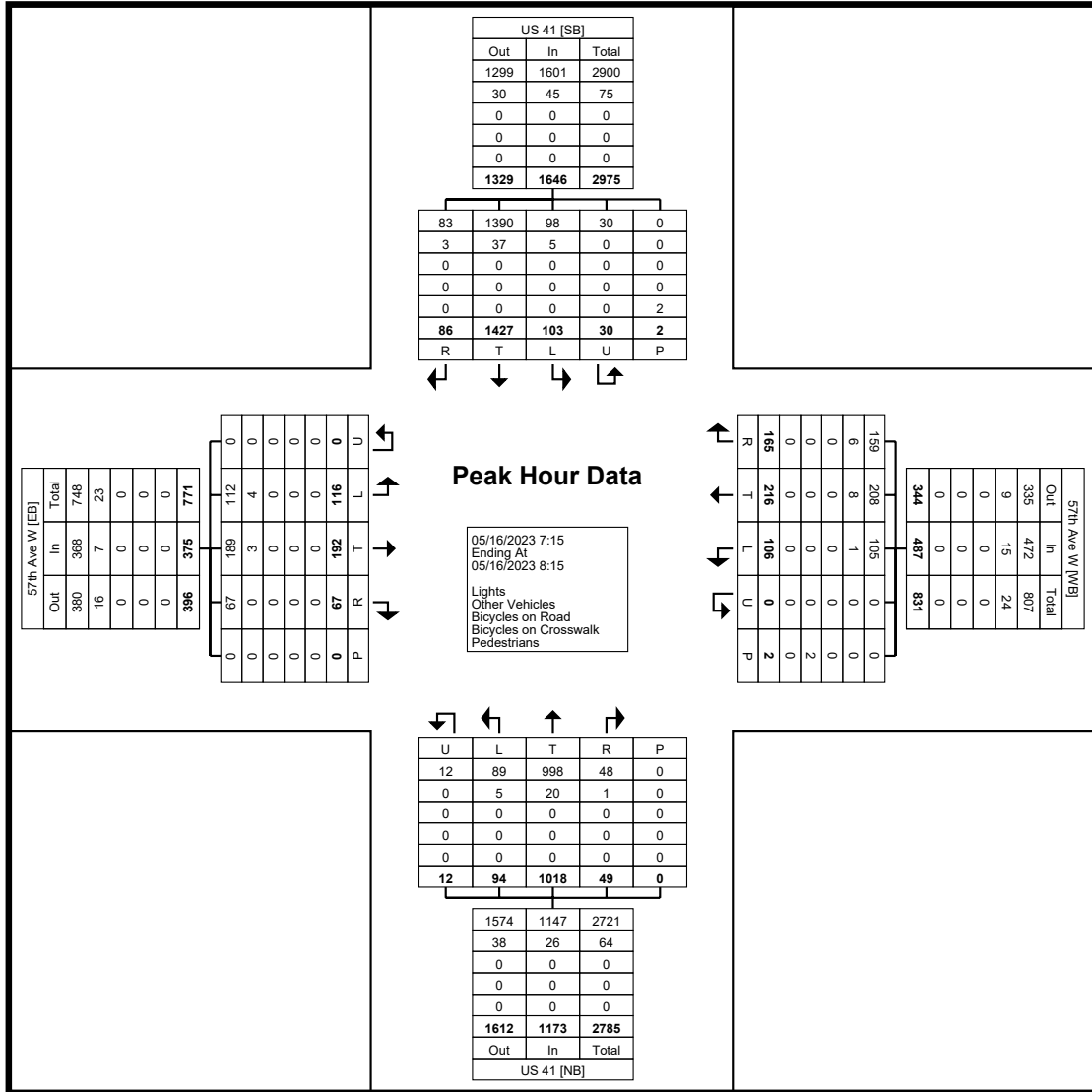
Start Time	57th Ave W Eastbound						57th Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	26	41	26	0	93	0	21	70	12	1	103	3	18	221	10	4	252	12	22	335	21	3	390	838
7:15	0	26	55	16	0	97	0	22	48	36	0	106	3	33	226	10	0	272	9	22	405	33	0	469	944
7:30	0	40	49	17	0	106	0	19	60	34	0	113	3	16	301	11	0	331	5	26	341	20	2	392	942
7:45	0	28	46	14	0	88	0	29	63	40	2	132	5	26	246	10	0	287	6	37	314	19	0	376	883
Hourly Total	0	120	191	73	0	384	0	91	241	122	3	454	14	93	994	41	4	1142	32	107	1395	93	5	1627	3607
8:00	0	22	42	20	0	84	0	36	45	55	0	136	1	19	245	18	0	283	10	18	367	14	0	409	912
8:15	0	26	30	22	0	78	0	26	51	24	0	101	3	22	250	12	1	287	8	21	362	17	1	408	874
8:30	0	26	31	14	0	71	0	28	45	39	1	112	6	19	197	9	1	231	8	30	341	24	1	403	817
8:45	0	22	43	15	0	80	0	28	43	40	4	111	3	30	225	13	0	271	4	24	346	24	1	398	860
Hourly Total	0	96	146	71	0	313	0	118	184	158	5	460	13	90	917	52	2	1072	30	93	1416	79	3	1618	3463
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	41	28	14	0	83	0	35	27	28	1	90	8	19	271	20	1	318	26	31	231	17	4	305	796
12:15	0	38	39	18	0	95	0	31	41	33	2	105	5	36	291	20	2	352	15	35	244	29	0	323	875
12:30	0	30	33	16	0	79	0	31	33	29	1	93	3	23	244	16	0	286	29	36	301	18	3	384	842
12:45	0	31	45	21	0	97	0	36	44	27	2	107	8	22	272	24	1	326	13	36	220	24	1	293	823
Hourly Total	0	140	145	69	0	354	0	133	145	117	6	395	24	100	1078	80	4	1282	83	138	996	88	8	1305	3336
13:00	0	32	30	16	3	78	0	30	32	22	1	84	4	17	243	22	1	286	17	37	257	27	0	338	786
13:15	0	24	51	15	0	90	0	31	35	26	0	92	6	23	274	15	1	318	14	32	222	23	2	291	791
13:30	1	25	31	17	2	74	0	28	32	27	1	87	5	21	258	18	3	302	21	26	255	19	0	321	784
13:45	0	36	33	19	0	88	0	19	39	28	1	86	6	32	270	13	0	321	13	30	230	16	1	289	784
Hourly Total	1	117	145	67	5	330	0	108	138	103	3	349	21	93	1045	68	5	1227	65	125	964	85	3	1239	3145
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	30	56	8	0	94	0	36	50	25	1	111	6	40	334	23	2	403	5	28	260	16	2	309	917
15:15	0	30	50	15	0	95	1	32	46	30	4	109	9	33	337	28	1	407	17	23	248	16	1	304	915
15:30	0	37	46	8	0	91	0	27	49	33	2	109	8	39	302	30	2	379	14	33	243	21	1	311	890
15:45	0	34	57	10	0	101	0	38	77	32	5	147	8	34	372	32	0	446	8	28	277	25	1	338	1032
Hourly Total	0	131	209	41	0	381	1	133	222	120	12	476	31	146	1345	113	5	1635	44	112	1028	78	5	1262	3754
16:00	0	36	45	20	3	101	1	27	77	46	0	151	7	49	347	27	1	430	15	41	232	22	1	310	992
16:15	0	33	54	16	2	103	0	44	69	36	1	149	8	51	327	28	1	414	8	32	232	45	1	317	983
16:30	0	35	47	14	1	96	0	30	56	38	1	124	3	48	389	38	1	478	14	34	279	34	0	361	1059
16:45	0	41	63	23	1	127	0	32	65	43	3	140	1	59	317	29	0	406	14	41	223	16	2	294	967
Hourly Total	0	145	209	73	7	427	1	133	267	163	5	564	19	207	1380	122	3	1728	51	148	966	117	4	1282	4001
17:00	0	26	57	17	2	100	0	45	64	36	0	145	8	44	354	41	2	447	11	25	277	26	1	339	1031
17:15	0	36	58	16	1	110	0	24	82	35	2	141	5	40	363	35	3	443	13	35	241	23	1	312	1006
17:30	0	19	57	12	3	88	0	28	73	44	3	145	6	51	294	25	3	376	15	37	205	33	0	290	899
17:45	0	28	54	6	4	88	0	27	72	34	4	133	9	45	385	30	3	469	5	35	278	26	2	344	1034
Hourly Total	0	109	226	51	10	386	0	124	291	149	9	564	28	180	1396	131	11	1735	44	132	1001	108	4	1285	3970
18:00	0	26	44	13	3	83	0	26	70	44	2	140	10	42	323	17	1	392	10	36	218	29	3	293	908
18:15	0	39	36	14	6	89	0	26	40	26	4	92	5	29	254	29	2	317	8	41	214	16	1	279	777
18:30	0	26	35	12	0	73	0	33	54	39	1	126	6	40	244	23	3	313	9	40	187	23	2	259	771
18:45	0	27	48	14	3	89	0	23	38	33	2	94	5	38	216	18	0	277	13	42	192	17	2	264	724
Hourly Total	0	118	163	53	12	334	0	108	202	142	9	452	26	149	1037	87	6	1299	40	159	811	85	8	1095	3180
Grand Total	1	976	1434	498	34	2909	2	948	1690	1074	52	3714	176	1058	9192	694	40	11120	389	1014	8577	733	40	10713	28456
Approach %	0.0	33.6	49.3	17.1	-	-	0.1	25.5	45.5	28.9	-	-	1.6	9.5	82.7	6.2	-	-	3.6	9.5	80.1	6.8	-	-	-
Total %	0.0	3.4	5.0	1.8	-	10.2	0.0	3.3	5.9	3.8	-	13.1	0.6	3.7	32.3	2.4	-	39.1	1.4	3.6	30.1	2.6	-	37.6	-
Lights	1	956	1403	491	-	2851	1	941	1646	1055	-	3643	176	1031	8993	670	-	10870	385	986	8375	722	-	10468	27832
% Lights	100.0	98.0	97.8	98.6	-	98.0	50.0	99.3	97.4	98.2	-	98.1	100.0	97.4	97.8	96.5	-	97.8	99.0	97.2	97.6	98.5	-	97.7	97.8
Other Vehicles	0	18	30	7	-	55	1	7	44	19	-	71	0	26	198	24	-	248	4	28	202	11	-	245	619
% Other Vehicles	0.0	1.8	2.1	1.4	-	1.9	50.0	0.7	2.6	1.8	-	1.9	0.0	2.5	2.2	3.5	-	2.2	1.0	2.8	2.4	1.5	-	2.3	2.2
Bicycles on Road	0	2	1	0	-	3	0	0	0	0	-	0	0	1	1	0	-	2	0	0	0	0	-	0	5
% Bicycles on Road	0.0	0.2	0.1	0.0	-	0.1	0.0	0.0	0.0	0.0	-	0.0	0.0	0.1	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	18	-	-	-	-	-	30	-	-	-	-	-	15	-	-	-	-	-	16	-	-
% Bicycles on Crosswalk	-	-	-	-	52.9	-	-	-	-	-	57.7	-	-	-	-	-	37.5	-	-	-	-	-	40.0	-	-
Pedestrians	-	-	-	-	16	-	-	-	-	-	22	-	-	-	-	-	25	-	-	-	-	-	24	-	-
% Pedestrians	-	-	-	-	47.1	-	-	-	-	-	42.3	-	-	-	-	-	62.5	-	-	-	-	-	60.0	-	-



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:15)

Start Time	57th Ave W Eastbound						57th Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15	0	26	55	16	0	97	0	22	48	36	0	106	3	33	226	10	0	272	9	22	405	33	0	469	944
7:30	0	40	49	17	0	106	0	19	60	34	0	113	3	16	301	11	0	331	5	26	341	20	2	392	942
7:45	0	28	46	14	0	88	0	29	63	40	2	132	5	26	246	10	0	287	6	37	314	19	0	376	883
8:00	0	22	42	20	0	84	0	36	45	55	0	136	1	19	245	18	0	283	10	18	367	14	0	409	912
<b>Total</b>	0	116	192	67	0	375	0	106	216	165	2	487	12	94	1018	49	0	1173	30	103	1427	86	2	1646	3681
Approach %	0.0	30.9	51.2	17.9	-	-	0.0	21.8	44.4	33.9	-	-	1.0	8.0	86.8	4.2	-	-	1.8	6.3	86.7	5.2	-	-	-
Total %	0.0	3.2	5.2	1.8	-	10.2	0.0	2.9	5.9	4.5	-	13.2	0.3	2.6	27.7	1.3	-	31.9	0.8	2.8	38.8	2.3	-	44.7	-
PHF	0.000	0.725	0.873	0.838	-	0.884	0.000	0.736	0.857	0.750	-	0.895	0.600	0.712	0.846	0.681	-	0.886	0.750	0.696	0.881	0.652	-	0.877	0.975
Lights	0	112	189	67	-	368	0	105	208	159	-	472	12	89	998	48	-	1147	30	98	1390	83	-	1601	3588
% Lights	-	96.6	98.4	100.0	-	98.1	-	99.1	96.3	96.4	-	96.9	100.0	94.7	98.0	98.0	-	97.8	100.0	95.1	97.4	96.5	-	97.3	97.5
Other Vehicles	0	4	3	0	-	7	0	1	8	6	-	15	0	5	20	1	-	26	0	5	37	3	-	45	93
% Other Vehicles	-	3.4	1.6	0.0	-	1.9	-	0.9	3.7	3.6	-	3.1	0.0	5.3	2.0	2.0	-	2.2	0.0	4.9	2.6	3.5	-	2.7	2.5
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	2	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



Turning Movement Peak Hour Data Plot (7:15)

US 41 at 57th Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 37\_US 41 at 57th  
Ave W  
Site Code: 37  
Start Date: 05/16/2023  
Page No: 5

### Turning Movement Peak Hour Data (12:00)

Start Time	57th Ave W Eastbound						57th Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	41	28	14	0	83	0	35	27	28	1	90	8	19	271	20	1	318	26	31	231	17	4	305	796
12:15	0	38	39	18	0	95	0	31	41	33	2	105	5	36	291	20	2	352	15	35	244	29	0	323	875
12:30	0	30	33	16	0	79	0	31	33	29	1	93	3	23	244	16	0	286	29	36	301	18	3	384	842
12:45	0	31	45	21	0	97	0	36	44	27	2	107	8	22	272	24	1	326	13	36	220	24	1	293	823
<b>Total</b>	<b>0</b>	<b>140</b>	<b>145</b>	<b>69</b>	<b>0</b>	<b>354</b>	<b>0</b>	<b>133</b>	<b>145</b>	<b>117</b>	<b>6</b>	<b>395</b>	<b>24</b>	<b>100</b>	<b>1078</b>	<b>80</b>	<b>4</b>	<b>1282</b>	<b>83</b>	<b>138</b>	<b>996</b>	<b>88</b>	<b>8</b>	<b>1305</b>	<b>3336</b>
Approach %	0.0	39.5	41.0	19.5	-	-	0.0	33.7	36.7	29.6	-	-	1.9	7.8	84.1	6.2	-	-	6.4	10.6	76.3	6.7	-	-	-
Total %	0.0	4.2	4.3	2.1	-	10.6	0.0	4.0	4.3	3.5	-	11.8	0.7	3.0	32.3	2.4	-	38.4	2.5	4.1	29.9	2.6	-	39.1	-
PHF	0.000	0.854	0.806	0.821	-	0.912	0.000	0.924	0.824	0.886	-	0.923	0.750	0.694	0.926	0.833	-	0.911	0.716	0.958	0.827	0.759	-	0.850	0.953
Lights	0	137	143	68	-	348	0	131	141	116	-	388	24	97	1060	80	-	1261	83	132	984	86	-	1285	3282
% Lights	-	97.9	98.6	98.6	-	98.3	-	98.5	97.2	99.1	-	98.2	100.0	97.0	98.3	100.0	-	98.4	100.0	95.7	98.8	97.7	-	98.5	98.4
Other Vehicles	0	3	2	1	-	6	0	2	4	1	-	7	0	3	18	0	-	21	0	6	12	2	-	20	54
% Other Vehicles	-	2.1	1.4	1.4	-	1.7	-	1.5	2.8	0.9	-	1.8	0.0	3.0	1.7	0.0	-	1.6	0.0	4.3	1.2	2.3	-	1.5	1.6
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	5	-	-	-	-	-	0	-	-	-	-	-	5	-	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	83.3	-	-	-	-	-	0.0	-	-	-	-	-	62.5	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	4	-	-	-	-	-	3	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	16.7	-	-	-	-	-	100.0	-	-	-	-	-	37.5	-	-





US 41 at 57th Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 37\_US 41 at 57th  
Ave W  
Site Code: 37  
Start Date: 05/16/2023  
Page No: 7

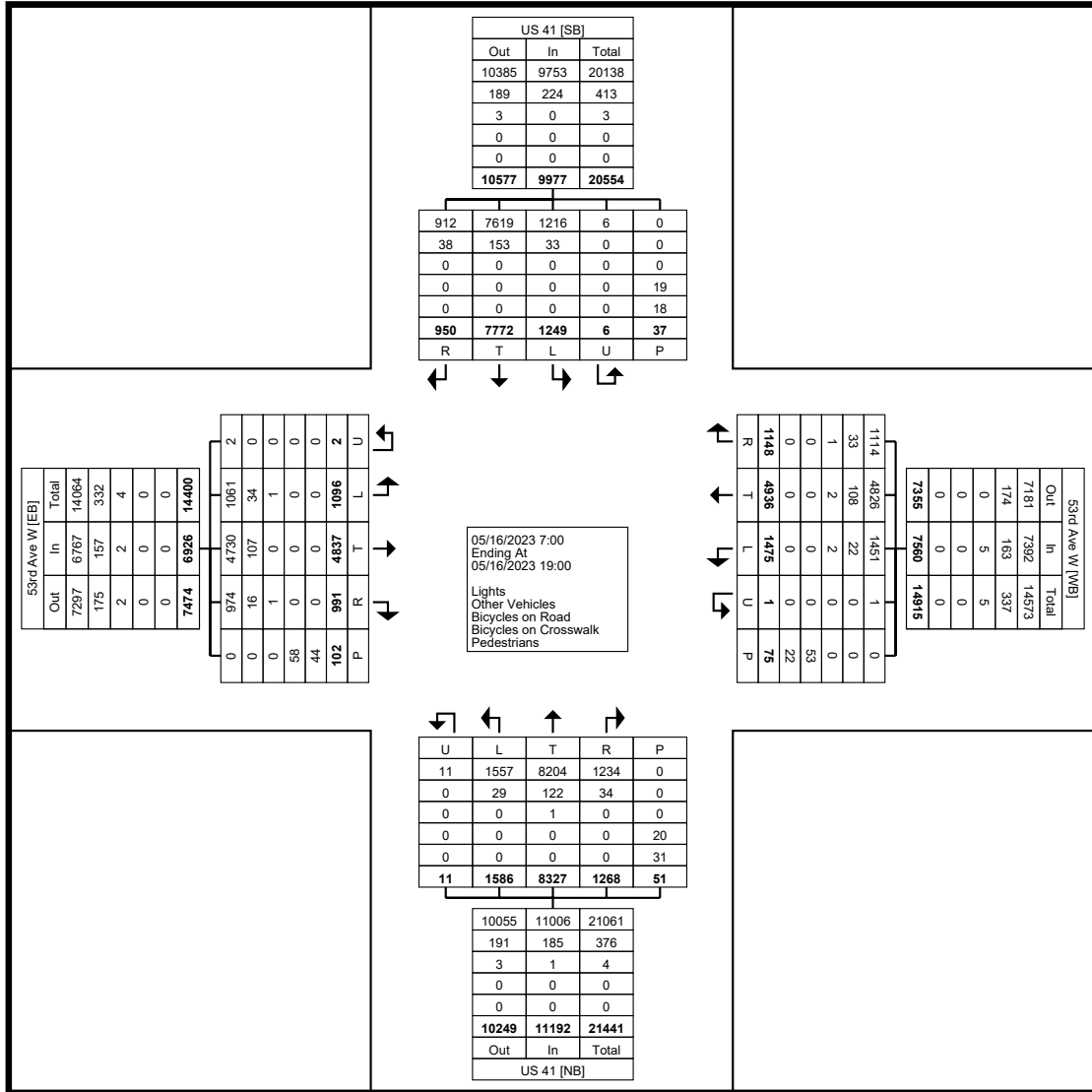
### Turning Movement Peak Hour Data (15:45)

Start Time	57th Ave W Eastbound						57th Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
15:45	0	34	57	10	0	101	0	38	77	32	5	147	8	34	372	32	0	446	8	28	277	25	1	338	1032
16:00	0	36	45	20	3	101	1	27	77	46	0	151	7	49	347	27	1	430	15	41	232	22	1	310	992
16:15	0	33	54	16	2	103	0	44	69	36	1	149	8	51	327	28	1	414	8	32	232	45	1	317	983
16:30	0	35	47	14	1	96	0	30	56	38	1	124	3	48	389	38	1	478	14	34	279	34	0	361	1059
<b>Total</b>	<b>0</b>	<b>138</b>	<b>203</b>	<b>60</b>	<b>6</b>	<b>401</b>	<b>1</b>	<b>139</b>	<b>279</b>	<b>152</b>	<b>7</b>	<b>571</b>	<b>26</b>	<b>182</b>	<b>1435</b>	<b>125</b>	<b>3</b>	<b>1768</b>	<b>45</b>	<b>135</b>	<b>1020</b>	<b>126</b>	<b>3</b>	<b>1326</b>	<b>4066</b>
Approach %	0.0	34.4	50.6	15.0	-	-	0.2	24.3	48.9	26.6	-	-	1.5	10.3	81.2	7.1	-	-	3.4	10.2	76.9	9.5	-	-	-
Total %	0.0	3.4	5.0	1.5	-	9.9	0.0	3.4	6.9	3.7	-	14.0	0.6	4.5	35.3	3.1	-	43.5	1.1	3.3	25.1	3.1	-	32.6	-
PHF	0.000	0.958	0.890	0.750	-	0.973	0.250	0.790	0.906	0.826	-	0.945	0.813	0.892	0.922	0.822	-	0.925	0.750	0.823	0.914	0.700	-	0.918	0.960
Lights	0	135	196	59	-	390	1	138	272	152	-	563	26	180	1389	119	-	1714	45	134	1002	126	-	1307	3974
% Lights	-	97.8	96.6	98.3	-	97.3	100.0	99.3	97.5	100.0	-	98.6	100.0	98.9	96.8	95.2	-	96.9	100.0	99.3	98.2	100.0	-	98.6	97.7
Other Vehicles	0	2	7	1	-	10	0	1	7	0	-	8	0	1	46	6	-	53	0	1	18	0	-	19	90
% Other Vehicles	-	1.4	3.4	1.7	-	2.5	0.0	0.7	2.5	0.0	-	1.4	0.0	0.5	3.2	4.8	-	3.0	0.0	0.7	1.8	0.0	-	1.4	2.2
Bicycles on Road	0	1	0	0	-	1	0	0	0	0	-	0	0	1	0	0	-	1	0	0	0	0	-	0	2
% Bicycles on Road	-	0.7	0.0	0.0	-	0.2	0.0	0.0	0.0	0.0	-	0.0	0.0	0.5	0.0	0.0	-	0.1	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	3	-	-	-	-	-	5	-	-	-	-	-	0	-	-	-	-	-	1	-	-
% Bicycles on Crosswalk	-	-	-	-	50.0	-	-	-	-	-	71.4	-	-	-	-	-	0.0	-	-	-	-	-	33.3	-	-
Pedestrians	-	-	-	-	3	-	-	-	-	-	2	-	-	-	-	-	3	-	-	-	-	-	2	-	-
% Pedestrians	-	-	-	-	50.0	-	-	-	-	-	28.6	-	-	-	-	-	100.0	-	-	-	-	-	66.7	-	-



### Turning Movement Data

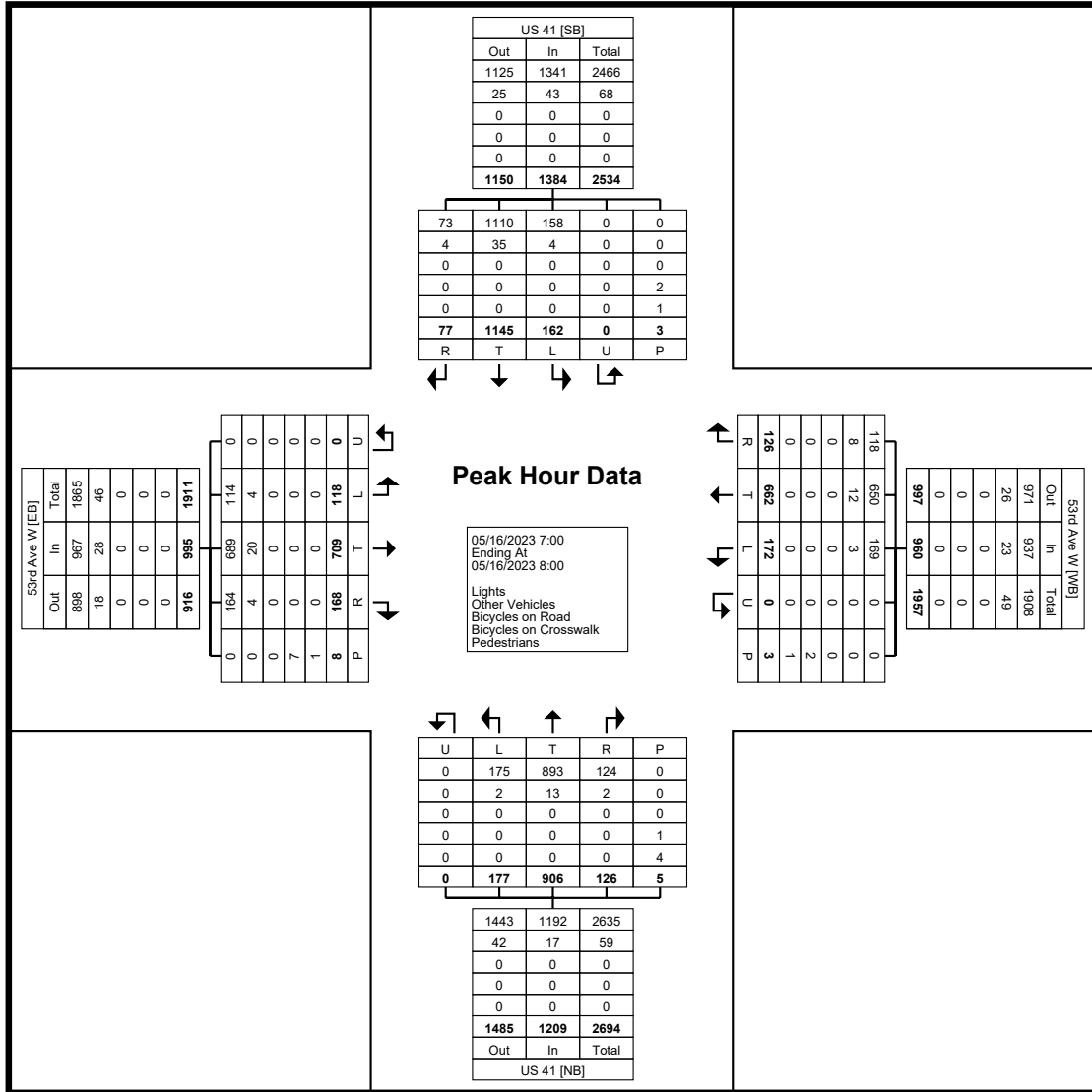
Start Time	53rd Ave W Eastbound						53rd Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	31	170	41	2	242	0	37	120	32	0	189	0	42	196	23	2	261	0	47	311	23	1	381	1073
7:15	0	27	184	46	4	257	0	48	160	29	1	237	0	38	191	23	2	252	0	46	290	19	0	355	1101
7:30	0	37	169	37	1	243	0	51	190	34	0	275	0	47	276	43	0	366	0	30	270	19	1	319	1203
7:45	0	23	186	44	1	253	0	36	192	31	2	259	0	50	243	37	1	330	0	39	274	16	1	329	1171
Hourly Total	0	118	709	168	8	995	0	172	662	126	3	960	0	177	906	126	5	1209	0	162	1145	77	3	1384	4548
8:00	0	31	147	63	1	241	0	47	156	37	2	240	0	30	197	25	1	252	0	25	256	20	0	301	1034
8:15	0	34	127	37	2	198	0	53	151	30	1	234	1	46	233	24	0	304	0	32	323	14	0	369	1105
8:30	0	27	120	36	0	183	1	52	192	37	0	282	0	45	200	29	0	274	0	39	286	16	0	341	1080
8:45	0	43	121	35	1	199	0	53	168	37	1	258	1	48	169	25	0	243	0	42	233	24	0	299	999
Hourly Total	0	135	515	171	4	821	1	205	667	141	4	1014	2	169	799	103	1	1073	0	138	1098	74	0	1310	4218
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	33	109	31	5	173	0	49	119	49	5	217	0	42	258	44	0	344	0	56	245	25	0	326	1060
12:15	0	49	131	29	3	209	0	49	110	53	2	212	0	56	253	47	1	356	0	36	254	24	1	314	1091
12:30	0	30	135	35	2	200	0	53	143	37	2	233	0	37	267	46	2	350	2	36	258	28	0	324	1107
12:45	0	26	93	20	0	139	0	47	128	34	0	209	0	49	254	33	2	336	0	51	220	39	0	310	994
Hourly Total	0	138	468	115	10	721	0	198	500	173	9	871	0	184	1032	170	5	1386	2	179	977	116	1	1274	4252
13:00	0	31	141	20	6	192	0	52	149	44	1	245	0	46	214	38	3	298	0	44	238	30	0	312	1047
13:15	0	42	115	24	2	181	0	40	135	35	1	210	0	53	260	49	1	362	0	51	250	27	1	328	1081
13:30	0	34	158	25	3	217	0	58	153	38	2	249	0	38	204	48	3	290	0	45	216	22	1	283	1039
13:45	0	37	144	28	7	209	0	30	139	34	2	203	1	51	271	37	0	360	0	49	222	38	2	309	1081
Hourly Total	0	144	558	97	18	799	0	180	576	151	6	907	1	188	949	172	7	1310	0	189	926	117	4	1232	4248
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	38	174	26	2	238	0	39	159	36	1	234	0	40	225	27	0	292	0	40	211	19	0	270	1034
15:15	0	23	176	30	5	229	0	47	140	30	3	217	0	53	267	35	2	355	0	36	246	49	3	331	1132
15:30	1	27	192	21	2	241	0	44	171	31	2	246	1	59	280	38	2	378	0	46	209	27	1	282	1147
15:45	1	35	166	31	1	233	0	48	147	38	7	233	0	49	241	41	10	331	0	37	220	32	1	289	1086
Hourly Total	2	123	708	108	10	941	0	178	617	135	13	930	1	201	1013	141	14	1356	0	159	886	127	5	1172	4399
16:00	0	25	159	35	7	219	0	46	156	36	4	238	1	66	342	48	3	457	0	35	284	30	5	349	1263
16:15	0	31	191	26	3	248	0	51	181	39	5	271	2	56	304	52	2	414	0	41	202	42	3	285	1218
16:30	0	47	160	36	1	243	0	41	182	32	2	255	2	47	324	55	2	428	0	33	264	36	0	333	1259
16:45	0	32	205	29	3	266	0	44	150	32	5	226	0	68	314	55	0	437	0	47	228	35	2	310	1239
Hourly Total	0	135	715	126	14	976	0	182	669	139	16	990	5	237	1284	210	7	1736	0	156	978	143	10	1277	4979
17:00	0	38	208	26	3	272	0	49	177	40	3	266	0	51	277	52	2	380	0	38	236	30	1	304	1222
17:15	0	38	148	11	2	197	0	49	150	29	3	228	2	63	323	49	0	437	0	39	255	35	0	329	1191
17:30	0	38	208	25	1	271	0	25	184	35	5	244	0	72	312	50	1	434	0	43	225	38	1	306	1255
17:45	0	36	146	23	6	205	0	58	198	27	3	283	0	56	326	38	2	420	0	30	254	33	6	317	1225
Hourly Total	0	150	710	85	12	945	0	181	709	131	14	1021	2	242	1238	189	5	1671	0	150	970	136	8	1256	4893
18:00	0	43	139	30	10	212	0	37	155	36	3	228	0	62	304	55	1	421	0	31	224	49	2	304	1165
18:15	0	40	119	32	3	191	0	47	150	41	3	238	0	58	296	32	1	386	0	34	173	37	2	244	1059
18:30	0	37	96	32	6	165	0	48	117	35	2	200	0	31	241	37	1	309	2	26	197	31	1	256	930
18:45	0	33	100	27	7	160	0	47	114	40	2	201	0	37	265	33	4	335	2	25	198	43	1	268	964
Hourly Total	0	153	454	121	26	728	0	179	536	152	10	867	0	188	1106	157	7	1451	4	116	792	160	6	1072	4118
Grand Total	2	1096	4837	991	102	6926	1	1475	4936	1148	75	7560	11	1586	8327	1268	51	11192	6	1249	7772	950	37	9977	35655
Approach %	0.0	15.8	69.8	14.3	-	-	0.0	19.5	65.3	15.2	-	-	0.1	14.2	74.4	11.3	-	-	0.1	12.5	77.9	9.5	-	-	-
Total %	0.0	3.1	13.6	2.8	-	19.4	0.0	4.1	13.8	3.2	-	21.2	0.0	4.4	23.4	3.6	-	31.4	0.0	3.5	21.8	2.7	-	28.0	-
Lights	2	1061	4730	974	-	6767	1	1451	4826	1114	-	7392	11	1557	8204	1234	-	11006	6	1216	7619	912	-	9753	34918
% Lights	100.0	96.8	97.8	98.3	-	97.7	100.0	98.4	97.8	97.0	-	97.8	100.0	98.2	98.5	97.3	-	98.3	100.0	97.4	98.0	96.0	-	97.8	97.9
Other Vehicles	0	34	107	16	-	157	0	22	108	33	-	163	0	29	122	34	-	185	0	33	153	38	-	224	729
% Other Vehicles	0.0	3.1	2.2	1.6	-	2.3	0.0	1.5	2.2	2.9	-	2.2	0.0	1.8	1.5	2.7	-	1.7	0.0	2.6	2.0	4.0	-	2.2	2.0
Bicycles on Road	0	1	0	1	-	2	0	2	2	1	-	5	0	0	1	0	-	1	0	0	0	0	-	0	8
% Bicycles on Road	0.0	0.1	0.0	0.1	-	0.0	0.0	0.1	0.0	0.1	-	0.1	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	58	-	-	-	-	-	53	-	-	-	-	-	20	-	-	-	-	-	19	-	-
% Bicycles on Crosswalk	-	-	-	-	56.9	-	-	-	-	-	70.7	-	-	-	-	-	39.2	-	-	-	-	-	51.4	-	-
Pedestrians	-	-	-	-	44	-	-	-	-	-	22	-	-	-	-	-	31	-	-	-	-	-	18	-	-
% Pedestrians	-	-	-	-	43.1	-	-	-	-	-	29.3	-	-	-	-	-	60.8	-	-	-	-	-	48.6	-	-



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:00)

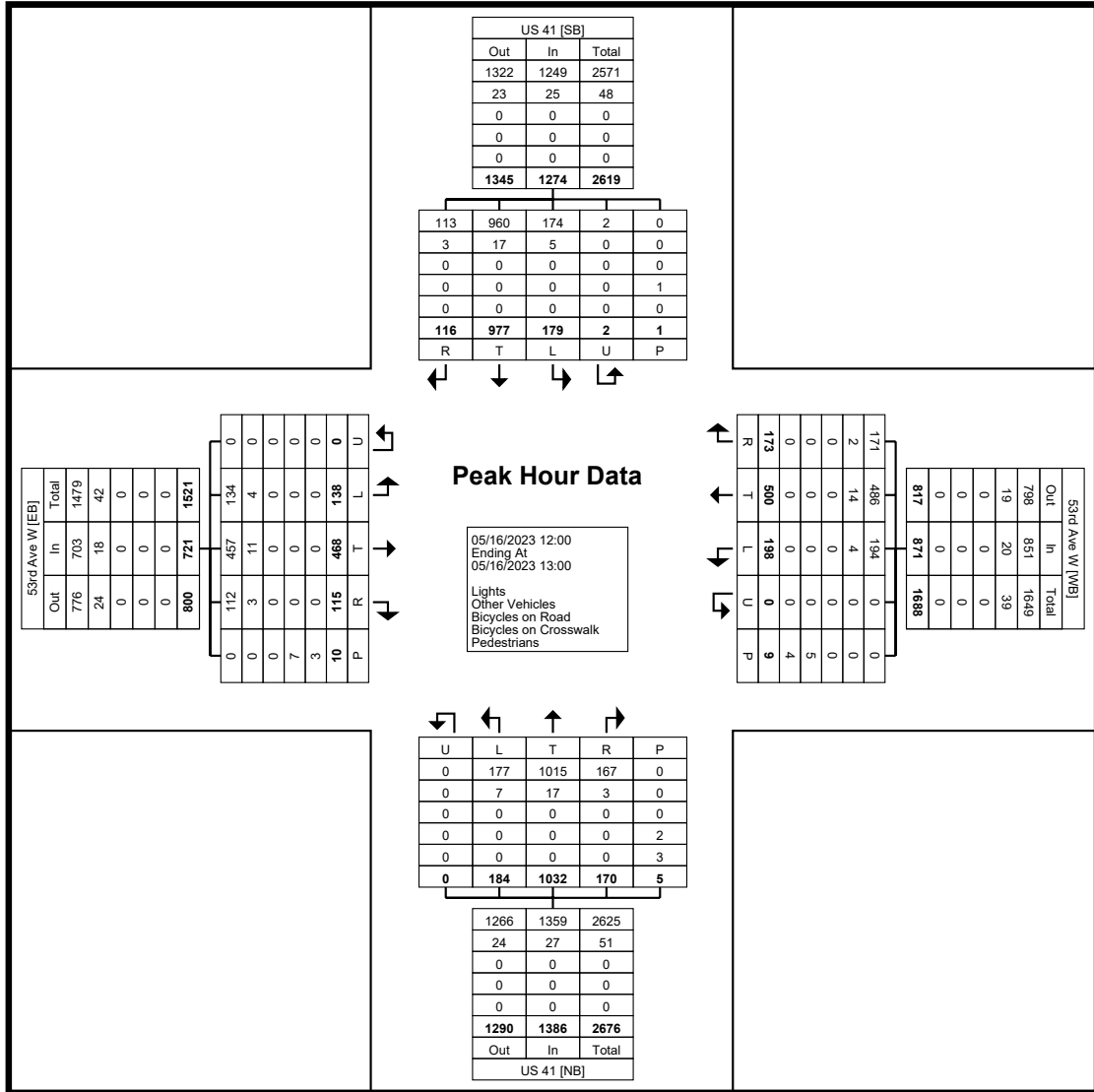
Start Time	53rd Ave W Eastbound						53rd Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	31	170	41	2	242	0	37	120	32	0	189	0	42	196	23	2	261	0	47	311	23	1	381	1073
7:15	0	27	184	46	4	257	0	48	160	29	1	237	0	38	191	23	2	252	0	46	290	19	0	355	1101
7:30	0	37	169	37	1	243	0	51	190	34	0	275	0	47	276	43	0	366	0	30	270	19	1	319	1203
7:45	0	23	186	44	1	253	0	36	192	31	2	259	0	50	243	37	1	330	0	39	274	16	1	329	1171
<b>Total</b>	<b>0</b>	<b>118</b>	<b>709</b>	<b>168</b>	<b>8</b>	<b>995</b>	<b>0</b>	<b>172</b>	<b>662</b>	<b>126</b>	<b>3</b>	<b>960</b>	<b>0</b>	<b>177</b>	<b>906</b>	<b>126</b>	<b>5</b>	<b>1209</b>	<b>0</b>	<b>162</b>	<b>1145</b>	<b>77</b>	<b>3</b>	<b>1384</b>	<b>4548</b>
Approach %	0.0	11.9	71.3	16.9	-	-	0.0	17.9	69.0	13.1	-	-	0.0	14.6	74.9	10.4	-	-	0.0	11.7	82.7	5.6	-	-	-
Total %	0.0	2.6	15.6	3.7	-	21.9	0.0	3.8	14.6	2.8	-	21.1	0.0	3.9	19.9	2.8	-	26.6	0.0	3.6	25.2	1.7	-	30.4	-
PHF	0.000	0.797	0.953	0.913	-	0.968	0.000	0.843	0.862	0.926	-	0.873	0.000	0.885	0.821	0.733	-	0.826	0.000	0.862	0.920	0.837	-	0.908	0.945
Lights	0	114	689	164	-	967	0	169	650	118	-	937	0	175	893	124	-	1192	0	158	1110	73	-	1341	4437
% Lights	-	96.6	97.2	97.6	-	97.2	-	98.3	98.2	93.7	-	97.6	-	98.9	98.6	98.4	-	98.6	-	97.5	96.9	94.8	-	96.9	97.6
Other Vehicles	0	4	20	4	-	28	0	3	12	8	-	23	0	2	13	2	-	17	0	4	35	4	-	43	111
% Other Vehicles	-	3.4	2.8	2.4	-	2.8	-	1.7	1.8	6.3	-	2.4	-	1.1	1.4	1.6	-	1.4	-	2.5	3.1	5.2	-	3.1	2.4
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	7	-	-	-	-	-	2	-	-	-	-	-	1	-	-	-	-	-	2	-	-
% Bicycles on Crosswalk	-	-	-	-	87.5	-	-	-	-	-	66.7	-	-	-	-	-	20.0	-	-	-	-	-	66.7	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	1	-	-	-	-	-	4	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	12.5	-	-	-	-	-	33.3	-	-	-	-	-	80.0	-	-	-	-	-	33.3	-	-



Turning Movement Peak Hour Data Plot (7:00)

### Turning Movement Peak Hour Data (12:00)

Start Time	53rd Ave W Eastbound						53rd Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	33	109	31	5	173	0	49	119	49	5	217	0	42	258	44	0	344	0	56	245	25	0	326	1060
12:15	0	49	131	29	3	209	0	49	110	53	2	212	0	56	253	47	1	356	0	36	254	24	1	314	1091
12:30	0	30	135	35	2	200	0	53	143	37	2	233	0	37	267	46	2	350	2	36	258	28	0	324	1107
12:45	0	26	93	20	0	139	0	47	128	34	0	209	0	49	254	33	2	336	0	51	220	39	0	310	994
<b>Total</b>	<b>0</b>	<b>138</b>	<b>468</b>	<b>115</b>	<b>10</b>	<b>721</b>	<b>0</b>	<b>198</b>	<b>500</b>	<b>173</b>	<b>9</b>	<b>871</b>	<b>0</b>	<b>184</b>	<b>1032</b>	<b>170</b>	<b>5</b>	<b>1386</b>	<b>2</b>	<b>179</b>	<b>977</b>	<b>116</b>	<b>1</b>	<b>1274</b>	<b>4252</b>
Approach %	0.0	19.1	64.9	16.0	-	-	0.0	22.7	57.4	19.9	-	-	0.0	13.3	74.5	12.3	-	-	0.2	14.1	76.7	9.1	-	-	-
Total %	0.0	3.2	11.0	2.7	-	17.0	0.0	4.7	11.8	4.1	-	20.5	0.0	4.3	24.3	4.0	-	32.6	0.0	4.2	23.0	2.7	-	30.0	-
PHF	0.000	0.704	0.867	0.821	-	0.862	0.000	0.934	0.874	0.816	-	0.935	0.000	0.821	0.966	0.904	-	0.973	0.250	0.799	0.947	0.744	-	0.977	0.960
Lights	0	134	457	112	-	703	0	194	486	171	-	851	0	177	1015	167	-	1359	2	174	960	113	-	1249	4162
% Lights	-	97.1	97.6	97.4	-	97.5	-	98.0	97.2	98.8	-	97.7	-	96.2	98.4	98.2	-	98.1	100.0	97.2	98.3	97.4	-	98.0	97.9
Other Vehicles	0	4	11	3	-	18	0	4	14	2	-	20	0	7	17	3	-	27	0	5	17	3	-	25	90
% Other Vehicles	-	2.9	2.4	2.6	-	2.5	-	2.0	2.8	1.2	-	2.3	-	3.8	1.6	1.8	-	1.9	0.0	2.8	1.7	2.6	-	2.0	2.1
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	7	-	-	-	-	-	5	-	-	-	-	-	2	-	-	-	-	-	1	-	-
% Bicycles on Crosswalk	-	-	-	-	70.0	-	-	-	-	-	55.6	-	-	-	-	-	40.0	-	-	-	-	-	100.0	-	-
Pedestrians	-	-	-	-	3	-	-	-	-	-	4	-	-	-	-	-	3	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	30.0	-	-	-	-	-	44.4	-	-	-	-	-	60.0	-	-	-	-	-	0.0	-	-

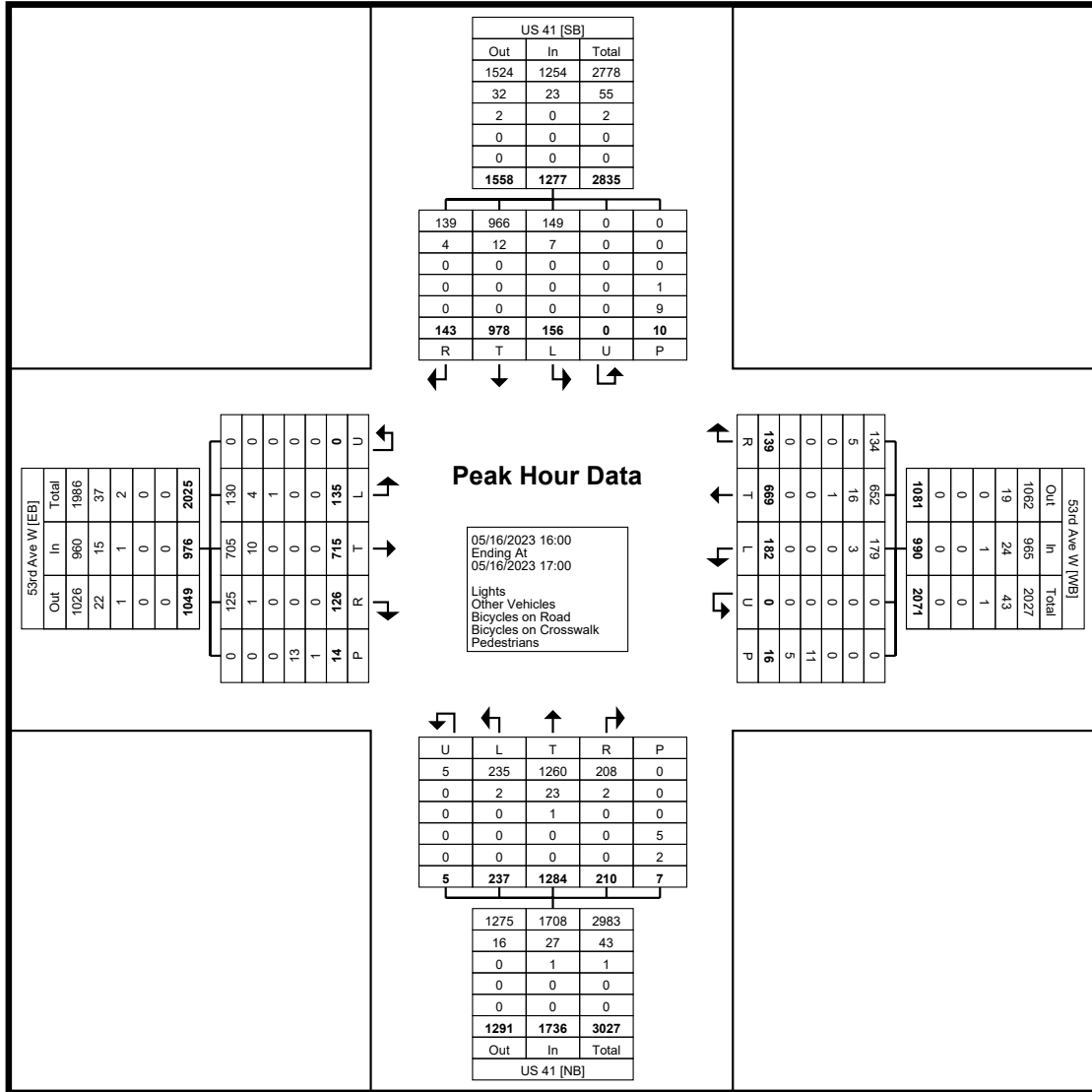


Turning Movement Peak Hour Data Plot (12:00)



### Turning Movement Peak Hour Data (16:00)

Start Time	53rd Ave W Eastbound						53rd Ave W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:00	0	25	159	35	7	219	0	46	156	36	4	238	1	66	342	48	3	457	0	35	284	30	5	349	1263
16:15	0	31	191	26	3	243	0	51	181	39	5	271	2	56	304	52	2	414	0	41	202	42	3	285	1218
16:30	0	47	160	36	1	243	0	41	182	32	2	255	2	47	324	55	2	428	0	33	264	36	0	333	1259
16:45	0	32	205	29	3	266	0	44	150	32	5	226	0	68	314	55	0	437	0	47	228	35	2	310	1239
<b>Total</b>	<b>0</b>	<b>135</b>	<b>715</b>	<b>126</b>	<b>14</b>	<b>976</b>	<b>0</b>	<b>182</b>	<b>669</b>	<b>139</b>	<b>16</b>	<b>990</b>	<b>5</b>	<b>237</b>	<b>1284</b>	<b>210</b>	<b>7</b>	<b>1736</b>	<b>0</b>	<b>156</b>	<b>978</b>	<b>143</b>	<b>10</b>	<b>1277</b>	<b>4979</b>
Approach %	0.0	13.8	73.3	12.9	-	-	0.0	18.4	67.6	14.0	-	-	0.3	13.7	74.0	12.1	-	-	0.0	12.2	76.6	11.2	-	-	-
Total %	0.0	2.7	14.4	2.5	-	19.6	0.0	3.7	13.4	2.8	-	19.9	0.1	4.8	25.8	4.2	-	34.9	0.0	3.1	19.6	2.9	-	25.6	-
PHF	0.000	0.718	0.872	0.875	-	0.917	0.000	0.892	0.919	0.891	-	0.913	0.625	0.871	0.939	0.955	-	0.950	0.000	0.830	0.861	0.851	-	0.915	0.986
Lights	0	130	705	125	-	960	0	179	652	134	-	965	5	235	1260	208	-	1708	0	149	966	139	-	1254	4887
% Lights	-	96.3	98.6	99.2	-	98.4	-	98.4	97.5	96.4	-	97.5	100.0	99.2	98.1	99.0	-	98.4	-	95.5	98.8	97.2	-	98.2	98.2
Other Vehicles	0	4	10	1	-	15	0	3	16	5	-	24	0	2	23	2	-	27	0	7	12	4	-	23	89
% Other Vehicles	-	3.0	1.4	0.8	-	1.5	-	1.6	2.4	3.6	-	2.4	0.0	0.8	1.8	1.0	-	1.6	-	4.5	1.2	2.8	-	1.8	1.8
Bicycles on Road	0	1	0	0	-	1	0	0	1	0	-	1	0	0	1	0	-	1	0	0	0	0	-	0	3
% Bicycles on Road	-	0.7	0.0	0.0	-	0.1	-	0.0	0.1	0.0	-	0.1	0.0	0.0	0.1	0.0	-	0.1	-	0.0	0.0	0.0	-	0.0	0.1
Bicycles on Crosswalk	-	-	-	-	13	-	-	-	-	-	11	-	-	-	-	-	5	-	-	-	-	-	1	-	-
% Bicycles on Crosswalk	-	-	-	-	92.9	-	-	-	-	-	68.8	-	-	-	-	-	71.4	-	-	-	-	-	10.0	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	5	-	-	-	-	-	2	-	-	-	-	-	9	-	-
% Pedestrians	-	-	-	-	7.1	-	-	-	-	-	31.3	-	-	-	-	-	28.6	-	-	-	-	-	90.0	-	-



Turning Movement Peak Hour Data Plot (16:00)

### Turning Movement Data

Start Time	49th Drive W Eastbound						49th Drive W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	2	0	7	2	9	0	3	0	2	0	5	0	8	209	3	0	220	0	3	365	7	0	375	609
7:15	0	4	0	7	2	11	0	4	0	5	1	9	0	12	288	0	1	300	0	0	340	3	1	343	663
7:30	0	2	0	5	1	7	0	1	0	5	2	6	0	8	332	3	0	343	0	3	308	7	0	318	674
7:45	0	7	0	4	1	11	0	2	0	3	2	5	0	3	282	0	0	285	0	2	305	4	0	311	612
Hourly Total	0	15	0	23	6	38	0	10	0	15	5	25	0	31	1111	6	1	1148	0	8	1318	21	1	1347	2558
8:00	0	5	0	4	1	9	0	3	0	6	1	9	0	10	292	3	1	305	0	4	261	6	0	271	594
8:15	0	3	0	6	0	9	0	2	0	7	1	9	0	5	281	0	0	286	0	3	349	7	0	359	663
8:30	0	8	0	4	1	12	0	0	1	3	2	4	0	6	242	1	0	249	0	1	326	13	0	340	605
8:45	0	1	2	9	3	12	0	1	0	6	1	7	1	13	276	1	0	291	0	2	316	6	0	324	634
Hourly Total	0	17	2	23	5	42	0	6	1	22	5	29	1	34	1091	5	1	1131	0	10	1252	32	0	1294	2496
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	14	0	16	2	30	0	4	1	10	4	15	0	12	288	2	0	302	0	5	285	12	0	302	649
12:15	0	14	0	20	0	34	0	7	1	5	0	13	0	20	347	6	1	373	0	5	316	14	0	335	755
12:30	0	10	0	13	1	23	0	8	0	4	8	12	0	19	300	3	0	322	0	4	261	6	0	271	628
12:45	0	11	1	17	0	29	0	2	0	11	1	13	0	9	310	3	1	322	0	9	287	9	0	305	669
Hourly Total	0	49	1	66	3	116	0	21	2	30	13	53	0	60	1245	14	2	1319	0	23	1149	41	0	1213	2701
13:00	0	12	0	22	1	34	0	6	0	10	5	16	0	17	263	4	0	284	0	9	259	8	0	276	610
13:15	0	15	0	14	1	29	0	9	0	2	3	11	0	15	298	4	0	317	0	3	315	17	0	335	692
13:30	0	11	0	9	1	20	0	4	1	7	3	12	0	8	287	5	0	300	0	1	261	5	0	267	599
13:45	0	11	0	21	3	32	0	1	0	2	3	3	1	18	296	4	0	319	0	4	289	5	0	298	652
Hourly Total	0	49	0	66	6	115	0	20	1	21	14	42	1	58	1144	17	0	1220	0	17	1124	35	0	1176	2553
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	14	2	9	2	25	0	6	0	9	3	15	0	16	339	8	0	363	1	7	289	7	0	304	707
15:15	0	7	0	12	2	19	0	4	0	7	3	11	0	6	341	1	0	348	0	7	291	16	0	314	692
15:30	0	13	2	5	2	20	0	3	1	7	2	11	0	9	345	6	0	360	0	5	288	8	0	301	692
15:45	0	10	0	11	1	21	0	11	0	6	5	17	0	20	351	10	0	381	0	4	275	9	0	288	707
Hourly Total	0	44	4	37	7	85	0	24	1	29	13	54	0	51	1376	25	0	1452	1	23	1143	40	0	1207	2798
16:00	0	12	2	17	0	31	0	2	0	5	5	7	0	13	354	4	0	371	0	6	303	12	0	321	730
16:15	0	12	0	10	3	22	0	7	1	13	2	21	0	12	376	5	0	393	0	10	270	14	0	294	730
16:30	0	15	0	22	3	37	0	4	0	8	4	12	0	20	382	7	0	409	0	6	311	5	0	322	780
16:45	0	11	2	22	2	35	0	8	0	6	0	14	0	16	322	9	0	347	0	6	280	13	0	299	695
Hourly Total	0	50	4	71	8	125	0	21	1	32	11	54	0	61	1434	25	0	1520	0	28	1164	44	0	1236	2935
17:00	0	10	1	12	2	23	0	7	0	8	2	15	0	15	396	8	0	419	0	5	280	7	0	292	749
17:15	0	14	1	14	2	29	0	2	0	6	2	8	1	12	358	5	0	376	0	9	334	13	0	356	769
17:30	0	14	0	13	1	27	0	4	0	9	6	13	0	18	329	6	1	353	0	4	292	15	1	311	704
17:45	0	20	0	6	0	26	0	5	0	7	3	12	0	19	384	3	0	406	0	10	295	9	0	314	758
Hourly Total	0	58	2	45	5	105	0	18	0	30	13	48	1	64	1467	22	1	1554	0	28	1201	44	1	1273	2980
18:00	0	12	1	12	1	25	0	12	2	6	0	20	0	21	321	3	0	345	0	10	283	17	0	310	700
18:15	0	16	2	11	2	29	0	6	1	5	2	12	0	24	351	3	0	378	0	4	259	21	1	284	703
18:30	0	16	0	18	0	34	0	4	1	5	0	10	0	23	285	4	1	312	0	8	250	15	0	273	629
18:45	0	16	0	18	3	34	0	6	1	6	0	13	0	22	283	6	0	311	0	3	254	10	0	267	625
Hourly Total	0	60	3	59	6	122	0	28	5	22	2	55	0	90	1240	16	1	1346	0	25	1046	63	1	1134	2657
Grand Total	0	342	16	390	46	748	0	148	11	201	76	360	3	449	10108	130	6	10690	1	162	9397	320	3	9880	21678
Approach %	0.0	45.7	2.1	52.1	-	-	0.0	41.1	3.1	55.8	-	-	0.0	4.2	94.6	1.2	-	-	0.0	1.6	95.1	3.2	-	-	-
Total %	0.0	1.6	0.1	1.8	-	3.5	0.0	0.7	0.1	0.9	-	1.7	0.0	2.1	46.6	0.6	-	49.3	0.0	0.7	43.3	1.5	-	45.6	-
Lights	0	339	16	379	-	734	0	147	11	196	-	354	3	446	9904	125	-	10478	1	161	9200	314	-	9676	21242
% Lights	-	99.1	100.0	97.2	-	98.1	-	99.3	100.0	97.5	-	98.3	100.0	99.3	98.0	96.2	-	98.0	100.0	99.4	97.9	98.1	-	97.9	98.0
Other Vehicles	0	3	0	11	-	14	0	1	0	4	-	5	0	3	201	4	-	208	0	1	197	6	-	204	431
% Other Vehicles	-	0.9	0.0	2.8	-	1.9	-	0.7	0.0	2.0	-	1.4	0.0	0.7	2.0	3.1	-	1.9	0.0	0.6	2.1	1.9	-	2.1	2.0
Bicycles on Road	0	0	0	0	-	0	0	0	0	1	-	1	0	0	3	1	-	4	0	0	0	0	-	0	5
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.5	-	0.3	0.0	0.0	0.0	0.8	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	17	-	-	-	-	-	46	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	37.0	-	-	-	-	-	60.5	-	-	-	-	-	16.7	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	29	-	-	-	-	-	30	-	-	-	-	-	5	-	-	-	-	-	3	-	-
% Pedestrians	-	-	-	-	63.0	-	-	-	-	-	39.5	-	-	-	-	-	83.3	-	-	-	-	-	100.0	-	-



### Turning Movement Peak Hour Data (7:00)

Start Time	49th Drive W Eastbound						49th Drive W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	2	0	7	2	9	0	3	0	2	0	5	0	8	209	3	0	220	0	3	365	7	0	375	609
7:15	0	4	0	7	2	11	0	4	0	5	1	9	0	12	288	0	1	300	0	0	340	3	1	343	663
7:30	0	2	0	5	1	7	0	1	0	5	2	6	0	8	332	3	0	343	0	3	308	7	0	318	674
7:45	0	7	0	4	1	11	0	2	0	3	2	5	0	3	282	0	0	285	0	2	305	4	0	311	612
<b>Total</b>	0	15	0	23	6	38	0	10	0	15	5	25	0	31	1111	6	1	1148	0	8	1318	21	1	1347	2558
Approach %	0.0	39.5	0.0	60.5	-	-	0.0	40.0	0.0	60.0	-	-	0.0	2.7	96.8	0.5	-	-	0.0	0.6	97.8	1.6	-	-	-
Total %	0.0	0.6	0.0	0.9	-	1.5	0.0	0.4	0.0	0.6	-	1.0	0.0	1.2	43.4	0.2	-	44.9	0.0	0.3	51.5	0.8	-	52.7	-
PHF	0.000	0.536	0.000	0.821	-	0.864	0.000	0.625	0.000	0.750	-	0.694	0.000	0.646	0.837	0.500	-	0.837	0.000	0.667	0.903	0.750	-	0.898	0.949
Lights	0	15	0	19	-	34	0	10	0	15	-	25	0	30	1082	6	-	1118	0	8	1275	19	-	1302	2479
% Lights	-	100.0	-	82.6	-	89.5	-	100.0	-	100.0	-	100.0	-	96.8	97.4	100.0	-	97.4	-	100.0	96.7	90.5	-	96.7	96.9
Other Vehicles	0	0	0	4	-	4	0	0	0	0	-	0	0	1	29	0	-	30	0	0	43	2	-	45	79
% Other Vehicles	-	0.0	-	17.4	-	10.5	-	0.0	-	0.0	-	0.0	-	3.2	2.6	0.0	-	2.6	-	0.0	3.3	9.5	-	3.3	3.1
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	3	-	-	-	-	-	5	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	50.0	-	-	-	-	-	100.0	-	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	3	-	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	50.0	-	-	-	-	-	0.0	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-



### Turning Movement Peak Hour Data (12:00)

Start Time	49th Drive W Eastbound						49th Drive W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	14	0	16	2	30	0	4	1	10	4	15	0	12	288	2	0	302	0	5	285	12	0	302	649
12:15	0	14	0	20	0	34	0	7	1	5	0	13	0	20	347	6	1	373	0	5	316	14	0	335	755
12:30	0	10	0	13	1	23	0	8	0	4	8	12	0	19	300	3	0	322	0	4	261	6	0	271	628
12:45	0	11	1	17	0	29	0	2	0	11	1	13	0	9	310	3	1	322	0	9	287	9	0	305	669
<b>Total</b>	<b>0</b>	<b>49</b>	<b>1</b>	<b>66</b>	<b>3</b>	<b>116</b>	<b>0</b>	<b>21</b>	<b>2</b>	<b>30</b>	<b>13</b>	<b>53</b>	<b>0</b>	<b>60</b>	<b>1245</b>	<b>14</b>	<b>2</b>	<b>1319</b>	<b>0</b>	<b>23</b>	<b>1149</b>	<b>41</b>	<b>0</b>	<b>1213</b>	<b>2701</b>
Approach %	0.0	42.2	0.9	56.9	-	-	0.0	39.6	3.8	56.6	-	-	0.0	4.5	94.4	1.1	-	-	0.0	1.9	94.7	3.4	-	-	-
Total %	0.0	1.8	0.0	2.4	-	4.3	0.0	0.8	0.1	1.1	-	2.0	0.0	2.2	46.1	0.5	-	48.8	0.0	0.9	42.5	1.5	-	44.9	-
PHF	0.000	0.875	0.250	0.825	-	0.853	0.000	0.656	0.500	0.682	-	0.883	0.000	0.750	0.897	0.583	-	0.884	0.000	0.639	0.909	0.732	-	0.905	0.894
Lights	0	49	1	63	-	113	0	20	2	29	-	51	0	60	1224	11	-	1295	0	23	1133	41	-	1197	2656
% Lights	-	100.0	100.0	95.5	-	97.4	-	95.2	100.0	96.7	-	96.2	-	100.0	98.3	78.6	-	98.2	-	100.0	98.6	100.0	-	98.7	98.3
Other Vehicles	0	0	0	3	-	3	0	1	0	1	-	2	0	0	21	2	-	23	0	0	16	0	-	16	44
% Other Vehicles	-	0.0	0.0	4.5	-	2.6	-	4.8	0.0	3.3	-	3.8	-	0.0	1.7	14.3	-	1.7	-	0.0	1.4	0.0	-	1.3	1.6
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	1	-	1	0	0	0	0	-	0	1
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	7.1	-	0.1	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	2	-	-	-	-	-	9	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	66.7	-	-	-	-	-	69.2	-	-	-	-	-	50.0	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	4	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	33.3	-	-	-	-	-	30.8	-	-	-	-	-	50.0	-	-	-	-	-	-	-	-





### Turning Movement Peak Hour Data (16:30)

Start Time	49th Drive W Eastbound						49th Drive W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:30	0	15	0	22	3	37	0	4	0	8	4	12	0	20	382	7	0	409	0	6	311	5	0	322	780
16:45	0	11	2	22	2	35	0	8	0	6	0	14	0	16	322	9	0	347	0	6	280	13	0	299	695
17:00	0	10	1	12	2	23	0	7	0	8	2	15	0	15	396	8	0	419	0	5	280	7	0	292	749
17:15	0	14	1	14	2	29	0	2	0	6	2	8	1	12	358	5	0	376	0	9	334	13	0	356	769
<b>Total</b>	0	50	4	70	9	124	0	21	0	28	8	49	1	63	1458	29	0	1551	0	26	1205	38	0	1269	2993
Approach %	0.0	40.3	3.2	56.5	-	-	0.0	42.9	0.0	57.1	-	-	0.1	4.1	94.0	1.9	-	-	0.0	2.0	95.0	3.0	-	-	-
Total %	0.0	1.7	0.1	2.3	-	4.1	0.0	0.7	0.0	0.9	-	1.6	0.0	2.1	48.7	1.0	-	51.8	0.0	0.9	40.3	1.3	-	42.4	-
PHF	0.000	0.833	0.500	0.795	-	0.838	0.000	0.656	0.000	0.875	-	0.817	0.250	0.788	0.920	0.806	-	0.925	0.000	0.722	0.902	0.731	-	0.891	0.959
Lights	0	50	4	70	-	124	0	21	0	26	-	47	1	63	1436	27	-	1527	0	26	1187	38	-	1251	2949
% Lights	-	100.0	100.0	100.0	-	100.0	-	100.0	-	92.9	-	95.9	100.0	100.0	98.5	93.1	-	98.5	-	100.0	98.5	100.0	-	98.6	98.5
Other Vehicles	0	0	0	0	-	0	0	0	0	2	-	2	0	0	22	2	-	24	0	0	18	0	-	18	44
% Other Vehicles	-	0.0	0.0	0.0	-	0.0	-	0.0	-	7.1	-	4.1	0.0	0.0	1.5	6.9	-	1.5	-	0.0	1.5	0.0	-	1.4	1.5
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	6	-	-	-	-	-	6	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	66.7	-	-	-	-	-	75.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	3	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	33.3	-	-	-	-	-	25.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-



### Turning Movement Data

Start Time	46th Ave Drive W Eastbound						Orlando Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	0	0	0	2	0	0	26	0	18	1	44	0	0	212	13	0	225	0	8	362	0	0	370	639
7:15	0	0	0	0	1	0	0	18	0	17	0	35	0	0	273	19	0	292	0	8	325	0	0	333	660
7:30	0	0	0	0	0	0	0	17	0	25	0	42	0	0	308	24	0	332	0	14	322	0	0	336	710
7:45	0	0	0	0	0	0	0	23	0	36	1	59	0	0	283	16	0	299	0	17	298	0	0	315	673
Hourly Total	0	0	0	0	3	0	0	84	0	96	2	180	0	0	1076	72	0	1148	0	47	1307	0	0	1354	2682
8:00	0	0	0	0	0	0	0	24	0	23	1	47	0	0	267	12	1	279	0	20	287	0	0	307	633
8:15	0	0	0	3	0	3	0	22	0	29	0	51	0	3	255	16	0	274	0	16	344	1	0	361	689
8:30	0	0	0	0	1	0	0	31	0	27	2	58	0	1	250	11	0	262	0	23	326	0	0	349	669
8:45	0	0	0	0	0	0	0	20	0	28	0	48	0	1	241	22	1	264	0	16	281	0	0	297	609
Hourly Total	0	0	0	3	1	3	0	97	0	107	3	204	0	5	1013	61	2	1079	0	75	1238	1	0	1314	2600
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	0	0	2	0	0	33	0	33	0	66	0	0	269	35	0	304	0	26	278	0	0	304	674
12:15	0	0	0	0	2	0	0	28	0	37	4	65	0	0	306	36	0	342	1	35	314	0	0	350	757
12:30	0	0	0	0	0	0	0	49	0	41	5	90	0	0	301	38	0	339	0	25	265	0	1	290	719
12:45	0	0	0	0	1	0	0	34	0	34	1	68	0	1	277	29	1	307	0	23	271	2	0	296	671
Hourly Total	0	0	0	0	5	0	0	144	0	145	10	289	0	1	1153	138	1	1292	1	109	1128	2	1	1240	2821
13:00	0	0	0	1	1	1	0	37	0	42	1	79	0	0	275	30	1	305	0	25	257	0	3	282	667
13:15	0	0	0	0	3	0	0	53	0	30	3	83	0	1	273	26	1	300	0	24	288	0	0	312	695
13:30	0	0	0	0	2	0	0	31	0	32	5	63	0	0	293	22	1	315	0	15	244	1	0	260	638
13:45	0	1	0	0	1	1	0	40	0	27	0	67	0	0	278	33	0	311	0	25	259	1	1	285	664
Hourly Total	0	1	0	1	7	2	0	161	0	131	9	292	0	1	1119	111	3	1231	0	89	1048	2	4	1139	2664
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	1	0	0	1	1	0	37	0	27	3	64	0	0	354	31	0	385	0	23	294	0	0	317	767
15:15	0	0	0	0	1	0	0	34	0	33	0	67	0	0	304	24	0	328	0	31	264	0	0	295	690
15:30	0	0	0	0	2	0	0	21	0	23	1	44	0	1	359	28	0	388	0	29	317	0	0	346	778
15:45	0	0	0	0	1	0	0	38	0	25	5	63	0	0	324	26	0	350	0	21	264	0	0	285	698
Hourly Total	0	1	0	0	5	1	0	130	0	108	9	238	0	1	1341	109	0	1451	0	104	1139	0	0	1243	2933
16:00	0	0	0	0	1	0	0	36	0	30	4	66	1	0	327	31	0	359	0	37	301	0	1	338	763
16:15	0	0	0	2	2	2	0	38	0	26	0	64	0	1	371	36	0	408	0	18	256	1	0	275	749
16:30	0	0	0	1	5	1	0	44	0	28	2	72	0	0	346	46	0	392	0	32	297	0	1	329	794
16:45	0	1	0	0	2	1	0	31	0	34	0	65	0	1	320	30	1	351	0	42	281	0	3	323	740
Hourly Total	0	1	0	3	10	4	0	149	0	118	6	267	1	2	1364	143	1	1510	0	129	1135	1	5	1265	3046
17:00	0	0	0	0	4	0	0	26	0	34	3	60	0	0	373	43	0	416	3	28	272	0	0	303	779
17:15	0	0	0	0	1	0	0	38	0	43	2	81	0	1	307	34	0	342	0	29	311	0	0	340	763
17:30	0	0	0	0	2	0	0	35	0	37	1	72	0	0	315	29	1	344	0	32	300	0	0	332	748
17:45	0	1	0	0	1	1	0	37	0	23	2	60	0	0	360	32	0	392	0	31	265	1	1	297	750
Hourly Total	0	1	0	0	8	1	0	136	0	137	8	273	0	1	1355	138	1	1494	3	120	1148	1	1	1272	3040
18:00	0	0	0	0	0	0	0	39	0	35	0	74	0	0	311	42	0	353	0	30	298	0	0	328	755
18:15	0	0	0	0	1	0	0	44	0	19	2	63	0	0	352	33	0	385	0	18	229	0	1	247	695
18:30	0	1	0	0	0	1	0	38	0	26	1	64	0	1	250	25	0	276	0	29	253	1	2	283	624
18:45	0	0	0	0	1	0	0	43	0	13	5	56	0	0	290	27	0	317	0	19	230	0	1	249	622
Hourly Total	0	1	0	0	2	1	0	164	0	93	8	257	0	1	1203	127	0	1331	0	96	1010	1	4	1107	2696
Grand Total	0	5	0	7	41	12	0	1065	0	935	55	2000	1	12	9624	899	8	10536	4	769	9153	8	15	9934	22482
Approach %	0.0	41.7	0.0	58.3	-	-	0.0	53.3	0.0	46.8	-	-	0.0	0.1	91.3	8.5	-	-	0.0	7.7	92.1	0.1	-	-	-
Total %	0.0	0.0	0.0	0.0	-	0.1	0.0	4.7	0.0	4.2	-	8.9	0.0	0.1	42.8	4.0	-	46.9	0.0	3.4	40.7	0.0	-	44.2	-
Lights	0	5	0	4	-	9	0	1056	0	925	-	1981	1	9	9448	892	-	10350	4	754	8926	6	-	9690	22030
% Lights	-	100.0	-	57.1	-	75.0	-	99.2	-	98.9	-	99.1	100.0	75.0	98.2	99.2	-	98.2	100.0	98.0	97.5	75.0	-	97.5	98.0
Other Vehicles	0	0	0	3	-	3	0	9	0	10	-	19	0	3	174	7	-	184	0	15	222	1	-	238	444
% Other Vehicles	-	0.0	-	42.9	-	25.0	-	0.8	-	1.1	-	1.0	0.0	25.0	1.8	0.8	-	1.7	0.0	2.0	2.4	12.5	-	2.4	2.0
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	2	0	-	2	0	0	5	1	-	6	8
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.1	12.5	-	0.1	0.0
Bicycles on Crosswalk	-	-	-	-	19	-	-	-	-	-	30	-	-	-	-	-	2	-	-	-	-	-	2	-	-
% Bicycles on Crosswalk	-	-	-	-	46.3	-	-	-	-	-	54.5	-	-	-	-	-	25.0	-	-	-	-	-	13.3	-	-
Pedestrians	-	-	-	-	22	-	-	-	-	-	25	-	-	-	-	-	6	-	-	-	-	-	13	-	-
% Pedestrians	-	-	-	-	53.7	-	-	-	-	-	45.5	-	-	-	-	-	75.0	-	-	-	-	-	86.7	-	-



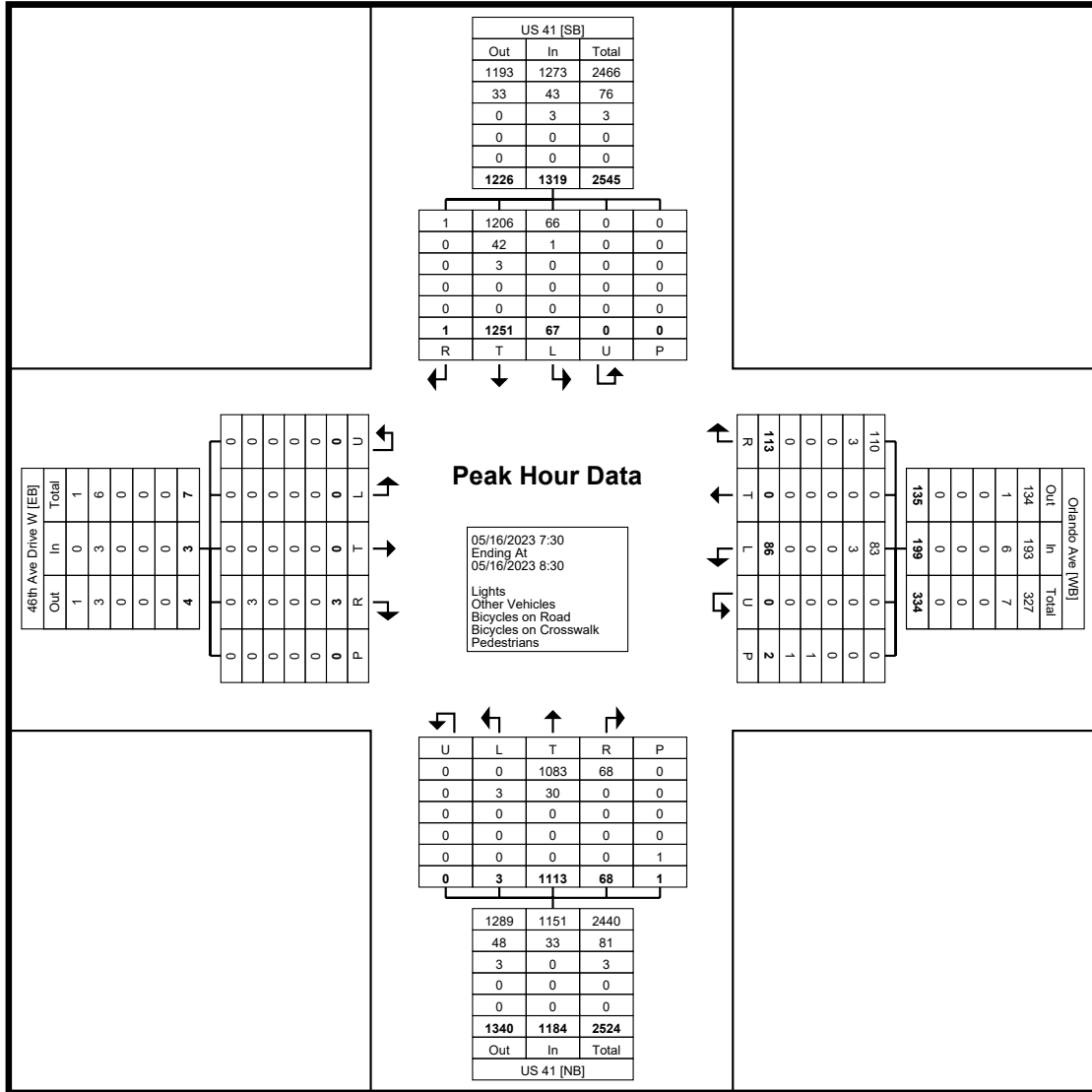
US 41 at Orlando Ave/46th Ave  
Drive W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 34 US 41 at  
Orlando Ave/46th Ave Drive W  
Site Code: 34  
Start Date: 05/16/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:30)

Start Time	46th Ave Drive W Eastbound						Orlando Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:30	0	0	0	0	0	0	0	17	0	25	0	42	0	0	308	24	0	332	0	14	322	0	0	336	710
7:45	0	0	0	0	0	0	0	23	0	36	1	59	0	0	283	16	0	299	0	17	298	0	0	315	673
8:00	0	0	0	0	0	0	0	24	0	23	1	47	0	0	267	12	1	279	0	20	287	0	0	307	633
8:15	0	0	0	3	0	3	0	22	0	29	0	51	0	3	255	16	0	274	0	16	344	1	0	361	689
<b>Total</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>86</b>	<b>0</b>	<b>113</b>	<b>2</b>	<b>199</b>	<b>0</b>	<b>3</b>	<b>1113</b>	<b>68</b>	<b>1</b>	<b>1184</b>	<b>0</b>	<b>67</b>	<b>1251</b>	<b>1</b>	<b>0</b>	<b>1319</b>	<b>2705</b>
Approach %	0.0	0.0	0.0	100.0	-	-	0.0	43.2	0.0	56.8	-	-	0.0	0.3	94.0	5.7	-	-	0.0	5.1	94.8	0.1	-	-	-
Total %	0.0	0.0	0.0	0.1	-	0.1	0.0	3.2	0.0	4.2	-	7.4	0.0	0.1	41.1	2.5	-	43.8	0.0	2.5	46.2	0.0	-	48.8	-
PHF	0.000	0.000	0.000	0.250	-	0.250	0.000	0.896	0.000	0.785	-	0.843	0.000	0.250	0.903	0.708	-	0.892	0.000	0.838	0.909	0.250	-	0.913	0.952
Lights	0	0	0	0	-	0	0	83	0	110	-	193	0	0	1083	68	-	1151	0	66	1206	1	-	1273	2617
% Lights	-	-	-	0.0	-	0.0	-	96.5	-	97.3	-	97.0	-	0.0	97.3	100.0	-	97.2	-	98.5	96.4	100.0	-	96.5	96.7
Other Vehicles	0	0	0	3	-	3	0	3	0	3	-	6	0	3	30	0	-	33	0	1	42	0	-	43	85
% Other Vehicles	-	-	-	100.0	-	100.0	-	3.5	-	2.7	-	3.0	-	100.0	2.7	0.0	-	2.8	-	1.5	3.4	0.0	-	3.3	3.1
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	3	0	-	3	3
% Bicycles on Road	-	-	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.2	0.0	-	0.2	0.1
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	50.0	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	50.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-



Turning Movement Peak Hour Data Plot (7:30)

### Turning Movement Peak Hour Data (12:00)

Start Time	46th Ave Drive W Eastbound						Orlando Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	0	0	0	2	0	0	33	0	33	0	66	0	0	269	35	0	304	0	26	278	0	0	304	674
12:15	0	0	0	0	2	0	0	28	0	37	4	65	0	0	306	36	0	342	1	35	314	0	0	350	757
12:30	0	0	0	0	0	0	0	49	0	41	5	90	0	0	301	38	0	339	0	25	265	0	1	290	719
12:45	0	0	0	0	1	0	0	34	0	34	1	68	0	1	277	29	1	307	0	23	271	2	0	296	671
<b>Total</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>5</b>	<b>0</b>	<b>0</b>	<b>144</b>	<b>0</b>	<b>145</b>	<b>10</b>	<b>289</b>	<b>0</b>	<b>1</b>	<b>1153</b>	<b>138</b>	<b>1</b>	<b>1292</b>	<b>1</b>	<b>109</b>	<b>1128</b>	<b>2</b>	<b>1</b>	<b>1240</b>	<b>2821</b>
Approach %	0.0	0.0	0.0	0.0	-	-	0.0	49.8	0.0	50.2	-	-	0.0	0.1	89.2	10.7	-	-	0.1	8.8	91.0	0.2	-	-	-
Total %	0.0	0.0	0.0	0.0	-	0.0	0.0	5.1	0.0	5.1	-	10.2	0.0	0.0	40.9	4.9	-	45.8	0.0	3.9	40.0	0.1	-	44.0	-
PHF	0.000	0.000	0.000	0.000	-	0.000	0.000	0.735	0.000	0.884	-	0.803	0.000	0.250	0.942	0.908	-	0.944	0.250	0.779	0.898	0.250	-	0.886	0.932
Lights	0	0	0	0	-	0	0	143	0	145	-	288	0	1	1136	136	-	1273	1	107	1106	1	-	1215	2776
% Lights	-	-	-	-	-	-	-	99.3	-	100.0	-	99.7	-	100.0	98.5	98.6	-	98.5	100.0	98.2	98.0	50.0	-	98.0	98.4
Other Vehicles	0	0	0	0	-	0	0	1	0	0	-	1	0	0	17	2	-	19	0	2	22	1	-	25	45
% Other Vehicles	-	-	-	-	-	-	-	0.7	-	0.0	-	0.3	-	0.0	1.5	1.4	-	1.5	0.0	1.8	2.0	50.0	-	2.0	1.6
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	-	-	-	-	-	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	2	-	-	-	-	-	6	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	40.0	-	-	-	-	-	60.0	-	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	3	-	-	-	-	-	4	-	-	-	-	-	1	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	60.0	-	-	-	-	-	40.0	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-





### Turning Movement Peak Hour Data (16:30)

Start Time	46th Ave Drive W Eastbound						Orlando Ave Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:30	0	0	0	1	5	1	0	44	0	28	2	72	0	0	346	46	0	392	0	32	297	0	1	329	794
16:45	0	1	0	0	2	1	0	31	0	34	0	65	0	1	320	30	1	351	0	42	281	0	3	323	740
17:00	0	0	0	0	4	0	0	26	0	34	3	60	0	0	373	43	0	416	3	28	272	0	0	303	779
17:15	0	0	0	0	1	0	0	38	0	43	2	81	0	1	307	34	0	342	0	29	311	0	0	340	763
<b>Total</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>1</b>	<b>12</b>	<b>2</b>	<b>0</b>	<b>139</b>	<b>0</b>	<b>139</b>	<b>7</b>	<b>278</b>	<b>0</b>	<b>2</b>	<b>1346</b>	<b>153</b>	<b>1</b>	<b>1501</b>	<b>3</b>	<b>131</b>	<b>1161</b>	<b>0</b>	<b>4</b>	<b>1295</b>	<b>3076</b>
Approach %	0.0	50.0	0.0	50.0	-	-	0.0	50.0	0.0	50.0	-	-	0.0	0.1	89.7	10.2	-	-	0.2	10.1	89.7	0.0	-	-	-
Total %	0.0	0.0	0.0	0.0	-	0.1	0.0	4.5	0.0	4.5	-	9.0	0.0	0.1	43.8	5.0	-	48.8	0.1	4.3	37.7	0.0	-	42.1	-
PHF	0.000	0.250	0.000	0.250	-	0.500	0.000	0.790	0.000	0.808	-	0.858	0.000	0.500	0.902	0.832	-	0.902	0.250	0.780	0.933	0.000	-	0.952	0.969
Lights	0	1	0	1	-	2	0	137	0	138	-	275	0	2	1325	152	-	1479	3	128	1147	0	-	1278	3034
% Lights	-	100.0	-	100.0	-	100.0	-	98.6	-	99.3	-	98.9	-	100.0	98.4	99.3	-	98.5	100.0	97.7	98.8	-	-	98.7	98.6
Other Vehicles	0	0	0	0	-	0	0	2	0	1	-	3	0	0	21	1	-	22	0	3	14	0	-	17	42
% Other Vehicles	-	0.0	-	0.0	-	0.0	-	1.4	-	0.7	-	1.1	-	0.0	1.6	0.7	-	1.5	0.0	2.3	1.2	-	-	1.3	1.4
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	-	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	4	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	2	-	-
% Bicycles on Crosswalk	-	-	-	-	33.3	-	-	-	-	-	28.6	-	-	-	-	-	0.0	-	-	-	-	-	50.0	-	-
Pedestrians	-	-	-	-	8	-	-	-	-	-	5	-	-	-	-	-	1	-	-	-	-	-	2	-	-
% Pedestrians	-	-	-	-	66.7	-	-	-	-	-	71.4	-	-	-	-	-	100.0	-	-	-	-	-	50.0	-	-



### Turning Movement Data

Start Time	Driveway Eastbound						Business Driveway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	3	0	1	1	4	0	0	0	7	0	7	0	2	248	2	0	252	0	11	347	0	0	358	621
7:15	0	0	0	1	0	1	0	0	0	5	0	5	0	0	285	1	0	286	0	11	345	0	0	356	648
7:30	0	1	0	0	1	1	0	0	0	6	1	6	0	0	325	2	0	327	0	10	338	0	1	348	682
7:45	0	0	0	1	1	1	0	0	0	8	1	8	0	0	326	4	0	330	0	9	306	1	0	316	655
Hourly Total	0	4	0	3	3	7	0	0	0	26	2	26	0	2	1184	9	0	1195	0	41	1336	1	1	1378	2606
8:00	0	0	0	2	3	2	0	1	0	4	1	5	0	2	277	5	0	284	0	9	303	0	0	312	603
8:15	0	0	0	0	0	0	0	0	0	5	1	5	0	1	281	3	0	285	0	8	353	0	0	361	651
8:30	0	0	0	0	1	0	0	0	0	9	1	9	0	1	277	3	0	281	0	14	330	1	0	345	635
8:45	0	0	0	0	0	0	0	0	0	12	1	12	0	1	244	7	0	252	0	20	320	3	0	343	607
Hourly Total	0	0	0	2	4	2	0	1	0	30	4	31	0	5	1079	18	0	1102	0	51	1306	4	0	1361	2496
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	0	0	1	0	0	2	0	21	0	23	0	4	304	15	0	323	0	31	305	0	0	336	682
12:15	0	0	0	4	2	4	0	4	0	17	2	21	0	1	321	13	0	335	0	28	328	0	0	356	716
12:30	0	0	0	3	0	3	0	0	0	19	5	19	0	4	336	18	0	358	0	28	302	0	0	330	710
12:45	0	0	0	1	1	1	0	5	0	22	0	27	0	0	290	13	0	303	1	25	283	1	0	310	641
Hourly Total	0	0	0	8	4	8	0	11	0	79	7	90	0	9	1251	59	0	1319	1	112	1218	1	0	1332	2749
13:00	1	1	0	1	5	3	0	3	0	26	6	29	0	2	284	23	0	309	1	24	284	0	0	309	650
13:15	0	1	0	2	3	3	0	3	0	21	1	24	0	2	301	9	0	312	2	34	297	1	0	334	673
13:30	0	0	0	1	3	1	0	3	0	25	7	28	1	2	291	14	1	308	1	29	277	3	1	310	647
13:45	0	1	0	4	1	5	0	2	0	18	1	20	0	3	290	16	0	309	1	19	255	0	0	275	609
Hourly Total	1	3	0	8	12	12	0	11	0	90	15	101	1	9	1166	62	1	1238	5	106	1113	4	1	1228	2579
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	0	0	2	3	2	0	1	0	12	1	13	0	2	359	9	0	370	0	12	313	2	0	327	712
15:15	1	0	0	3	1	4	0	3	0	4	1	7	0	5	329	9	0	343	0	17	288	0	0	305	659
15:30	0	1	0	2	2	3	0	1	0	9	3	10	0	1	365	10	3	376	0	21	329	1	0	351	740
15:45	0	1	0	2	3	3	0	1	0	10	5	11	1	1	314	10	0	326	0	24	298	1	1	323	663
Hourly Total	1	2	0	9	9	12	0	6	0	35	10	41	1	9	1367	38	3	1415	0	74	1228	4	1	1306	2774
16:00	0	1	0	1	1	2	0	2	0	8	3	10	0	2	351	14	2	367	0	12	343	0	0	355	734
16:15	0	0	0	3	3	3	0	1	0	20	0	21	0	2	336	12	0	350	0	15	291	2	0	308	682
16:30	0	1	0	1	4	2	0	1	0	12	1	13	1	6	350	20	0	377	0	5	330	1	0	336	728
16:45	0	0	0	1	6	1	0	0	0	7	1	7	0	6	355	12	0	373	0	14	328	0	0	342	723
Hourly Total	0	2	0	6	14	8	0	4	0	47	5	51	1	16	1392	58	2	1467	0	46	1292	3	0	1341	2867
17:00	0	1	0	1	4	2	0	0	0	17	0	17	0	1	371	14	1	386	0	15	310	1	0	326	731
17:15	0	0	0	2	3	2	0	1	0	15	1	16	0	6	344	16	3	366	0	14	324	2	0	340	724
17:30	0	0	0	3	1	3	0	0	0	7	0	7	0	5	356	25	0	386	0	4	319	1	0	324	720
17:45	0	1	0	0	1	1	0	0	0	10	1	10	0	2	348	16	0	366	0	15	317	0	0	332	709
Hourly Total	0	2	0	6	9	8	0	1	0	49	2	50	0	14	1419	71	4	1504	0	48	1270	4	0	1322	2884
18:00	0	0	0	3	4	3	0	1	0	18	0	19	1	5	351	22	0	379	1	14	311	0	1	326	727
18:15	0	0	0	2	1	2	0	1	0	20	1	21	0	4	337	14	2	355	0	16	262	0	0	278	656
18:30	0	0	0	0	1	0	0	0	0	17	0	17	0	1	259	5	0	265	4	21	273	1	0	299	581
18:45	0	0	0	3	1	3	0	1	0	9	4	10	1	3	279	12	1	295	5	12	245	0	0	262	570
Hourly Total	0	0	0	8	7	8	0	3	0	64	5	67	2	13	1226	53	3	1294	10	63	1091	1	1	1165	2534
Grand Total	2	13	0	50	62	65	0	37	0	420	50	457	5	77	10084	368	13	10534	16	541	9854	22	4	10433	21489
Approach %	3.1	20.0	0.0	76.9	-	-	0.0	8.1	0.0	91.9	-	-	0.0	0.7	95.7	3.5	-	-	0.2	5.2	94.5	0.2	-	-	-
Total %	0.0	0.1	0.0	0.2	-	0.3	0.0	0.2	0.0	2.0	-	2.1	0.0	0.4	46.9	1.7	-	49.0	0.1	2.5	45.9	0.1	-	48.6	-
Lights	1	11	0	48	-	60	0	35	0	413	-	448	5	73	9908	363	-	10349	15	535	9630	22	-	10202	21059
% Lights	50.0	84.6	-	96.0	-	92.3	-	94.6	-	98.3	-	98.0	100.0	94.8	98.3	98.6	-	98.2	93.8	98.9	97.7	100.0	-	97.8	98.0
Other Vehicles	0	2	0	2	-	4	0	2	0	5	-	7	0	4	172	4	-	180	0	5	222	0	-	227	418
% Other Vehicles	0.0	15.4	-	4.0	-	6.2	-	5.4	-	1.2	-	1.5	0.0	5.2	1.7	1.1	-	1.7	0.0	0.9	2.3	0.0	-	2.2	1.9
Bicycles on Road	1	0	0	0	-	1	0	0	0	2	-	2	0	0	4	1	-	5	1	1	2	0	-	4	12
% Bicycles on Road	50.0	0.0	-	0.0	-	1.5	-	0.0	-	0.5	-	0.4	0.0	0.0	0.0	0.3	-	0.0	6.3	0.2	0.0	0.0	-	0.0	0.1
Bicycles on Crosswalk	-	-	-	-	24	-	-	-	-	18	-	-	-	-	-	-	3	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	38.7	-	-	-	-	36.0	-	-	-	-	-	-	23.1	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	38	-	-	-	-	32	-	-	-	-	-	-	10	-	-	-	-	-	4	-	-
% Pedestrians	-	-	-	-	61.3	-	-	-	-	64.0	-	-	-	-	-	-	76.9	-	-	-	-	-	100.0	-	-



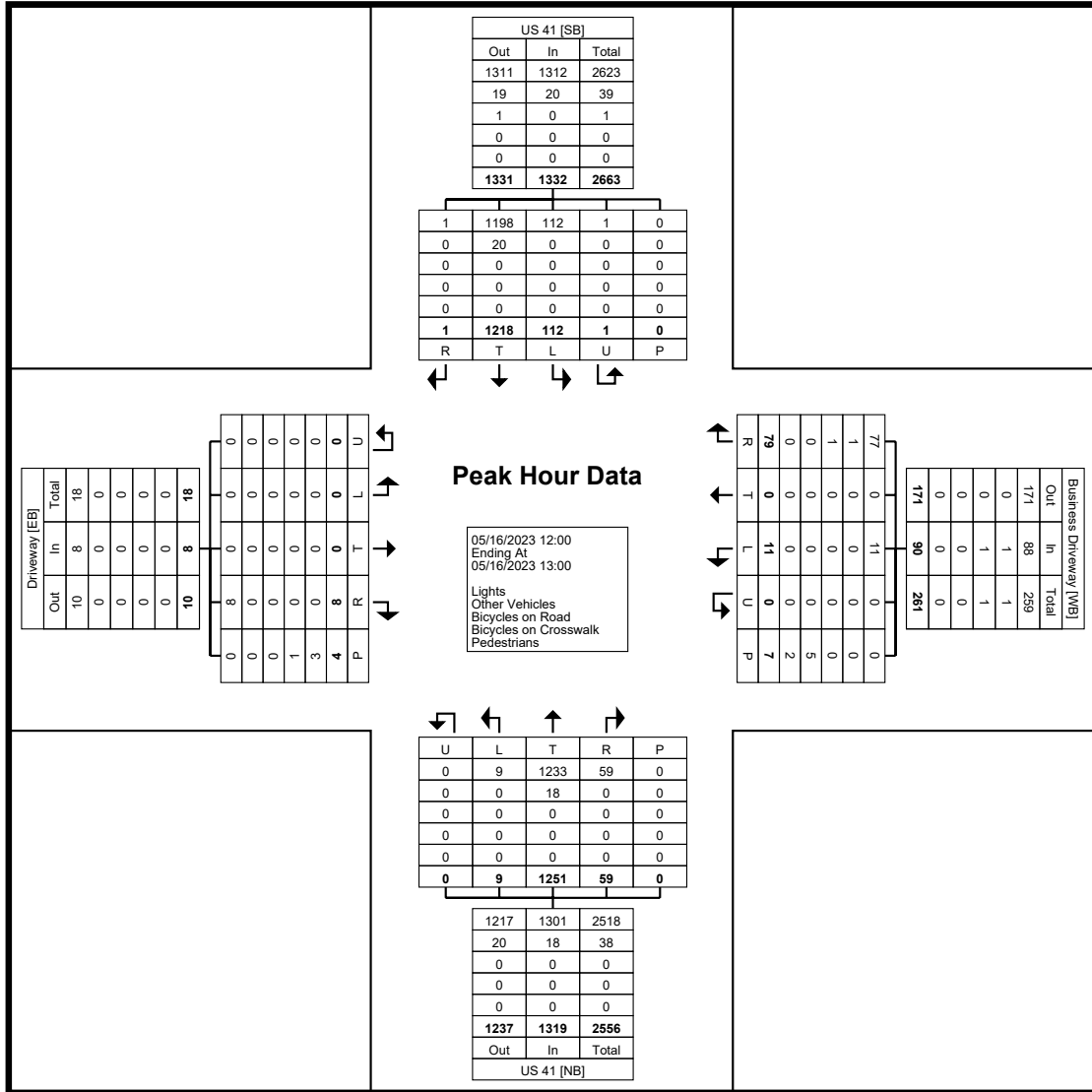
### Turning Movement Peak Hour Data (7:00)

Start Time	Driveway Eastbound						Business Driveway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	3	0	1	1	4	0	0	0	7	0	7	0	2	248	2	0	252	0	11	347	0	0	358	621
7:15	0	0	0	1	0	1	0	0	0	5	0	5	0	0	285	1	0	286	0	11	345	0	0	356	648
7:30	0	1	0	0	1	1	0	0	0	6	1	6	0	0	325	2	0	327	0	10	338	0	1	348	682
7:45	0	0	0	1	1	1	0	0	0	8	1	8	0	0	326	4	0	330	0	9	306	1	0	316	655
<b>Total</b>	<b>0</b>	<b>4</b>	<b>0</b>	<b>3</b>	<b>3</b>	<b>7</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>26</b>	<b>2</b>	<b>26</b>	<b>0</b>	<b>2</b>	<b>1184</b>	<b>9</b>	<b>0</b>	<b>1195</b>	<b>0</b>	<b>41</b>	<b>1336</b>	<b>1</b>	<b>1</b>	<b>1378</b>	<b>2606</b>
Approach %	0.0	57.1	0.0	42.9	-	-	0.0	0.0	0.0	100.0	-	-	0.0	0.2	99.1	0.8	-	-	0.0	3.0	97.0	0.1	-	-	-
Total %	0.0	0.2	0.0	0.1	-	0.3	0.0	0.0	0.0	1.0	-	1.0	0.0	0.1	45.4	0.3	-	45.9	0.0	1.6	51.3	0.0	-	52.9	-
PHF	0.000	0.333	0.000	0.750	-	0.438	0.000	0.000	0.000	0.813	-	0.813	0.000	0.250	0.908	0.563	-	0.905	0.000	0.932	0.963	0.250	-	0.962	0.955
Lights	0	3	0	3	-	6	0	0	0	26	-	26	0	1	1154	9	-	1164	0	39	1292	1	-	1332	2528
% Lights	-	75.0	-	100.0	-	85.7	-	-	-	100.0	-	100.0	-	50.0	97.5	100.0	-	97.4	-	95.1	96.7	100.0	-	96.7	97.0
Other Vehicles	0	1	0	0	-	1	0	0	0	0	-	0	0	1	29	0	-	30	0	1	43	0	-	44	75
% Other Vehicles	-	25.0	-	0.0	-	14.3	-	-	-	0.0	-	0.0	-	50.0	2.4	0.0	-	2.5	-	2.4	3.2	0.0	-	3.2	2.9
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	1	0	-	1	0	1	1	0	-	2	3
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	-	-	0.0	-	0.0	-	0.0	0.1	0.0	-	0.1	-	2.4	0.1	0.0	-	0.1	0.1
Bicycles on Crosswalk	-	-	-	-	3	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	100.0	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	0.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



### Turning Movement Peak Hour Data (12:00)

Start Time	Driveway Eastbound						Business Driveway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	0	0	0	1	0	0	2	0	21	0	23	0	4	304	15	0	323	0	31	305	0	0	336	682
12:15	0	0	0	4	2	4	0	4	0	17	2	21	0	1	321	13	0	335	0	28	328	0	0	356	716
12:30	0	0	0	3	0	3	0	0	0	19	5	19	0	4	336	18	0	358	0	28	302	0	0	330	710
12:45	0	0	0	1	1	1	0	5	0	22	0	27	0	0	290	13	0	303	1	25	283	1	0	310	641
<b>Total</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>8</b>	<b>4</b>	<b>8</b>	<b>0</b>	<b>11</b>	<b>0</b>	<b>79</b>	<b>7</b>	<b>90</b>	<b>0</b>	<b>9</b>	<b>1251</b>	<b>59</b>	<b>0</b>	<b>1319</b>	<b>1</b>	<b>112</b>	<b>1218</b>	<b>1</b>	<b>0</b>	<b>1332</b>	<b>2749</b>
Approach %	0.0	0.0	0.0	100.0	-	-	0.0	12.2	0.0	87.8	-	-	0.0	0.7	94.8	4.5	-	-	0.1	8.4	91.4	0.1	-	-	-
Total %	0.0	0.0	0.0	0.3	-	0.3	0.0	0.4	0.0	2.9	-	3.3	0.0	0.3	45.5	2.1	-	48.0	0.0	4.1	44.3	0.0	-	48.5	-
PHF	0.000	0.000	0.000	0.500	-	0.500	0.000	0.550	0.000	0.898	-	0.833	0.000	0.563	0.931	0.819	-	0.921	0.250	0.903	0.928	0.250	-	0.935	0.960
Lights	0	0	0	8	-	8	0	11	0	77	-	88	0	9	1233	59	-	1301	1	112	1198	1	-	1312	2709
% Lights	-	-	-	100.0	-	100.0	-	100.0	-	97.5	-	97.8	-	100.0	98.6	100.0	-	98.6	100.0	100.0	98.4	100.0	-	98.5	98.5
Other Vehicles	0	0	0	0	-	0	0	0	0	1	-	1	0	0	18	0	-	18	0	0	20	0	-	20	39
% Other Vehicles	-	-	-	0.0	-	0.0	-	0.0	-	1.3	-	1.1	-	0.0	1.4	0.0	-	1.4	0.0	0.0	1.6	0.0	-	1.5	1.4
Bicycles on Road	0	0	0	0	-	0	0	0	0	1	-	1	0	0	0	0	-	0	0	0	0	0	-	0	1
% Bicycles on Road	-	-	-	0.0	-	0.0	-	0.0	-	1.3	-	1.1	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	1	-	-	-	-	-	5	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	25.0	-	-	-	-	-	71.4	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	3	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	75.0	-	-	-	-	-	28.6	-	-	-	-	-	-	-	-	-	-	-	-	-	-



Turning Movement Peak Hour Data Plot (12:00)



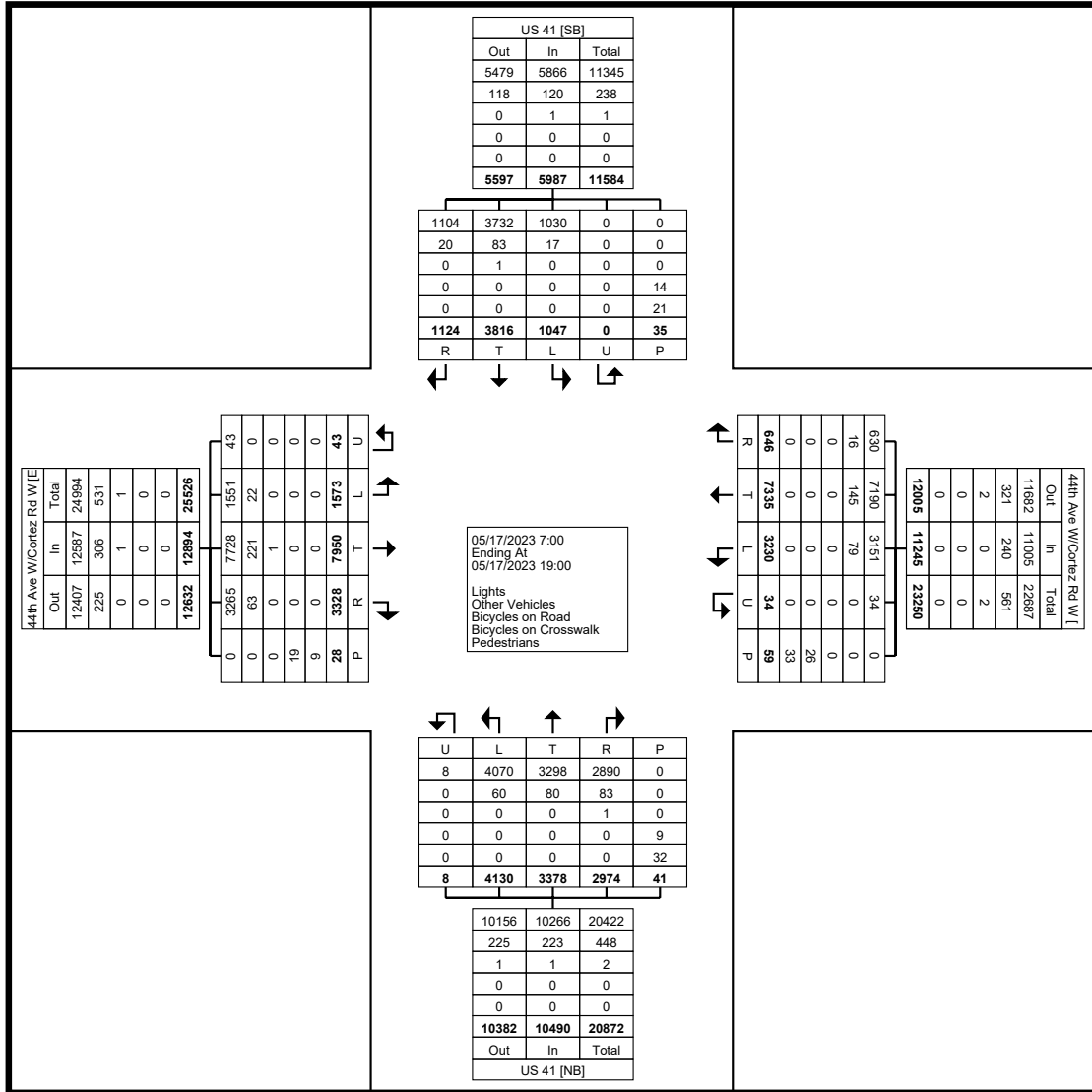
### Turning Movement Peak Hour Data (16:30)

Start Time	Driveway Eastbound						Business Driveway Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:30	0	1	0	1	4	2	0	1	0	12	1	13	1	6	350	20	0	377	0	5	330	1	0	336	728
16:45	0	0	0	1	6	1	0	0	0	7	1	7	0	6	355	12	0	373	0	14	328	0	0	342	723
17:00	0	1	0	1	4	2	0	0	0	17	0	17	0	1	371	14	1	386	0	15	310	1	0	326	731
17:15	0	0	0	2	3	2	0	1	0	15	1	16	0	6	344	16	3	366	0	14	324	2	0	340	724
<b>Total</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>5</b>	<b>17</b>	<b>7</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>51</b>	<b>3</b>	<b>53</b>	<b>1</b>	<b>19</b>	<b>1420</b>	<b>62</b>	<b>4</b>	<b>1502</b>	<b>0</b>	<b>48</b>	<b>1292</b>	<b>4</b>	<b>0</b>	<b>1344</b>	<b>2906</b>
Approach %	0.0	28.6	0.0	71.4	-	-	0.0	3.8	0.0	96.2	-	-	0.1	1.3	94.5	4.1	-	-	0.0	3.6	96.1	0.3	-	-	-
Total %	0.0	0.1	0.0	0.2	-	0.2	0.0	0.1	0.0	1.8	-	1.8	0.0	0.7	48.9	2.1	-	51.7	0.0	1.7	44.5	0.1	-	46.2	-
PHF	0.000	0.500	0.000	0.625	-	0.875	0.000	0.500	0.000	0.750	-	0.779	0.250	0.792	0.957	0.775	-	0.973	0.000	0.800	0.979	0.500	-	0.982	0.994
Lights	0	2	0	5	-	7	0	2	0	51	-	53	1	19	1398	60	-	1478	0	48	1276	4	-	1328	2866
% Lights	-	100.0	-	100.0	-	100.0	-	100.0	-	100.0	-	100.0	100.0	100.0	98.5	96.8	-	98.4	-	100.0	98.8	100.0	-	98.8	98.6
Other Vehicles	0	0	0	0	-	0	0	0	0	0	-	0	0	0	22	2	-	24	0	0	16	0	-	16	40
% Other Vehicles	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	1.5	3.2	-	1.6	-	0.0	1.2	0.0	-	1.2	1.4
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	8	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	47.1	-	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	9	-	-	-	-	-	3	-	-	-	-	-	4	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	52.9	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-



### Turning Movement Data

Start Time	44th Ave W/Cortez Rd W Eastbound						44th Ave W/Cortez Rd W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	34	200	96	0	330	0	113	180	11	0	304	0	99	89	73	0	261	0	24	156	26	1	206	1101
7:15	0	40	266	118	2	424	0	99	227	3	2	329	0	127	79	74	0	280	0	25	144	44	0	213	1246
7:30	0	26	295	104	1	425	1	108	260	12	3	381	0	74	106	95	0	275	0	27	135	29	2	191	1272
7:45	1	50	257	84	3	392	0	125	241	15	0	381	0	148	113	122	0	383	0	28	112	38	0	178	1334
Hourly Total	1	150	1018	402	6	1571	1	445	908	41	5	1395	0	448	387	364	0	1199	0	104	547	137	3	788	4953
8:00	1	51	248	118	0	418	0	96	275	15	0	386	0	134	90	71	3	295	0	25	112	40	3	177	1276
8:15	0	45	220	108	0	373	0	125	250	15	7	390	0	78	106	84	0	268	0	27	138	42	0	207	1238
8:30	0	44	208	84	1	336	0	105	190	8	0	303	0	128	72	78	2	278	0	22	124	50	0	196	1113
8:45	2	48	204	113	1	367	0	128	234	16	0	378	0	102	87	83	2	272	0	25	101	37	0	163	1180
Hourly Total	3	188	880	423	2	1494	0	454	949	54	7	1457	0	442	355	316	7	1113	0	99	475	169	3	743	4807
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	42	244	130	0	416	3	119	240	17	3	379	0	98	101	105	0	304	0	31	93	35	1	159	1258
12:15	1	50	218	107	0	376	1	67	186	17	2	271	0	134	99	98	0	331	0	44	136	28	1	208	1186
12:30	0	49	265	112	0	426	6	101	237	26	1	370	0	101	102	106	2	309	0	33	92	43	1	168	1273
12:45	1	55	222	105	2	383	6	96	192	28	3	322	1	135	85	101	0	322	0	39	135	29	0	203	1230
Hourly Total	2	196	949	454	2	1601	16	383	855	88	9	1342	1	468	387	410	2	1266	0	147	456	135	3	738	4947
13:00	1	36	255	107	0	399	1	107	213	26	0	347	1	117	113	118	0	349	0	36	97	24	0	157	1252
13:15	1	47	194	109	1	351	1	84	180	25	2	290	1	151	110	101	0	363	0	42	121	43	2	206	1210
13:30	5	32	295	103	0	435	3	69	219	25	4	316	2	119	110	98	0	329	0	38	99	35	2	172	1252
13:45	5	48	240	111	0	404	1	68	179	16	2	264	1	150	105	98	1	354	0	30	107	34	0	171	1193
Hourly Total	12	163	984	430	1	1589	6	328	791	92	8	1217	5	537	438	415	1	1395	0	146	424	136	4	706	4907
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	55	274	91	2	420	0	87	191	22	2	300	0	118	141	121	5	380	0	33	122	27	1	182	1282
15:15	1	52	316	131	3	500	0	81	249	18	1	348	0	142	107	95	1	344	0	31	90	37	3	158	1350
15:30	0	57	263	105	1	425	0	104	203	28	2	335	0	130	110	86	6	326	0	44	114	39	0	197	1283
15:45	0	51	288	103	2	442	0	103	259	29	5	391	0	144	85	92	0	321	0	35	105	32	1	172	1326
Hourly Total	1	215	1141	430	8	1787	0	375	902	97	10	1374	0	534	443	394	12	1371	0	143	431	135	5	709	5241
16:00	2	57	216	101	1	376	0	100	231	21	0	352	0	115	136	113	1	364	0	37	121	37	1	195	1287
16:15	2	60	253	96	1	411	0	97	265	24	0	386	0	156	113	83	1	352	0	37	104	19	0	160	1309
16:30	2	53	294	78	1	427	0	109	230	27	2	366	0	155	128	92	0	375	0	34	167	46	0	247	1415
16:45	3	55	270	109	1	437	0	87	236	16	3	339	0	168	109	81	2	358	0	31	114	37	2	182	1316
Hourly Total	9	225	1033	384	4	1651	0	393	962	88	5	1443	0	594	486	369	4	1449	0	139	506	139	3	784	5327
17:00	0	63	246	95	1	404	0	128	228	26	2	382	0	132	126	87	0	345	0	45	128	46	3	219	1350
17:15	0	73	256	101	0	430	0	94	318	17	6	429	0	150	85	95	2	330	0	26	118	39	3	183	1372
17:30	1	55	267	106	1	429	0	138	256	26	1	420	0	145	126	96	1	367	0	26	152	42	2	220	1436
17:45	2	65	274	104	0	445	0	124	286	21	2	431	0	170	109	78	0	357	0	32	119	19	0	170	1403
Hourly Total	3	256	1043	406	2	1708	0	484	1088	90	11	1662	0	597	446	356	3	1399	0	129	517	146	8	792	5561
18:00	3	58	232	108	1	401	2	120	204	31	1	357	0	135	125	90	6	350	0	35	128	39	1	202	1310
18:15	6	43	231	111	1	391	1	76	227	20	1	324	1	134	98	101	2	334	0	43	98	34	2	175	1224
18:30	2	30	231	90	1	353	2	88	184	22	1	296	0	102	115	84	3	301	0	34	130	26	1	190	1140
18:45	1	49	208	90	0	348	6	84	265	23	1	378	1	139	98	75	1	313	0	28	104	28	2	160	1199
Hourly Total	12	180	902	399	3	1493	11	368	880	96	4	1355	2	510	436	350	12	1298	0	140	460	127	6	727	4873
Grand Total	43	1573	7950	3328	28	12894	34	3230	7335	646	59	11245	8	4130	3378	2974	41	10490	0	1047	3816	1124	35	5987	40616
Approach %	0.3	12.2	61.7	25.8	-	-	0.3	28.7	65.2	5.7	-	-	0.1	39.4	32.2	28.4	-	-	0.0	17.5	63.7	18.8	-	-	-
Total %	0.1	3.9	19.6	8.2	-	31.7	0.1	8.0	18.1	1.6	-	27.7	0.0	10.2	8.3	7.3	-	25.8	0.0	2.6	9.4	2.8	-	14.7	-
Lights	43	1551	7728	3265	-	12587	34	3151	7190	630	-	11005	8	4070	3298	2890	-	10266	0	1030	3732	1104	-	5866	39724
% Lights	100.0	98.6	97.2	98.1	-	97.6	100.0	97.6	98.0	97.5	-	97.9	100.0	98.5	97.6	97.2	-	97.9	-	98.4	97.8	98.2	-	98.0	97.8
Other Vehicles	0	22	221	63	-	306	0	79	145	16	-	240	0	60	80	83	-	223	0	17	83	20	-	120	889
% Other Vehicles	0.0	1.4	2.8	1.9	-	2.4	0.0	2.4	2.0	2.5	-	2.1	0.0	1.5	2.4	2.8	-	2.1	-	1.6	2.2	1.8	-	2.0	2.2
Bicycles on Road	0	0	1	0	-	1	0	0	0	0	-	0	0	0	0	1	-	1	0	0	1	0	-	1	3
% Bicycles on Road	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	19	-	-	-	-	26	-	-	-	-	-	9	-	-	-	-	-	-	14	-	-
% Bicycles on Crosswalk	-	-	-	-	67.9	-	-	-	-	44.1	-	-	-	-	-	22.0	-	-	-	-	-	-	40.0	-	-
Pedestrians	-	-	-	-	9	-	-	-	-	33	-	-	-	-	-	32	-	-	-	-	-	-	21	-	-
% Pedestrians	-	-	-	-	32.1	-	-	-	-	55.9	-	-	-	-	-	78.0	-	-	-	-	-	-	60.0	-	-



Turning Movement Data Plot

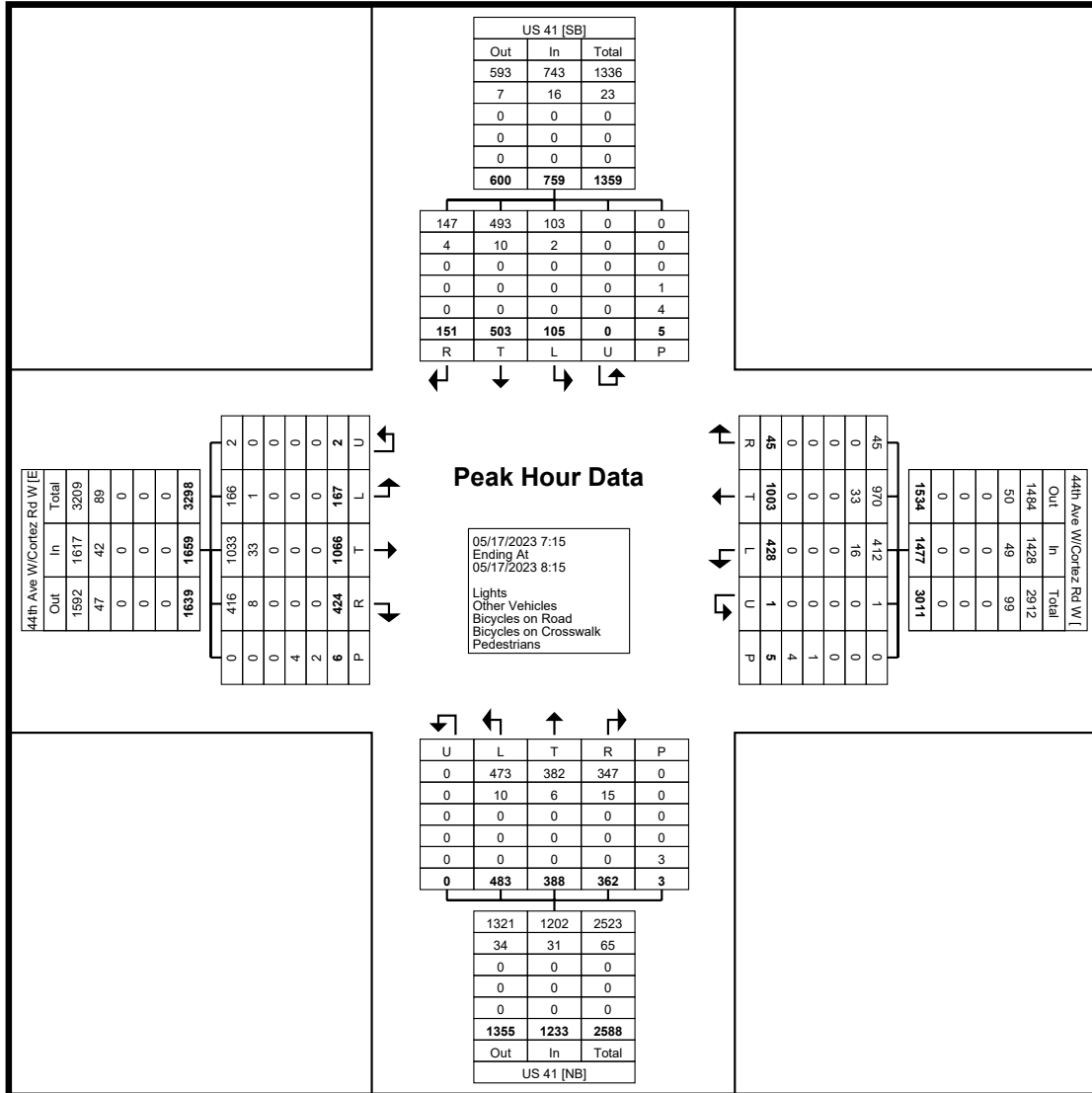
US 41 at 44th Ave W/Cortez Rd  
W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 32 US 41 at 44th  
Ave W/Cortez Rd W  
Site Code: 32  
Start Date: 05/17/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:15)

Start Time	44th Ave W/Cortez Rd W Eastbound						44th Ave W/Cortez Rd W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15	0	40	266	118	2	424	0	99	227	3	2	329	0	127	79	74	0	280	0	25	144	44	0	213	1246
7:30	0	26	295	104	1	425	1	108	260	12	3	381	0	74	106	95	0	275	0	27	135	29	2	191	1272
7:45	1	50	257	84	3	392	0	125	241	15	0	381	0	148	113	122	0	383	0	28	112	38	0	178	1334
8:00	1	51	248	118	0	418	0	96	275	15	0	386	0	134	90	71	3	295	0	25	112	40	3	177	1276
<b>Total</b>	<b>2</b>	<b>167</b>	<b>1066</b>	<b>424</b>	<b>6</b>	<b>1659</b>	<b>1</b>	<b>428</b>	<b>1003</b>	<b>45</b>	<b>5</b>	<b>1477</b>	<b>0</b>	<b>483</b>	<b>388</b>	<b>362</b>	<b>3</b>	<b>1233</b>	<b>0</b>	<b>105</b>	<b>503</b>	<b>151</b>	<b>5</b>	<b>759</b>	<b>5128</b>
Approach %	0.1	10.1	64.3	25.6	-	-	0.1	29.0	67.9	3.0	-	-	0.0	39.2	31.5	29.4	-	-	0.0	13.8	66.3	19.9	-	-	-
Total %	0.0	3.3	20.8	8.3	-	32.4	0.0	8.3	19.6	0.9	-	28.8	0.0	9.4	7.6	7.1	-	24.0	0.0	2.0	9.8	2.9	-	14.8	-
PHF	0.500	0.819	0.903	0.898	-	0.976	0.250	0.856	0.912	0.750	-	0.957	0.000	0.816	0.858	0.742	-	0.805	0.000	0.938	0.873	0.858	-	0.891	0.961
Lights	2	166	1033	416	-	1617	1	412	970	45	-	1428	0	473	382	347	-	1202	0	103	493	147	-	743	4990
% Lights	100.0	99.4	96.9	98.1	-	97.5	100.0	96.3	96.7	100.0	-	96.7	-	97.9	98.5	95.9	-	97.5	-	98.1	98.0	97.4	-	97.9	97.3
Other Vehicles	0	1	33	8	-	42	0	16	33	0	-	49	0	10	6	15	-	31	0	2	10	4	-	16	138
% Other Vehicles	0.0	0.6	3.1	1.9	-	2.5	0.0	3.7	3.3	0.0	-	3.3	-	2.1	1.5	4.1	-	2.5	-	1.9	2.0	2.6	-	2.1	2.7
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	4	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	1	-	-
% Bicycles on Crosswalk	-	-	-	-	66.7	-	-	-	-	-	20.0	-	-	-	-	-	0.0	-	-	-	-	-	20.0	-	-
Pedestrians	-	-	-	-	2	-	-	-	-	-	4	-	-	-	-	-	3	-	-	-	-	-	4	-	-
% Pedestrians	-	-	-	-	33.3	-	-	-	-	-	80.0	-	-	-	-	-	100.0	-	-	-	-	-	80.0	-	-



Turning Movement Peak Hour Data Plot (7:15)

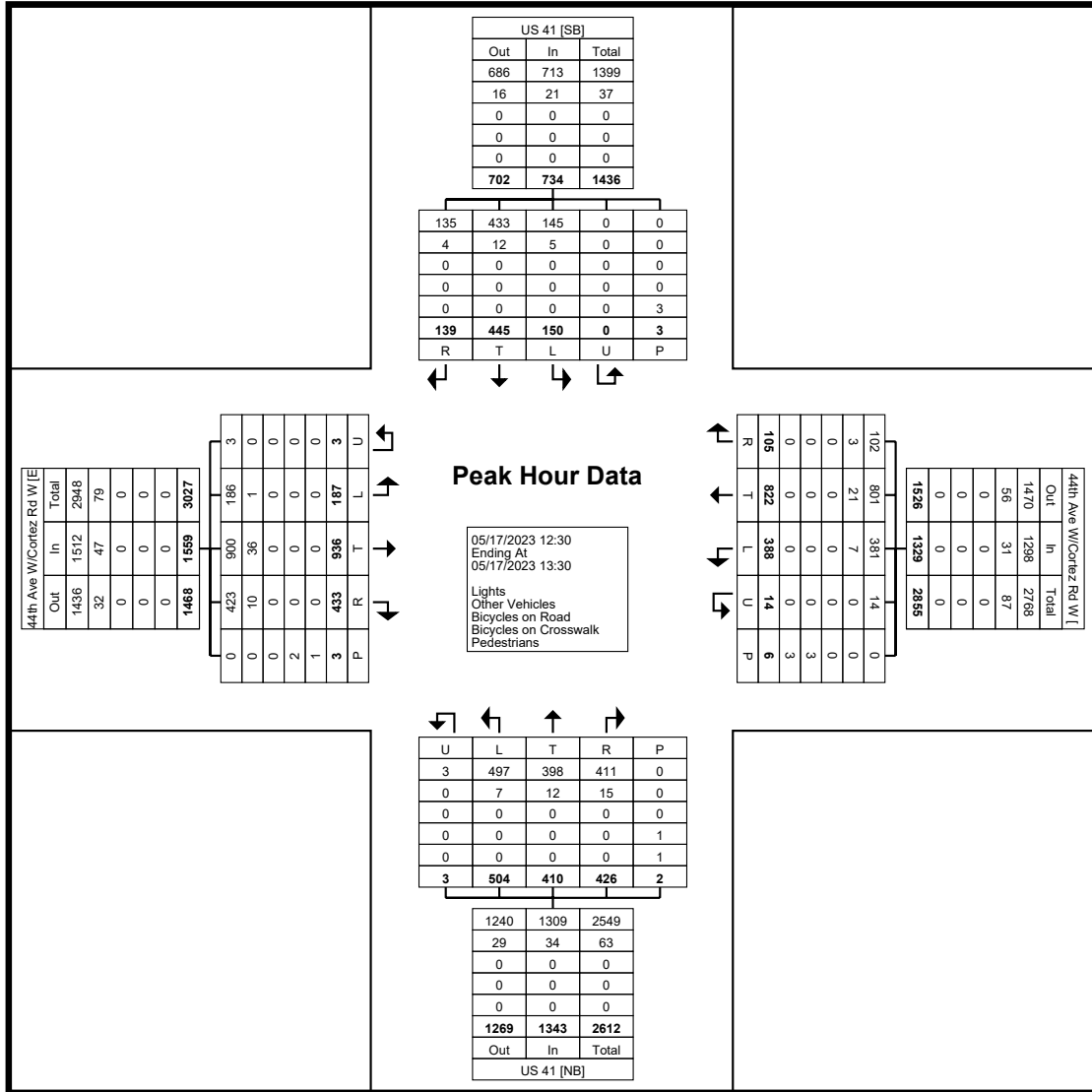
US 41 at 44th Ave W/Cortez Rd W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 32 US 41 at 44th Ave W/Cortez Rd W  
Site Code: 32  
Start Date: 05/17/2023  
Page No: 5

### Turning Movement Peak Hour Data (12:30)

Start Time	44th Ave W/Cortez Rd W Eastbound						44th Ave W/Cortez Rd W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:30	0	49	265	112	0	426	6	101	237	26	1	370	0	101	102	106	2	309	0	33	92	43	1	168	1273
12:45	1	55	222	105	2	383	6	96	192	28	3	322	1	135	85	101	0	322	0	39	135	29	0	203	1230
13:00	1	36	255	107	0	399	1	107	213	26	0	347	1	117	113	118	0	349	0	36	97	24	0	157	1252
13:15	1	47	194	109	1	351	1	84	180	25	2	290	1	151	110	101	0	363	0	42	121	43	2	206	1210
<b>Total</b>	<b>3</b>	<b>187</b>	<b>936</b>	<b>433</b>	<b>3</b>	<b>1559</b>	<b>14</b>	<b>388</b>	<b>822</b>	<b>105</b>	<b>6</b>	<b>1329</b>	<b>3</b>	<b>504</b>	<b>410</b>	<b>426</b>	<b>2</b>	<b>1343</b>	<b>0</b>	<b>150</b>	<b>445</b>	<b>139</b>	<b>3</b>	<b>734</b>	<b>4965</b>
Approach %	0.2	12.0	60.0	27.8	-	-	1.1	29.2	61.9	7.9	-	-	0.2	37.5	30.5	31.7	-	-	0.0	20.4	60.6	18.9	-	-	-
Total %	0.1	3.8	18.9	8.7	-	31.4	0.3	7.8	16.6	2.1	-	26.8	0.1	10.2	8.3	8.6	-	27.0	0.0	3.0	9.0	2.8	-	14.8	-
PHF	0.750	0.850	0.883	0.967	-	0.915	0.583	0.907	0.867	0.938	-	0.898	0.750	0.834	0.907	0.903	-	0.925	0.000	0.893	0.824	0.808	-	0.891	0.975
Lights	3	186	900	423	-	1512	14	381	801	102	-	1298	3	497	398	411	-	1309	0	145	433	135	-	713	4832
% Lights	100.0	99.5	96.2	97.7	-	97.0	100.0	98.2	97.4	97.1	-	97.7	100.0	98.6	97.1	96.5	-	97.5	-	96.7	97.3	97.1	-	97.1	97.3
Other Vehicles	0	1	36	10	-	47	0	7	21	3	-	31	0	7	12	15	-	34	0	5	12	4	-	21	133
% Other Vehicles	0.0	0.5	3.8	2.3	-	3.0	0.0	1.8	2.6	2.9	-	2.3	0.0	1.4	2.9	3.5	-	2.5	-	3.3	2.7	2.9	-	2.9	2.7
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	2	-	-	-	-	-	3	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	66.7	-	-	-	-	-	50.0	-	-	-	-	-	50.0	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	3	-	-	-	-	-	1	-	-	-	-	-	3	-	-
% Pedestrians	-	-	-	-	33.3	-	-	-	-	-	50.0	-	-	-	-	-	50.0	-	-	-	-	-	100.0	-	-



Turning Movement Peak Hour Data Plot (12:30)



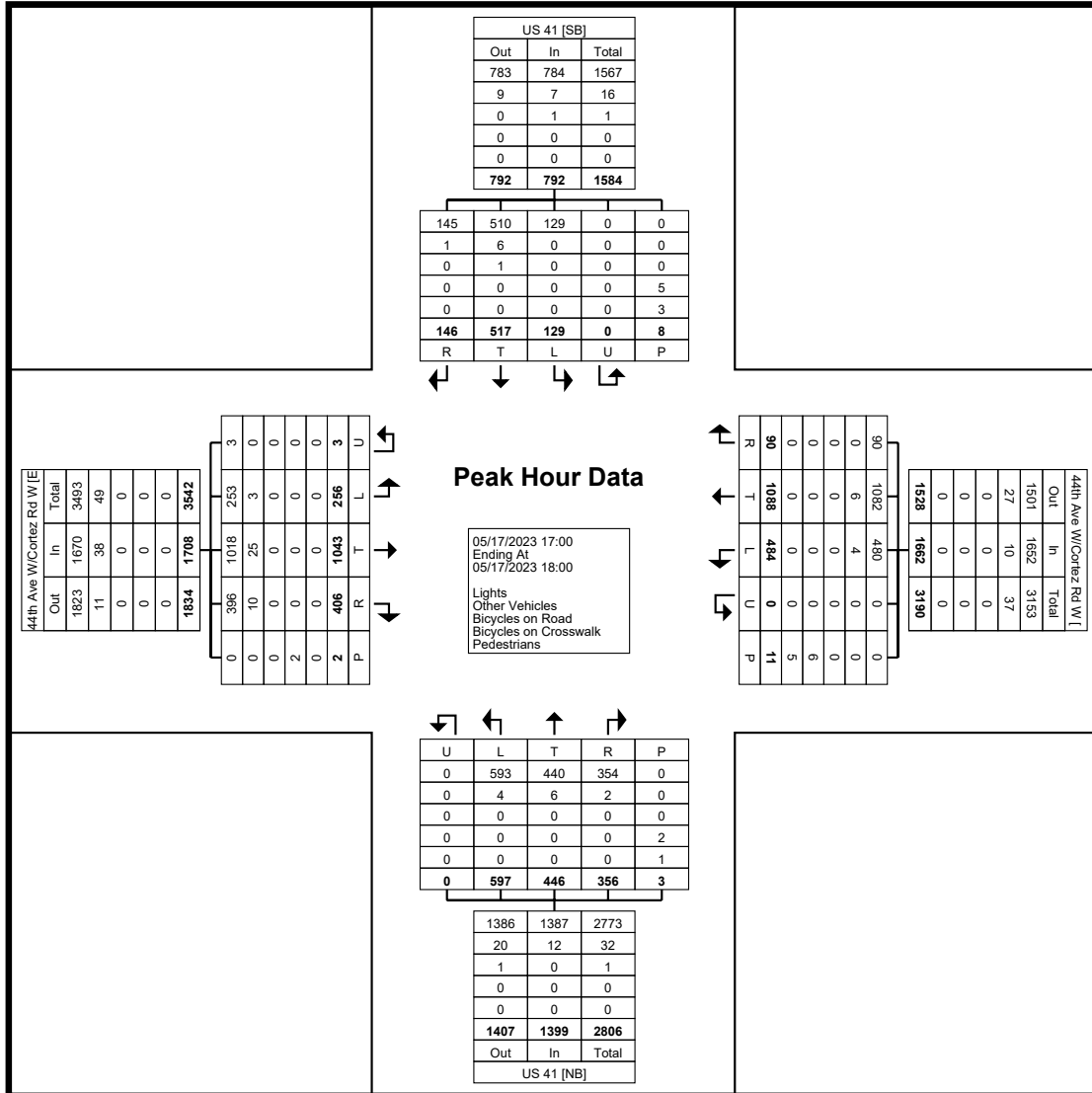
US 41 at 44th Ave W/Cortez Rd W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 32 US 41 at 44th Ave W/Cortez Rd W  
Site Code: 32  
Start Date: 05/17/2023  
Page No: 7

### Turning Movement Peak Hour Data (17:00)

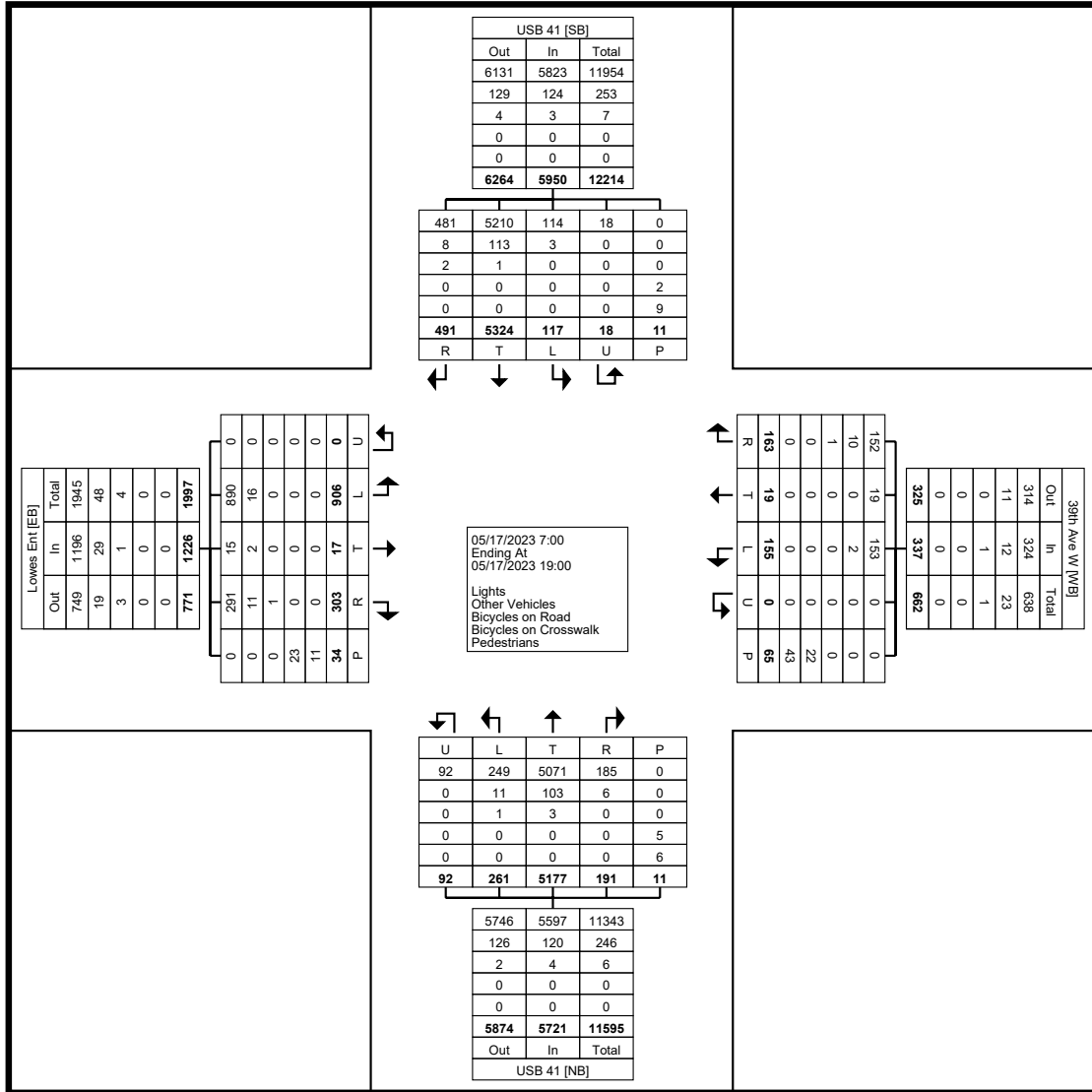
Start Time	44th Ave W/Cortez Rd W Eastbound						44th Ave W/Cortez Rd W Westbound						US 41 Northbound						US 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
17:00	0	63	246	95	1	404	0	128	228	26	2	382	0	132	126	87	0	345	0	45	128	46	3	219	1350
17:15	0	73	256	101	0	430	0	94	318	17	6	429	0	150	85	95	2	330	0	26	118	39	3	183	1372
17:30	1	55	267	106	1	429	0	138	256	26	1	420	0	145	126	96	1	367	0	26	152	42	2	220	1436
17:45	2	65	274	104	0	445	0	124	286	21	2	431	0	170	109	78	0	357	0	32	119	19	0	170	1403
<b>Total</b>	<b>3</b>	<b>256</b>	<b>1043</b>	<b>406</b>	<b>2</b>	<b>1708</b>	<b>0</b>	<b>484</b>	<b>1088</b>	<b>90</b>	<b>11</b>	<b>1662</b>	<b>0</b>	<b>597</b>	<b>446</b>	<b>356</b>	<b>3</b>	<b>1399</b>	<b>0</b>	<b>129</b>	<b>517</b>	<b>146</b>	<b>8</b>	<b>792</b>	<b>5561</b>
Approach %	0.2	15.0	61.1	23.8	-	-	0.0	29.1	65.5	5.4	-	-	0.0	42.7	31.9	25.4	-	-	0.0	16.3	65.3	18.4	-	-	-
Total %	0.1	4.6	18.8	7.3	-	30.7	0.0	8.7	19.6	1.6	-	29.9	0.0	10.7	8.0	6.4	-	25.2	0.0	2.3	9.3	2.6	-	14.2	-
PHF	0.375	0.877	0.952	0.958	-	0.960	0.000	0.877	0.855	0.865	-	0.964	0.000	0.878	0.885	0.927	-	0.953	0.000	0.717	0.850	0.793	-	0.900	0.968
Lights	3	253	1018	396	-	1670	0	480	1082	90	-	1652	0	593	440	354	-	1387	0	129	510	145	-	784	5493
% Lights	100.0	98.8	97.6	97.5	-	97.8	-	99.2	99.4	100.0	-	99.4	-	99.3	98.7	99.4	-	99.1	-	100.0	98.6	99.3	-	99.0	98.8
Other Vehicles	0	3	25	10	-	38	0	4	6	0	-	10	0	4	6	2	-	12	0	0	6	1	-	7	67
% Other Vehicles	0.0	1.2	2.4	2.5	-	2.2	-	0.8	0.6	0.0	-	0.6	-	0.7	1.3	0.6	-	0.9	-	0.0	1.2	0.7	-	0.9	1.2
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	1	0	-	1	1
% Bicycles on Road	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.2	0.0	-	0.1	0.0
Bicycles on Crosswalk	-	-	-	-	2	-	-	-	-	-	6	-	-	-	-	-	2	-	-	-	-	-	5	-	-
% Bicycles on Crosswalk	-	-	-	-	100.0	-	-	-	-	-	54.5	-	-	-	-	-	66.7	-	-	-	-	-	62.5	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	5	-	-	-	-	-	1	-	-	-	-	-	3	-	-
% Pedestrians	-	-	-	-	0.0	-	-	-	-	-	45.5	-	-	-	-	-	33.3	-	-	-	-	-	37.5	-	-



Turning Movement Peak Hour Data Plot (17:00)

### Turning Movement Data

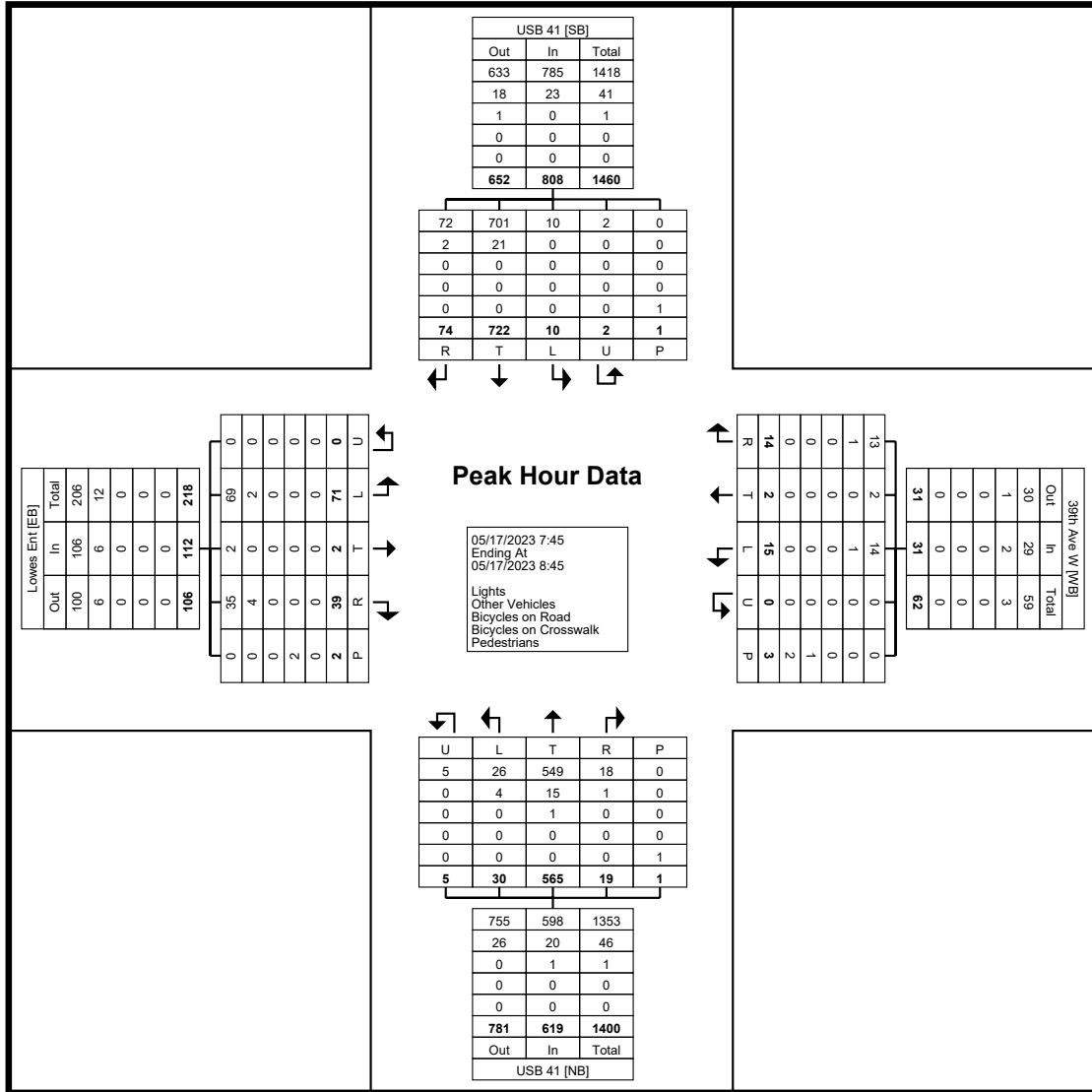
Start Time	Lowes Ent Eastbound						39th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	10	0	15	0	25	0	2	1	4	3	7	2	5	108	4	0	119	0	0	184	9	0	193	344
7:15	0	7	0	13	1	20	0	5	0	6	3	11	0	3	133	3	0	139	0	3	206	11	0	220	390
7:30	0	18	0	7	0	25	0	8	0	10	3	18	1	2	135	2	0	140	0	5	178	2	0	185	368
7:45	0	20	0	6	2	26	0	4	0	7	0	11	0	10	156	5	1	171	0	4	150	23	1	177	385
Hourly Total	0	55	0	41	3	96	0	19	1	27	9	47	3	20	532	14	1	569	0	12	718	45	1	775	1487
8:00	0	15	0	11	0	26	0	4	1	4	1	9	1	4	140	7	0	152	0	3	206	18	0	227	414
8:15	0	20	1	9	0	30	0	2	1	1	2	4	2	9	142	4	0	157	1	1	172	16	0	190	381
8:30	0	16	1	13	0	30	0	5	0	2	0	7	2	7	127	3	0	139	1	2	194	17	0	214	390
8:45	0	24	0	7	0	31	0	5	0	2	2	7	4	5	132	4	1	145	2	3	149	16	1	170	353
Hourly Total	0	75	2	40	0	117	0	16	2	9	5	27	9	25	541	18	1	593	4	9	721	67	1	801	1538
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	40	0	6	1	46	0	6	0	9	3	15	5	9	128	4	0	146	0	4	155	15	0	174	381
12:15	0	34	0	11	0	45	0	2	0	4	1	6	2	6	158	9	0	175	0	5	170	21	0	196	422
12:30	0	38	0	13	1	51	0	7	3	4	1	14	3	8	149	6	0	166	0	0	162	17	1	179	410
12:45	0	42	2	13	0	57	0	3	1	7	4	11	7	10	164	7	3	188	1	2	145	20	1	168	424
Hourly Total	0	154	2	43	2	199	0	18	4	24	9	46	17	33	599	26	3	675	1	11	632	73	2	717	1637
13:00	0	28	1	10	0	39	0	6	1	2	3	9	3	9	151	6	0	169	0	7	143	19	3	169	386
13:15	0	23	0	14	0	37	0	5	1	3	1	9	6	8	184	8	0	206	0	2	155	17	0	174	426
13:30	0	33	1	11	1	45	0	9	1	4	1	14	2	11	144	6	0	163	4	7	149	17	0	177	399
13:45	0	25	1	11	1	37	0	3	1	1	5	5	3	11	148	13	0	175	0	0	152	14	0	166	383
Hourly Total	0	109	3	46	2	158	0	23	4	10	10	37	14	39	627	33	0	713	4	16	599	67	3	686	1594
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	31	2	11	2	44	0	6	1	9	2	16	5	13	189	5	0	212	1	5	139	10	2	155	427
15:15	0	17	0	6	3	23	0	6	0	7	1	13	3	10	171	6	1	190	0	3	169	16	0	188	414
15:30	0	44	0	10	2	54	0	5	1	2	1	8	2	6	156	10	0	174	0	9	158	13	0	180	416
15:45	0	23	1	9	0	33	0	9	0	7	3	16	4	11	171	12	0	198	4	6	180	11	0	201	448
Hourly Total	0	115	3	36	7	154	0	26	2	25	7	53	14	40	687	33	1	774	5	23	646	50	2	724	1705
16:00	0	35	1	18	1	54	0	3	0	7	3	10	1	13	196	8	1	218	0	5	148	18	0	171	453
16:15	0	31	1	5	3	37	0	4	0	6	4	10	2	12	186	8	0	208	0	5	159	20	0	184	439
16:30	0	30	1	8	2	39	0	4	0	8	3	12	1	11	202	7	0	221	2	4	174	10	0	190	462
16:45	0	26	1	9	2	36	0	4	0	5	0	9	5	4	184	5	0	198	0	1	194	15	0	210	453
Hourly Total	0	122	4	40	8	166	0	15	0	26	10	41	9	40	768	28	1	845	2	15	675	63	0	755	1807
17:00	0	43	1	5	3	49	0	8	1	8	1	17	3	6	209	3	0	221	1	5	176	12	0	194	481
17:15	0	36	1	9	3	46	0	6	0	3	3	9	8	5	169	11	2	193	0	6	182	10	0	198	446
17:30	0	31	0	6	3	37	0	4	1	7	1	12	3	8	183	3	0	197	0	4	194	23	1	221	467
17:45	0	29	0	12	2	41	0	5	1	5	1	11	2	9	192	5	0	208	0	5	174	22	0	201	461
Hourly Total	0	139	2	32	11	173	0	23	3	23	6	49	16	28	753	22	2	819	1	20	726	67	1	814	1855
18:00	0	35	1	4	0	40	0	1	1	1	1	3	5	6	201	4	0	216	0	3	169	12	0	184	443
18:15	0	30	0	7	0	37	0	6	0	3	3	9	3	10	153	2	0	168	1	1	139	20	0	161	375
18:30	0	42	0	7	1	49	0	6	1	8	2	15	0	9	154	6	1	169	0	5	161	14	0	180	413
18:45	0	30	0	7	0	37	0	2	1	7	3	10	2	11	162	5	1	180	0	2	138	13	1	153	380
Hourly Total	0	137	1	25	1	163	0	15	3	19	9	37	10	36	670	17	2	733	1	11	607	59	1	678	1611
Grand Total	0	906	17	303	34	1226	0	155	19	163	65	337	92	261	5177	191	11	5721	18	117	5324	491	11	5950	13234
Approach %	0.0	73.9	1.4	24.7	-	-	0.0	46.0	5.6	48.4	-	-	1.6	4.6	90.5	3.3	-	-	0.3	2.0	89.5	8.3	-	-	-
Total %	0.0	6.8	0.1	2.3	-	9.3	0.0	1.2	0.1	1.2	-	2.5	0.7	2.0	39.1	1.4	-	43.2	0.1	0.9	40.2	3.7	-	45.0	-
Lights	0	890	15	291	-	1196	0	153	19	152	-	324	92	249	5071	185	-	5597	18	114	5210	481	-	5823	12940
% Lights	-	98.2	88.2	96.0	-	97.6	-	98.7	100.0	93.3	-	96.1	100.0	95.4	98.0	96.9	-	97.8	100.0	97.4	97.9	98.0	-	97.9	97.8
Other Vehicles	0	16	2	11	-	29	0	2	0	10	-	12	0	11	103	6	-	120	0	3	113	8	-	124	285
% Other Vehicles	-	1.8	11.8	3.6	-	2.4	-	1.3	0.0	6.1	-	3.6	0.0	4.2	2.0	3.1	-	2.1	0.0	2.6	2.1	1.6	-	2.1	2.2
Bicycles on Road	0	0	0	1	-	1	0	0	0	1	-	1	0	1	3	0	-	4	0	0	1	2	-	3	9
% Bicycles on Road	-	0.0	0.0	0.3	-	0.1	-	0.0	0.0	0.6	-	0.3	0.0	0.4	0.1	0.0	-	0.1	0.0	0.0	0.4	-	0.1	0.1	0.1
Bicycles on Crosswalk	-	-	-	-	23	-	-	-	-	-	22	-	-	-	-	-	5	-	-	-	-	-	2	-	-
% Bicycles on Crosswalk	-	-	-	-	67.6	-	-	-	-	-	33.8	-	-	-	-	-	45.5	-	-	-	-	-	18.2	-	-
Pedestrians	-	-	-	-	11	-	-	-	-	-	43	-	-	-	-	-	6	-	-	-	-	-	9	-	-
% Pedestrians	-	-	-	-	32.4	-	-	-	-	-	66.2	-	-	-	-	-	54.5	-	-	-	-	-	81.8	-	-



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:45)

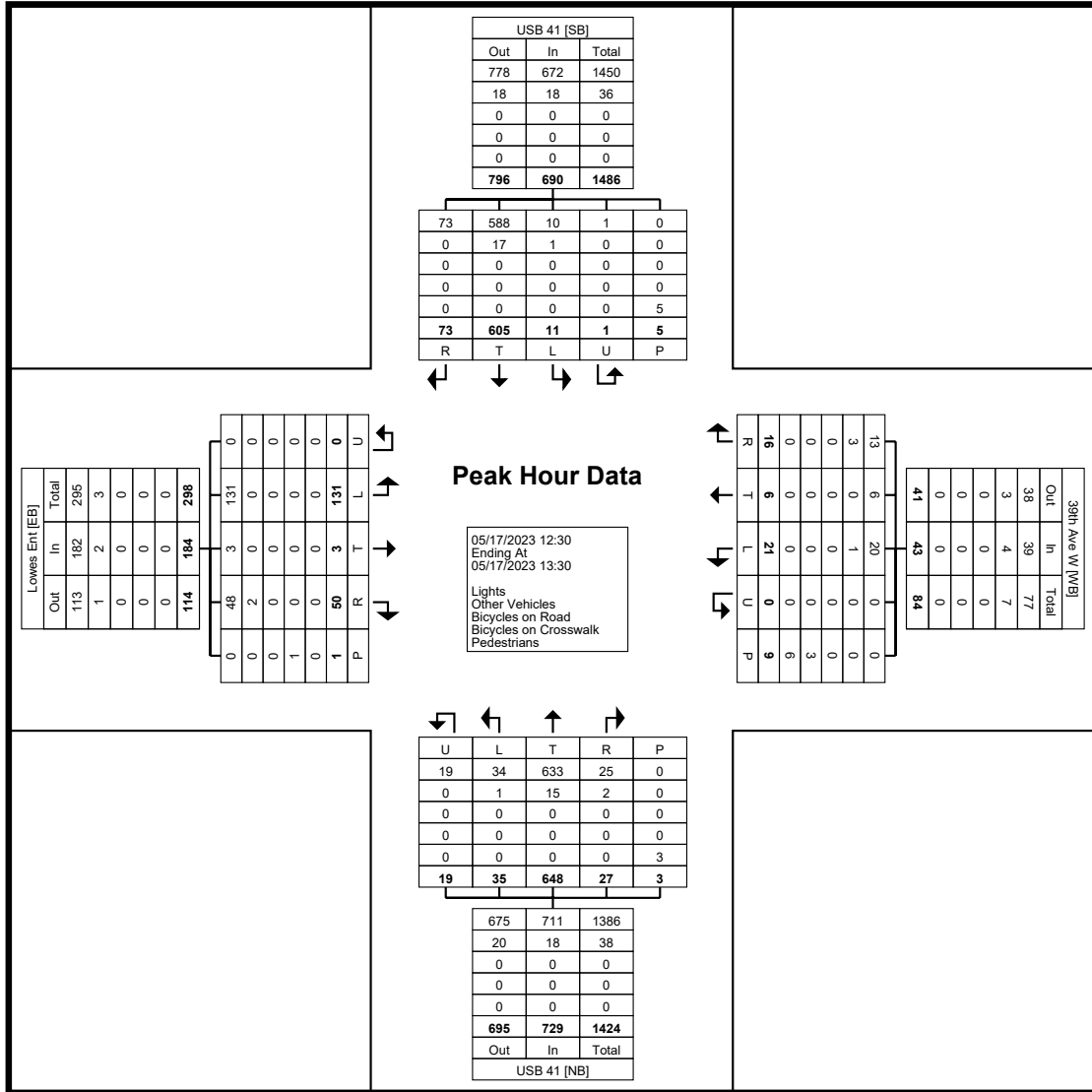
Start Time	Lowe's Ent Eastbound						39th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:45	0	20	0	6	2	26	0	4	0	7	0	11	0	10	156	5	1	171	0	4	150	23	1	177	385
8:00	0	15	0	11	0	26	0	4	1	4	1	9	1	4	140	7	0	152	0	3	206	18	0	227	414
8:15	0	20	1	9	0	30	0	2	1	1	2	4	2	9	142	4	0	157	1	1	172	16	0	190	381
8:30	0	16	1	13	0	30	0	5	0	2	0	7	2	7	127	3	0	139	1	2	194	17	0	214	390
<b>Total</b>	<b>0</b>	<b>71</b>	<b>2</b>	<b>39</b>	<b>2</b>	<b>112</b>	<b>0</b>	<b>15</b>	<b>2</b>	<b>14</b>	<b>3</b>	<b>31</b>	<b>5</b>	<b>30</b>	<b>565</b>	<b>19</b>	<b>1</b>	<b>619</b>	<b>2</b>	<b>10</b>	<b>722</b>	<b>74</b>	<b>1</b>	<b>808</b>	<b>1570</b>
Approach %	0.0	63.4	1.8	34.8	-	-	0.0	48.4	6.5	45.2	-	-	0.8	4.8	91.3	3.1	-	-	0.2	1.2	89.4	9.2	-	-	-
Total %	0.0	4.5	0.1	2.5	-	7.1	0.0	1.0	0.1	0.9	-	2.0	0.3	1.9	36.0	1.2	-	39.4	0.1	0.6	46.0	4.7	-	51.5	-
PHF	0.000	0.888	0.500	0.750	-	0.933	0.000	0.750	0.500	0.500	-	0.705	0.625	0.750	0.905	0.679	-	0.905	0.500	0.625	0.876	0.804	-	0.890	0.948
Lights	0	69	2	35	-	106	0	14	2	13	-	29	5	26	549	18	-	598	2	10	701	72	-	785	1518
% Lights	-	97.2	100.0	89.7	-	94.6	-	93.3	100.0	92.9	-	93.5	100.0	86.7	97.2	94.7	-	96.6	100.0	100.0	97.1	97.3	-	97.2	96.7
Other Vehicles	0	2	0	4	-	6	0	1	0	1	-	2	0	4	15	1	-	20	0	0	21	2	-	23	51
% Other Vehicles	-	2.8	0.0	10.3	-	5.4	-	6.7	0.0	7.1	-	6.5	0.0	13.3	2.7	5.3	-	3.2	0.0	0.0	2.9	2.7	-	2.8	3.2
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	1	0	-	1	0	0	0	0	-	0	1
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.2	0.0	-	0.2	0.0	0.0	0.0	0.0	-	0.0	0.1
Bicycles on Crosswalk	-	-	-	-	2	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	100.0	-	-	-	-	-	33.3	-	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	2	-	-	-	-	-	1	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	0.0	-	-	-	-	-	66.7	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-



Turning Movement Peak Hour Data Plot (7:45)

### Turning Movement Peak Hour Data (12:30)

Start Time	Lowe's Ent Eastbound						39th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:30	0	38	0	13	1	51	0	7	3	4	1	14	3	8	149	6	0	166	0	0	162	17	1	179	410
12:45	0	42	2	13	0	57	0	3	1	7	4	11	7	10	164	7	3	188	1	2	145	20	1	168	424
13:00	0	28	1	10	0	39	0	6	1	2	3	9	3	9	151	6	0	169	0	7	143	19	3	169	386
13:15	0	23	0	14	0	37	0	5	1	3	1	9	6	8	184	8	0	206	0	2	155	17	0	174	426
<b>Total</b>	<b>0</b>	<b>131</b>	<b>3</b>	<b>50</b>	<b>1</b>	<b>184</b>	<b>0</b>	<b>21</b>	<b>6</b>	<b>16</b>	<b>9</b>	<b>43</b>	<b>19</b>	<b>35</b>	<b>648</b>	<b>27</b>	<b>3</b>	<b>729</b>	<b>1</b>	<b>11</b>	<b>605</b>	<b>73</b>	<b>5</b>	<b>690</b>	<b>1646</b>
Approach %	0.0	71.2	1.6	27.2	-	-	0.0	48.8	14.0	37.2	-	-	2.6	4.8	88.9	3.7	-	-	0.1	1.6	87.7	10.6	-	-	-
Total %	0.0	8.0	0.2	3.0	-	11.2	0.0	1.3	0.4	1.0	-	2.6	1.2	2.1	39.4	1.6	-	44.3	0.1	0.7	36.8	4.4	-	41.9	-
PHF	0.000	0.780	0.375	0.893	-	0.807	0.000	0.750	0.500	0.571	-	0.768	0.679	0.875	0.880	0.844	-	0.885	0.250	0.393	0.934	0.913	-	0.964	0.966
Lights	0	131	3	48	-	182	0	20	6	13	-	39	19	34	633	25	-	711	1	10	588	73	-	672	1604
% Lights	-	100.0	100.0	96.0	-	98.9	-	95.2	100.0	81.3	-	90.7	100.0	97.1	97.7	92.6	-	97.5	100.0	90.9	97.2	100.0	-	97.4	97.4
Other Vehicles	0	0	0	2	-	2	0	1	0	3	-	4	0	1	15	2	-	18	0	1	17	0	-	18	42
% Other Vehicles	-	0.0	0.0	4.0	-	1.1	-	4.8	0.0	18.8	-	9.3	0.0	2.9	2.3	7.4	-	2.5	0.0	9.1	2.8	0.0	-	2.6	2.6
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	1	-	-	-	-	-	3	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	100.0	-	-	-	-	-	33.3	-	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	6	-	-	-	-	-	3	-	-	-	-	-	5	-	-
% Pedestrians	-	-	-	-	0.0	-	-	-	-	-	66.7	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-

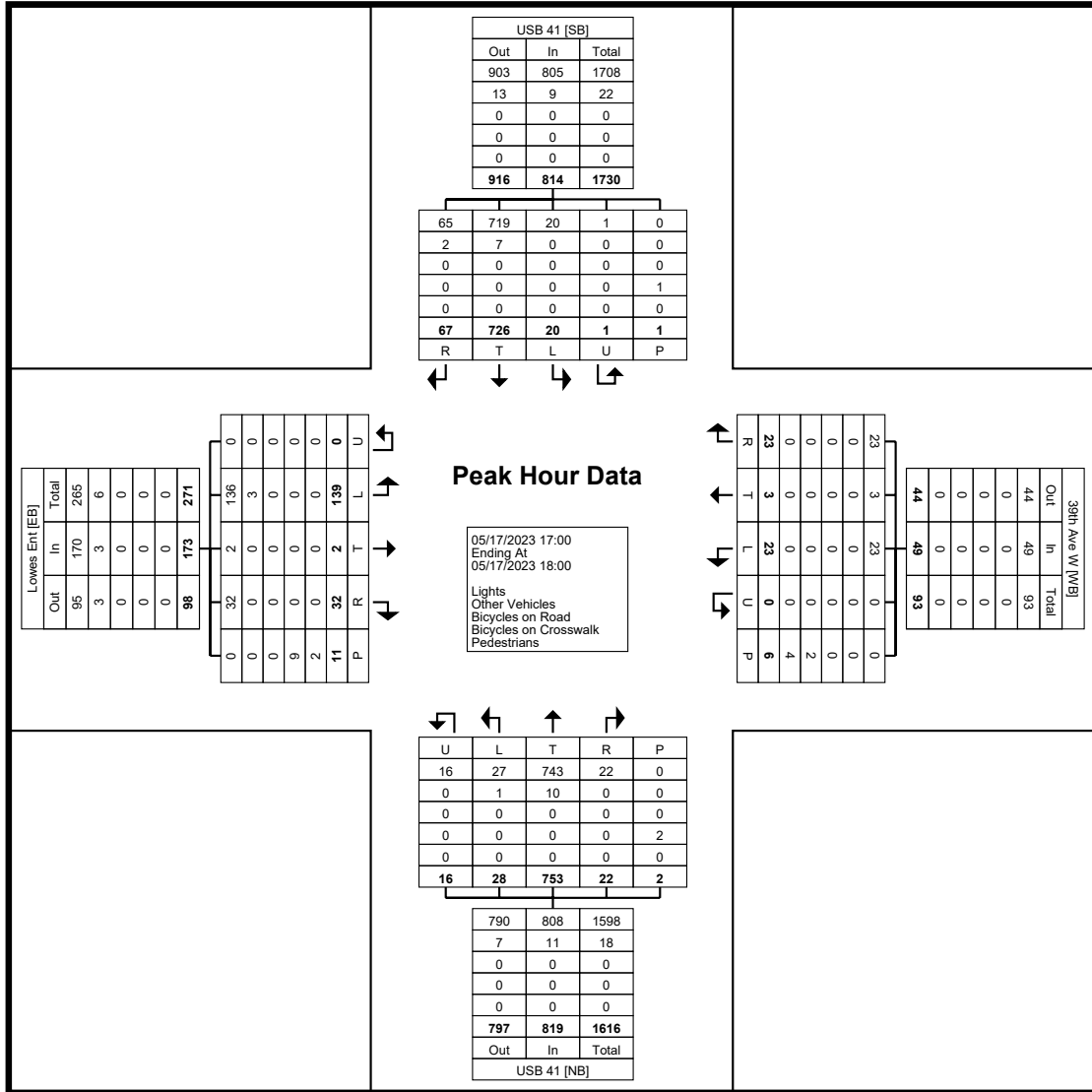


Turning Movement Peak Hour Data Plot (12:30)



### Turning Movement Peak Hour Data (17:00)

Start Time	Lowe's Ent Eastbound						39th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
17:00	0	43	1	5	3	49	0	8	1	8	1	17	3	6	209	3	0	221	1	5	176	12	0	194	481
17:15	0	36	1	9	3	46	0	6	0	3	3	9	8	5	169	11	2	193	0	6	182	10	0	198	446
17:30	0	31	0	6	3	37	0	4	1	7	1	12	3	8	183	3	0	197	0	4	194	23	1	221	467
17:45	0	29	0	12	2	41	0	5	1	5	1	11	2	9	192	5	0	208	0	5	174	22	0	201	461
<b>Total</b>	<b>0</b>	<b>139</b>	<b>2</b>	<b>32</b>	<b>11</b>	<b>173</b>	<b>0</b>	<b>23</b>	<b>3</b>	<b>23</b>	<b>6</b>	<b>49</b>	<b>16</b>	<b>28</b>	<b>753</b>	<b>22</b>	<b>2</b>	<b>819</b>	<b>1</b>	<b>20</b>	<b>726</b>	<b>67</b>	<b>1</b>	<b>814</b>	<b>1855</b>
Approach %	0.0	80.3	1.2	18.5	-	-	0.0	46.9	6.1	46.9	-	-	2.0	3.4	91.9	2.7	-	-	0.1	2.5	89.2	8.2	-	-	-
Total %	0.0	7.5	0.1	1.7	-	9.3	0.0	1.2	0.2	1.2	-	2.6	0.9	1.5	40.6	1.2	-	44.2	0.1	1.1	39.1	3.6	-	43.9	-
PHF	0.000	0.808	0.500	0.667	-	0.883	0.000	0.719	0.750	0.719	-	0.721	0.500	0.778	0.901	0.500	-	0.926	0.250	0.833	0.936	0.728	-	0.921	0.964
Lights	0	136	2	32	-	170	0	23	3	23	-	49	16	27	743	22	-	808	1	20	719	65	-	805	1832
% Lights	-	97.8	100.0	100.0	-	98.3	-	100.0	100.0	100.0	-	100.0	100.0	96.4	98.7	100.0	-	98.7	100.0	100.0	99.0	97.0	-	98.9	98.8
Other Vehicles	0	3	0	0	-	3	0	0	0	0	-	0	0	1	10	0	-	11	0	0	7	2	-	9	23
% Other Vehicles	-	2.2	0.0	0.0	-	1.7	-	0.0	0.0	0.0	-	0.0	0.0	3.6	1.3	0.0	-	1.3	0.0	0.0	1.0	3.0	-	1.1	1.2
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	9	-	-	-	-	-	2	-	-	-	-	-	2	-	-	-	-	-	1	-	-
% Bicycles on Crosswalk	-	-	-	-	81.8	-	-	-	-	-	33.3	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-
Pedestrians	-	-	-	-	2	-	-	-	-	-	4	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	18.2	-	-	-	-	-	66.7	-	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-



Turning Movement Peak Hour Data Plot (17:00)

### Turning Movement Data

Start Time	Driveway Eastbound						35th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	8	0	14	0	22	0	4	0	5	0	9	1	2	127	2	0	132	0	3	160	5	0	168	331
7:15	0	4	0	4	1	8	0	3	0	2	2	5	2	5	149	0	0	156	0	1	176	0	0	177	346
7:30	0	5	0	4	0	9	0	7	0	5	0	12	1	5	170	2	0	178	0	3	162	4	0	169	368
7:45	0	6	0	10	4	16	0	4	0	2	1	6	3	1	192	3	1	199	0	1	152	5	0	158	379
Hourly Total	0	23	0	32	5	55	0	18	0	14	3	32	7	13	638	7	1	665	0	8	650	14	0	672	1424
8:00	0	7	0	8	1	15	0	0	0	3	1	3	0	0	164	0	0	164	0	0	220	5	0	225	407
8:15	0	2	0	3	2	5	0	3	0	4	2	7	0	2	159	3	0	164	0	3	198	5	0	206	382
8:30	0	4	0	3	2	7	0	2	0	3	4	5	2	2	140	2	0	146	1	2	200	2	0	205	363
8:45	0	0	0	5	21	5	0	2	0	2	4	4	5	2	158	0	0	165	0	0	176	2	0	178	352
Hourly Total	0	13	0	19	26	32	0	7	0	12	11	19	7	6	621	5	0	639	1	5	794	14	0	814	1504
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	1	0	3	3	4	0	3	0	4	5	7	3	1	171	1	1	176	0	1	192	10	0	203	390
12:15	0	3	0	6	2	9	0	0	0	1	2	1	4	7	185	1	0	197	0	2	168	9	0	179	386
12:30	0	2	0	5	0	7	0	3	0	2	4	5	3	7	182	2	0	194	0	2	175	2	0	179	385
12:45	0	2	0	7	2	9	0	0	0	3	1	3	5	4	198	0	1	207	0	1	168	0	0	169	388
Hourly Total	0	8	0	21	7	29	0	6	0	10	12	16	15	19	736	4	2	774	0	6	703	21	0	730	1549
13:00	0	1	0	1	2	2	0	2	0	1	0	3	3	4	172	3	0	182	0	0	168	3	0	171	358
13:15	0	3	2	6	1	11	0	2	0	2	0	4	3	2	200	2	0	207	0	0	159	7	0	166	388
13:30	0	3	0	4	7	7	1	1	0	2	1	4	2	2	175	1	0	180	0	5	172	4	0	181	372
13:45	0	3	0	6	5	9	0	1	0	2	0	3	2	0	175	3	2	180	1	2	165	4	0	172	364
Hourly Total	0	10	2	17	15	29	1	6	0	7	1	14	10	8	722	9	2	749	1	7	664	18	0	690	1482
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	2	0	1	4	3	0	2	0	2	1	4	6	6	218	0	0	230	1	0	159	6	0	166	403
15:15	0	2	0	9	6	11	0	2	0	4	5	6	5	4	191	3	0	203	0	3	182	12	0	197	417
15:30	0	6	0	7	6	13	0	0	2	5	1	7	4	9	175	2	0	190	0	2	189	6	0	197	407
15:45	0	3	0	2	6	5	0	3	0	1	2	4	3	5	193	1	0	202	1	2	193	7	0	203	414
Hourly Total	0	13	0	19	22	32	0	7	2	12	9	21	18	24	777	6	0	825	2	7	723	31	0	763	1641
16:00	0	3	0	4	3	7	0	1	0	1	5	2	5	4	222	4	0	235	0	3	186	5	0	194	438
16:15	0	4	0	3	4	7	0	2	0	3	7	5	4	7	211	6	0	228	0	2	197	11	0	210	450
16:30	0	3	0	10	3	13	0	3	0	4	7	7	5	6	216	6	0	233	0	3	203	6	0	212	465
16:45	0	0	0	3	5	3	0	3	0	1	4	4	6	4	201	8	0	219	0	4	186	8	0	198	424
Hourly Total	0	10	0	20	15	30	0	9	0	9	23	18	20	21	850	24	0	915	0	12	772	30	0	814	1777
17:00	0	3	0	9	8	12	0	2	0	1	2	3	4	8	224	4	1	240	0	1	196	8	1	205	460
17:15	0	0	0	5	3	5	0	2	0	1	4	3	3	10	205	3	0	221	0	1	222	6	0	229	458
17:30	0	3	1	9	9	13	0	1	1	3	2	5	2	9	195	6	2	212	0	3	220	10	0	233	463
17:45	0	3	1	6	5	10	0	2	0	3	4	5	3	4	219	3	2	229	0	5	198	12	0	215	459
Hourly Total	0	9	2	29	25	40	0	7	1	8	12	16	12	31	843	16	5	902	0	10	836	36	1	882	1840
18:00	0	5	0	10	2	15	0	0	0	6	3	6	5	12	203	7	2	227	0	3	171	7	1	181	429
18:15	0	7	0	4	6	11	0	3	0	0	3	3	2	2	187	3	0	194	0	4	149	8	0	161	369
18:30	0	3	0	6	2	9	0	0	0	6	3	6	3	4	180	9	0	196	0	6	197	8	0	211	422
18:45	0	2	0	7	2	9	0	3	0	3	0	6	7	5	187	5	0	204	0	1	155	6	0	162	381
Hourly Total	0	17	0	27	12	44	0	6	0	15	9	21	17	23	757	24	2	821	0	14	672	29	1	715	1601
Grand Total	0	103	4	184	127	291	1	66	3	87	80	157	106	145	5944	95	12	6290	4	69	5814	193	2	6080	12818
Approach %	0.0	35.4	1.4	63.2	-	-	0.6	42.0	1.9	55.4	-	-	1.7	2.3	94.5	1.5	-	-	0.1	1.1	95.6	3.2	-	-	-
Total %	0.0	0.8	0.0	1.4	-	2.3	0.0	0.5	0.0	0.7	-	1.2	0.8	1.1	46.4	0.7	-	49.1	0.0	0.5	45.4	1.5	-	47.4	-
Lights	0	102	4	182	-	288	1	64	2	84	-	151	105	143	5814	92	-	6154	4	69	5661	191	-	5925	12518
% Lights	-	99.0	100.0	98.9	-	99.0	100.0	97.0	66.7	96.6	-	96.2	99.1	98.6	97.8	96.8	-	97.8	100.0	100.0	97.4	99.0	-	97.5	97.7
Other Vehicles	0	0	0	2	-	2	0	2	1	3	-	6	1	1	123	3	-	128	0	0	149	2	-	151	287
% Other Vehicles	-	0.0	0.0	1.1	-	0.7	0.0	3.0	33.3	3.4	-	3.8	0.9	0.7	2.1	3.2	-	2.0	0.0	0.0	2.6	1.0	-	2.5	2.2
Bicycles on Road	0	1	0	0	-	1	0	0	0	0	-	0	0	1	7	0	-	8	0	0	4	0	-	4	13
% Bicycles on Road	-	1.0	0.0	0.0	-	0.3	0.0	0.0	0.0	0.0	-	0.0	0.0	0.7	0.1	0.0	-	0.1	0.0	0.0	0.1	0.0	-	0.1	0.1
Bicycles on Crosswalk	-	-	-	-	59	-	-	-	-	-	26	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	46.5	-	-	-	-	-	32.5	-	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	68	-	-	-	-	-	54	-	-	-	-	-	12	-	-	-	-	-	2	-	-
% Pedestrians	-	-	-	-	53.5	-	-	-	-	-	67.5	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-



### Turning Movement Peak Hour Data (7:30)

Start Time	Driveway Eastbound						35th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:30	0	5	0	4	0	9	0	7	0	5	0	12	1	5	170	2	0	178	0	3	162	4	0	169	368
7:45	0	6	0	10	4	16	0	4	0	2	1	6	3	1	192	3	1	199	0	1	152	5	0	158	379
8:00	0	7	0	8	1	15	0	0	0	3	1	3	0	0	164	0	0	164	0	0	220	5	0	225	407
8:15	0	2	0	3	2	5	0	3	0	4	2	7	0	2	159	3	0	164	0	3	198	5	0	206	382
<b>Total</b>	<b>0</b>	<b>20</b>	<b>0</b>	<b>25</b>	<b>7</b>	<b>45</b>	<b>0</b>	<b>14</b>	<b>0</b>	<b>14</b>	<b>4</b>	<b>28</b>	<b>4</b>	<b>8</b>	<b>685</b>	<b>8</b>	<b>1</b>	<b>705</b>	<b>0</b>	<b>7</b>	<b>732</b>	<b>19</b>	<b>0</b>	<b>758</b>	<b>1536</b>
Approach %	0.0	44.4	0.0	55.6	-	-	0.0	50.0	0.0	50.0	-	-	0.6	1.1	97.2	1.1	-	-	0.0	0.9	96.6	2.5	-	-	-
Total %	0.0	1.3	0.0	1.6	-	2.9	0.0	0.9	0.0	0.9	-	1.8	0.3	0.5	44.6	0.5	-	45.9	0.0	0.5	47.7	1.2	-	49.3	-
PHF	0.000	0.714	0.000	0.625	-	0.703	0.000	0.500	0.000	0.700	-	0.583	0.333	0.400	0.892	0.667	-	0.886	0.000	0.583	0.832	0.950	-	0.842	0.943
Lights	0	20	0	25	-	45	0	13	0	12	-	25	4	8	665	8	-	685	0	7	706	19	-	732	1487
% Lights	-	100.0	-	100.0	-	100.0	-	92.9	-	85.7	-	89.3	100.0	100.0	97.1	100.0	-	97.2	-	100.0	96.4	100.0	-	96.6	96.8
Other Vehicles	0	0	0	0	-	0	0	1	0	2	-	3	0	0	18	0	-	18	0	0	26	0	-	26	47
% Other Vehicles	-	0.0	-	0.0	-	0.0	-	7.1	-	14.3	-	10.7	0.0	0.0	2.6	0.0	-	2.6	-	0.0	3.6	0.0	-	3.4	3.1
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	2	0	-	2	0	0	0	0	-	0	2
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	0.3	0.0	-	0.3	-	0.0	0.0	0.0	-	0.0	0.1
Bicycles on Crosswalk	-	-	-	-	4	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	57.1	-	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	3	-	-	-	-	-	4	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	42.9	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-



### Turning Movement Peak Hour Data (12:00)

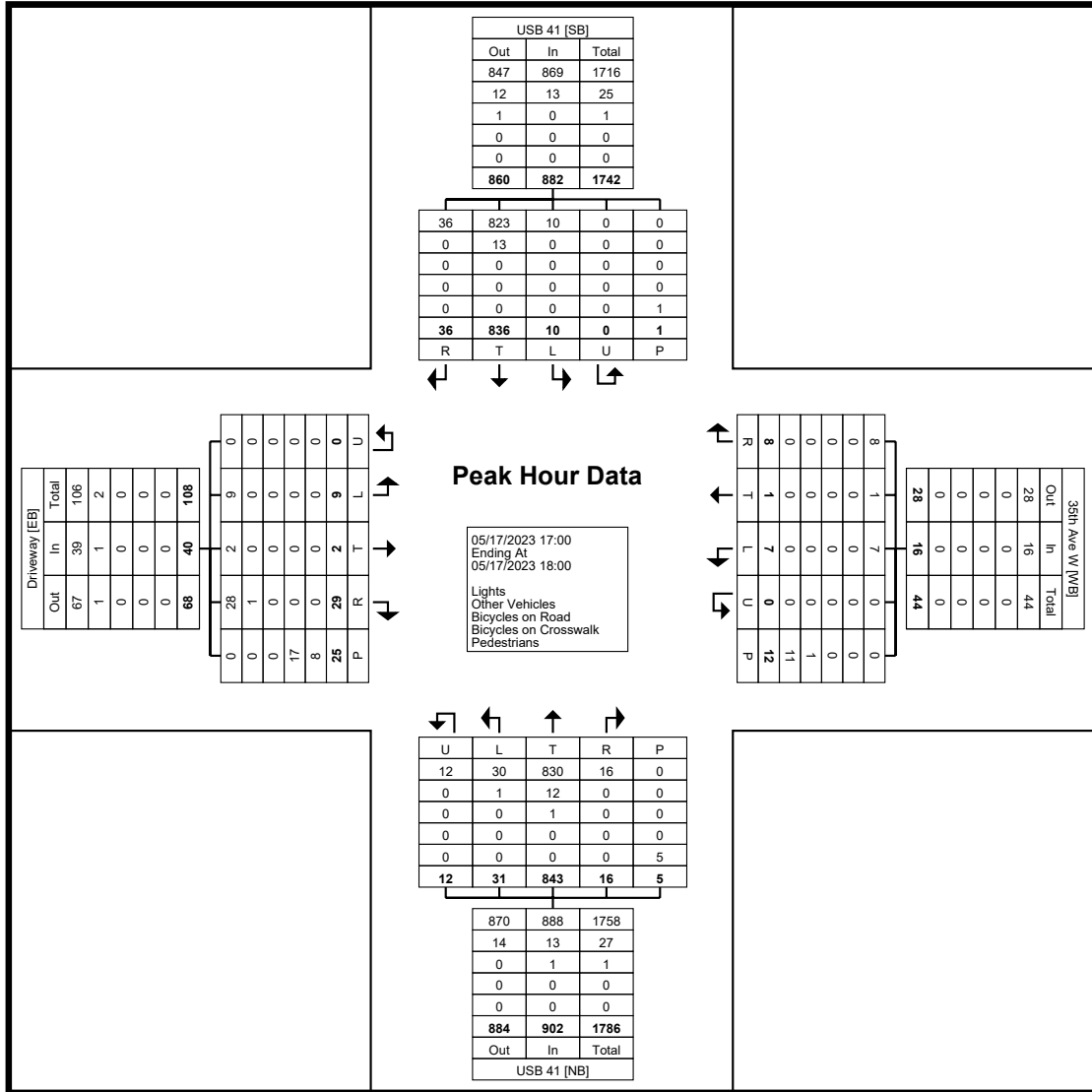
Start Time	Driveway Eastbound						35th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	1	0	3	3	4	0	3	0	4	5	7	3	1	171	1	1	176	0	1	192	10	0	203	390
12:15	0	3	0	6	2	9	0	0	0	1	2	1	4	7	185	1	0	197	0	2	168	9	0	179	386
12:30	0	2	0	5	0	7	0	3	0	2	4	5	3	7	182	2	0	194	0	2	175	2	0	179	385
12:45	0	2	0	7	2	9	0	0	0	3	1	3	5	4	198	0	1	207	0	1	168	0	0	169	388
<b>Total</b>	<b>0</b>	<b>8</b>	<b>0</b>	<b>21</b>	<b>7</b>	<b>29</b>	<b>0</b>	<b>6</b>	<b>0</b>	<b>10</b>	<b>12</b>	<b>16</b>	<b>15</b>	<b>19</b>	<b>736</b>	<b>4</b>	<b>2</b>	<b>774</b>	<b>0</b>	<b>6</b>	<b>703</b>	<b>21</b>	<b>0</b>	<b>730</b>	<b>1549</b>
Approach %	0.0	27.6	0.0	72.4	-	-	0.0	37.5	0.0	62.5	-	-	1.9	2.5	95.1	0.5	-	-	0.0	0.8	96.3	2.9	-	-	-
Total %	0.0	0.5	0.0	1.4	-	1.9	0.0	0.4	0.0	0.6	-	1.0	1.0	1.2	47.5	0.3	-	50.0	0.0	0.4	45.4	1.4	-	47.1	-
PHF	0.000	0.667	0.000	0.750	-	0.806	0.000	0.500	0.000	0.625	-	0.571	0.750	0.679	0.929	0.500	-	0.935	0.000	0.750	0.915	0.525	-	0.899	0.993
Lights	0	8	0	21	-	29	0	6	0	9	-	15	15	19	718	4	-	756	0	6	689	21	-	716	1516
% Lights	-	100.0	-	100.0	-	100.0	-	100.0	-	90.0	-	93.8	100.0	100.0	97.6	100.0	-	97.7	-	100.0	98.0	100.0	-	98.1	97.9
Other Vehicles	0	0	0	0	-	0	0	0	0	1	-	1	0	0	18	0	-	18	0	0	14	0	-	14	33
% Other Vehicles	-	0.0	-	0.0	-	0.0	-	0.0	-	10.0	-	6.3	0.0	0.0	2.4	0.0	-	2.3	-	0.0	2.0	0.0	-	1.9	2.1
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	3	-	-	-	-	-	8	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	42.9	-	-	-	-	-	66.7	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	4	-	-	-	-	-	4	-	-	-	-	-	2	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	57.1	-	-	-	-	-	33.3	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-





### Turning Movement Peak Hour Data (17:00)

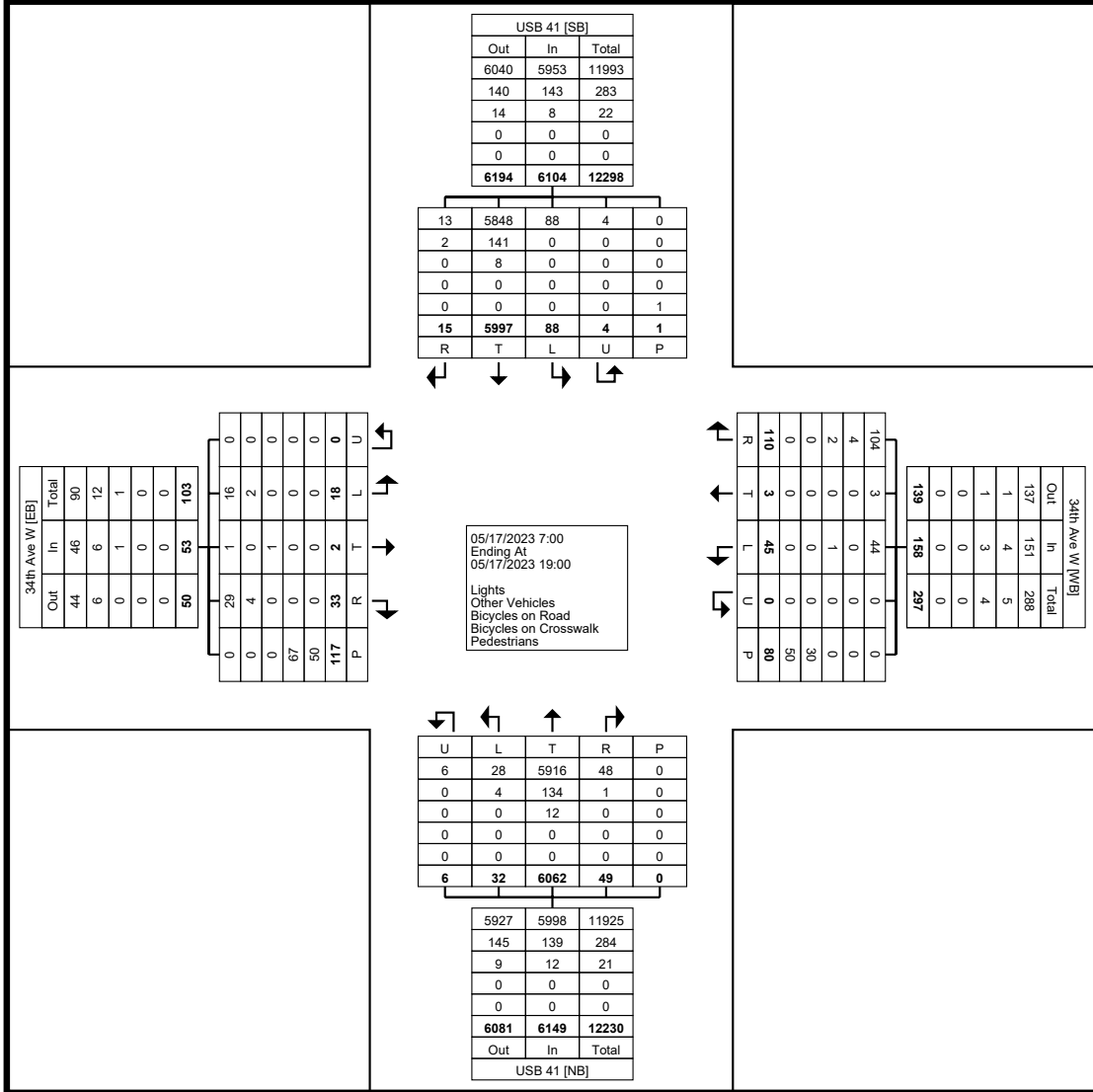
Start Time	Driveway Eastbound						35th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
17:00	0	3	0	9	8	12	0	2	0	1	2	3	4	8	224	4	1	240	0	1	196	8	1	205	460
17:15	0	0	0	5	3	5	0	2	0	1	4	3	3	10	205	3	0	221	0	1	222	6	0	229	458
17:30	0	3	1	9	9	13	0	1	1	3	2	5	2	9	195	6	2	212	0	3	220	10	0	233	463
17:45	0	3	1	6	5	10	0	2	0	3	4	5	3	4	219	3	2	229	0	5	198	12	0	215	459
<b>Total</b>	<b>0</b>	<b>9</b>	<b>2</b>	<b>29</b>	<b>25</b>	<b>40</b>	<b>0</b>	<b>7</b>	<b>1</b>	<b>8</b>	<b>12</b>	<b>16</b>	<b>12</b>	<b>31</b>	<b>843</b>	<b>16</b>	<b>5</b>	<b>902</b>	<b>0</b>	<b>10</b>	<b>836</b>	<b>36</b>	<b>1</b>	<b>882</b>	<b>1840</b>
Approach %	0.0	22.5	5.0	72.5	-	-	0.0	43.8	6.3	50.0	-	-	1.3	3.4	93.5	1.8	-	-	0.0	1.1	94.8	4.1	-	-	-
Total %	0.0	0.5	0.1	1.6	-	2.2	0.0	0.4	0.1	0.4	-	0.9	0.7	1.7	45.8	0.9	-	49.0	0.0	0.5	45.4	2.0	-	47.9	-
PHF	0.000	0.750	0.500	0.806	-	0.769	0.000	0.875	0.250	0.667	-	0.800	0.750	0.775	0.941	0.667	-	0.940	0.000	0.500	0.941	0.750	-	0.946	0.994
Lights	0	9	2	28	-	39	0	7	1	8	-	16	12	30	830	16	-	888	0	10	823	36	-	869	1812
% Lights	-	100.0	100.0	96.6	-	97.5	-	100.0	100.0	100.0	-	100.0	100.0	96.8	98.5	100.0	-	98.4	-	100.0	98.4	100.0	-	98.5	98.5
Other Vehicles	0	0	0	1	-	1	0	0	0	0	-	0	0	1	12	0	-	13	0	0	13	0	-	13	27
% Other Vehicles	-	0.0	0.0	3.4	-	2.5	-	0.0	0.0	0.0	-	0.0	0.0	3.2	1.4	0.0	-	1.4	-	0.0	1.6	0.0	-	1.5	1.5
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	1	0	-	1	0	0	0	0	-	0	1
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.1	0.0	-	0.1	-	0.0	0.0	0.0	-	0.0	0.1
Bicycles on Crosswalk	-	-	-	-	17	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	68.0	-	-	-	-	-	8.3	-	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	8	-	-	-	-	-	11	-	-	-	-	-	5	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	32.0	-	-	-	-	-	91.7	-	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-



Turning Movement Peak Hour Data Plot (17:00)

### Turning Movement Data

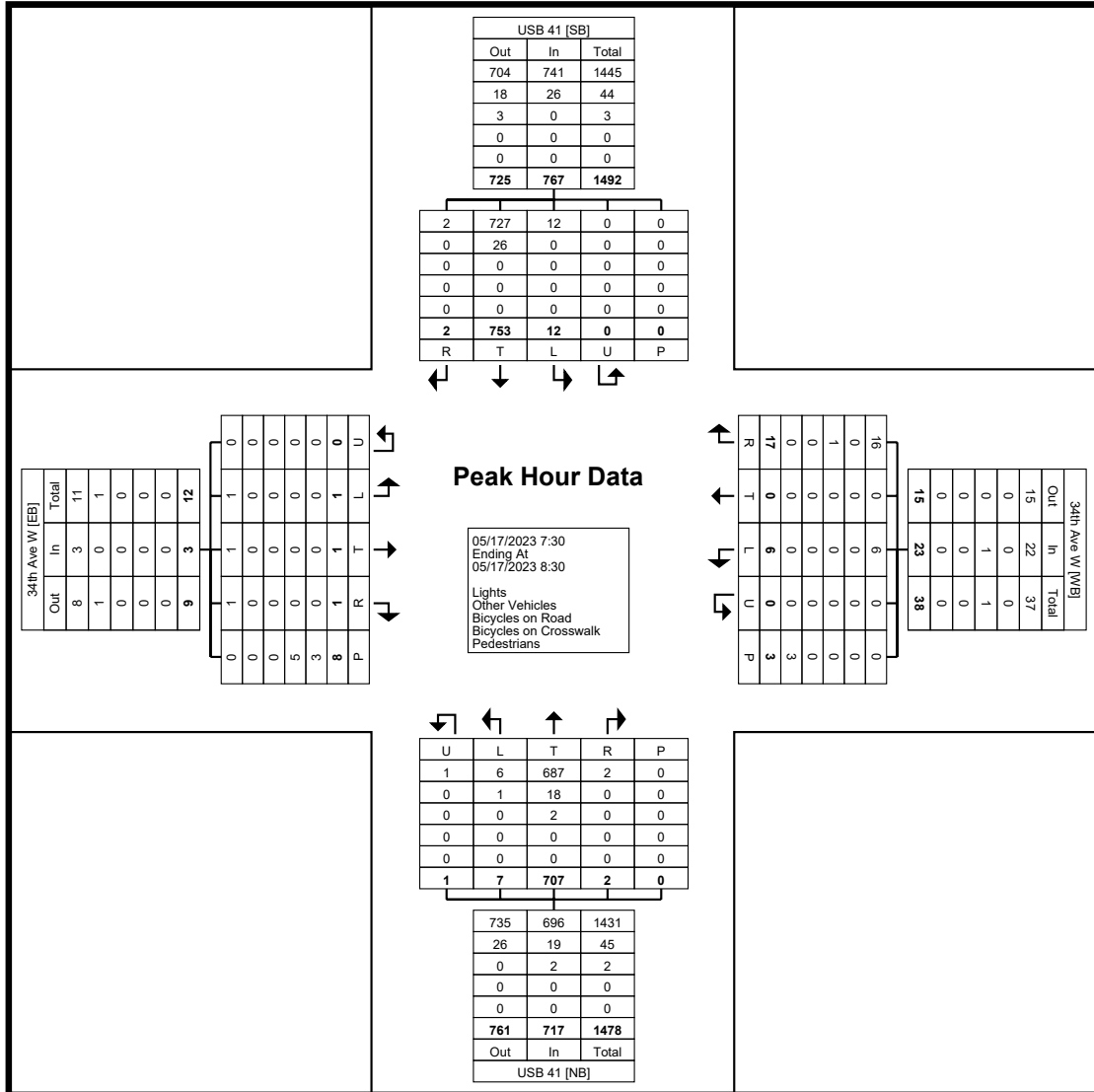
Start Time	34th Ave W Eastbound						34th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	0	0	0	1	0	0	3	0	5	0	8	0	1	156	2	0	159	0	3	168	0	0	171	338
7:15	0	0	0	0	2	0	0	4	1	7	0	12	0	0	154	0	0	154	0	3	168	0	0	171	337
7:30	0	0	0	0	0	0	0	2	0	4	0	6	1	0	173	1	0	175	0	6	168	0	0	174	355
7:45	0	0	0	0	5	0	0	3	0	5	0	8	0	0	172	0	0	172	0	2	161	0	0	163	343
Hourly Total	0	0	0	0	8	0	0	12	1	21	0	34	1	1	655	3	0	660	0	14	665	0	0	679	1373
8:00	0	1	0	0	2	1	0	1	0	4	0	5	0	1	197	0	0	198	0	2	221	1	0	224	428
8:15	0	0	1	1	1	2	0	0	0	4	3	4	0	6	165	1	0	172	0	2	203	1	0	206	384
8:30	0	1	0	1	1	2	0	0	0	2	6	2	0	0	134	1	0	135	0	0	205	0	0	205	344
8:45	0	0	0	1	3	1	0	0	0	2	5	2	0	0	166	1	0	167	0	0	179	2	1	181	351
Hourly Total	0	2	1	3	7	6	0	1	0	12	14	13	0	7	662	3	0	672	0	4	808	4	1	816	1507
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	0	1	4	1	0	0	0	3	4	3	0	1	184	2	0	187	0	1	199	1	0	201	392
12:15	0	0	0	2	1	2	0	1	0	4	2	5	0	1	187	3	0	191	0	1	186	0	0	187	385
12:30	0	0	0	0	4	0	0	2	0	2	3	4	0	0	191	1	0	192	0	0	175	1	0	176	372
12:45	0	2	0	3	4	5	0	0	0	0	2	0	0	4	191	2	0	197	1	3	169	0	0	173	375
Hourly Total	0	2	0	6	13	8	0	3	0	9	11	12	0	6	753	8	0	767	1	5	729	2	0	737	1524
13:00	0	0	0	4	2	4	0	1	0	3	0	4	1	1	185	0	0	187	0	1	165	0	0	166	361
13:15	0	0	0	1	1	1	0	0	0	1	5	1	0	4	192	0	0	196	0	0	161	1	0	162	360
13:30	0	0	0	0	3	0	0	1	0	3	0	4	0	2	184	0	0	186	0	7	181	1	0	189	379
13:45	0	0	0	1	8	1	0	1	0	5	2	6	0	0	172	1	0	173	0	1	165	2	0	168	348
Hourly Total	0	0	0	6	14	6	0	3	0	12	7	15	1	7	733	1	0	742	0	9	672	4	0	685	1448
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	0	0	3	4	3	0	1	0	3	2	4	0	1	212	0	0	213	0	1	160	0	0	161	381
15:15	0	0	0	1	6	1	0	1	0	1	3	2	0	1	204	3	0	208	0	3	196	0	0	199	410
15:30	0	1	0	1	2	2	0	4	0	3	2	7	0	3	166	2	0	171	0	2	191	0	0	193	373
15:45	0	1	0	2	1	3	0	1	0	3	1	4	0	0	202	2	0	204	0	2	194	2	0	198	409
Hourly Total	0	2	0	7	13	9	0	7	0	10	8	17	0	5	784	7	0	796	0	8	741	2	0	751	1573
16:00	0	1	0	3	6	4	0	2	0	1	4	3	0	2	214	2	0	218	0	4	202	1	0	207	432
16:15	0	0	0	3	4	3	0	2	0	0	3	2	0	0	215	2	0	217	0	4	188	0	0	192	414
16:30	0	0	0	1	2	1	0	0	0	5	8	5	0	2	216	2	0	220	1	2	201	1	0	205	431
16:45	0	0	0	1	4	1	0	1	0	6	3	7	0	0	206	1	0	207	0	8	221	0	0	229	444
Hourly Total	0	1	0	8	16	9	0	5	0	12	18	17	0	4	851	7	0	862	1	18	812	2	0	833	1721
17:00	0	1	0	1	7	2	0	2	1	6	4	9	1	0	224	1	0	226	0	5	191	0	0	196	433
17:15	0	0	0	0	5	0	0	1	0	2	3	3	0	1	199	2	0	202	0	5	217	0	0	222	427
17:30	0	1	1	0	6	2	0	0	0	5	4	5	0	0	195	2	0	197	0	5	221	1	0	227	431
17:45	0	0	0	1	6	1	0	2	0	4	3	6	0	0	230	7	0	237	1	5	222	0	0	228	472
Hourly Total	0	2	1	2	24	5	0	5	1	17	14	23	1	1	848	12	0	862	1	20	851	1	0	873	1763
18:00	0	2	0	1	6	3	0	3	0	2	3	5	1	1	210	0	0	212	1	0	184	0	0	185	405
18:15	0	5	0	0	9	5	0	2	1	5	3	8	0	0	190	2	0	192	0	4	161	0	0	165	370
18:30	0	1	0	0	4	1	0	3	0	4	2	7	1	0	177	3	0	181	0	3	214	0	0	217	406
18:45	0	1	0	0	3	1	0	1	0	6	0	7	1	0	199	3	0	203	0	3	160	0	0	163	374
Hourly Total	0	9	0	1	22	10	0	9	1	17	8	27	3	1	776	8	0	788	1	10	719	0	0	730	1555
Grand Total	0	18	2	33	117	53	0	45	3	110	80	158	6	32	6062	49	0	6149	4	88	5997	15	1	6104	12464
Approach %	0.0	34.0	3.8	62.3	-	-	0.0	28.5	1.9	69.6	-	-	0.1	0.5	98.6	0.8	-	-	0.1	1.4	98.2	0.2	-	-	-
Total %	0.0	0.1	0.0	0.3	-	0.4	0.0	0.4	0.0	0.9	-	1.3	0.0	0.3	48.6	0.4	-	49.3	0.0	0.7	48.1	0.1	-	49.0	-
Lights	0	16	1	29	-	46	0	44	3	104	-	151	6	28	5916	48	-	5998	4	88	5848	13	-	5953	12148
% Lights	-	88.9	50.0	87.9	-	86.8	-	97.8	100.0	94.5	-	95.6	100.0	87.5	97.6	98.0	-	97.5	100.0	100.0	97.5	86.7	-	97.5	97.5
Other Vehicles	0	2	0	4	-	6	0	0	0	4	-	4	0	4	134	1	-	139	0	0	141	2	-	143	292
% Other Vehicles	-	11.1	0.0	12.1	-	11.3	-	0.0	0.0	3.6	-	2.5	0.0	12.5	2.2	2.0	-	2.3	0.0	0.0	2.4	13.3	-	2.3	2.3
Bicycles on Road	0	0	1	0	-	1	0	1	0	2	-	3	0	0	12	0	-	12	0	0	8	0	-	8	24
% Bicycles on Road	-	0.0	50.0	0.0	-	1.9	-	2.2	0.0	1.8	-	1.9	0.0	0.0	0.2	0.0	-	0.2	0.0	0.0	0.1	0.0	-	0.1	0.2
Bicycles on Crosswalk	-	-	-	-	67	-	-	-	-	-	30	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	57.3	-	-	-	-	-	37.5	-	-	-	-	-	-	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	50	-	-	-	-	-	50	-	-	-	-	-	0	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	42.7	-	-	-	-	-	62.5	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:30)

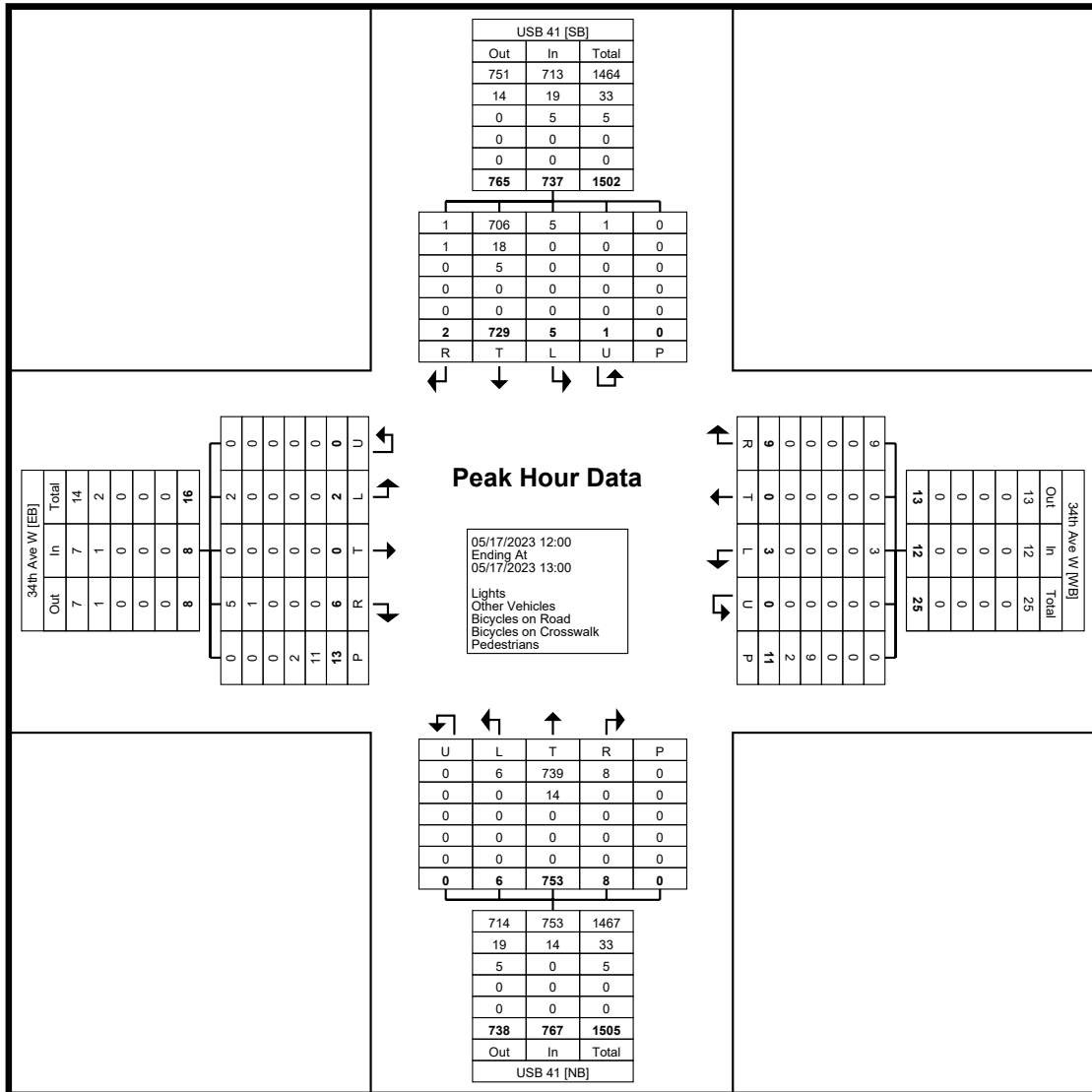
Start Time	34th Ave W Eastbound						34th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:30	0	0	0	0	0	0	0	2	0	4	0	6	1	0	173	1	0	175	0	6	168	0	0	174	355
7:45	0	0	0	0	5	0	0	3	0	5	0	8	0	0	172	0	0	172	0	2	161	0	0	163	343
8:00	0	1	0	0	2	1	0	1	0	4	0	5	0	1	197	0	0	198	0	2	221	1	0	224	428
8:15	0	0	1	1	1	2	0	0	0	4	3	4	0	6	165	1	0	172	0	2	203	1	0	206	384
<b>Total</b>	<b>0</b>	<b>1</b>	<b>1</b>	<b>1</b>	<b>8</b>	<b>3</b>	<b>0</b>	<b>6</b>	<b>0</b>	<b>17</b>	<b>3</b>	<b>23</b>	<b>1</b>	<b>7</b>	<b>707</b>	<b>2</b>	<b>0</b>	<b>717</b>	<b>0</b>	<b>12</b>	<b>753</b>	<b>2</b>	<b>0</b>	<b>767</b>	<b>1510</b>
Approach %	0.0	33.3	33.3	33.3	-	-	0.0	26.1	0.0	73.9	-	-	0.1	1.0	98.6	0.3	-	-	0.0	1.6	98.2	0.3	-	-	-
Total %	0.0	0.1	0.1	0.1	-	0.2	0.0	0.4	0.0	1.1	-	1.5	0.1	0.5	46.8	0.1	-	47.5	0.0	0.8	49.9	0.1	-	50.8	-
PHF	0.000	0.250	0.250	0.250	-	0.375	0.000	0.500	0.000	0.850	-	0.719	0.250	0.292	0.897	0.500	-	0.905	0.000	0.500	0.852	0.500	-	0.856	0.882
Lights	0	1	1	1	-	3	0	6	0	16	-	22	1	6	687	2	-	696	0	12	727	2	-	741	1462
% Lights	-	100.0	100.0	100.0	-	100.0	-	100.0	-	94.1	-	95.7	100.0	85.7	97.2	100.0	-	97.1	-	100.0	96.5	100.0	-	96.6	96.8
Other Vehicles	0	0	0	0	-	0	0	0	0	0	-	0	0	1	18	0	-	19	0	0	26	0	-	26	45
% Other Vehicles	-	0.0	0.0	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	14.3	2.5	0.0	-	2.6	-	0.0	3.5	0.0	-	3.4	3.0
Bicycles on Road	0	0	0	0	-	0	0	0	0	1	-	1	0	0	2	0	-	2	0	0	0	0	-	0	3
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	-	5.9	-	4.3	0.0	0.0	0.3	0.0	-	0.3	-	0.0	0.0	0.0	-	0.0	0.2
Bicycles on Crosswalk	-	-	-	-	5	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	62.5	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	3	-	-	-	-	-	3	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	37.5	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-



Turning Movement Peak Hour Data Plot (7:30)

### Turning Movement Peak Hour Data (12:00)

Start Time	34th Ave W Eastbound						34th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	0	0	1	4	1	0	0	0	3	4	3	0	1	184	2	0	187	0	1	199	1	0	201	392
12:15	0	0	0	2	1	2	0	1	0	4	2	5	0	1	187	3	0	191	0	1	186	0	0	187	385
12:30	0	0	0	0	4	0	0	2	0	2	3	4	0	0	191	1	0	192	0	0	175	1	0	176	372
12:45	0	2	0	3	4	5	0	0	0	0	2	0	0	4	191	2	0	197	1	3	169	0	0	173	375
<b>Total</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>6</b>	<b>13</b>	<b>8</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>9</b>	<b>11</b>	<b>12</b>	<b>0</b>	<b>6</b>	<b>753</b>	<b>8</b>	<b>0</b>	<b>767</b>	<b>1</b>	<b>5</b>	<b>729</b>	<b>2</b>	<b>0</b>	<b>737</b>	<b>1524</b>
Approach %	0.0	25.0	0.0	75.0	-	-	0.0	25.0	0.0	75.0	-	-	0.0	0.8	98.2	1.0	-	-	0.1	0.7	98.9	0.3	-	-	-
Total %	0.0	0.1	0.0	0.4	-	0.5	0.0	0.2	0.0	0.6	-	0.8	0.0	0.4	49.4	0.5	-	50.3	0.1	0.3	47.8	0.1	-	48.4	-
PHF	0.000	0.250	0.000	0.500	-	0.400	0.000	0.375	0.000	0.563	-	0.600	0.000	0.375	0.986	0.667	-	0.973	0.250	0.417	0.916	0.500	-	0.917	0.972
Lights	0	2	0	5	-	7	0	3	0	9	-	12	0	6	739	8	-	753	1	5	706	1	-	713	1485
% Lights	-	100.0	-	83.3	-	87.5	-	100.0	-	100.0	-	100.0	-	100.0	98.1	100.0	-	98.2	100.0	100.0	96.8	50.0	-	96.7	97.4
Other Vehicles	0	0	0	1	-	1	0	0	0	0	-	0	0	0	14	0	-	14	0	0	18	1	-	19	34
% Other Vehicles	-	0.0	-	16.7	-	12.5	-	0.0	-	0.0	-	0.0	-	0.0	1.9	0.0	-	1.8	0.0	0.0	2.5	50.0	-	2.6	2.2
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	5	0	-	5	5
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.7	0.0	-	0.7	0.3
Bicycles on Crosswalk	-	-	-	-	2	-	-	-	-	-	9	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	15.4	-	-	-	-	-	81.8	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	11	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	84.6	-	-	-	-	-	18.2	-	-	-	-	-	-	-	-	-	-	-	-	-	-



Turning Movement Peak Hour Data Plot (12:00)



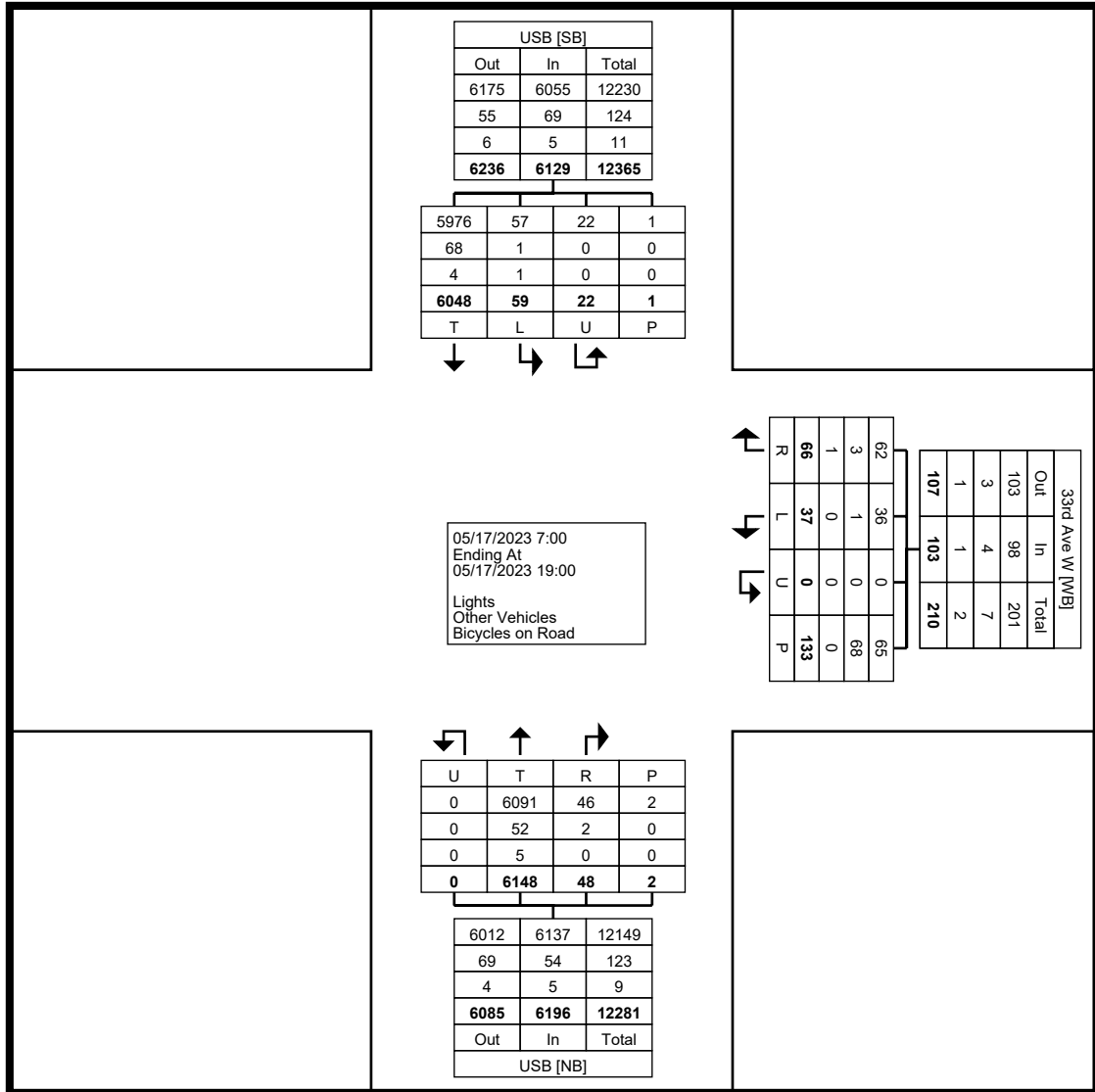
### Turning Movement Peak Hour Data (17:00)

Start Time	34th Ave W Eastbound						34th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
17:00	0	1	0	1	7	2	0	2	1	6	4	9	1	0	224	1	0	226	0	5	191	0	0	196	433
17:15	0	0	0	0	5	0	0	1	0	2	3	3	0	1	199	2	0	202	0	5	217	0	0	222	427
17:30	0	1	1	0	6	2	0	0	0	5	4	5	0	0	195	2	0	197	0	5	221	1	0	227	431
17:45	0	0	0	1	6	1	0	2	0	4	3	6	0	0	230	7	0	237	1	5	222	0	0	228	472
<b>Total</b>	<b>0</b>	<b>2</b>	<b>1</b>	<b>2</b>	<b>24</b>	<b>5</b>	<b>0</b>	<b>5</b>	<b>1</b>	<b>17</b>	<b>14</b>	<b>23</b>	<b>1</b>	<b>1</b>	<b>848</b>	<b>12</b>	<b>0</b>	<b>862</b>	<b>1</b>	<b>20</b>	<b>851</b>	<b>1</b>	<b>0</b>	<b>873</b>	<b>1763</b>
Approach %	0.0	40.0	20.0	40.0	-	-	0.0	21.7	4.3	73.9	-	-	0.1	0.1	98.4	1.4	-	-	0.1	2.3	97.5	0.1	-	-	-
Total %	0.0	0.1	0.1	0.1	-	0.3	0.0	0.3	0.1	1.0	-	1.3	0.1	0.1	48.1	0.7	-	48.9	0.1	1.1	48.3	0.1	-	49.5	-
PHF	0.000	0.500	0.250	0.500	-	0.625	0.000	0.625	0.250	0.708	-	0.639	0.250	0.250	0.922	0.429	-	0.909	0.250	1.000	0.958	0.250	-	0.957	0.934
Lights	0	2	0	2	-	4	0	5	1	17	-	23	1	1	838	12	-	852	1	20	839	1	-	861	1740
% Lights	-	100.0	0.0	100.0	-	80.0	-	100.0	100.0	100.0	-	100.0	100.0	100.0	98.8	100.0	-	98.8	100.0	100.0	98.6	100.0	-	98.6	98.7
Other Vehicles	0	0	0	0	-	0	0	0	0	0	-	0	0	0	10	0	-	10	0	0	12	0	-	12	22
% Other Vehicles	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	1.2	0.0	-	1.2	0.0	0.0	1.4	0.0	-	1.4	1.2
Bicycles on Road	0	0	1	0	-	1	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	1
% Bicycles on Road	-	0.0	100.0	0.0	-	20.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.1
Bicycles on Crosswalk	-	-	-	-	17	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	70.8	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	7	-	-	-	-	-	14	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	29.2	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-



### Turning Movement Data

Start Time	33rd Ave W Westbound					USB Northbound					USB Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	
7:00	0	1	0	1	1	0	149	1	0	150	0	4	158	0	162	313
7:15	0	1	3	2	4	0	162	0	0	162	0	0	184	0	184	350
7:30	0	0	1	4	1	0	173	2	0	175	1	1	173	0	175	351
7:45	0	0	4	4	4	0	190	2	1	192	1	0	160	0	161	357
Hourly Total	0	2	8	11	10	0	674	5	1	679	2	5	675	0	682	1371
8:00	0	2	3	3	5	0	195	2	0	197	1	0	218	0	219	421
8:15	0	0	2	6	2	0	157	2	0	159	0	1	202	0	203	364
8:30	0	1	2	6	3	0	140	1	0	141	0	2	218	0	220	364
8:45	0	2	3	5	5	0	166	0	0	166	0	3	167	0	170	341
Hourly Total	0	5	10	20	15	0	658	5	0	663	1	6	805	0	812	1490
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	2	4	2	0	192	0	0	192	0	3	200	0	203	397
12:15	0	3	2	4	5	0	178	2	0	180	4	1	175	0	180	365
12:30	0	1	1	3	2	0	195	0	0	195	1	1	181	0	183	380
12:45	0	0	3	3	3	0	192	3	0	195	0	0	161	0	161	359
Hourly Total	0	4	8	14	12	0	757	5	0	762	5	5	717	0	727	1501
13:00	0	2	0	0	2	0	192	1	0	193	0	1	167	0	168	363
13:15	0	1	2	4	3	0	193	3	0	196	0	0	170	0	170	369
13:30	0	1	0	1	1	0	186	0	0	186	0	1	174	0	175	362
13:45	0	1	1	4	2	0	170	4	0	174	0	1	167	0	168	344
Hourly Total	0	5	3	9	8	0	741	8	0	749	0	3	678	0	681	1438
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	1	1	5	2	0	220	2	0	222	1	2	157	0	160	384
15:15	0	1	6	3	7	0	200	4	0	204	1	3	195	0	199	410
15:30	0	2	1	7	3	0	173	1	0	174	0	2	197	0	199	376
15:45	0	4	2	2	6	0	193	3	0	196	0	4	203	0	207	409
Hourly Total	0	8	10	17	18	0	786	10	0	796	2	11	752	0	765	1579
16:00	0	2	0	5	2	0	218	0	0	218	1	1	190	0	192	412
16:15	0	0	2	6	2	0	218	1	0	219	0	1	200	0	201	422
16:30	0	2	2	7	4	0	226	2	0	228	0	5	216	0	221	453
16:45	0	3	3	8	6	0	220	1	1	221	2	1	214	1	217	444
Hourly Total	0	7	7	26	14	0	882	4	1	886	3	8	820	1	831	1731
17:00	0	1	3	4	4	0	217	2	0	219	2	2	202	0	206	429
17:15	0	1	3	2	4	0	216	0	0	216	0	5	224	0	229	449
17:30	0	0	4	5	4	0	190	2	0	192	0	3	233	0	236	432
17:45	0	1	4	5	5	0	223	2	0	225	1	4	225	0	230	460
Hourly Total	0	3	14	16	17	0	846	6	0	852	3	14	884	0	901	1770
18:00	0	0	2	4	2	0	222	1	0	223	1	1	174	0	176	401
18:15	0	0	0	6	0	0	208	1	0	209	1	1	169	0	171	380
18:30	0	1	1	9	2	0	177	1	0	178	2	2	207	0	211	391
18:45	0	2	3	1	5	0	197	2	0	199	2	3	167	0	172	376
Hourly Total	0	3	6	20	9	0	804	5	0	809	6	7	717	0	730	1548
Grand Total	0	37	66	133	103	0	6148	48	2	6196	22	59	6048	1	6129	12428
Approach %	0.0	35.9	64.1	-	-	0.0	99.2	0.8	-	-	0.4	1.0	98.7	-	-	-
Total %	0.0	0.3	0.5	-	0.8	0.0	49.5	0.4	-	49.9	0.2	0.5	48.7	-	49.3	-
Lights	0	36	62	65	98	0	6091	46	2	6137	22	57	5976	1	6055	12290
% Lights	-	97.3	93.9	48.9	95.1	-	99.1	95.8	100.0	99.0	100.0	96.6	98.8	100.0	98.8	98.9
Other Vehicles	0	1	3	68	4	0	52	2	0	54	0	1	68	0	69	127
% Other Vehicles	-	2.7	4.5	51.1	3.9	-	0.8	4.2	0.0	0.9	0.0	1.7	1.1	0.0	1.1	1.0
Bicycles on Road	0	0	1	0	1	0	5	0	0	5	0	1	4	0	5	11
% Bicycles on Road	-	0.0	1.5	0.0	1.0	-	0.1	0.0	0.0	0.1	0.0	1.7	0.1	0.0	0.1	0.1



Turning Movement Data Plot

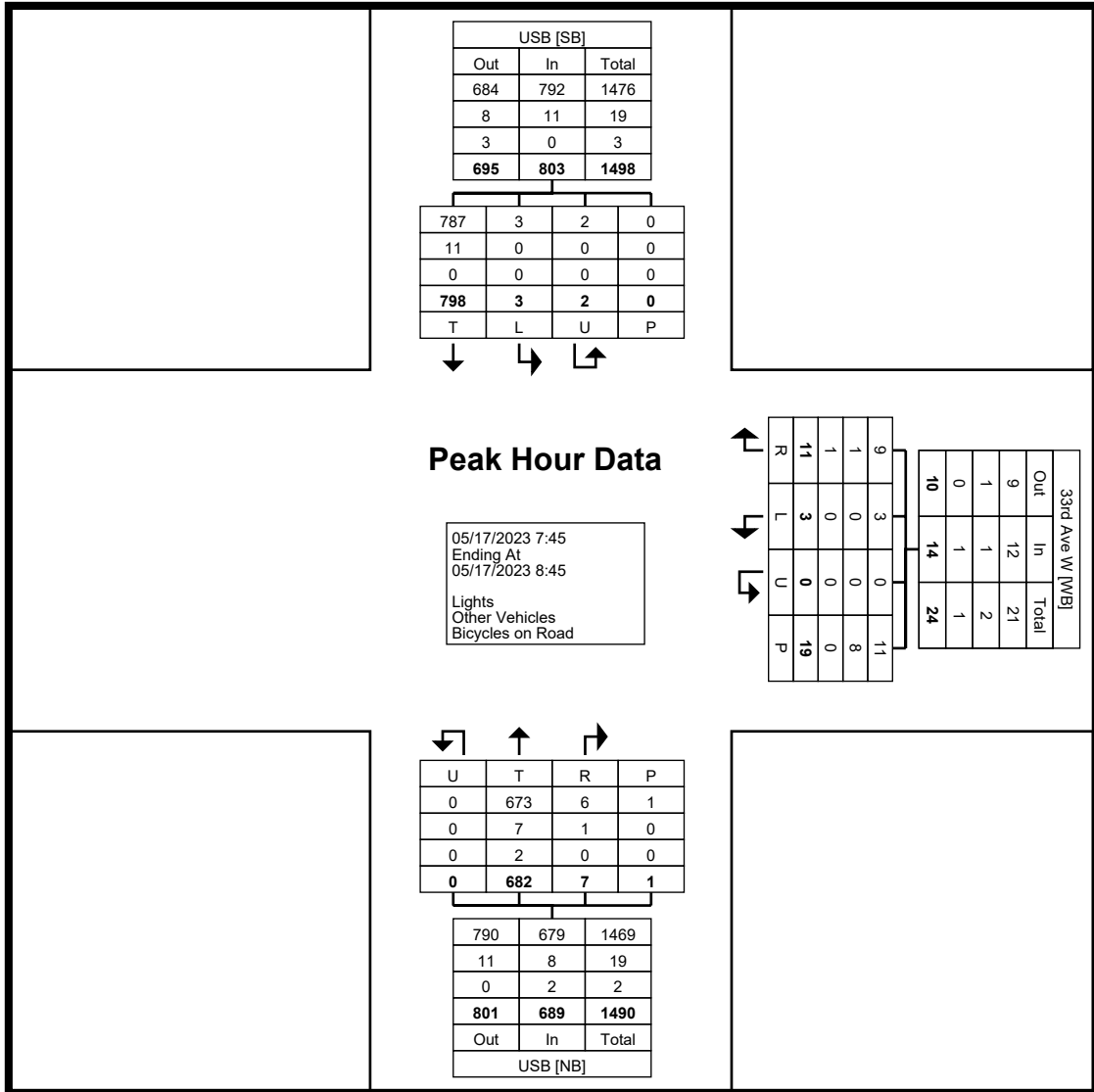
USB 41 & 33rd Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 28\_USB 41 & 33rd  
Ave W  
Site Code: 28  
Start Date: 05/17/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:45)

Start Time	33rd Ave W Westbound					USB Northbound					USB Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	
7:45	0	0	4	4	4	0	190	2	1	192	1	0	160	0	161	357
8:00	0	2	3	3	5	0	195	2	0	197	1	0	218	0	219	421
8:15	0	0	2	6	2	0	157	2	0	159	0	1	202	0	203	364
8:30	0	1	2	6	3	0	140	1	0	141	0	2	218	0	220	364
<b>Total</b>	<b>0</b>	<b>3</b>	<b>11</b>	<b>19</b>	<b>14</b>	<b>0</b>	<b>682</b>	<b>7</b>	<b>1</b>	<b>689</b>	<b>2</b>	<b>3</b>	<b>798</b>	<b>0</b>	<b>803</b>	<b>1506</b>
Approach %	0.0	21.4	78.6	-	-	0.0	99.0	1.0	-	-	0.2	0.4	99.4	-	-	-
Total %	0.0	0.2	0.7	-	0.9	0.0	45.3	0.5	-	45.8	0.1	0.2	53.0	-	53.3	-
PHF	0.000	0.375	0.688	-	0.700	0.000	0.874	0.875	-	0.874	0.500	0.375	0.915	-	0.913	0.894
Lights	0	3	9	11	12	0	673	6	1	679	2	3	787	0	792	1483
% Lights	-	100.0	81.8	57.9	85.7	-	98.7	85.7	100.0	98.5	100.0	100.0	98.6	-	98.6	98.5
Other Vehicles	0	0	1	8	1	0	7	1	0	8	0	0	11	0	11	20
% Other Vehicles	-	0.0	9.1	42.1	7.1	-	1.0	14.3	0.0	1.2	0.0	0.0	1.4	-	1.4	1.3
Bicycles on Road	0	0	1	0	1	0	2	0	0	2	0	0	0	0	0	3
% Bicycles on Road	-	0.0	9.1	0.0	7.1	-	0.3	0.0	0.0	0.3	0.0	0.0	0.0	-	0.0	0.2



Turning Movement Peak Hour Data Plot (7:45)

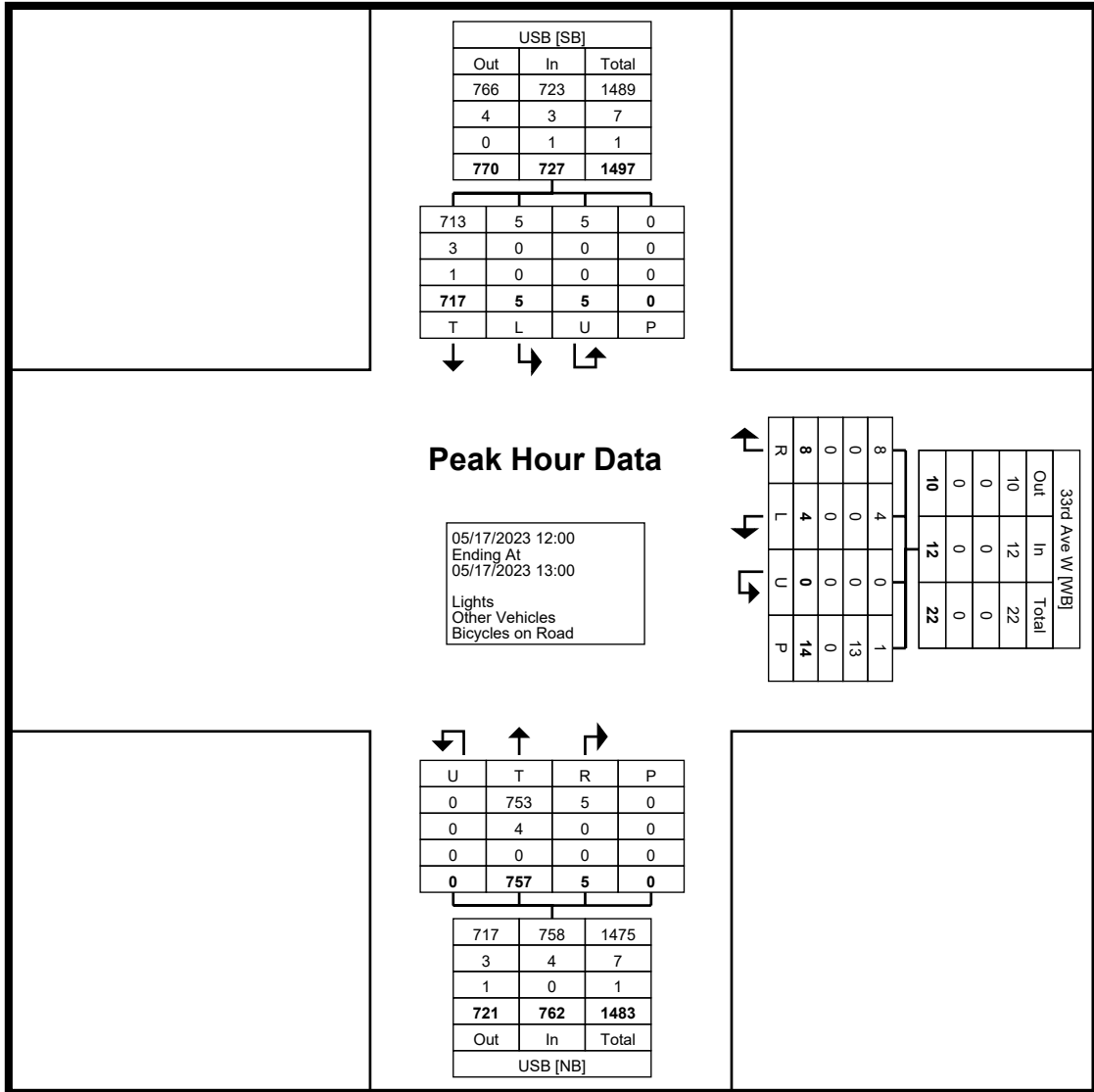
USB 41 & 33rd Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 28\_USB 41 & 33rd  
Ave W  
Site Code: 28  
Start Date: 05/17/2023  
Page No: 5

### Turning Movement Peak Hour Data (12:00)

Start Time	33rd Ave W Westbound					USB Northbound					USB Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	
12:00	0	0	2	4	2	0	192	0	0	192	0	3	200	0	203	397
12:15	0	3	2	4	5	0	178	2	0	180	4	1	175	0	180	365
12:30	0	1	1	3	2	0	195	0	0	195	1	1	181	0	183	380
12:45	0	0	3	3	3	0	192	3	0	195	0	0	161	0	161	359
<b>Total</b>	<b>0</b>	<b>4</b>	<b>8</b>	<b>14</b>	<b>12</b>	<b>0</b>	<b>757</b>	<b>5</b>	<b>0</b>	<b>762</b>	<b>5</b>	<b>5</b>	<b>717</b>	<b>0</b>	<b>727</b>	<b>1501</b>
<b>Approach %</b>	<b>0.0</b>	<b>33.3</b>	<b>66.7</b>	<b>-</b>	<b>-</b>	<b>0.0</b>	<b>99.3</b>	<b>0.7</b>	<b>-</b>	<b>-</b>	<b>0.7</b>	<b>0.7</b>	<b>98.6</b>	<b>-</b>	<b>-</b>	<b>-</b>
<b>Total %</b>	<b>0.0</b>	<b>0.3</b>	<b>0.5</b>	<b>-</b>	<b>0.8</b>	<b>0.0</b>	<b>50.4</b>	<b>0.3</b>	<b>-</b>	<b>50.8</b>	<b>0.3</b>	<b>0.3</b>	<b>47.8</b>	<b>-</b>	<b>48.4</b>	<b>-</b>
<b>PHF</b>	<b>0.000</b>	<b>0.333</b>	<b>0.667</b>	<b>-</b>	<b>0.600</b>	<b>0.000</b>	<b>0.971</b>	<b>0.417</b>	<b>-</b>	<b>0.977</b>	<b>0.313</b>	<b>0.417</b>	<b>0.896</b>	<b>-</b>	<b>0.895</b>	<b>0.945</b>
<b>Lights</b>	<b>0</b>	<b>4</b>	<b>8</b>	<b>1</b>	<b>12</b>	<b>0</b>	<b>753</b>	<b>5</b>	<b>0</b>	<b>758</b>	<b>5</b>	<b>5</b>	<b>713</b>	<b>0</b>	<b>723</b>	<b>1493</b>
<b>% Lights</b>	<b>-</b>	<b>100.0</b>	<b>100.0</b>	<b>7.1</b>	<b>100.0</b>	<b>-</b>	<b>99.5</b>	<b>100.0</b>	<b>-</b>	<b>99.5</b>	<b>100.0</b>	<b>100.0</b>	<b>99.4</b>	<b>-</b>	<b>99.4</b>	<b>99.5</b>
<b>Other Vehicles</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>13</b>	<b>0</b>	<b>0</b>	<b>4</b>	<b>0</b>	<b>0</b>	<b>4</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>3</b>	<b>7</b>
<b>% Other Vehicles</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>	<b>92.9</b>	<b>0.0</b>	<b>-</b>	<b>0.5</b>	<b>0.0</b>	<b>-</b>	<b>0.5</b>	<b>0.0</b>	<b>0.0</b>	<b>0.4</b>	<b>-</b>	<b>0.4</b>	<b>0.5</b>
<b>Bicycles on Road</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>1</b>	<b>1</b>
<b>% Bicycles on Road</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>	<b>0.0</b>	<b>0.0</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>	<b>0.0</b>	<b>0.1</b>	<b>-</b>	<b>0.1</b>	<b>0.1</b>



Turning Movement Peak Hour Data Plot (12:00)



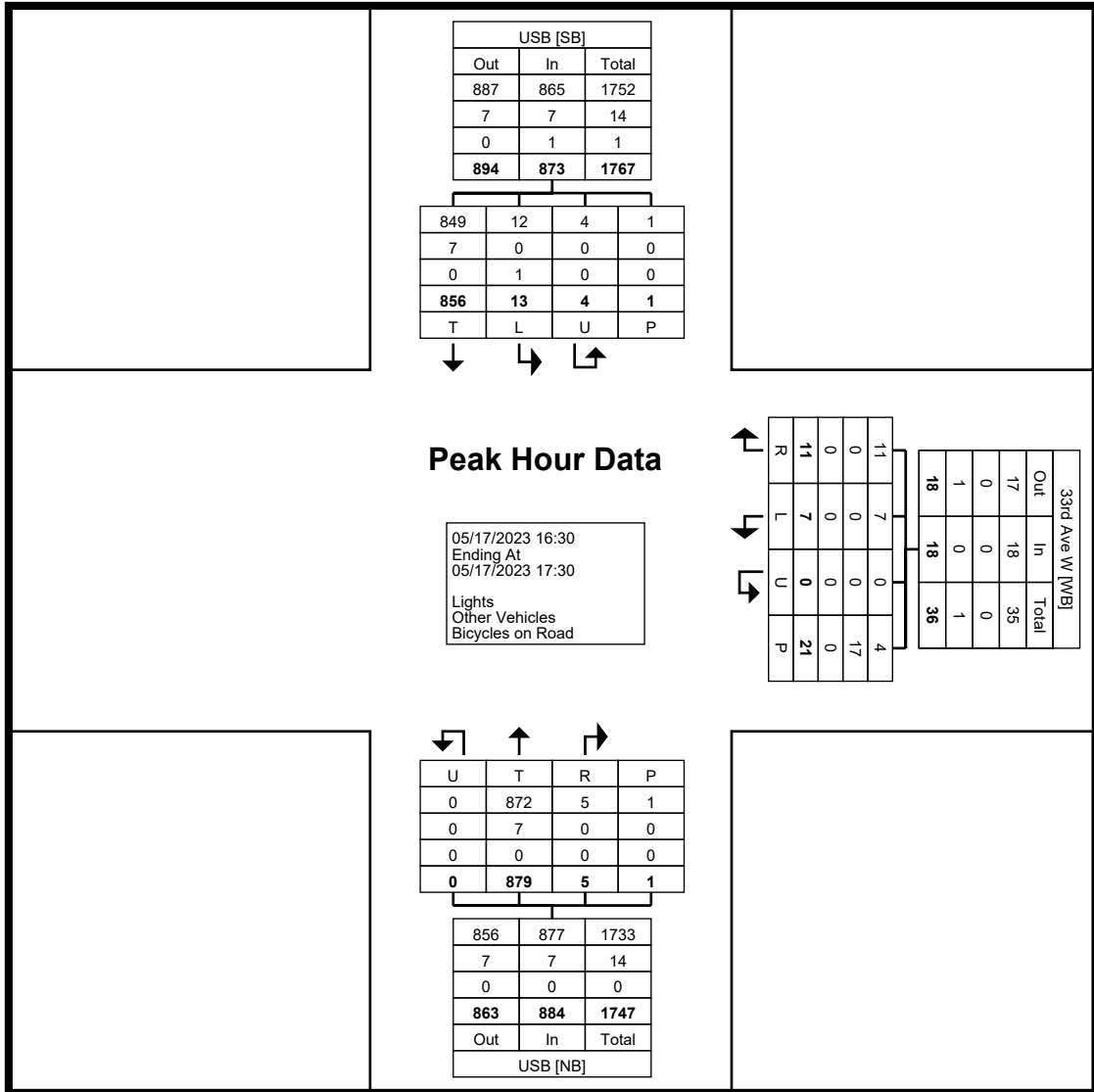
USB 41 & 33rd Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 28\_USB 41 & 33rd  
Ave W  
Site Code: 28  
Start Date: 05/17/2023  
Page No: 7

### Turning Movement Peak Hour Data (16:30)

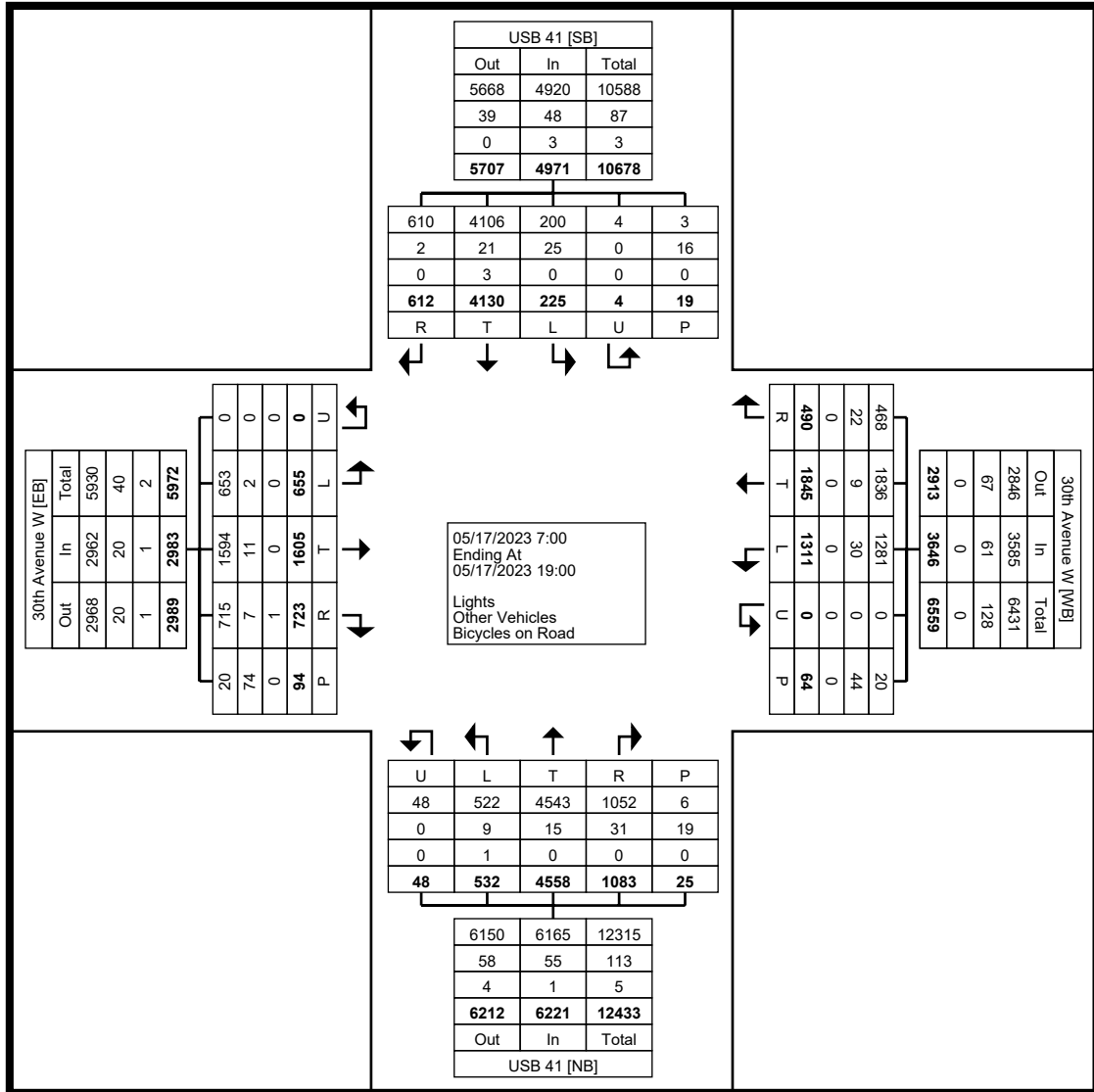
Start Time	33rd Ave W Westbound					USB Northbound					USB Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	
16:30	0	2	2	7	4	0	226	2	0	228	0	5	216	0	221	453
16:45	0	3	3	8	6	0	220	1	1	221	2	1	214	1	217	444
17:00	0	1	3	4	4	0	217	2	0	219	2	2	202	0	206	429
17:15	0	1	3	2	4	0	216	0	0	216	0	5	224	0	229	449
<b>Total</b>	<b>0</b>	<b>7</b>	<b>11</b>	<b>21</b>	<b>18</b>	<b>0</b>	<b>879</b>	<b>5</b>	<b>1</b>	<b>884</b>	<b>4</b>	<b>13</b>	<b>856</b>	<b>1</b>	<b>873</b>	<b>1775</b>
Approach %	0.0	38.9	61.1	-	-	0.0	99.4	0.6	-	-	0.5	1.5	98.1	-	-	-
Total %	0.0	0.4	0.6	-	1.0	0.0	49.5	0.3	-	49.8	0.2	0.7	48.2	-	49.2	-
PHF	0.000	0.583	0.917	-	0.750	0.000	0.972	0.625	-	0.969	0.500	0.650	0.955	-	0.953	0.980
Lights	0	7	11	4	18	0	872	5	1	877	4	12	849	1	865	1760
% Lights	-	100.0	100.0	19.0	100.0	-	99.2	100.0	100.0	99.2	100.0	92.3	99.2	100.0	99.1	99.2
Other Vehicles	0	0	0	17	0	0	7	0	0	7	0	0	7	0	7	14
% Other Vehicles	-	0.0	0.0	81.0	0.0	-	0.8	0.0	0.0	0.8	0.0	0.0	0.8	0.0	0.8	0.8
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
% Bicycles on Road	-	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	7.7	0.0	0.0	0.1	0.1



Turning Movement Peak Hour Data Plot (16:30)

### Turning Movement Data

Start Time	30th Avenue W Eastbound						30th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	15	68	24	3	107	0	29	32	13	1	74	0	16	99	29	0	144	0	10	123	23	0	156	481
7:15	0	17	66	23	3	106	0	43	40	13	0	96	4	25	106	34	0	169	0	9	115	13	0	137	508
7:30	0	20	72	27	0	119	0	35	55	10	0	100	1	10	133	37	0	181	0	12	118	22	2	152	552
7:45	0	24	87	20	5	131	0	33	67	11	1	111	2	20	131	43	1	196	0	7	107	23	0	137	575
Hourly Total	0	76	293	94	11	463	0	140	194	47	2	381	7	71	469	143	1	690	0	38	463	81	2	582	2116
8:00	0	26	60	27	2	113	0	53	65	24	1	142	3	18	131	40	0	192	0	11	153	27	1	191	638
8:15	0	15	54	29	1	98	0	35	67	14	4	116	1	17	108	29	0	155	0	5	146	21	0	172	541
8:30	0	14	40	21	0	75	0	41	47	9	4	97	0	14	107	22	1	143	0	6	144	20	1	170	485
8:45	0	15	39	13	2	67	0	22	48	9	2	79	2	14	123	29	1	168	0	3	147	20	0	170	484
Hourly Total	0	70	193	90	5	353	0	151	227	56	11	434	6	63	469	120	2	658	0	25	590	88	2	703	2148
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	15	30	20	1	65	0	36	42	13	3	91	0	13	143	44	0	200	0	1	146	15	0	162	518
12:15	0	13	31	21	1	65	0	25	26	12	2	63	1	15	137	27	0	180	0	9	130	17	2	156	464
12:30	0	20	37	19	1	76	0	36	29	12	3	77	2	15	152	37	1	206	0	6	124	20	0	150	509
12:45	0	15	33	15	2	63	0	36	39	18	0	93	1	16	126	35	0	178	0	13	120	6	1	139	473
Hourly Total	0	63	131	75	5	269	0	133	136	55	8	324	4	59	558	143	1	764	0	29	520	58	3	607	1964
13:00	0	12	37	21	1	70	0	27	40	8	0	75	0	14	158	25	2	197	1	7	121	13	0	142	484
13:15	0	12	22	12	1	46	0	42	29	13	2	84	2	8	149	34	2	193	0	8	113	7	1	128	451
13:30	0	17	40	23	4	80	0	35	45	13	0	93	3	15	146	28	2	192	0	6	122	16	2	144	509
13:45	0	12	49	25	3	86	0	32	41	7	3	80	0	17	122	31	2	170	0	4	117	9	1	130	466
Hourly Total	0	53	148	81	9	282	0	136	155	41	5	332	5	54	575	118	8	752	1	25	473	45	4	544	1910
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	24	71	28	3	123	0	32	61	18	1	111	2	17	149	46	0	214	0	7	100	20	0	127	575
15:15	0	21	67	25	6	113	0	50	73	16	3	139	0	16	167	34	2	217	0	9	125	20	2	154	623
15:30	0	39	66	26	1	131	0	58	92	16	2	166	0	15	122	28	1	165	0	10	114	29	0	153	615
15:45	0	38	61	25	2	124	0	45	78	26	2	149	1	16	128	39	1	184	0	10	126	15	0	151	608
Hourly Total	0	122	265	104	12	491	0	185	304	76	8	565	3	64	566	147	4	780	0	36	465	84	2	585	2421
16:00	0	34	47	20	4	101	0	53	54	19	3	126	1	24	176	29	1	230	0	5	119	20	3	144	601
16:15	0	28	48	21	2	97	0	55	67	16	3	138	1	19	171	31	0	222	0	5	133	22	0	160	617
16:30	0	18	51	29	3	98	0	40	90	19	3	149	2	19	170	35	1	226	0	6	148	15	0	169	642
16:45	0	30	61	24	2	115	0	59	73	18	2	150	0	22	156	35	0	213	1	3	134	18	0	156	634
Hourly Total	0	110	207	94	11	411	0	207	284	72	11	563	4	84	673	130	2	891	1	19	534	75	3	629	2494
17:00	0	28	68	34	7	130	0	41	71	18	1	130	0	20	153	34	0	207	0	6	134	27	0	167	634
17:15	0	34	50	23	9	107	0	55	90	20	4	165	2	14	188	30	1	234	1	2	140	30	0	173	679
17:30	0	25	61	35	4	121	0	57	73	23	2	153	4	14	140	35	0	193	0	3	153	30	0	186	653
17:45	0	19	47	20	3	86	0	55	87	14	1	156	2	12	169	40	2	223	1	7	149	22	0	179	644
Hourly Total	0	106	226	112	23	444	0	208	321	75	8	604	8	60	650	139	3	857	2	18	576	109	0	705	2610
18:00	0	20	46	25	8	91	0	43	53	23	3	119	3	21	178	39	2	241	0	5	114	15	0	134	585
18:15	0	12	29	16	7	57	0	38	49	15	7	102	1	22	142	41	0	206	0	11	113	24	0	148	513
18:30	0	9	33	20	1	62	0	39	57	17	1	113	5	16	132	28	2	181	0	8	159	23	2	190	546
18:45	0	14	34	12	2	60	0	31	65	13	0	109	2	18	146	35	0	201	0	11	123	10	1	144	514
Hourly Total	0	55	142	73	18	270	0	151	224	68	11	443	11	77	598	143	4	829	0	35	509	72	3	616	2158
Grand Total	0	655	1605	723	94	2983	0	1311	1845	490	64	3646	48	532	4558	1083	25	6221	4	225	4130	612	19	4971	17821
Approach %	0.0	22.0	53.8	24.2	-	-	0.0	36.0	50.6	13.4	-	-	0.8	8.6	73.3	17.4	-	-	0.1	4.5	83.1	12.3	-	-	-
Total %	0.0	3.7	9.0	4.1	-	16.7	0.0	7.4	10.4	2.7	-	20.5	0.3	3.0	25.6	6.1	-	34.9	0.0	1.3	23.2	3.4	-	27.9	-
Lights	0	653	1594	715	20	2962	0	1281	1836	468	20	3585	48	522	4543	1052	6	6165	4	200	4106	610	3	4920	17632
% Lights	-	99.7	99.3	98.9	21.3	99.3	-	97.7	99.5	95.5	31.3	98.3	100.0	98.1	99.7	97.1	24.0	99.1	100.0	88.9	99.4	99.7	15.8	99.0	98.9
Other Vehicles	0	2	11	7	74	20	0	30	9	22	44	61	0	9	15	31	19	55	0	25	21	2	16	48	184
% Other Vehicles	-	0.3	0.7	1.0	78.7	0.7	-	2.3	0.5	4.5	68.8	1.7	0.0	1.7	0.3	2.9	76.0	0.9	0.0	11.1	0.5	0.3	84.2	1.0	1.0
Bicycles on Road	0	0	0	1	0	1	0	0	0	0	0	0	0	1	0	0	0	1	0	0	3	0	0	3	5
% Bicycles on Road	-	0.0	0.0	0.1	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.0	0.0	0.1	0.0



Turning Movement Data Plot

USB 41 at 30th Avenue W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 27\_USB 41 at  
30th Avenue W  
Site Code: 27  
Start Date: 05/17/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:30)

Start Time	30th Avenue W Eastbound						30th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:30	0	20	72	27	0	119	0	35	55	10	0	100	1	10	133	37	0	181	0	12	118	22	2	152	552
7:45	0	24	87	20	5	131	0	33	67	11	1	111	2	20	131	43	1	196	0	7	107	23	0	137	575
8:00	0	26	60	27	2	113	0	53	65	24	1	142	3	18	131	40	0	192	0	11	153	27	1	191	638
8:15	0	15	54	29	1	98	0	35	67	14	4	116	1	17	108	29	0	155	0	5	146	21	0	172	541
<b>Total</b>	<b>0</b>	<b>85</b>	<b>273</b>	<b>103</b>	<b>8</b>	<b>461</b>	<b>0</b>	<b>156</b>	<b>254</b>	<b>59</b>	<b>6</b>	<b>469</b>	<b>7</b>	<b>65</b>	<b>503</b>	<b>149</b>	<b>1</b>	<b>724</b>	<b>0</b>	<b>35</b>	<b>524</b>	<b>93</b>	<b>3</b>	<b>652</b>	<b>2306</b>
Approach %	0.0	18.4	59.2	22.3	-	-	0.0	33.3	54.2	12.6	-	-	1.0	9.0	69.5	20.6	-	-	0.0	5.4	80.4	14.3	-	-	-
Total %	0.0	3.7	11.8	4.5	-	20.0	0.0	6.8	11.0	2.6	-	20.3	0.3	2.8	21.8	6.5	-	31.4	0.0	1.5	22.7	4.0	-	28.3	-
PHF	0.000	0.817	0.784	0.888	-	0.880	0.000	0.736	0.948	0.615	-	0.826	0.583	0.813	0.945	0.866	-	0.923	0.000	0.729	0.856	0.861	-	0.853	0.904
Lights	0	85	270	102	2	457	0	150	252	56	2	458	7	63	500	144	0	714	0	30	522	92	0	644	2273
% Lights	-	100.0	98.9	99.0	25.0	99.1	-	96.2	99.2	94.9	33.3	97.7	100.0	96.9	99.4	96.6	0.0	98.6	-	85.7	99.6	98.9	0.0	98.8	98.6
Other Vehicles	0	0	3	1	6	4	0	6	2	3	4	11	0	2	3	5	1	10	0	5	2	1	3	8	33
% Other Vehicles	-	0.0	1.1	1.0	75.0	0.9	-	3.8	0.8	5.1	66.7	2.3	0.0	3.1	0.6	3.4	100.0	1.4	-	14.3	0.4	1.1	100.0	1.2	1.4
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



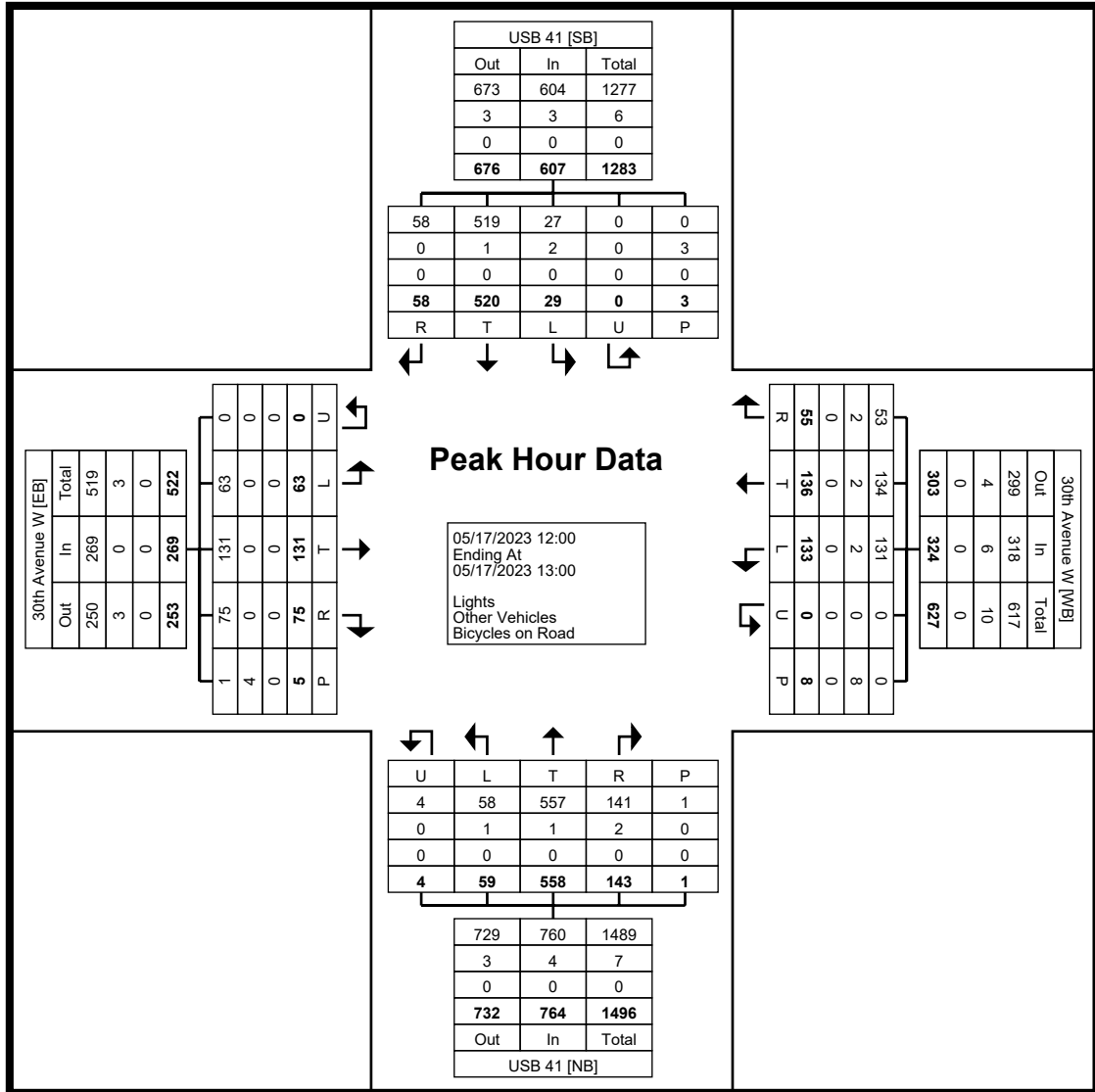
USB 41 at 30th Avenue W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 27\_USB 41 at  
30th Avenue W  
Site Code: 27  
Start Date: 05/17/2023  
Page No: 5

### Turning Movement Peak Hour Data (12:00)

Start Time	30th Avenue W Eastbound						30th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	15	30	20	1	65	0	36	42	13	3	91	0	13	143	44	0	200	0	1	146	15	0	162	518
12:15	0	13	31	21	1	65	0	25	26	12	2	63	1	15	137	27	0	180	0	9	130	17	2	156	464
12:30	0	20	37	19	1	76	0	36	29	12	3	77	2	15	152	37	1	206	0	6	124	20	0	150	509
12:45	0	15	33	15	2	63	0	36	39	18	0	93	1	16	126	35	0	178	0	13	120	6	1	139	473
<b>Total</b>	0	63	131	75	5	269	0	133	136	55	8	324	4	59	558	143	1	764	0	29	520	58	3	607	1964
Approach %	0.0	23.4	48.7	27.9	-	-	0.0	41.0	42.0	17.0	-	-	0.5	7.7	73.0	18.7	-	-	0.0	4.8	85.7	9.6	-	-	-
Total %	0.0	3.2	6.7	3.8	-	13.7	0.0	6.8	6.9	2.8	-	16.5	0.2	3.0	28.4	7.3	-	38.9	0.0	1.5	26.5	3.0	-	30.9	-
PHF	0.000	0.788	0.885	0.893	-	0.885	0.000	0.924	0.810	0.764	-	0.871	0.500	0.922	0.918	0.813	-	0.927	0.000	0.558	0.890	0.725	-	0.937	0.948
Lights	0	63	131	75	1	269	0	131	134	53	0	318	4	58	557	141	1	760	0	27	519	58	0	604	1951
% Lights	-	100.0	100.0	100.0	20.0	100.0	-	98.5	98.5	96.4	0.0	98.1	100.0	98.3	99.8	98.6	100.0	99.5	-	93.1	99.8	100.0	0.0	99.5	99.3
Other Vehicles	0	0	0	0	4	0	0	2	2	2	8	6	0	1	1	2	0	4	0	2	1	0	3	3	13
% Other Vehicles	-	0.0	0.0	0.0	80.0	0.0	-	1.5	1.5	3.6	100.0	1.9	0.0	1.7	0.2	1.4	0.0	0.5	-	6.9	0.2	0.0	100.0	0.5	0.7
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (12:00)



USB 41 at 30th Avenue W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 27\_USB 41 at  
30th Avenue W  
Site Code: 27  
Start Date: 05/17/2023  
Page No: 7

### Turning Movement Peak Hour Data (17:00)

Start Time	30th Avenue W Eastbound						30th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
17:00	0	28	68	34	7	130	0	41	71	18	1	130	0	20	153	34	0	207	0	6	134	27	0	167	634
17:15	0	34	50	23	9	107	0	55	90	20	4	165	2	14	188	30	1	234	1	2	140	30	0	173	679
17:30	0	25	61	35	4	121	0	57	73	23	2	153	4	14	140	35	0	193	0	3	153	30	0	186	653
17:45	0	19	47	20	3	86	0	55	87	14	1	156	2	12	169	40	2	223	1	7	149	22	0	179	644
<b>Total</b>	<b>0</b>	<b>106</b>	<b>226</b>	<b>112</b>	<b>23</b>	<b>444</b>	<b>0</b>	<b>208</b>	<b>321</b>	<b>75</b>	<b>8</b>	<b>604</b>	<b>8</b>	<b>60</b>	<b>650</b>	<b>139</b>	<b>3</b>	<b>857</b>	<b>2</b>	<b>18</b>	<b>576</b>	<b>109</b>	<b>0</b>	<b>705</b>	<b>2610</b>
Approach %	0.0	23.9	50.9	25.2	-	-	0.0	34.4	53.1	12.4	-	-	0.9	7.0	75.8	16.2	-	-	0.3	2.6	81.7	15.5	-	-	-
Total %	0.0	4.1	8.7	4.3	-	17.0	0.0	8.0	12.3	2.9	-	23.1	0.3	2.3	24.9	5.3	-	32.8	0.1	0.7	22.1	4.2	-	27.0	-
PHF	0.000	0.779	0.831	0.800	-	0.854	0.000	0.912	0.892	0.815	-	0.915	0.500	0.750	0.864	0.869	-	0.916	0.500	0.643	0.941	0.908	-	0.948	0.961
Lights	0	104	226	112	4	442	0	206	321	71	2	598	8	60	649	131	0	848	2	15	571	109	0	697	2585
% Lights	-	98.1	100.0	100.0	17.4	99.5	-	99.0	100.0	94.7	25.0	99.0	100.0	100.0	99.8	94.2	0.0	98.9	100.0	83.3	99.1	100.0	-	98.9	99.0
Other Vehicles	0	2	0	0	19	2	0	2	0	4	6	6	0	0	1	8	3	9	0	3	4	0	0	7	24
% Other Vehicles	-	1.9	0.0	0.0	82.6	0.5	-	1.0	0.0	5.3	75.0	1.0	0.0	0.0	0.2	5.8	100.0	1.1	0.0	16.7	0.7	0.0	-	1.0	0.9
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.0	-	0.1	0.0



USB 41 (14th St W) at 29th Ave  
W turnout NB to EB  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 26\_USB 41 (14th  
St W) at 29th Ave W turnout NB  
to EB  
Site Code: 26  
Start Date: 07/06/2023  
Page No: 1

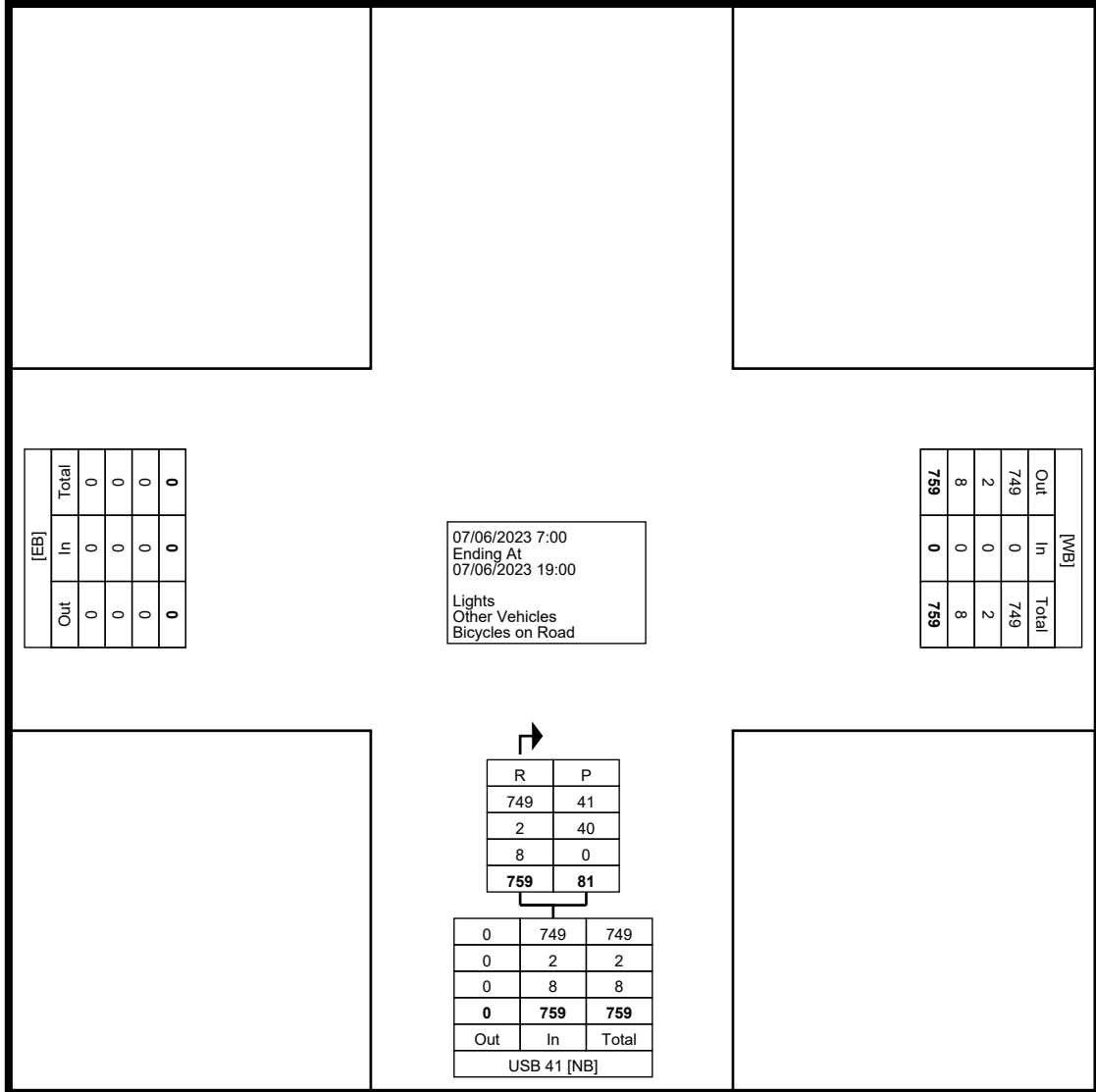
### Turning Movement Data

Start Time	USB 41 Northbound		App. Total	Int. Total
	Right	Peds		
7:00	24	0	24	24
7:15	16	2	16	16
7:30	22	1	22	22
7:45	22	1	22	22
<b>Hourly Total</b>	<b>84</b>	<b>4</b>	<b>84</b>	<b>84</b>
8:00	12	1	12	12
8:15	20	3	20	20
8:30	20	1	20	20
8:45	30	6	30	30
<b>Hourly Total</b>	<b>82</b>	<b>11</b>	<b>82</b>	<b>82</b>
*** BREAK ***	-	-	-	-
12:00	20	3	20	20
12:15	23	3	23	23
12:30	18	1	18	18
12:45	16	2	16	16
<b>Hourly Total</b>	<b>77</b>	<b>9</b>	<b>77</b>	<b>77</b>
13:00	31	1	31	31
13:15	23	2	23	23
13:30	20	3	20	20
13:45	18	2	18	18
<b>Hourly Total</b>	<b>92</b>	<b>8</b>	<b>92</b>	<b>92</b>
*** BREAK ***	-	-	-	-
15:00	21	0	21	21
15:15	24	2	24	24
15:30	21	1	21	21
15:45	34	5	34	34
<b>Hourly Total</b>	<b>100</b>	<b>8</b>	<b>100</b>	<b>100</b>
16:00	18	7	18	18
16:15	35	1	35	35
16:30	34	3	34	34
16:45	26	4	26	26
<b>Hourly Total</b>	<b>113</b>	<b>15</b>	<b>113</b>	<b>113</b>
17:00	33	5	33	33
17:15	26	4	26	26
17:30	26	3	26	26
17:45	18	4	18	18
<b>Hourly Total</b>	<b>103</b>	<b>16</b>	<b>103</b>	<b>103</b>
18:00	21	2	21	21
18:15	36	1	36	36
18:30	25	4	25	25
18:45	26	3	26	26
<b>Hourly Total</b>	<b>108</b>	<b>10</b>	<b>108</b>	<b>108</b>
<b>Grand Total</b>	<b>759</b>	<b>81</b>	<b>759</b>	<b>759</b>
Approach %	100.0	-	-	-
Total %	100.0	-	100.0	-
Lights	749	41	749	749
% Lights	98.7	50.6	98.7	98.7
Other Vehicles	2	40	2	2
% Other Vehicles	0.3	49.4	0.3	0.3
Bicycles on Road	8	0	8	8
% Bicycles on Road	1.1	0.0	1.1	1.1

USB 41 (14th St W) at 29th Ave  
 W turnout NB to EB  
 Weekday TMC

Florida Transportation Engineering, Inc.  
 (FTE)  
 8250 Pascal Dr  
 Punta Gorda, Florida, United States 33950  
 (800) 639-4851

Count Name: 26\_USB 41 (14th  
 St W) at 29th Ave W turnout NB  
 to EB  
 Site Code: 26  
 Start Date: 07/06/2023  
 Page No: 2



Turning Movement Data Plot

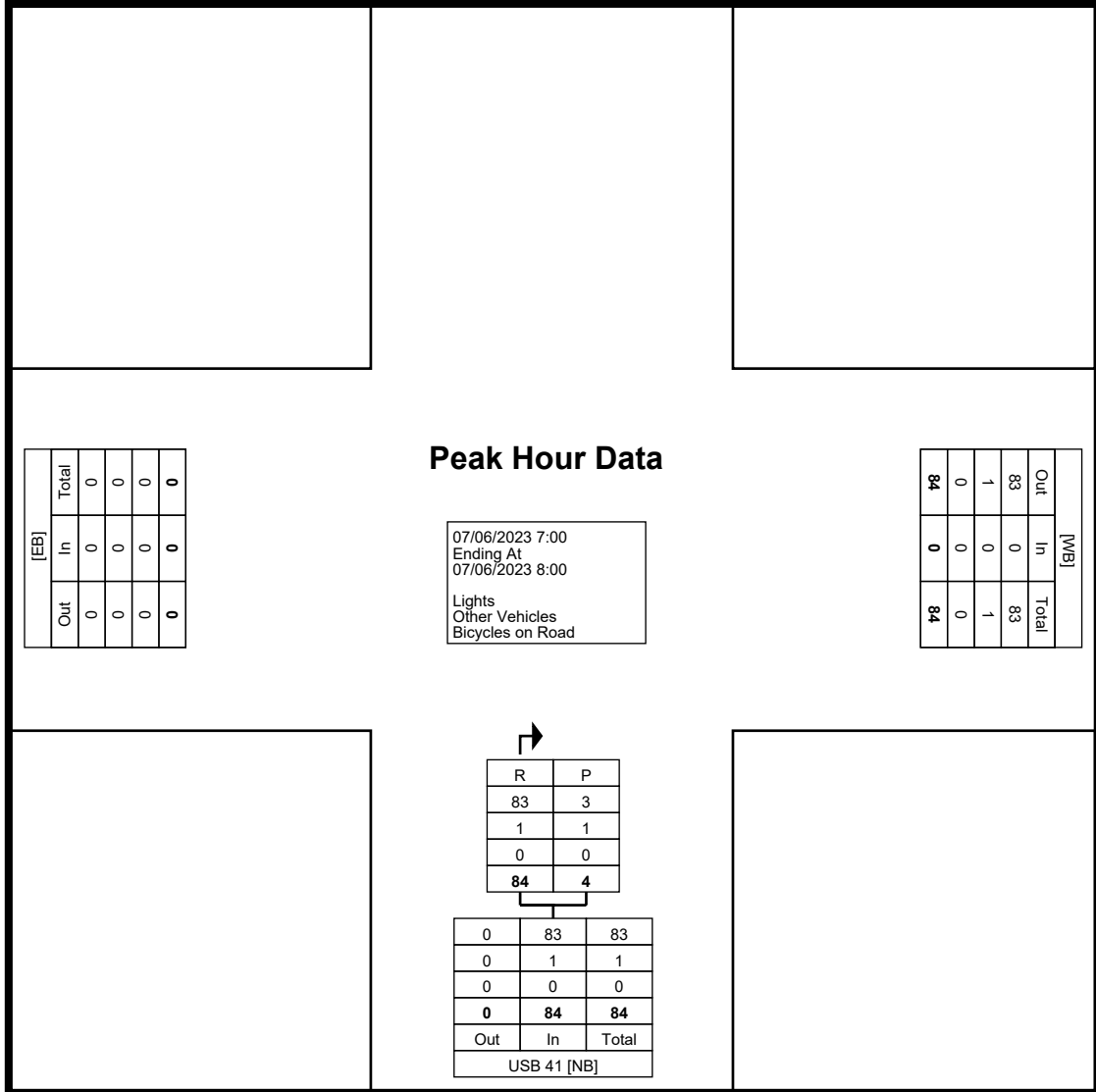
USB 41 (14th St W) at 29th Ave  
W turnout NB to EB  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 26\_USB 41 (14th  
St W) at 29th Ave W turnout NB  
to EB  
Site Code: 26  
Start Date: 07/06/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:00)

Start Time	USB 41 Northbound		App. Total	Int. Total
	Right	Peds		
7:00	24	0	24	24
7:15	16	2	16	16
7:30	22	1	22	22
7:45	22	1	22	22
Total	84	4	84	84
Approach %	100.0	-	-	-
Total %	100.0	-	100.0	-
PHF	0.875	-	0.875	0.875
Lights	83	3	83	83
% Lights	98.8	75.0	98.8	98.8
Other Vehicles	1	1	1	1
% Other Vehicles	1.2	25.0	1.2	1.2
Bicycles on Road	0	0	0	0
% Bicycles on Road	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (7:00)

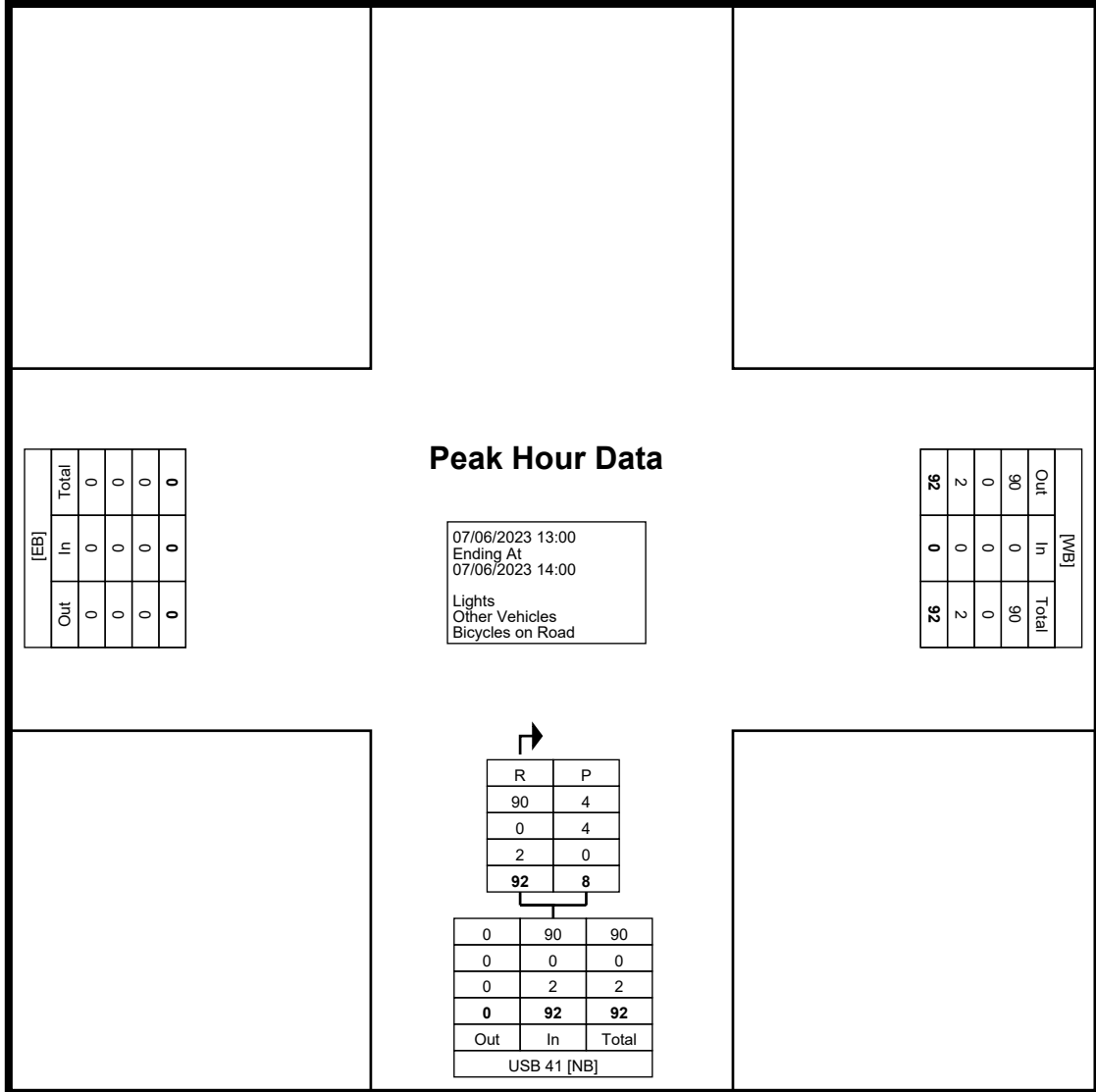
USB 41 (14th St W) at 29th Ave  
W turnout NB to EB  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 26\_USB 41 (14th  
St W) at 29th Ave W turnout NB  
to EB  
Site Code: 26  
Start Date: 07/06/2023  
Page No: 5

### Turning Movement Peak Hour Data (13:00)

Start Time	USB 41 Northbound		App. Total	Int. Total
	Right	Peds		
13:00	31	1	31	31
13:15	23	2	23	23
13:30	20	3	20	20
13:45	18	2	18	18
Total	92	8	92	92
Approach %	100.0	-	-	-
Total %	100.0	-	100.0	-
PHF	0.742	-	0.742	0.742
Lights	90	4	90	90
% Lights	97.8	50.0	97.8	97.8
Other Vehicles	0	4	0	0
% Other Vehicles	0.0	50.0	0.0	0.0
Bicycles on Road	2	0	2	2
% Bicycles on Road	2.2	0.0	2.2	2.2



Turning Movement Peak Hour Data Plot (13:00)



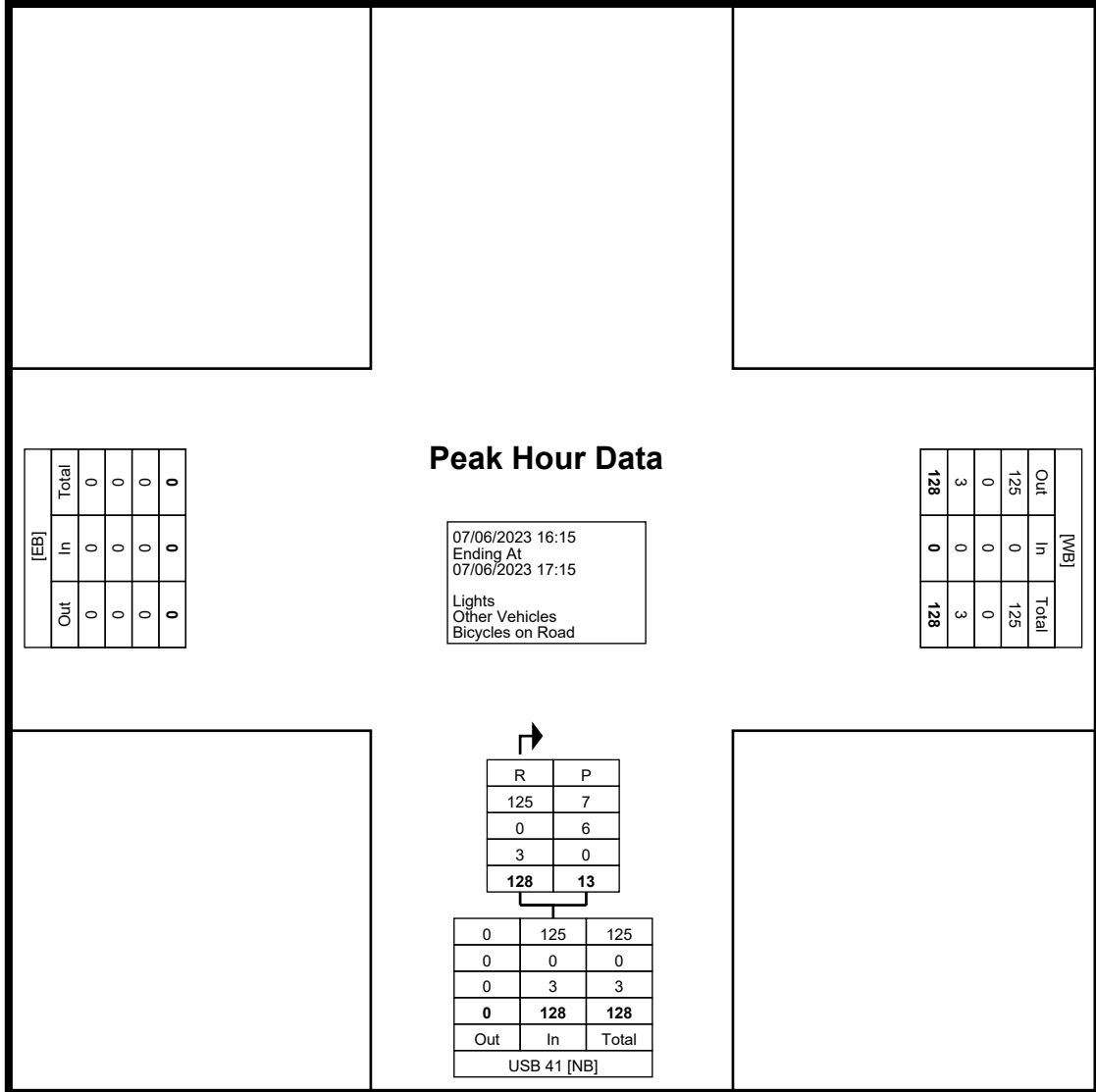
USB 41 (14th St W) at 29th Ave  
W turnout NB to EB  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 26\_USB 41 (14th  
St W) at 29th Ave W turnout NB  
to EB  
Site Code: 26  
Start Date: 07/06/2023  
Page No: 7

### Turning Movement Peak Hour Data (16:15)

Start Time	USB 41 Northbound		App. Total	Int. Total
	Right	Peds		
16:15	35	1	35	35
16:30	34	3	34	34
16:45	26	4	26	26
17:00	33	5	33	33
Total	128	13	128	128
Approach %	100.0	-	-	-
Total %	100.0	-	100.0	-
PHF	0.914	-	0.914	0.914
Lights	125	7	125	125
% Lights	97.7	53.8	97.7	97.7
Other Vehicles	0	6	0	0
% Other Vehicles	0.0	46.2	0.0	0.0
Bicycles on Road	3	0	3	3
% Bicycles on Road	2.3	0.0	2.3	2.3



Turning Movement Peak Hour Data Plot (16:15)

### Turning Movement Data

Start Time	Driveway Eastbound						29th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	0	0	2	4	2	0	11	1	3	4	15	0	0	101	0	0	101	0	1	119	1	0	121	239
7:15	0	0	0	0	3	0	0	15	0	1	0	16	0	0	108	0	0	108	1	2	133	1	0	137	261
7:30	0	0	0	2	0	2	0	12	0	1	1	13	0	0	151	0	0	151	0	2	126	0	0	128	294
7:45	0	1	0	1	2	2	0	11	0	0	1	11	0	0	131	0	0	131	0	1	136	1	1	138	282
Hourly Total	0	1	0	5	9	6	0	49	1	5	6	55	0	0	491	0	0	491	1	6	514	3	1	524	1076
8:00	0	2	0	0	2	2	0	18	0	0	2	18	1	1	114	0	0	116	0	2	140	1	0	143	279
8:15	0	1	0	0	1	1	0	19	0	2	4	21	0	1	141	0	0	142	0	2	127	0	0	129	293
8:30	0	0	0	1	9	1	0	19	0	3	6	22	0	1	119	0	0	120	0	1	137	1	2	139	282
8:45	0	1	1	0	1	2	0	16	1	4	6	21	0	1	148	0	0	149	0	0	160	0	0	160	332
Hourly Total	0	4	1	1	13	6	0	72	1	9	18	82	1	4	522	0	0	527	0	5	564	2	2	571	1186
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	0	1	3	1	0	13	0	1	3	14	0	1	152	1	0	154	0	0	159	0	0	159	328
12:15	0	1	0	2	1	3	1	21	0	2	4	24	0	0	154	0	0	154	0	1	138	0	0	139	320
12:30	0	0	1	0	2	1	0	9	0	3	0	12	0	1	139	1	0	141	0	5	138	2	0	145	299
12:45	0	0	0	0	2	0	0	14	0	2	1	16	1	0	172	0	0	173	0	1	144	0	0	145	334
Hourly Total	0	1	1	3	8	5	1	57	0	8	8	66	1	2	617	2	0	622	0	7	579	2	0	588	1281
13:00	0	2	0	2	0	4	0	12	1	6	2	19	1	0	160	0	0	161	0	1	123	2	0	126	310
13:15	0	0	0	1	1	1	0	25	0	2	3	27	0	0	167	0	0	167	0	0	147	1	1	148	343
13:30	0	0	0	0	1	0	0	21	0	5	4	26	0	0	171	0	0	171	0	0	118	0	0	118	315
13:45	0	0	2	0	1	2	0	15	0	2	5	17	0	0	135	0	0	135	0	0	138	0	1	138	292
Hourly Total	0	2	2	3	3	7	0	73	1	15	14	89	1	0	633	0	0	634	0	1	526	3	2	530	1260
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	1	0	0	3	1	0	19	0	8	0	27	0	1	160	0	0	161	1	0	162	3	4	166	355
15:15	0	0	0	1	2	1	0	12	0	3	1	15	1	0	199	0	0	200	0	0	135	0	0	135	351
15:30	0	1	0	0	5	1	0	16	8	2	0	26	0	0	186	1	0	187	0	2	147	0	0	149	363
15:45	0	0	0	1	1	1	0	18	0	4	8	22	0	1	161	0	0	162	0	3	149	1	0	153	338
Hourly Total	0	2	0	2	11	4	0	65	8	17	9	90	1	2	706	1	0	710	1	5	593	4	4	603	1407
16:00	0	0	0	3	6	3	0	13	0	3	0	16	0	1	192	0	0	193	0	2	141	1	1	144	356
16:15	0	1	0	0	2	1	0	18	1	6	2	25	0	1	215	0	0	216	0	2	130	0	0	132	374
16:30	0	0	0	2	3	2	0	21	0	4	4	25	1	0	199	0	0	200	0	2	171	0	0	173	400
16:45	0	0	0	2	8	2	0	22	0	3	6	25	0	1	208	0	1	209	1	1	159	1	2	162	398
Hourly Total	0	1	0	7	19	8	0	74	1	16	12	91	1	3	814	0	1	818	1	7	601	2	3	611	1528
17:00	0	0	0	0	6	0	0	20	0	12	3	32	2	0	184	0	0	186	0	0	200	0	0	200	418
17:15	0	0	0	0	4	0	0	20	0	6	3	26	1	0	224	0	0	225	0	0	182	0	0	182	433
17:30	0	0	0	0	6	0	0	22	0	4	4	26	0	0	189	0	0	189	0	0	139	0	0	139	354
17:45	0	0	0	2	1	2	0	18	0	5	1	23	1	0	177	0	0	178	0	1	136	4	0	141	344
Hourly Total	0	0	0	2	17	2	0	80	0	27	11	107	4	0	774	0	0	778	0	1	657	4	0	662	1549
18:00	0	0	0	2	4	2	0	20	0	1	2	21	1	0	187	0	0	188	0	2	124	4	0	130	341
18:15	0	0	2	0	4	2	0	16	0	3	0	19	0	0	163	0	0	163	0	0	141	0	0	141	325
18:30	0	0	0	0	4	0	0	12	0	5	4	17	1	0	132	0	0	133	0	0	123	1	1	124	274
18:45	0	0	0	0	1	0	0	23	0	2	4	25	1	1	160	0	0	162	0	0	120	0	0	120	307
Hourly Total	0	0	2	2	13	4	0	71	0	11	10	82	3	1	642	0	0	646	0	2	508	5	1	515	1247
Grand Total	0	11	6	25	93	42	1	541	12	108	88	662	12	12	5199	3	1	5226	3	34	4542	25	13	4604	10534
Approach %	0.0	26.2	14.3	59.5	-	-	0.2	81.7	1.8	16.3	-	-	0.2	0.2	99.5	0.1	-	-	0.1	0.7	98.7	0.5	-	-	-
Total %	0.0	0.1	0.1	0.2	-	0.4	0.0	5.1	0.1	1.0	-	6.3	0.1	0.1	49.4	0.0	-	49.6	0.0	0.3	43.1	0.2	-	43.7	-
Lights	0	11	6	25	-	42	1	539	10	106	-	656	12	12	5090	3	-	5117	3	33	4459	25	-	4520	10335
% Lights	-	100.0	100.0	100.0	-	100.0	100.0	99.6	83.3	98.1	-	99.1	100.0	100.0	97.9	100.0	-	97.9	100.0	97.1	98.2	100.0	-	98.2	98.1
Other Vehicles	0	0	0	0	-	0	0	2	2	2	-	6	0	0	102	0	-	102	0	1	79	0	-	80	188
% Other Vehicles	-	0.0	0.0	0.0	-	0.0	0.0	0.4	16.7	1.9	-	0.9	0.0	0.0	2.0	0.0	-	2.0	0.0	2.9	1.7	0.0	-	1.7	1.8
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	7	0	-	7	0	0	4	0	-	4	11
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.1	0.0	-	0.1	0.0	0.0	0.1	0.0	-	0.1	0.1
Bicycles on Crosswalk	-	-	-	-	61	-	-	-	-	-	31	-	-	-	-	-	1	-	-	-	-	-	2	-	-
% Bicycles on Crosswalk	-	-	-	-	65.6	-	-	-	-	-	35.2	-	-	-	-	-	100.0	-	-	-	-	-	15.4	-	-
Pedestrians	-	-	-	-	32	-	-	-	-	-	57	-	-	-	-	-	0	-	-	-	-	-	11	-	-
% Pedestrians	-	-	-	-	34.4	-	-	-	-	-	64.8	-	-	-	-	-	0.0	-	-	-	-	-	84.6	-	-



USB 41 (14th St W) & 29th Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 25 USB 41 (14th St W) & 29th Ave W  
Site Code: 25  
Start Date: 07/06/2023  
Page No: 3

### Turning Movement Peak Hour Data (8:00)

Start Time	Driveway Eastbound						29th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
8:00	0	2	0	0	2	2	0	18	0	0	2	18	1	1	114	0	0	116	0	2	140	1	0	143	279
8:15	0	1	0	0	1	1	0	19	0	2	4	21	0	1	141	0	0	142	0	2	127	0	0	129	293
8:30	0	0	0	1	9	1	0	19	0	3	6	22	0	1	119	0	0	120	0	1	137	1	2	139	282
8:45	0	1	1	0	1	2	0	16	1	4	6	21	0	1	148	0	0	149	0	0	160	0	0	160	332
<b>Total</b>	<b>0</b>	<b>4</b>	<b>1</b>	<b>1</b>	<b>13</b>	<b>6</b>	<b>0</b>	<b>72</b>	<b>1</b>	<b>9</b>	<b>18</b>	<b>82</b>	<b>1</b>	<b>4</b>	<b>522</b>	<b>0</b>	<b>0</b>	<b>527</b>	<b>0</b>	<b>5</b>	<b>564</b>	<b>2</b>	<b>2</b>	<b>571</b>	<b>1186</b>
Approach %	0.0	66.7	16.7	16.7	-	-	0.0	87.8	1.2	11.0	-	-	0.2	0.8	99.1	0.0	-	-	0.0	0.9	98.8	0.4	-	-	-
Total %	0.0	0.3	0.1	0.1	-	0.5	0.0	6.1	0.1	0.8	-	6.9	0.1	0.3	44.0	0.0	-	44.4	0.0	0.4	47.6	0.2	-	48.1	-
PHF	0.000	0.500	0.250	0.250	-	0.750	0.000	0.947	0.250	0.563	-	0.932	0.250	1.000	0.882	0.000	-	0.884	0.000	0.625	0.881	0.500	-	0.892	0.893
Lights	0	4	1	1	-	6	0	72	1	8	-	81	1	4	505	0	-	510	0	4	547	2	-	553	1150
% Lights	-	100.0	100.0	100.0	-	100.0	-	100.0	100.0	88.9	-	98.8	100.0	100.0	96.7	-	-	96.8	-	80.0	97.0	100.0	-	96.8	97.0
Other Vehicles	0	0	0	0	-	0	0	0	0	1	-	1	0	0	16	0	-	16	0	1	16	0	-	17	34
% Other Vehicles	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	11.1	-	1.2	0.0	0.0	3.1	-	-	3.0	-	20.0	2.8	0.0	-	3.0	2.9
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	1	0	-	1	0	0	1	0	-	1	2
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.2	-	-	0.2	-	0.0	0.2	0.0	-	0.2	0.2
Bicycles on Crosswalk	-	-	-	-	9	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	69.2	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	4	-	-	-	-	-	18	-	-	-	-	-	0	-	-	-	-	-	2	-	-
% Pedestrians	-	-	-	-	30.8	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



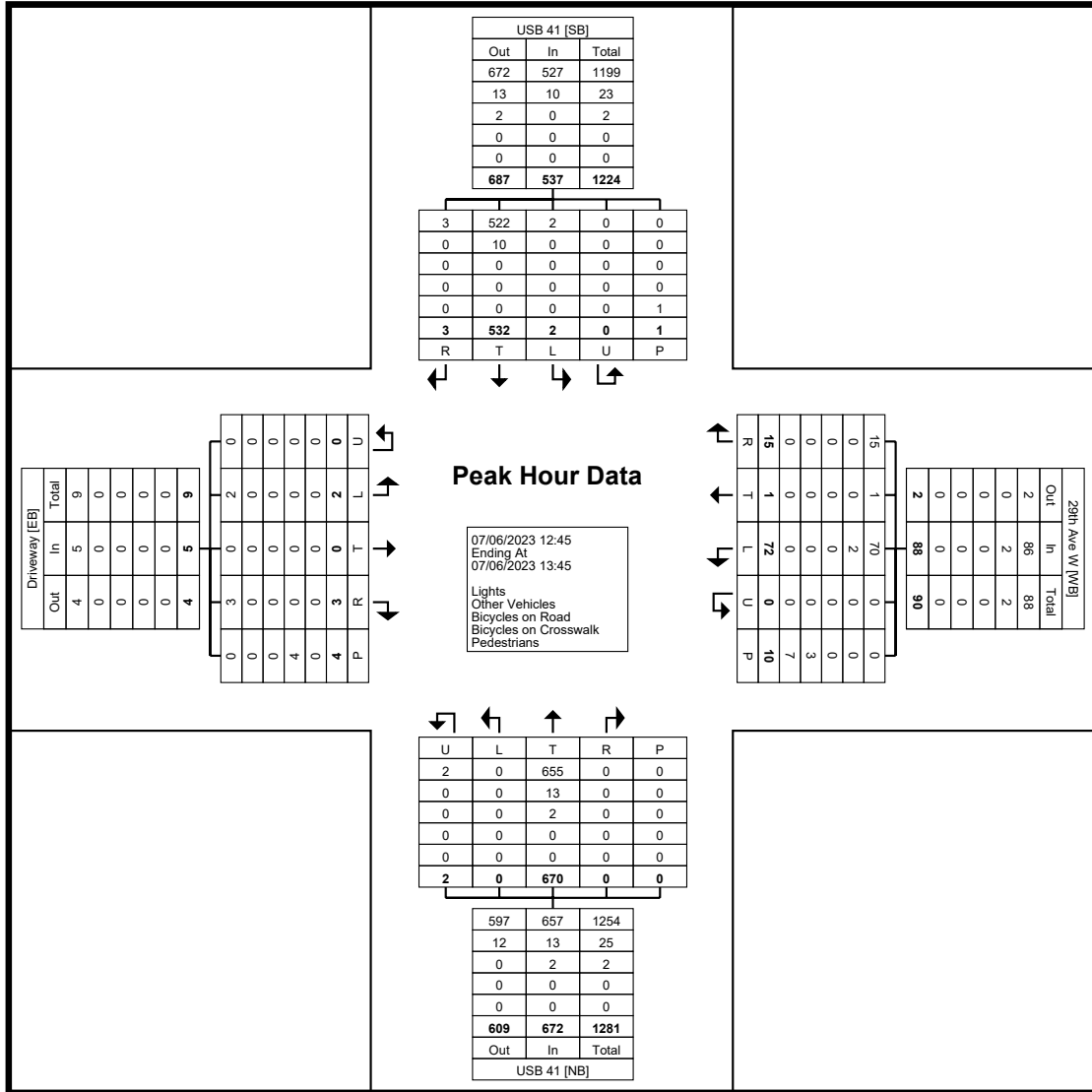
USB 41 (14th St W) & 29th Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 25 USB 41 (14th St W) & 29th Ave W  
Site Code: 25  
Start Date: 07/06/2023  
Page No: 5

### Turning Movement Peak Hour Data (12:45)

Start Time	Driveway Eastbound						29th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:45	0	0	0	0	2	0	0	14	0	2	1	16	1	0	172	0	0	173	0	1	144	0	0	145	334
13:00	0	2	0	2	0	4	0	12	1	6	2	19	1	0	160	0	0	161	0	1	123	2	0	126	310
13:15	0	0	0	1	1	1	0	25	0	2	3	27	0	0	167	0	0	167	0	0	147	1	1	148	343
13:30	0	0	0	0	1	0	0	21	0	5	4	26	0	0	171	0	0	171	0	0	118	0	0	118	315
<b>Total</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>3</b>	<b>4</b>	<b>5</b>	<b>0</b>	<b>72</b>	<b>1</b>	<b>15</b>	<b>10</b>	<b>88</b>	<b>2</b>	<b>0</b>	<b>670</b>	<b>0</b>	<b>0</b>	<b>672</b>	<b>0</b>	<b>2</b>	<b>532</b>	<b>3</b>	<b>1</b>	<b>537</b>	<b>1302</b>
Approach %	0.0	40.0	0.0	60.0	-	-	0.0	81.8	1.1	17.0	-	-	0.3	0.0	99.7	0.0	-	-	0.0	0.4	99.1	0.6	-	-	-
Total %	0.0	0.2	0.0	0.2	-	0.4	0.0	5.5	0.1	1.2	-	6.8	0.2	0.0	51.5	0.0	-	51.6	0.0	0.2	40.9	0.2	-	41.2	-
PHF	0.000	0.250	0.000	0.375	-	0.313	0.000	0.720	0.250	0.625	-	0.815	0.500	0.000	0.974	0.000	-	0.971	0.000	0.500	0.905	0.375	-	0.907	0.949
Lights	0	2	0	3	-	5	0	70	1	15	-	86	2	0	655	0	-	657	0	2	522	3	-	527	1275
% Lights	-	100.0	-	100.0	-	100.0	-	97.2	100.0	100.0	-	97.7	100.0	-	97.8	-	-	97.8	-	100.0	98.1	100.0	-	98.1	97.9
Other Vehicles	0	0	0	0	-	0	0	2	0	0	-	2	0	0	13	0	-	13	0	0	10	0	-	10	25
% Other Vehicles	-	0.0	-	0.0	-	0.0	-	2.8	0.0	0.0	-	2.3	0.0	-	1.9	-	-	1.9	-	0.0	1.9	0.0	-	1.9	1.9
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	2	0	-	2	0	0	0	0	-	0	2
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	-	0.3	-	-	0.3	-	0.0	0.0	0.0	-	0.0	0.2
Bicycles on Crosswalk	-	-	-	-	4	-	-	-	-	-	3	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	100.0	-	-	-	-	-	30.0	-	-	-	-	-	-	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	0	-	-	-	-	-	7	-	-	-	-	-	0	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	0.0	-	-	-	-	-	70.0	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



Turning Movement Peak Hour Data Plot (12:45)



USB 41 (14th St W) & 29th Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 25 USB 41 (14th St W) & 29th Ave W  
Site Code: 25  
Start Date: 07/06/2023  
Page No: 7

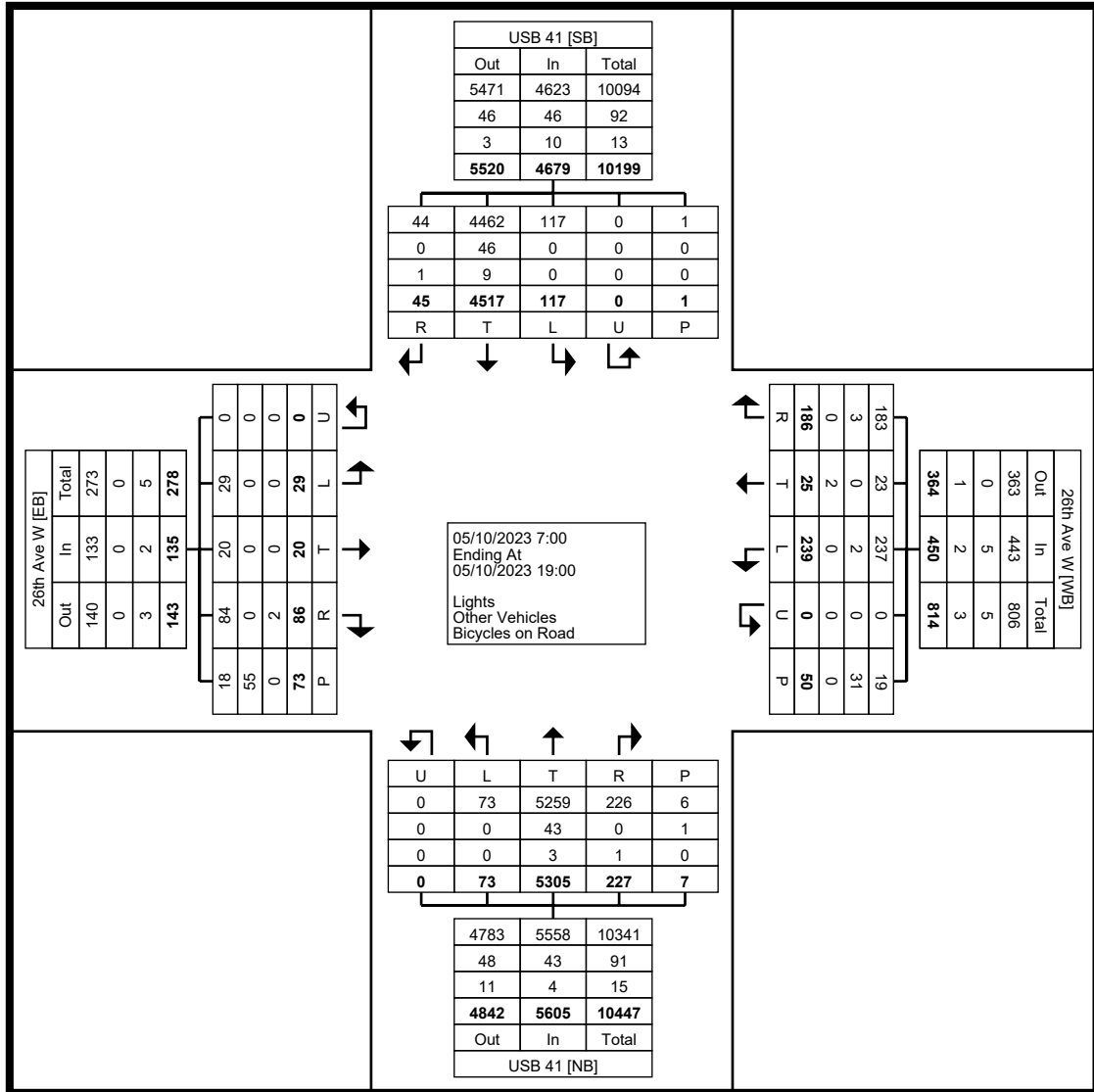
### Turning Movement Peak Hour Data (16:30)

Start Time	Driveway Eastbound						29th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:30	0	0	0	2	3	2	0	21	0	4	4	25	1	0	199	0	0	200	0	2	171	0	0	173	400
16:45	0	0	0	2	8	2	0	22	0	3	6	25	0	1	208	0	1	209	1	1	159	1	2	162	398
17:00	0	0	0	0	6	0	0	20	0	12	3	32	2	0	184	0	0	186	0	0	200	0	0	200	418
17:15	0	0	0	0	4	0	0	20	0	6	3	26	1	0	224	0	0	225	0	0	182	0	0	182	433
<b>Total</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>4</b>	<b>21</b>	<b>4</b>	<b>0</b>	<b>83</b>	<b>0</b>	<b>25</b>	<b>16</b>	<b>108</b>	<b>4</b>	<b>1</b>	<b>815</b>	<b>0</b>	<b>1</b>	<b>820</b>	<b>1</b>	<b>3</b>	<b>712</b>	<b>1</b>	<b>2</b>	<b>717</b>	<b>1649</b>
Approach %	0.0	0.0	0.0	100.0	-	-	0.0	76.9	0.0	23.1	-	-	0.5	0.1	99.4	0.0	-	-	0.1	0.4	99.3	0.1	-	-	-
Total %	0.0	0.0	0.0	0.2	-	0.2	0.0	5.0	0.0	1.5	-	6.5	0.2	0.1	49.4	0.0	-	49.7	0.1	0.2	43.2	0.1	-	43.5	-
PHF	0.000	0.000	0.000	0.500	-	0.500	0.000	0.943	0.000	0.521	-	0.844	0.500	0.250	0.910	0.000	-	0.911	0.250	0.375	0.890	0.250	-	0.896	0.952
Lights	0	0	0	4	-	4	0	83	0	25	-	108	4	1	800	0	-	805	1	3	701	1	-	706	1623
% Lights	-	-	-	100.0	-	100.0	-	100.0	-	100.0	-	100.0	100.0	100.0	98.2	-	-	98.2	100.0	100.0	98.5	100.0	-	98.5	98.4
Other Vehicles	0	0	0	0	-	0	0	0	0	0	-	0	0	0	14	0	-	14	0	0	9	0	-	9	23
% Other Vehicles	-	-	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	1.7	-	-	1.7	0.0	0.0	1.3	0.0	-	1.3	1.4
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	1	0	-	1	0	0	2	0	-	2	3
% Bicycles on Road	-	-	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	0.1	-	-	0.1	0.0	0.0	0.3	0.0	-	0.3	0.2
Bicycles on Crosswalk	-	-	-	-	15	-	-	-	-	-	8	-	-	-	-	-	1	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	71.4	-	-	-	-	-	50.0	-	-	-	-	-	100.0	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	6	-	-	-	-	-	8	-	-	-	-	-	0	-	-	-	-	-	2	-	-
% Pedestrians	-	-	-	-	28.6	-	-	-	-	-	50.0	-	-	-	-	-	0.0	-	-	-	-	-	100.0	-	-



### Turning Movement Data

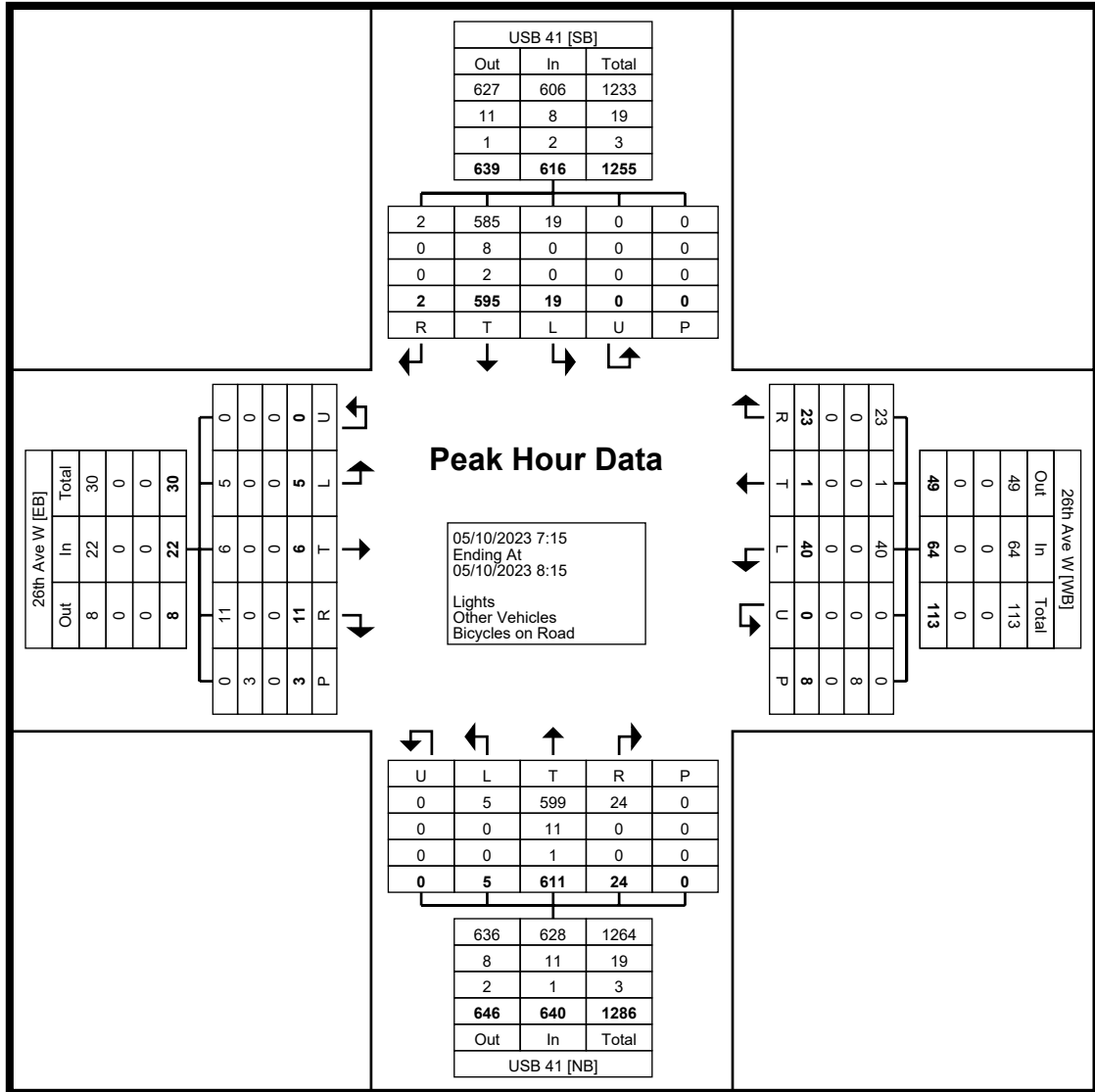
Start Time	26th Ave W Eastbound						26th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	1	1	8	2	10	0	8	0	4	2	12	0	0	119	3	0	122	0	0	138	3	0	141	285
7:15	0	3	1	5	2	9	0	12	0	11	1	23	0	3	149	5	0	157	0	6	151	2	0	159	348
7:30	0	1	1	2	1	4	0	8	0	2	4	10	0	1	138	12	0	151	0	4	134	0	0	138	303
7:45	0	0	4	1	0	5	0	10	0	5	1	15	0	1	162	3	0	166	0	5	145	0	0	150	336
Hourly Total	0	5	7	16	5	28	0	38	0	22	8	60	0	5	568	23	0	596	0	15	568	5	0	588	1272
8:00	0	1	0	3	0	4	0	10	1	5	2	16	0	0	162	4	0	166	0	4	165	0	0	169	355
8:15	0	1	0	1	1	2	0	5	1	6	2	12	0	2	139	6	0	147	0	1	163	2	0	166	327
8:30	0	1	0	1	2	2	0	6	0	8	0	14	0	2	108	7	0	117	0	1	167	2	0	170	303
8:45	0	1	0	2	2	3	0	4	1	10	2	15	0	1	146	4	0	151	0	4	145	0	0	149	318
Hourly Total	0	4	0	7	5	11	0	25	3	29	6	57	0	5	555	21	0	581	0	10	640	4	0	654	1303
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	0	2	4	2	0	8	1	9	1	18	0	0	151	7	0	158	0	1	153	0	0	154	332
12:15	0	0	0	0	3	0	0	3	0	5	1	8	0	1	157	8	0	166	0	5	138	0	1	143	317
12:30	0	0	0	1	3	1	0	4	1	5	0	10	0	1	137	3	0	141	0	2	127	2	0	131	283
12:45	0	1	0	1	1	2	0	6	0	7	3	13	0	1	139	4	0	144	0	1	131	2	0	134	293
Hourly Total	0	1	0	4	11	5	0	21	2	26	5	49	0	3	584	22	0	609	0	9	549	4	1	562	1225
13:00	0	0	0	2	0	2	0	10	1	4	1	15	0	1	144	4	0	149	0	3	133	0	0	136	302
13:15	0	1	1	1	1	3	0	1	1	1	1	3	0	2	181	10	0	193	0	3	136	2	0	141	340
13:30	0	0	1	1	4	2	0	2	1	1	5	4	0	0	131	7	0	138	0	5	128	3	0	136	280
13:45	0	1	0	2	1	3	0	5	1	7	3	13	0	1	133	3	0	137	0	3	142	0	0	145	298
Hourly Total	0	2	2	6	6	10	0	18	4	13	10	35	0	4	589	24	0	617	0	14	539	5	0	558	1220
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	0	1	2	0	3	0	5	1	5	0	11	0	1	160	9	0	170	0	6	144	1	0	151	335
15:15	0	1	1	5	1	7	0	10	0	8	0	18	0	4	161	18	0	183	0	7	121	3	0	131	339
15:30	0	2	0	4	5	6	0	7	1	5	1	13	0	3	179	9	0	191	0	6	151	2	0	159	369
15:45	0	0	1	1	4	2	0	14	0	7	0	21	0	2	194	6	1	202	0	4	136	2	0	142	367
Hourly Total	0	3	3	12	10	18	0	36	2	25	1	63	0	10	694	42	1	746	0	23	552	8	0	583	1410
16:00	0	0	0	3	7	3	0	14	0	7	1	21	0	2	228	7	1	237	0	5	151	2	0	158	419
16:15	0	1	0	2	3	3	0	9	0	2	1	11	0	3	225	12	0	240	0	0	129	4	0	133	387
16:30	0	1	1	7	1	9	0	8	1	4	1	13	0	5	175	6	1	186	0	4	141	3	0	148	356
16:45	0	0	0	5	3	5	0	9	1	6	3	16	0	4	181	8	0	193	0	4	154	2	0	160	374
Hourly Total	0	2	1	17	14	20	0	40	2	19	6	61	0	14	809	33	2	856	0	13	575	11	0	599	1536
17:00	0	0	2	1	4	3	0	7	1	7	0	15	0	3	239	9	0	251	0	7	178	1	0	186	455
17:15	0	3	1	2	3	6	0	9	3	7	2	19	0	2	221	3	0	226	0	7	157	0	0	164	415
17:30	0	0	0	3	6	3	0	4	3	11	2	18	0	4	205	8	0	217	0	6	155	0	0	161	399
17:45	0	1	0	3	0	4	0	7	0	8	5	15	0	6	182	10	0	198	0	2	125	3	0	130	347
Hourly Total	0	4	3	9	13	16	0	27	7	33	9	67	0	15	847	30	0	892	0	22	615	4	0	641	1616
18:00	0	2	0	6	4	8	0	10	1	2	1	13	0	2	183	5	4	190	0	3	123	2	0	128	339
18:15	0	1	0	4	3	5	0	4	2	8	0	14	0	5	160	5	0	170	0	4	121	1	0	126	315
18:30	0	4	3	3	0	10	0	12	1	4	4	17	0	5	154	12	0	171	0	2	122	1	0	125	323
18:45	0	1	1	2	2	4	0	8	1	5	0	14	0	5	162	10	0	177	0	2	113	0	0	115	310
Hourly Total	0	8	4	15	9	27	0	34	5	19	5	58	0	17	659	32	4	708	0	11	479	4	0	494	1287
Grand Total	0	29	20	86	73	135	0	239	25	186	50	450	0	73	5305	227	7	5605	0	117	4517	45	1	4679	10869
Approach %	0.0	21.5	14.8	63.7	-	-	0.0	53.1	5.6	41.3	-	-	0.0	1.3	94.6	4.0	-	-	0.0	2.5	96.5	1.0	-	-	-
Total %	0.0	0.3	0.2	0.8	-	1.2	0.0	2.2	0.2	1.7	-	4.1	0.0	0.7	48.8	2.1	-	51.6	0.0	1.1	41.6	0.4	-	43.0	-
Lights	0	29	20	84	18	133	0	237	23	183	19	443	0	73	5259	226	6	5558	0	117	4462	44	1	4623	10757
% Lights	-	100.0	100.0	97.7	24.7	98.5	-	99.2	92.0	98.4	38.0	98.4	-	100.0	99.1	99.6	85.7	99.2	-	100.0	98.8	97.8	100.0	98.8	99.0
Other Vehicles	0	0	0	0	55	0	0	2	0	3	31	5	0	0	43	0	1	43	0	0	46	0	0	46	94
% Other Vehicles	-	0.0	0.0	0.0	75.3	0.0	-	0.8	0.0	1.6	62.0	1.1	-	0.0	0.8	0.0	14.3	0.8	-	0.0	1.0	0.0	0.0	1.0	0.9
Bicycles on Road	0	0	0	2	0	2	0	0	2	0	0	2	0	0	3	1	0	4	0	0	9	1	0	10	18
% Bicycles on Road	-	0.0	0.0	2.3	0.0	1.5	-	0.0	8.0	0.0	0.0	0.4	-	0.0	0.1	0.4	0.0	0.1	-	0.0	0.2	2.2	0.0	0.2	0.2



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:15)

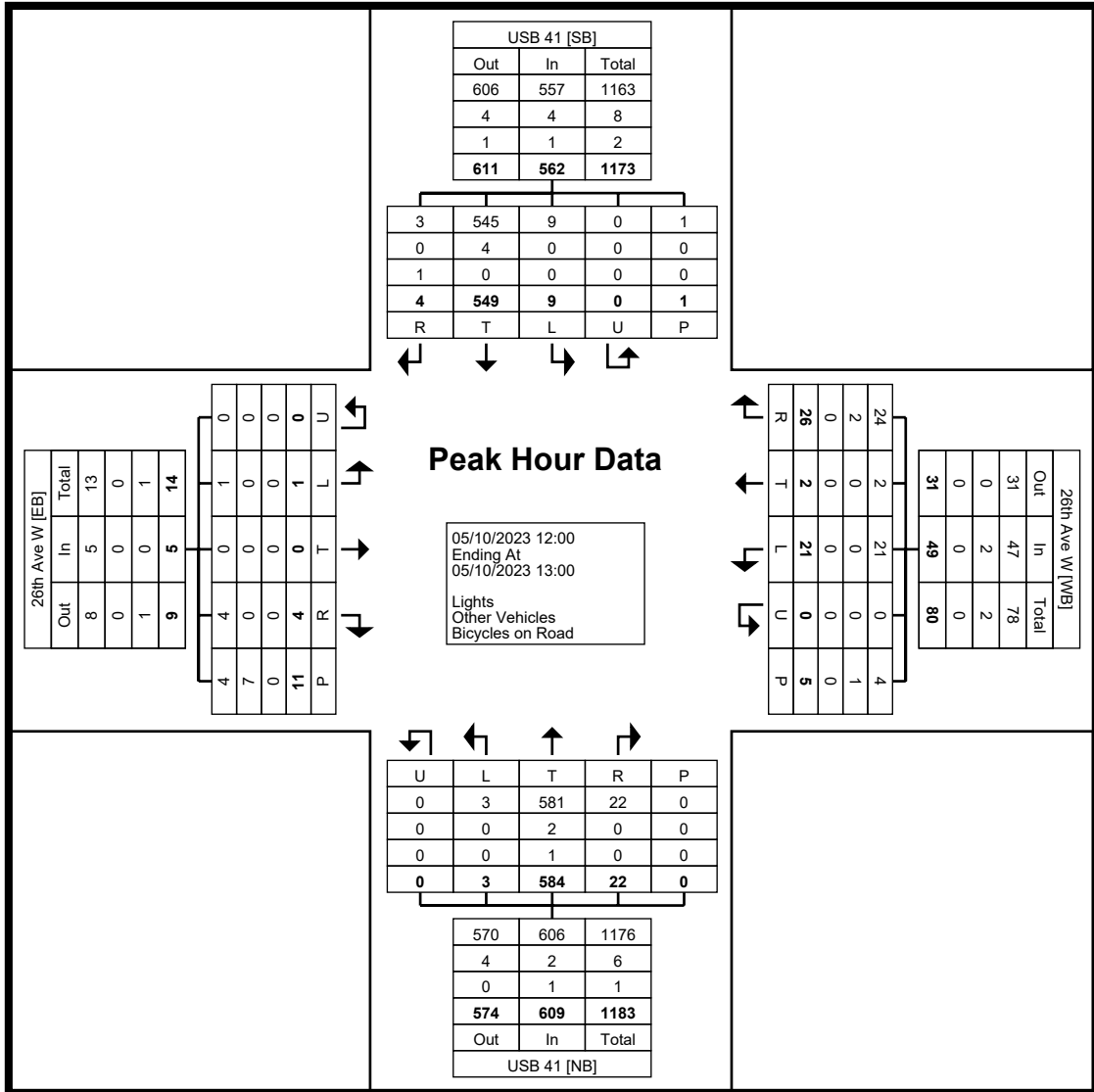
Start Time	26th Ave W Eastbound						26th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15	0	3	1	5	2	9	0	12	0	11	1	23	0	3	149	5	0	157	0	6	151	2	0	159	348
7:30	0	1	1	2	1	4	0	8	0	2	4	10	0	1	138	12	0	151	0	4	134	0	0	138	303
7:45	0	0	4	1	0	5	0	10	0	5	1	15	0	1	162	3	0	166	0	5	145	0	0	150	336
8:00	0	1	0	3	0	4	0	10	1	5	2	16	0	0	162	4	0	166	0	4	165	0	0	169	355
<b>Total</b>	<b>0</b>	<b>5</b>	<b>6</b>	<b>11</b>	<b>3</b>	<b>22</b>	<b>0</b>	<b>40</b>	<b>1</b>	<b>23</b>	<b>8</b>	<b>64</b>	<b>0</b>	<b>5</b>	<b>611</b>	<b>24</b>	<b>0</b>	<b>640</b>	<b>0</b>	<b>19</b>	<b>595</b>	<b>2</b>	<b>0</b>	<b>616</b>	<b>1342</b>
Approach %	0.0	22.7	27.3	50.0	-	-	0.0	62.5	1.6	35.9	-	-	0.0	0.8	95.5	3.8	-	-	0.0	3.1	96.6	0.3	-	-	-
Total %	0.0	0.4	0.4	0.8	-	1.6	0.0	3.0	0.1	1.7	-	4.8	0.0	0.4	45.5	1.8	-	47.7	0.0	1.4	44.3	0.1	-	45.9	-
PHF	0.000	0.417	0.375	0.550	-	0.611	0.000	0.833	0.250	0.523	-	0.696	0.000	0.417	0.943	0.500	-	0.964	0.000	0.792	0.902	0.250	-	0.911	0.945
Lights	0	5	6	11	0	22	0	40	1	23	0	64	0	5	599	24	0	628	0	19	585	2	0	606	1320
% Lights	-	100.0	100.0	100.0	0.0	100.0	-	100.0	100.0	100.0	0.0	100.0	-	100.0	98.0	100.0	-	98.1	-	100.0	98.3	100.0	-	98.4	98.4
Other Vehicles	0	0	0	0	3	0	0	0	0	0	8	0	0	0	11	0	0	11	0	0	8	0	0	8	19
% Other Vehicles	-	0.0	0.0	0.0	100.0	0.0	-	0.0	0.0	0.0	100.0	0.0	-	0.0	1.8	0.0	-	1.7	-	0.0	1.3	0.0	-	1.3	1.4
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	2	0	0	2	3
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.2	0.0	-	0.2	-	0.0	0.3	0.0	-	0.3	0.2



Turning Movement Peak Hour Data Plot (7:15)

### Turning Movement Peak Hour Data (12:00)

Start Time	26th Ave W Eastbound						26th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	0	0	2	4	2	0	8	1	9	1	18	0	0	151	7	0	158	0	1	153	0	0	154	332
12:15	0	0	0	0	3	0	0	3	0	5	1	8	0	1	157	8	0	166	0	5	138	0	1	143	317
12:30	0	0	0	1	3	1	0	4	1	5	0	10	0	1	137	3	0	141	0	2	127	2	0	131	283
12:45	0	1	0	1	1	2	0	6	0	7	3	13	0	1	139	4	0	144	0	1	131	2	0	134	293
<b>Total</b>	0	1	0	4	11	5	0	21	2	26	5	49	0	3	584	22	0	609	0	9	549	4	1	562	1225
Approach %	0.0	20.0	0.0	80.0	-	-	0.0	42.9	4.1	53.1	-	-	0.0	0.5	95.9	3.6	-	-	0.0	1.6	97.7	0.7	-	-	-
Total %	0.0	0.1	0.0	0.3	-	0.4	0.0	1.7	0.2	2.1	-	4.0	0.0	0.2	47.7	1.8	-	49.7	0.0	0.7	44.8	0.3	-	45.9	-
PHF	0.000	0.250	0.000	0.500	-	0.625	0.000	0.656	0.500	0.722	-	0.681	0.000	0.750	0.930	0.688	-	0.917	0.000	0.450	0.897	0.500	-	0.912	0.922
Lights	0	1	0	4	4	5	0	21	2	24	4	47	0	3	581	22	0	606	0	9	545	3	1	557	1215
% Lights	-	100.0	-	100.0	36.4	100.0	-	100.0	100.0	92.3	80.0	95.9	-	100.0	99.5	100.0	-	99.5	-	100.0	99.3	75.0	100.0	99.1	99.2
Other Vehicles	0	0	0	0	7	0	0	0	0	2	1	2	0	0	2	0	0	2	0	0	4	0	0	4	8
% Other Vehicles	-	0.0	-	0.0	63.6	0.0	-	0.0	0.0	7.7	20.0	4.1	-	0.0	0.3	0.0	-	0.3	-	0.0	0.7	0.0	0.0	0.7	0.7
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	1	0	1	2
% Bicycles on Road	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.2	0.0	-	0.2	-	0.0	0.0	25.0	0.0	0.2	0.2

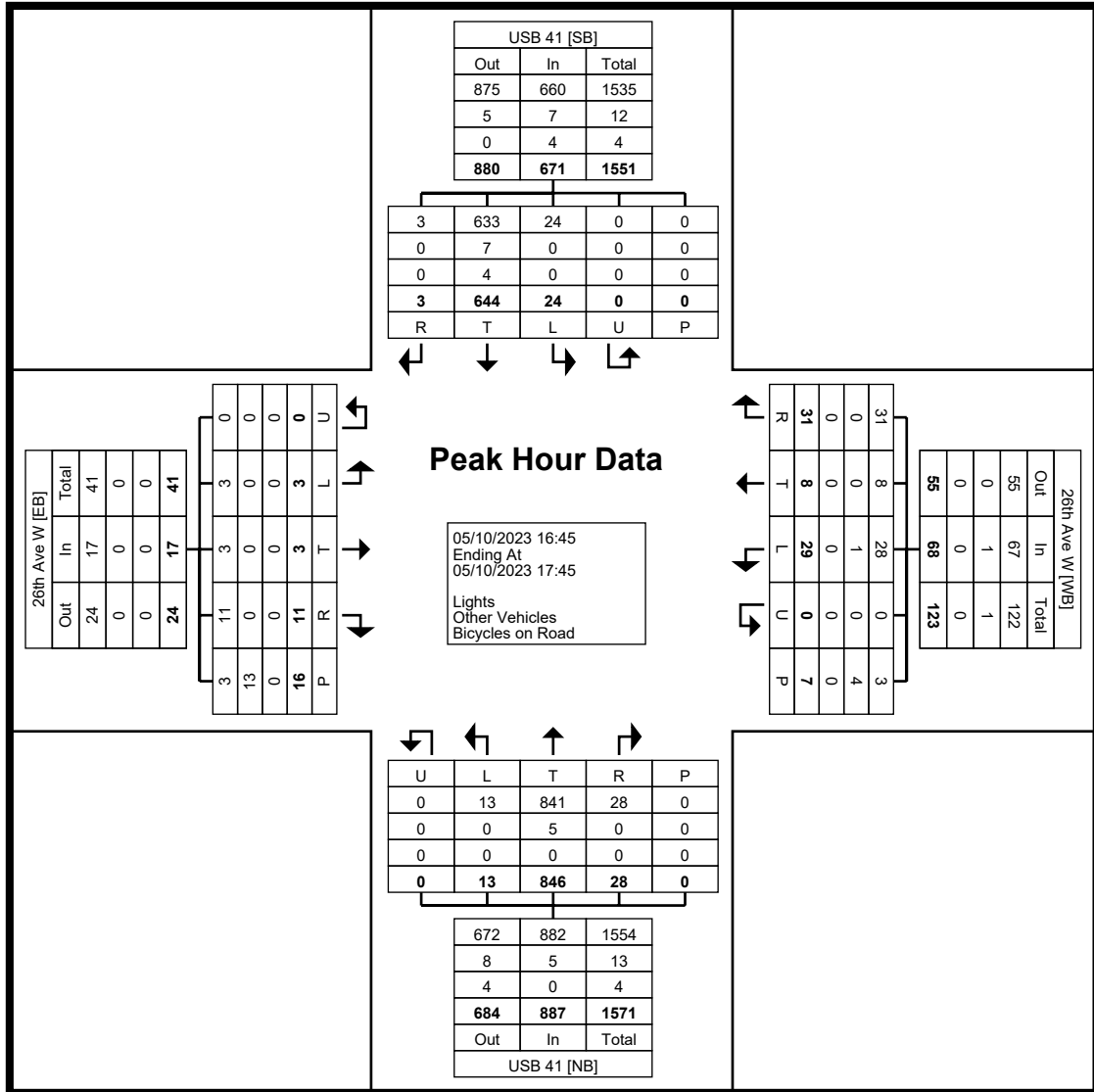


Turning Movement Peak Hour Data Plot (12:00)



### Turning Movement Peak Hour Data (16:45)

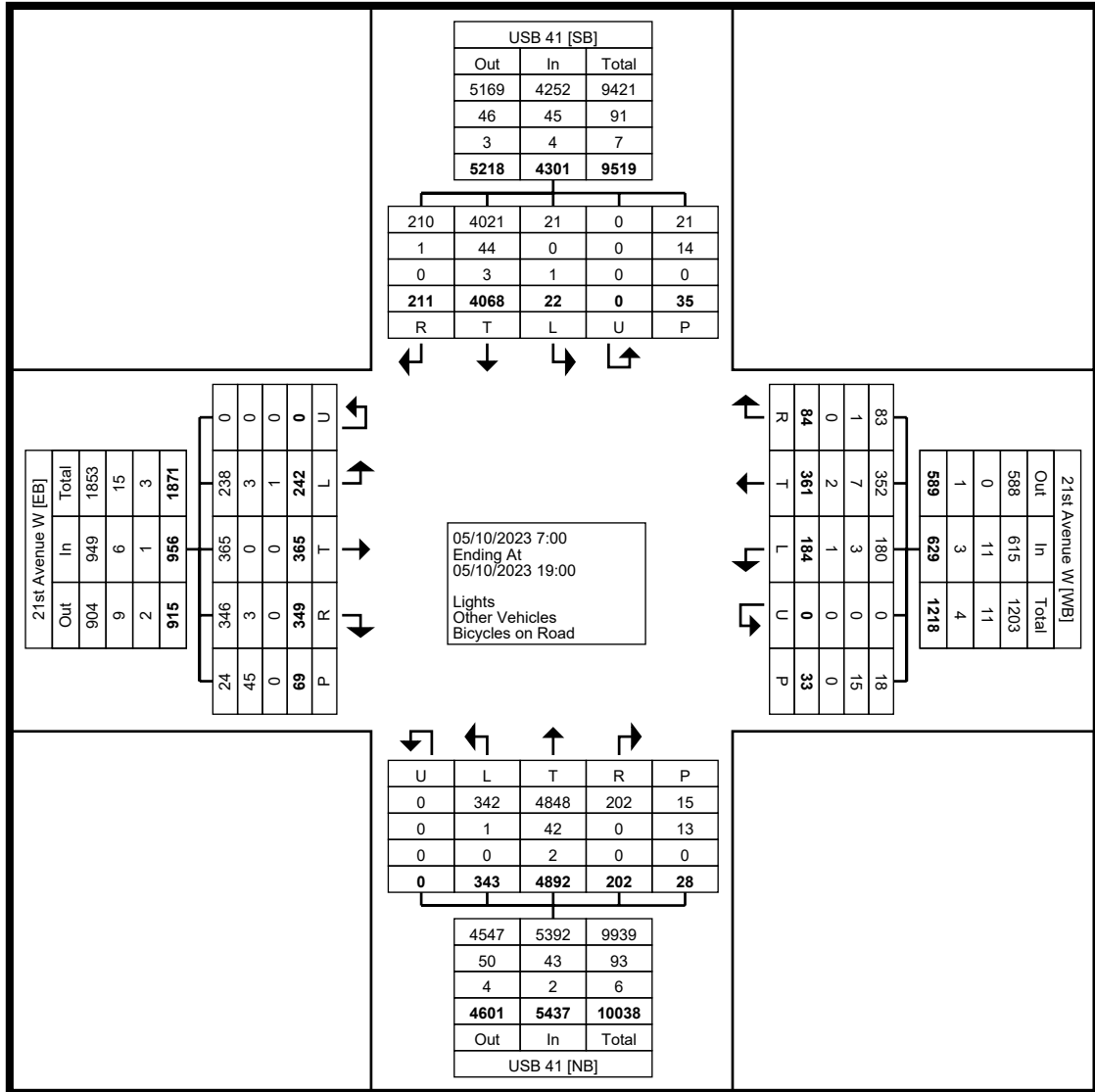
Start Time	26th Ave W Eastbound						26th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:45	0	0	0	5	3	5	0	9	1	6	3	16	0	4	181	8	0	193	0	4	154	2	0	160	374
17:00	0	0	2	1	4	3	0	7	1	7	0	15	0	3	239	9	0	251	0	7	178	1	0	186	455
17:15	0	3	1	2	3	6	0	9	3	7	2	19	0	2	221	3	0	226	0	7	157	0	0	164	415
17:30	0	0	0	3	6	3	0	4	3	11	2	18	0	4	205	8	0	217	0	6	155	0	0	161	399
<b>Total</b>	0	3	3	11	16	17	0	29	8	31	7	68	0	13	846	28	0	887	0	24	644	3	0	671	1643
Approach %	0.0	17.6	17.6	64.7	-	-	0.0	42.6	11.8	45.6	-	-	0.0	1.5	95.4	3.2	-	-	0.0	3.6	96.0	0.4	-	-	-
Total %	0.0	0.2	0.2	0.7	-	1.0	0.0	1.8	0.5	1.9	-	4.1	0.0	0.8	51.5	1.7	-	54.0	0.0	1.5	39.2	0.2	-	40.8	-
PHF	0.000	0.250	0.375	0.550	-	0.708	0.000	0.806	0.667	0.705	-	0.895	0.000	0.813	0.885	0.778	-	0.883	0.000	0.857	0.904	0.375	-	0.902	0.903
Lights	0	3	3	11	3	17	0	28	8	31	3	67	0	13	841	28	0	882	0	24	633	3	0	660	1626
% Lights	-	100.0	100.0	100.0	18.8	100.0	-	96.6	100.0	100.0	42.9	98.5	-	100.0	99.4	100.0	-	99.4	-	100.0	98.3	100.0	-	98.4	99.0
Other Vehicles	0	0	0	0	13	0	0	1	0	0	4	1	0	0	5	0	0	5	0	0	7	0	0	7	13
% Other Vehicles	-	0.0	0.0	0.0	81.3	0.0	-	3.4	0.0	0.0	57.1	1.5	-	0.0	0.6	0.0	-	0.6	-	0.0	1.1	0.0	-	1.0	0.8
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	4	4
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.6	0.0	-	0.6	0.2



Turning Movement Peak Hour Data Plot (16:45)

### Turning Movement Data

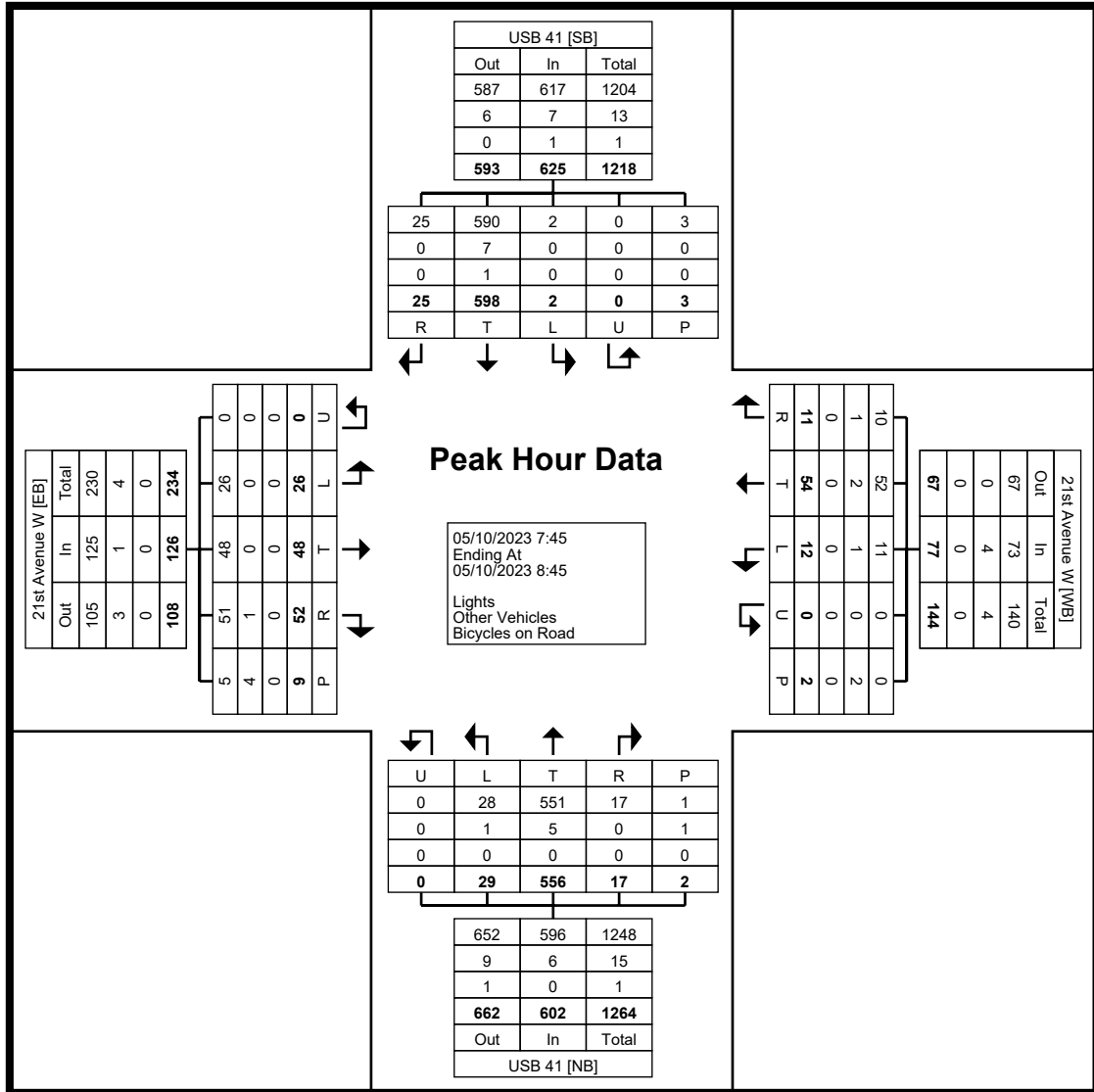
Start Time	21st Avenue W Eastbound						21st Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	8	6	12	5	26	0	4	4	6	2	14	0	7	120	3	1	130	0	0	123	1	0	124	294
7:15	0	6	13	13	3	32	0	7	6	1	0	14	0	5	133	2	1	140	0	0	146	4	0	150	336
7:30	0	9	6	8	5	23	0	6	8	5	1	19	0	11	131	3	3	145	0	0	117	0	1	117	304
7:45	0	6	18	8	5	32	0	4	19	4	1	27	0	7	160	4	1	171	0	0	143	9	3	152	382
Hourly Total	0	29	43	41	18	113	0	21	37	16	4	74	0	30	544	12	6	586	0	0	529	14	4	543	1316
8:00	0	8	13	14	0	35	0	0	13	5	0	18	0	6	157	4	1	167	0	0	154	7	0	161	381
8:15	0	6	10	12	3	28	0	4	12	0	0	16	0	9	132	6	0	147	0	0	147	2	0	149	340
8:30	0	6	7	18	1	31	0	4	10	2	1	16	0	7	107	3	0	117	0	2	154	7	0	163	327
8:45	0	3	17	6	2	26	0	5	11	2	2	18	0	8	139	5	0	152	0	2	143	5	1	150	346
Hourly Total	0	23	47	50	6	120	0	13	46	9	3	68	0	30	535	18	1	583	0	4	598	21	1	623	1394
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	7	7	8	1	22	0	2	6	3	3	11	0	11	135	8	0	154	0	1	135	4	2	140	327
12:15	0	7	8	11	1	26	0	9	8	5	2	22	0	8	131	11	0	150	0	1	116	8	1	125	323
12:30	0	5	4	12	2	21	0	5	9	3	0	17	0	13	118	4	0	135	0	0	107	8	1	115	288
12:45	0	3	10	10	1	23	0	7	7	2	2	16	0	13	135	4	1	152	0	2	121	7	0	130	321
Hourly Total	0	22	29	41	5	92	0	23	30	13	7	66	0	45	519	27	1	591	0	4	479	27	4	510	1259
13:00	0	10	8	5	0	23	0	2	8	1	2	11	0	11	114	8	0	133	0	0	113	8	1	121	288
13:15	0	18	6	7	1	31	0	11	3	1	0	15	0	11	175	7	0	193	0	1	124	3	0	128	367
13:30	0	5	11	13	0	29	0	4	9	0	2	13	0	6	123	5	0	134	0	0	113	4	0	117	293
13:45	0	6	7	7	1	20	0	6	11	4	2	21	0	10	119	8	0	137	0	1	126	5	0	132	310
Hourly Total	0	39	32	32	2	103	0	23	31	6	6	60	0	38	531	28	0	597	0	2	476	20	1	498	1258
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	6	12	5	1	23	0	6	12	3	0	21	0	6	154	9	1	169	0	0	143	14	3	157	370
15:15	0	11	8	13	0	32	0	6	8	2	0	16	0	23	147	5	1	175	0	0	117	6	2	123	346
15:30	0	10	17	14	3	41	0	9	20	4	0	33	0	13	155	13	1	181	0	0	122	9	1	131	386
15:45	0	11	16	9	2	36	0	6	15	2	0	23	0	12	185	8	1	205	0	1	133	5	1	139	403
Hourly Total	0	38	53	41	6	132	0	27	55	11	0	93	0	54	641	35	4	730	0	1	515	34	7	550	1505
16:00	0	9	14	6	4	29	0	3	12	2	2	17	0	15	197	7	1	219	0	0	137	4	1	141	406
16:15	0	7	12	14	3	33	0	7	18	2	1	27	0	13	196	5	3	214	0	0	110	5	1	115	389
16:30	0	9	10	13	2	32	0	8	14	3	0	25	0	7	179	5	2	191	0	1	133	6	3	140	388
16:45	0	9	22	13	1	44	0	7	14	1	2	22	0	9	158	9	0	176	0	1	135	7	2	143	385
Hourly Total	0	34	58	46	10	138	0	25	58	8	5	91	0	44	730	26	6	800	0	2	515	22	7	539	1568
17:00	0	11	10	11	1	32	0	3	17	3	2	23	0	17	217	13	1	247	0	1	171	16	0	188	490
17:15	0	7	14	13	4	34	0	9	20	3	0	32	0	12	224	3	2	239	0	2	138	11	2	151	456
17:30	0	8	16	9	6	33	0	8	18	1	0	27	0	14	192	6	0	212	0	0	131	9	0	140	412
17:45	0	8	14	14	1	36	0	4	12	1	1	17	0	7	170	3	1	180	0	1	115	4	2	120	353
Hourly Total	0	34	54	47	12	135	0	24	67	8	3	99	0	50	803	25	4	878	0	4	555	40	4	599	1711
18:00	0	6	12	11	5	29	0	8	10	1	1	19	0	15	164	8	0	187	0	1	103	8	3	112	347
18:15	0	4	16	15	3	35	0	10	8	4	2	22	0	14	148	10	4	172	0	2	112	9	2	123	352
18:30	0	6	10	12	0	28	0	6	9	5	1	20	0	4	139	6	0	149	0	2	96	8	0	106	303
18:45	0	7	11	13	2	31	0	4	10	3	1	17	0	19	138	7	2	164	0	0	90	8	2	98	310
Hourly Total	0	23	49	51	10	123	0	28	37	13	5	78	0	52	589	31	6	672	0	5	401	33	7	439	1312
Grand Total	0	242	365	349	69	956	0	184	361	84	33	629	0	343	4892	202	28	5437	0	22	4068	211	35	4301	11323
Approach %	0.0	25.3	38.2	36.5	-	-	0.0	29.3	57.4	13.4	-	-	0.0	6.3	90.0	3.7	-	-	0.0	0.5	94.6	4.9	-	-	-
Total %	0.0	2.1	3.2	3.1	-	8.4	0.0	1.6	3.2	0.7	-	5.6	0.0	3.0	43.2	1.8	-	48.0	0.0	0.2	35.9	1.9	-	38.0	-
Lights	0	238	365	346	24	949	0	180	352	83	18	615	0	342	4848	202	15	5392	0	21	4021	210	21	4252	11208
% Lights	-	98.3	100.0	99.1	34.8	99.3	-	97.8	97.5	98.8	54.5	97.8	-	99.7	99.1	100.0	53.6	99.2	-	95.5	98.8	99.5	60.0	98.9	99.0
Other Vehicles	0	3	0	3	45	6	0	3	7	1	15	11	0	1	42	0	13	43	0	0	44	1	14	45	105
% Other Vehicles	-	1.2	0.0	0.9	65.2	0.6	-	1.6	1.9	1.2	45.5	1.7	-	0.3	0.9	0.0	46.4	0.8	-	0.0	1.1	0.5	40.0	1.0	0.9
Bicycles on Road	0	1	0	0	0	1	0	1	2	0	0	3	0	0	2	0	0	2	0	1	3	0	0	4	10
% Bicycles on Road	-	0.4	0.0	0.0	0.0	0.1	-	0.5	0.6	0.0	0.0	0.5	-	0.0	0.0	0.0	0.0	0.0	-	4.5	0.1	0.0	0.0	0.1	0.1



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:45)

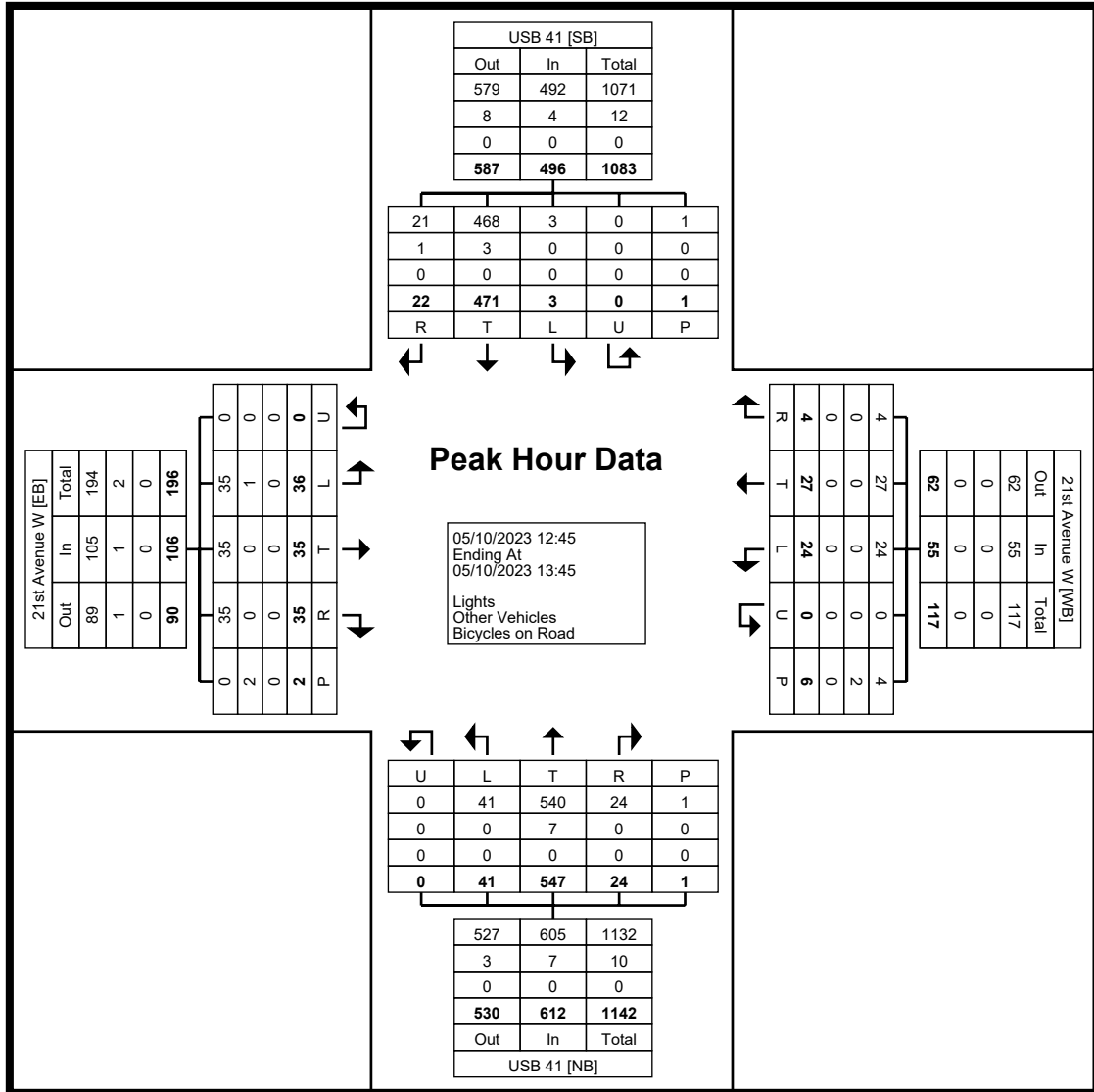
Start Time	21st Avenue W Eastbound						21st Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:45	0	6	18	8	5	32	0	4	19	4	1	27	0	7	160	4	1	171	0	0	143	9	3	152	382
8:00	0	8	13	14	0	35	0	0	13	5	0	18	0	6	157	4	1	167	0	0	154	7	0	161	381
8:15	0	6	10	12	3	28	0	4	12	0	0	16	0	9	132	6	0	147	0	0	147	2	0	149	340
8:30	0	6	7	18	1	31	0	4	10	2	1	16	0	7	107	3	0	117	0	2	154	7	0	163	327
<b>Total</b>	0	26	48	52	9	126	0	12	54	11	2	77	0	29	556	17	2	602	0	2	598	25	3	625	1430
Approach %	0.0	20.6	38.1	41.3	-	-	0.0	15.6	70.1	14.3	-	-	0.0	4.8	92.4	2.8	-	-	0.0	0.3	95.7	4.0	-	-	-
Total %	0.0	1.8	3.4	3.6	-	8.8	0.0	0.8	3.8	0.8	-	5.4	0.0	2.0	38.9	1.2	-	42.1	0.0	0.1	41.8	1.7	-	43.7	-
PHF	0.000	0.813	0.667	0.722	-	0.900	0.000	0.750	0.711	0.550	-	0.713	0.000	0.806	0.869	0.708	-	0.880	0.000	0.250	0.971	0.694	-	0.959	0.936
Lights	0	26	48	51	5	125	0	11	52	10	0	73	0	28	551	17	1	596	0	2	590	25	3	617	1411
% Lights	-	100.0	100.0	98.1	55.6	99.2	-	91.7	96.3	90.9	0.0	94.8	-	96.6	99.1	100.0	50.0	99.0	-	100.0	98.7	100.0	100.0	98.7	98.7
Other Vehicles	0	0	0	1	4	1	0	1	2	1	2	4	0	1	5	0	1	6	0	0	7	0	0	7	18
% Other Vehicles	-	0.0	0.0	1.9	44.4	0.8	-	8.3	3.7	9.1	100.0	5.2	-	3.4	0.9	0.0	50.0	1.0	-	0.0	1.2	0.0	0.0	1.1	1.3
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.2	0.0	0.0	0.2	0.1



Turning Movement Peak Hour Data Plot (7:45)

### Turning Movement Peak Hour Data (12:45)

Start Time	21st Avenue W Eastbound						21st Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:45	0	3	10	10	1	23	0	7	7	2	2	16	0	13	135	4	1	152	0	2	121	7	0	130	321
13:00	0	10	8	5	0	23	0	2	8	1	2	11	0	11	114	8	0	133	0	0	113	8	1	121	288
13:15	0	18	6	7	1	31	0	11	3	1	0	15	0	11	175	7	0	193	0	1	124	3	0	128	367
13:30	0	5	11	13	0	29	0	4	9	0	2	13	0	6	123	5	0	134	0	0	113	4	0	117	293
<b>Total</b>	0	36	35	35	2	106	0	24	27	4	6	55	0	41	547	24	1	612	0	3	471	22	1	496	1269
Approach %	0.0	34.0	33.0	33.0	-	-	0.0	43.6	49.1	7.3	-	-	0.0	6.7	89.4	3.9	-	-	0.0	0.6	95.0	4.4	-	-	-
Total %	0.0	2.8	2.8	2.8	-	8.4	0.0	1.9	2.1	0.3	-	4.3	0.0	3.2	43.1	1.9	-	48.2	0.0	0.2	37.1	1.7	-	39.1	-
PHF	0.000	0.500	0.795	0.673	-	0.855	0.000	0.545	0.750	0.500	-	0.859	0.000	0.788	0.781	0.750	-	0.793	0.000	0.375	0.950	0.688	-	0.954	0.864
Lights	0	35	35	35	0	105	0	24	27	4	4	55	0	41	540	24	1	605	0	3	468	21	1	492	1257
% Lights	-	97.2	100.0	100.0	0.0	99.1	-	100.0	100.0	100.0	66.7	100.0	-	100.0	98.7	100.0	100.0	98.9	-	100.0	99.4	95.5	100.0	99.2	99.1
Other Vehicles	0	1	0	0	2	1	0	0	0	0	2	0	0	0	7	0	0	7	0	0	3	1	0	4	12
% Other Vehicles	-	2.8	0.0	0.0	100.0	0.9	-	0.0	0.0	0.0	33.3	0.0	-	0.0	1.3	0.0	0.0	1.1	-	0.0	0.6	4.5	0.0	0.8	0.9
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (12:45)



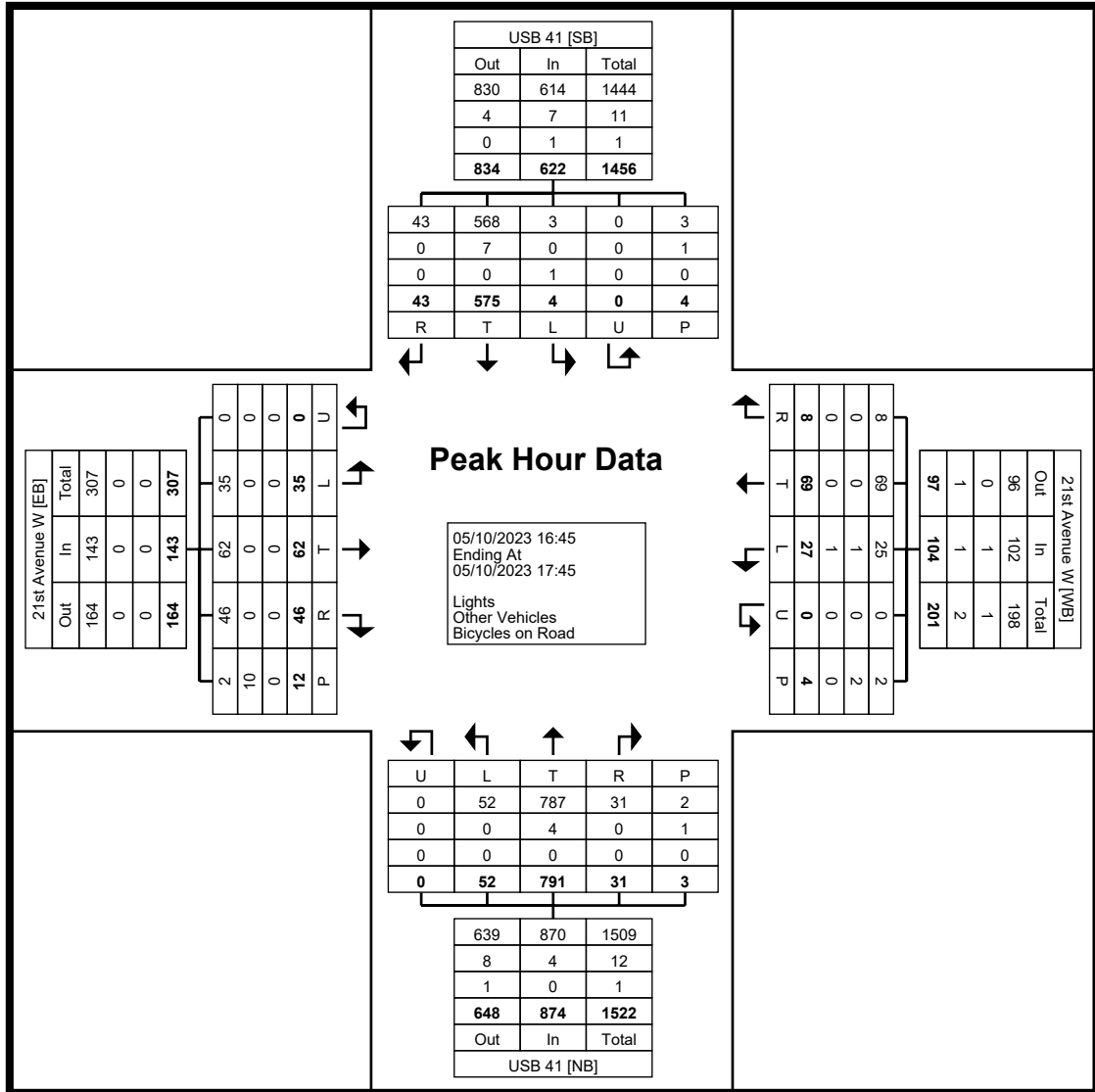
USB 41 at 21st Avenue W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 23\_USB 41 at 21st  
Avenue W  
Site Code: 23  
Start Date: 05/10/2023  
Page No: 7

### Turning Movement Peak Hour Data (16:45)

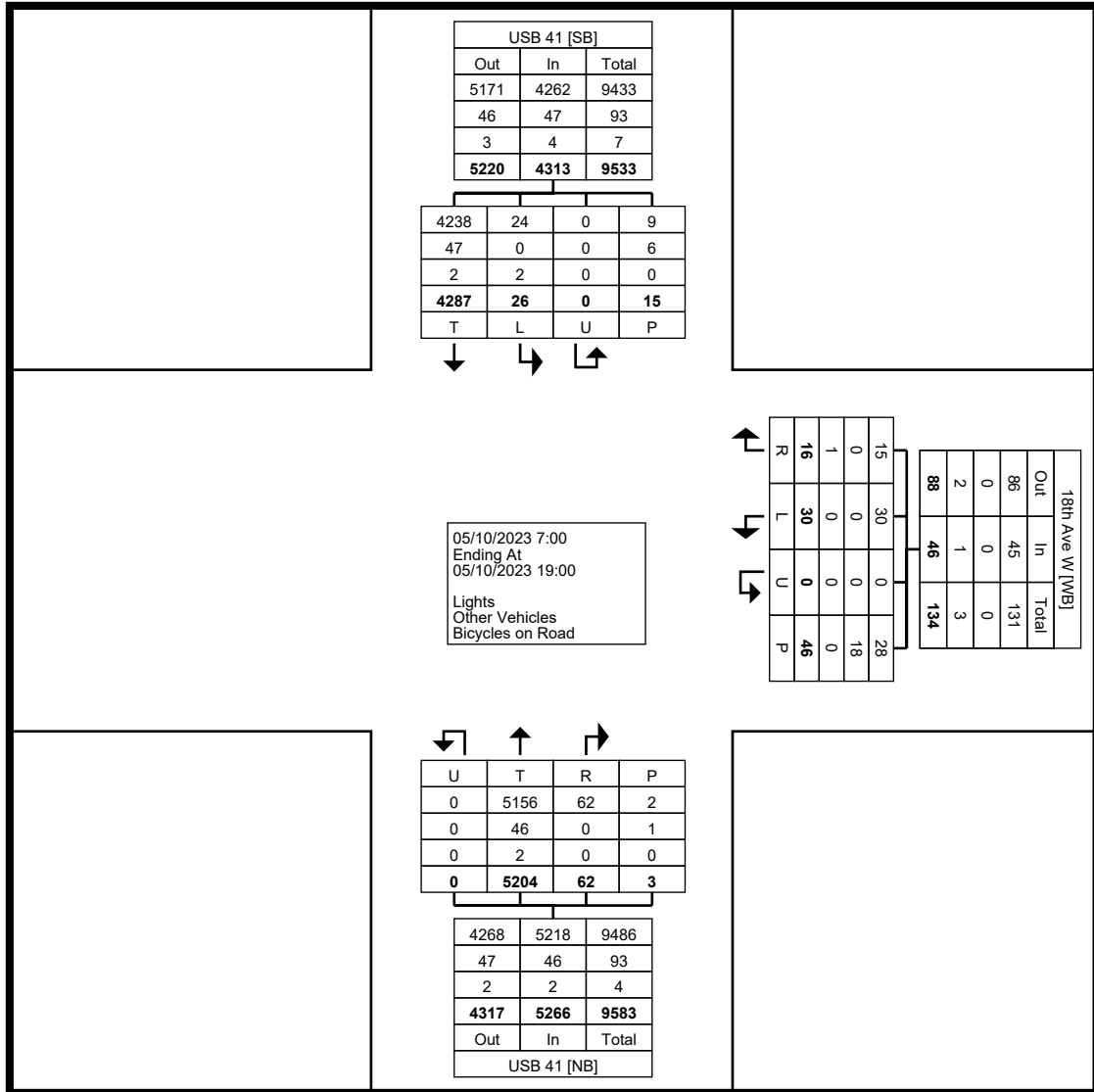
Start Time	21st Avenue W Eastbound						21st Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:45	0	9	22	13	1	44	0	7	14	1	2	22	0	9	158	9	0	176	0	1	135	7	2	143	385
17:00	0	11	10	11	1	32	0	3	17	3	2	23	0	17	217	13	1	247	0	1	171	16	0	188	490
17:15	0	7	14	13	4	34	0	9	20	3	0	32	0	12	224	3	2	239	0	2	138	11	2	151	456
17:30	0	8	16	9	6	33	0	8	18	1	0	27	0	14	192	6	0	212	0	0	131	9	0	140	412
<b>Total</b>	0	35	62	46	12	143	0	27	69	8	4	104	0	52	791	31	3	874	0	4	575	43	4	622	1743
Approach %	0.0	24.5	43.4	32.2	-	-	0.0	26.0	66.3	7.7	-	-	0.0	5.9	90.5	3.5	-	-	0.0	0.6	92.4	6.9	-	-	-
Total %	0.0	2.0	3.6	2.6	-	8.2	0.0	1.5	4.0	0.5	-	6.0	0.0	3.0	45.4	1.8	-	50.1	0.0	0.2	33.0	2.5	-	35.7	-
PHF	0.000	0.795	0.705	0.885	-	0.813	0.000	0.750	0.863	0.667	-	0.813	0.000	0.765	0.883	0.596	-	0.885	0.000	0.500	0.841	0.672	-	0.827	0.889
Lights	0	35	62	46	2	143	0	25	69	8	2	102	0	52	787	31	2	870	0	3	568	43	3	614	1729
% Lights	-	100.0	100.0	100.0	16.7	100.0	-	92.6	100.0	100.0	50.0	98.1	-	100.0	99.5	100.0	66.7	99.5	-	75.0	98.8	100.0	75.0	98.7	99.2
Other Vehicles	0	0	0	0	10	0	0	1	0	0	2	1	0	0	4	0	1	4	0	0	7	0	1	7	12
% Other Vehicles	-	0.0	0.0	0.0	83.3	0.0	-	3.7	0.0	0.0	50.0	1.0	-	0.0	0.5	0.0	33.3	0.5	-	0.0	1.2	0.0	25.0	1.1	0.7
Bicycles on Road	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	1	0	0	0	1	2
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	3.7	0.0	0.0	0.0	1.0	-	0.0	0.0	0.0	0.0	0.0	-	25.0	0.0	0.0	0.0	0.2	0.1



Turning Movement Peak Hour Data Plot (16:45)

### Turning Movement Data

Start Time	18th Ave W Westbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	
7:00	0	1	0	2	1	0	140	0	0	140	0	2	116	1	118	259
7:15	0	2	0	0	2	0	146	1	0	147	0	0	138	0	138	287
7:30	0	1	0	1	1	0	147	0	0	147	0	0	123	0	123	271
7:45	0	1	0	1	1	0	177	0	0	177	0	0	146	0	146	324
Hourly Total	0	5	0	4	5	0	610	1	0	611	0	2	523	1	525	1141
8:00	0	0	1	2	1	0	175	2	0	177	0	0	170	0	170	348
8:15	0	0	0	0	0	0	138	2	0	140	0	2	161	2	163	303
8:30	0	1	0	3	1	0	115	2	0	117	0	1	149	0	150	268
8:45	0	1	2	0	3	0	156	1	0	157	0	1	152	0	153	313
Hourly Total	0	2	3	5	5	0	584	7	0	591	0	4	632	2	636	1232
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	1	2	4	3	0	143	1	0	144	0	0	139	0	139	286
12:15	0	2	1	3	3	0	146	5	1	151	0	1	122	1	123	277
12:30	0	1	0	1	1	0	135	1	0	136	0	0	110	3	110	247
12:45	0	0	1	4	1	0	134	1	0	135	0	1	119	0	120	256
Hourly Total	0	4	4	12	8	0	558	8	1	566	0	2	490	4	492	1066
13:00	0	0	1	3	1	0	137	0	0	137	0	1	116	0	117	255
13:15	0	2	0	1	2	0	175	4	0	179	0	0	127	0	127	308
13:30	0	0	0	1	0	0	142	1	0	143	0	0	109	0	109	252
13:45	0	1	1	4	2	0	121	0	0	121	0	2	133	0	135	258
Hourly Total	0	3	2	9	5	0	575	5	0	580	0	3	485	0	488	1073
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	0	1	0	1	0	173	3	0	176	0	1	158	0	159	336
15:15	0	1	0	0	1	0	154	2	0	156	0	3	133	1	136	293
15:30	0	0	0	0	0	0	180	1	0	181	0	2	142	1	144	325
15:45	0	2	0	2	2	0	200	4	0	204	0	1	141	2	142	348
Hourly Total	0	3	1	2	4	0	707	10	0	717	0	7	574	4	581	1302
16:00	0	0	2	0	2	0	200	2	0	202	0	2	134	0	136	340
16:15	0	2	1	2	3	0	190	3	0	193	0	1	115	1	116	312
16:30	0	2	0	2	2	0	191	1	0	192	0	0	143	0	143	337
16:45	0	0	1	0	1	0	153	1	0	154	0	0	147	1	147	302
Hourly Total	0	4	4	4	8	0	734	7	0	741	0	3	539	2	542	1291
17:00	0	1	0	2	1	0	228	4	0	232	0	1	183	0	184	417
17:15	0	0	0	1	0	0	210	6	1	216	0	1	155	0	156	372
17:30	0	2	1	1	3	0	208	2	1	210	0	0	134	1	134	347
17:45	0	1	1	1	2	0	180	1	0	181	0	1	134	0	135	318
Hourly Total	0	4	2	5	6	0	826	13	2	839	0	3	606	1	609	1454
18:00	0	2	0	2	2	0	179	2	0	181	0	2	115	0	117	300
18:15	0	1	0	0	1	0	144	3	0	147	0	0	119	0	119	267
18:30	0	1	0	1	1	0	156	3	0	159	0	0	102	0	102	262
18:45	0	1	0	2	1	0	131	3	0	134	0	0	102	1	102	237
Hourly Total	0	5	0	5	5	0	610	11	0	621	0	2	438	1	440	1066
Grand Total	0	30	16	46	46	0	5204	62	3	5266	0	26	4287	15	4313	9625
Approach %	0.0	65.2	34.8	-	-	0.0	98.8	1.2	-	-	0.0	0.6	99.4	-	-	-
Total %	0.0	0.3	0.2	-	0.5	0.0	54.1	0.6	-	54.7	0.0	0.3	44.5	-	44.8	-
Lights	0	30	15	28	45	0	5156	62	2	5218	0	24	4238	9	4262	9525
% Lights	-	100.0	93.8	60.9	97.8	-	99.1	100.0	66.7	99.1	-	92.3	98.9	60.0	98.8	99.0
Other Vehicles	0	0	0	18	0	0	46	0	1	46	0	0	47	6	47	93
% Other Vehicles	-	0.0	0.0	39.1	0.0	-	0.9	0.0	33.3	0.9	-	0.0	1.1	40.0	1.1	1.0
Bicycles on Road	0	0	1	0	1	0	2	0	0	2	0	2	2	0	4	7
% Bicycles on Road	-	0.0	6.3	0.0	2.2	-	0.0	0.0	0.0	0.0	-	7.7	0.0	0.0	0.1	0.1



Turning Movement Data Plot

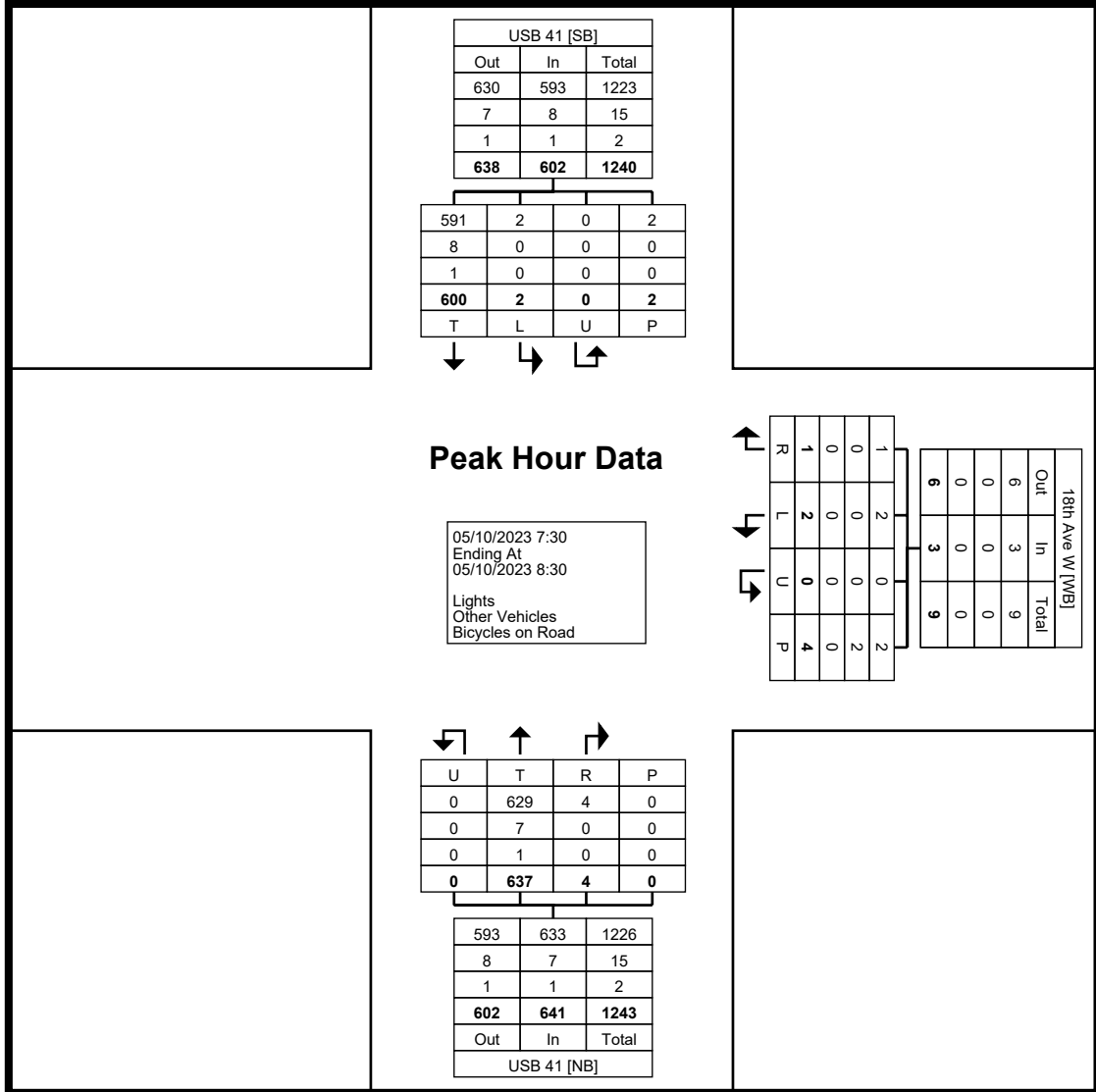
USB 41 & 18th Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 22\_USB 41 & 18th  
Ave W  
Site Code: 22  
Start Date: 05/10/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:30)

Start Time	18th Ave W Westbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	
7:30	0	1	0	1	1	0	147	0	0	147	0	0	123	0	123	271
7:45	0	1	0	1	1	0	177	0	0	177	0	0	146	0	146	324
8:00	0	0	1	2	1	0	175	2	0	177	0	0	170	0	170	348
8:15	0	0	0	0	0	0	138	2	0	140	0	2	161	2	163	303
<b>Total</b>	<b>0</b>	<b>2</b>	<b>1</b>	<b>4</b>	<b>3</b>	<b>0</b>	<b>637</b>	<b>4</b>	<b>0</b>	<b>641</b>	<b>0</b>	<b>2</b>	<b>600</b>	<b>2</b>	<b>602</b>	<b>1246</b>
Approach %	0.0	66.7	33.3	-	-	0.0	99.4	0.6	-	-	0.0	0.3	99.7	-	-	-
Total %	0.0	0.2	0.1	-	0.2	0.0	51.1	0.3	-	51.4	0.0	0.2	48.2	-	48.3	-
PHF	0.000	0.500	0.250	-	0.750	0.000	0.900	0.500	-	0.905	0.000	0.250	0.882	-	0.885	0.895
Lights	0	2	1	2	3	0	629	4	0	633	0	2	591	2	593	1229
% Lights	-	100.0	100.0	50.0	100.0	-	98.7	100.0	-	98.8	-	100.0	98.5	100.0	98.5	98.6
Other Vehicles	0	0	0	2	0	0	7	0	0	7	0	0	8	0	8	15
% Other Vehicles	-	0.0	0.0	50.0	0.0	-	1.1	0.0	-	1.1	-	0.0	1.3	0.0	1.3	1.2
Bicycles on Road	0	0	0	0	0	0	1	0	0	1	0	0	1	0	1	2
% Bicycles on Road	-	0.0	0.0	0.0	0.0	-	0.2	0.0	-	0.2	-	0.0	0.2	0.0	0.2	0.2



Turning Movement Peak Hour Data Plot (7:30)

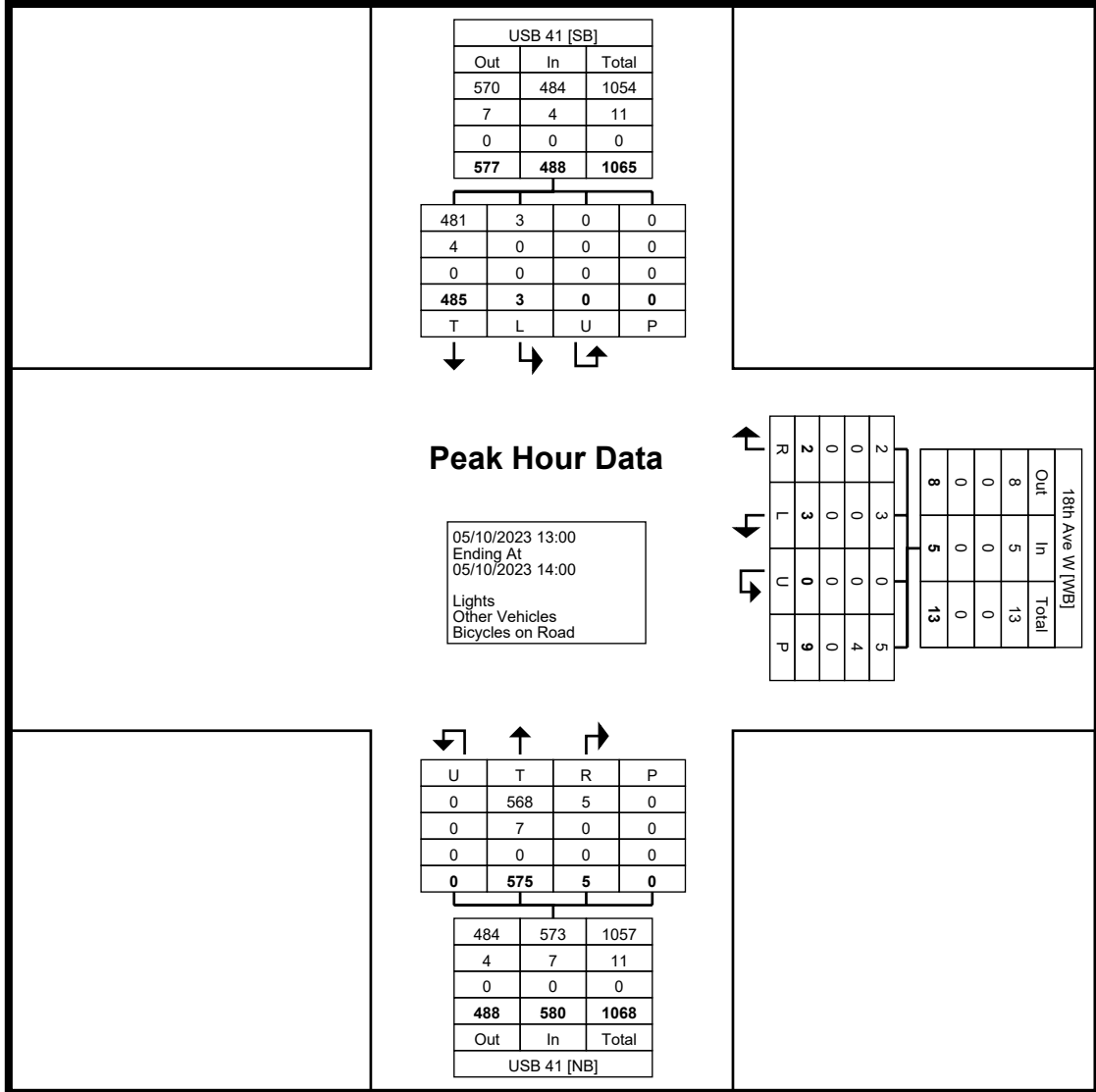
USB 41 & 18th Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 22\_USB 41 & 18th  
Ave W  
Site Code: 22  
Start Date: 05/10/2023  
Page No: 5

### Turning Movement Peak Hour Data (13:00)

Start Time	18th Ave W Westbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	
13:00	0	0	1	3	1	0	137	0	0	137	0	1	116	0	117	255
13:15	0	2	0	1	2	0	175	4	0	179	0	0	127	0	127	308
13:30	0	0	0	1	0	0	142	1	0	143	0	0	109	0	109	252
13:45	0	1	1	4	2	0	121	0	0	121	0	2	133	0	135	258
<b>Total</b>	<b>0</b>	<b>3</b>	<b>2</b>	<b>9</b>	<b>5</b>	<b>0</b>	<b>575</b>	<b>5</b>	<b>0</b>	<b>580</b>	<b>0</b>	<b>3</b>	<b>485</b>	<b>0</b>	<b>488</b>	<b>1073</b>
Approach %	0.0	60.0	40.0	-	-	0.0	99.1	0.9	-	-	0.0	0.6	99.4	-	-	-
Total %	0.0	0.3	0.2	-	0.5	0.0	53.6	0.5	-	54.1	0.0	0.3	45.2	-	45.5	-
PHF	0.000	0.375	0.500	-	0.625	0.000	0.821	0.313	-	0.810	0.000	0.375	0.912	-	0.904	0.871
Lights	0	3	2	5	5	0	568	5	0	573	0	3	481	0	484	1062
% Lights	-	100.0	100.0	55.6	100.0	-	98.8	100.0	-	98.8	-	100.0	99.2	-	99.2	99.0
Other Vehicles	0	0	0	4	0	0	7	0	0	7	0	0	4	0	4	11
% Other Vehicles	-	0.0	0.0	44.4	0.0	-	1.2	0.0	-	1.2	-	0.0	0.8	-	0.8	1.0
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.0



Turning Movement Peak Hour Data Plot (13:00)



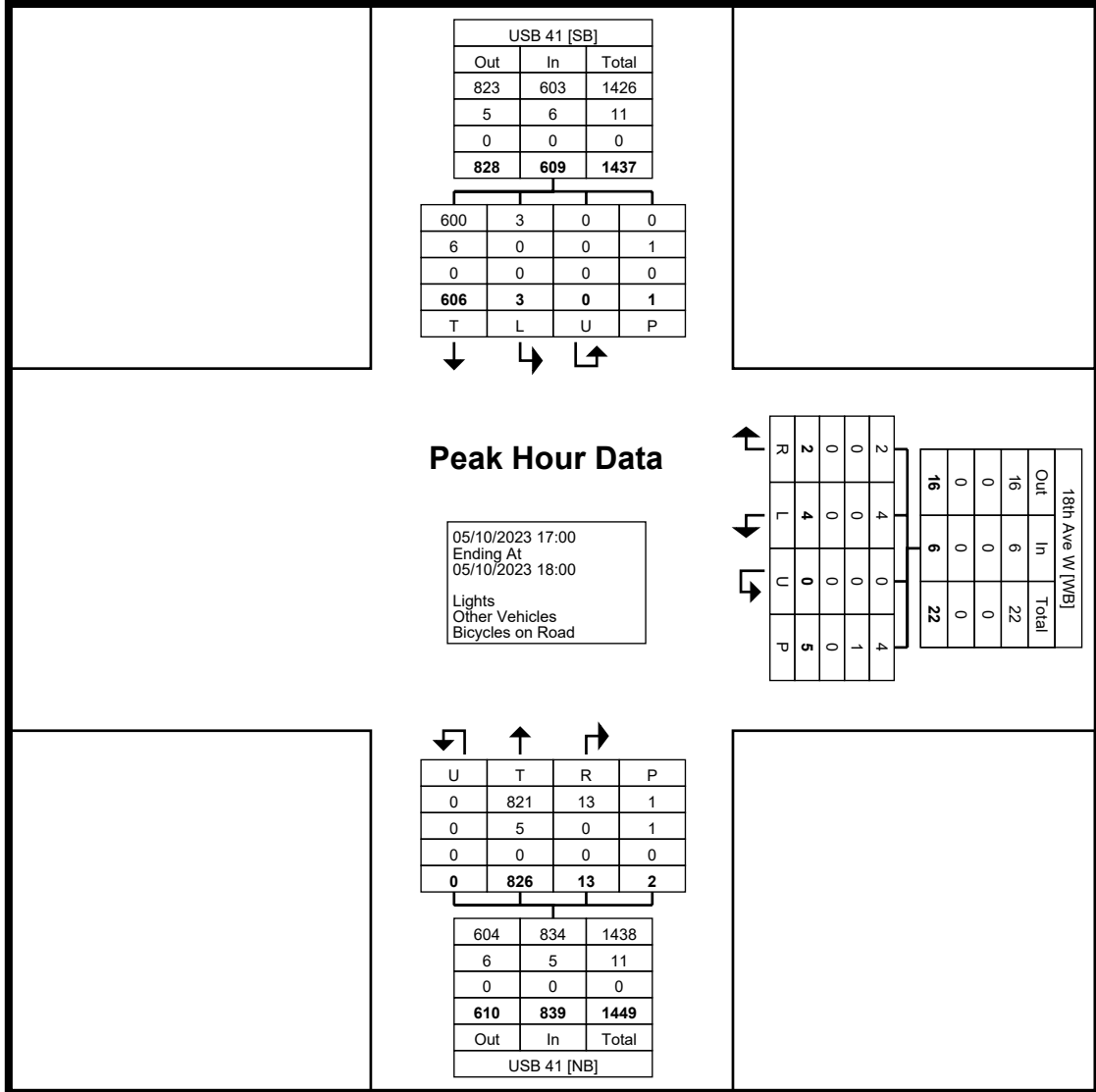
USB 41 & 18th Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 22\_USB 41 & 18th  
Ave W  
Site Code: 22  
Start Date: 05/10/2023  
Page No: 7

### Turning Movement Peak Hour Data (17:00)

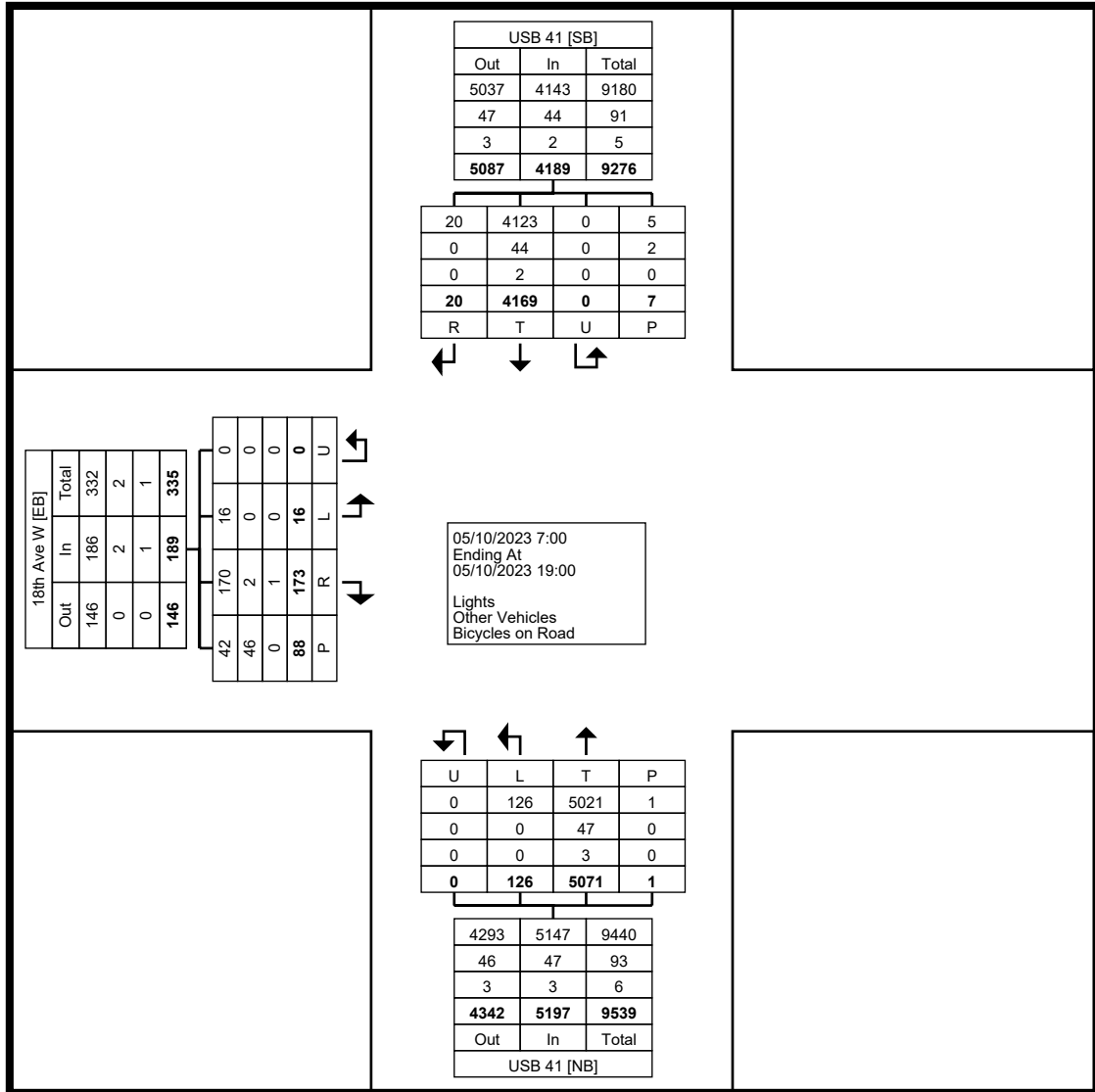
Start Time	18th Ave W Westbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	
17:00	0	1	0	2	1	0	228	4	0	232	0	1	183	0	184	417
17:15	0	0	0	1	0	0	210	6	1	216	0	1	155	0	156	372
17:30	0	2	1	1	3	0	208	2	1	210	0	0	134	1	134	347
17:45	0	1	1	1	2	0	180	1	0	181	0	1	134	0	135	318
<b>Total</b>	<b>0</b>	<b>4</b>	<b>2</b>	<b>5</b>	<b>6</b>	<b>0</b>	<b>826</b>	<b>13</b>	<b>2</b>	<b>839</b>	<b>0</b>	<b>3</b>	<b>606</b>	<b>1</b>	<b>609</b>	<b>1454</b>
<b>Approach %</b>	<b>0.0</b>	<b>66.7</b>	<b>33.3</b>	<b>-</b>	<b>-</b>	<b>0.0</b>	<b>98.5</b>	<b>1.5</b>	<b>-</b>	<b>-</b>	<b>0.0</b>	<b>0.5</b>	<b>99.5</b>	<b>-</b>	<b>-</b>	<b>-</b>
<b>Total %</b>	<b>0.0</b>	<b>0.3</b>	<b>0.1</b>	<b>-</b>	<b>0.4</b>	<b>0.0</b>	<b>56.8</b>	<b>0.9</b>	<b>-</b>	<b>57.7</b>	<b>0.0</b>	<b>0.2</b>	<b>41.7</b>	<b>-</b>	<b>41.9</b>	<b>-</b>
<b>PHF</b>	<b>0.000</b>	<b>0.500</b>	<b>0.500</b>	<b>-</b>	<b>0.500</b>	<b>0.000</b>	<b>0.906</b>	<b>0.542</b>	<b>-</b>	<b>0.904</b>	<b>0.000</b>	<b>0.750</b>	<b>0.828</b>	<b>-</b>	<b>0.827</b>	<b>0.872</b>
<b>Lights</b>	<b>0</b>	<b>4</b>	<b>2</b>	<b>4</b>	<b>6</b>	<b>0</b>	<b>821</b>	<b>13</b>	<b>1</b>	<b>834</b>	<b>0</b>	<b>3</b>	<b>600</b>	<b>0</b>	<b>603</b>	<b>1443</b>
<b>% Lights</b>	<b>-</b>	<b>100.0</b>	<b>100.0</b>	<b>80.0</b>	<b>100.0</b>	<b>-</b>	<b>99.4</b>	<b>100.0</b>	<b>50.0</b>	<b>99.4</b>	<b>-</b>	<b>100.0</b>	<b>99.0</b>	<b>0.0</b>	<b>99.0</b>	<b>99.2</b>
<b>Other Vehicles</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>5</b>	<b>0</b>	<b>1</b>	<b>5</b>	<b>0</b>	<b>0</b>	<b>6</b>	<b>1</b>	<b>6</b>	<b>11</b>
<b>% Other Vehicles</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>	<b>20.0</b>	<b>0.0</b>	<b>-</b>	<b>0.6</b>	<b>0.0</b>	<b>50.0</b>	<b>0.6</b>	<b>-</b>	<b>0.0</b>	<b>1.0</b>	<b>100.0</b>	<b>1.0</b>	<b>0.8</b>
<b>Bicycles on Road</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>% Bicycles on Road</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>	<b>0.0</b>	<b>0.0</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>	<b>0.0</b>	<b>0.0</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>	<b>0.0</b>	<b>0.0</b>	<b>0.0</b>



Turning Movement Peak Hour Data Plot (17:00)

### Turning Movement Data

Start Time	18th Ave W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
7:00	0	1	6	4	7	0	6	132	0	138	0	117	1	0	118	263
7:15	0	1	5	1	6	0	5	140	0	145	0	138	1	1	139	290
7:30	0	0	6	3	6	0	2	141	0	143	0	118	1	0	119	268
7:45	0	0	10	6	10	0	3	169	0	172	0	138	0	0	138	320
Hourly Total	0	2	27	14	29	0	16	582	0	598	0	511	3	1	514	1141
8:00	0	0	9	3	9	0	8	172	0	180	0	156	2	0	158	347
8:15	0	0	9	5	9	0	0	137	0	137	0	151	1	0	152	298
8:30	0	1	5	1	6	0	1	116	0	117	0	143	1	0	144	267
8:45	0	1	2	4	3	0	2	150	0	152	0	150	2	1	152	307
Hourly Total	0	2	25	13	27	0	11	575	0	586	0	600	6	1	606	1219
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	4	1	4	0	4	147	0	151	0	142	0	0	142	297
12:15	0	2	1	1	3	0	2	138	0	140	0	124	0	0	124	267
12:30	0	0	6	2	6	0	2	139	0	141	0	107	0	0	107	254
12:45	0	1	5	3	6	0	5	129	0	134	0	118	0	1	118	258
Hourly Total	0	3	16	7	19	0	13	553	0	566	0	491	0	1	491	1076
13:00	0	1	6	1	7	0	5	131	0	136	0	111	0	0	111	254
13:15	0	0	2	1	2	0	2	173	0	175	0	127	0	0	127	304
13:30	0	0	4	4	4	0	0	143	0	143	0	110	0	0	110	257
13:45	0	0	4	2	4	0	3	117	0	120	0	127	0	0	127	251
Hourly Total	0	1	16	8	17	0	10	564	0	574	0	475	0	0	475	1066
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	0	11	1	11	0	4	164	0	168	0	153	0	0	153	332
15:15	0	1	8	0	9	0	5	155	0	160	0	124	1	0	125	294
15:30	0	1	3	1	4	0	1	172	0	173	0	142	0	0	142	319
15:45	0	1	8	2	9	0	4	200	1	204	0	131	3	0	134	347
Hourly Total	0	3	30	4	33	0	14	691	1	705	0	550	4	0	554	1292
16:00	0	0	4	5	4	0	5	192	0	197	0	140	0	1	140	341
16:15	0	0	9	1	9	0	1	195	0	196	0	107	0	0	107	312
16:30	0	0	4	3	4	0	2	184	0	186	0	141	2	1	143	333
16:45	0	1	4	3	5	0	4	152	0	156	0	143	0	0	143	304
Hourly Total	0	1	21	12	22	0	12	723	0	735	0	531	2	2	533	1290
17:00	0	2	10	4	12	0	9	215	0	224	0	177	1	0	178	414
17:15	0	0	4	4	4	0	10	203	0	213	0	149	1	0	150	367
17:30	0	1	3	3	4	0	11	190	0	201	0	132	0	1	132	337
17:45	0	0	4	5	4	0	5	179	0	184	0	130	1	0	131	319
Hourly Total	0	3	21	16	24	0	35	787	0	822	0	588	3	1	591	1437
18:00	0	0	5	3	5	0	5	171	0	176	0	110	2	0	112	293
18:15	0	0	4	1	4	0	4	146	0	150	0	114	0	1	114	268
18:30	0	1	5	7	6	0	4	151	0	155	0	99	0	0	99	260
18:45	0	0	3	3	3	0	2	128	0	130	0	100	0	0	100	233
Hourly Total	0	1	17	14	18	0	15	596	0	611	0	423	2	1	425	1054
Grand Total	0	16	173	88	189	0	126	5071	1	5197	0	4169	20	7	4189	9575
Approach %	0.0	8.5	91.5	-	-	0.0	2.4	97.6	-	-	0.0	99.5	0.5	-	-	-
Total %	0.0	0.2	1.8	-	2.0	0.0	1.3	53.0	-	54.3	0.0	43.5	0.2	-	43.7	-
Lights	0	16	170	42	186	0	126	5021	1	5147	0	4123	20	5	4143	9476
% Lights	-	100.0	98.3	47.7	98.4	-	100.0	99.0	100.0	99.0	-	98.9	100.0	71.4	98.9	99.0
Other Vehicles	0	0	2	46	2	0	0	47	0	47	0	44	0	2	44	93
% Other Vehicles	-	0.0	1.2	52.3	1.1	-	0.0	0.9	0.0	0.9	-	1.1	0.0	28.6	1.1	1.0
Bicycles on Road	0	0	1	0	1	0	0	3	0	3	0	2	0	0	2	6
% Bicycles on Road	-	0.0	0.6	0.0	0.5	-	0.0	0.1	0.0	0.1	-	0.0	0.0	0.0	0.0	0.1



Turning Movement Data Plot

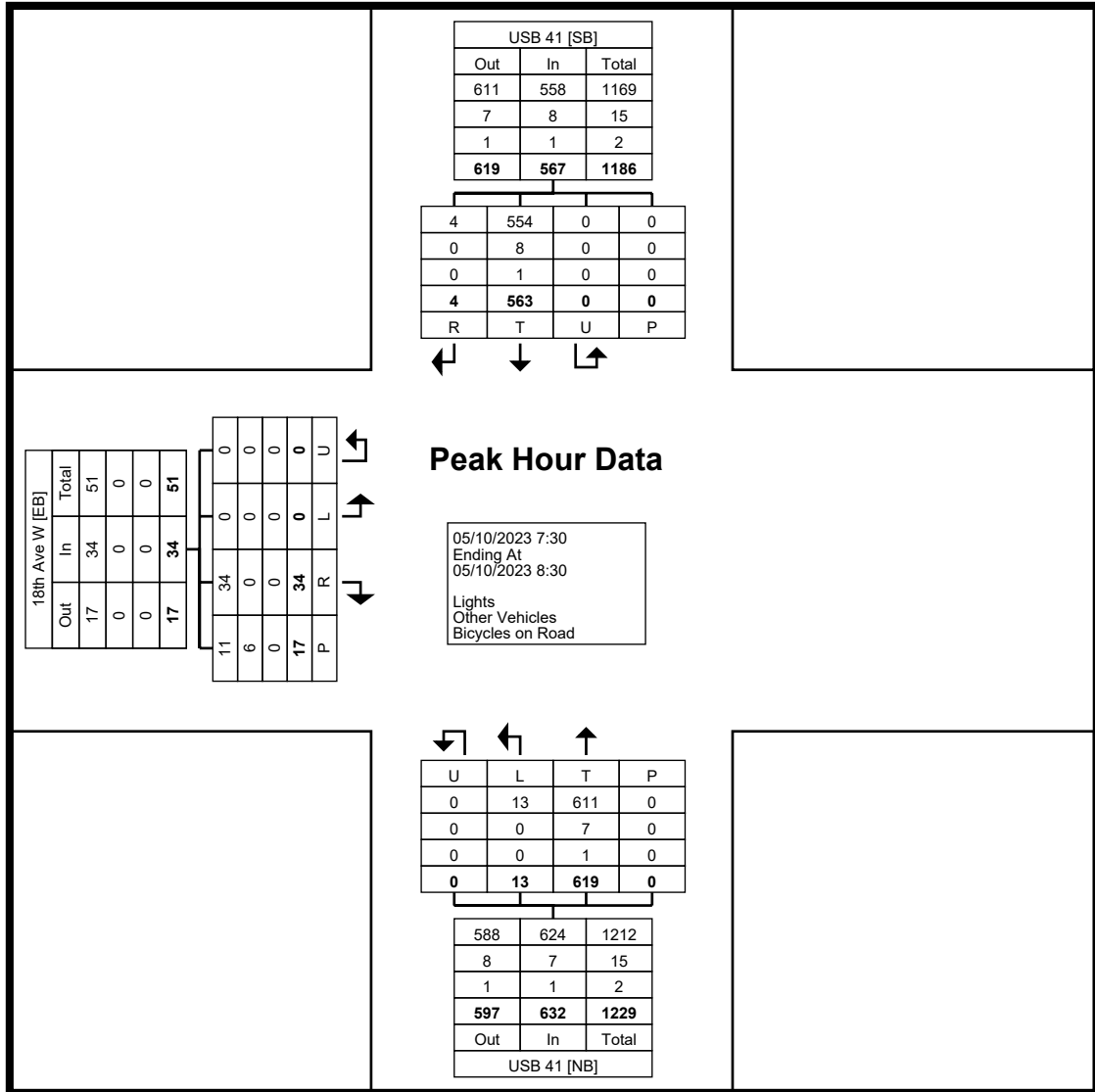
USB 41 & 18th Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 21\_USB 41 & 18th  
Ave W  
Site Code: 21  
Start Date: 05/10/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:30)

Start Time	18th Ave W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
7:30	0	0	6	3	6	0	2	141	0	143	0	118	1	0	119	268
7:45	0	0	10	6	10	0	3	169	0	172	0	138	0	0	138	320
8:00	0	0	9	3	9	0	8	172	0	180	0	156	2	0	158	347
8:15	0	0	9	5	9	0	0	137	0	137	0	151	1	0	152	298
<b>Total</b>	<b>0</b>	<b>0</b>	<b>34</b>	<b>17</b>	<b>34</b>	<b>0</b>	<b>13</b>	<b>619</b>	<b>0</b>	<b>632</b>	<b>0</b>	<b>563</b>	<b>4</b>	<b>0</b>	<b>567</b>	<b>1233</b>
Approach %	0.0	0.0	100.0	-	-	0.0	2.1	97.9	-	-	0.0	99.3	0.7	-	-	-
Total %	0.0	0.0	2.8	-	2.8	0.0	1.1	50.2	-	51.3	0.0	45.7	0.3	-	46.0	-
PHF	0.000	0.000	0.850	-	0.850	0.000	0.406	0.900	-	0.878	0.000	0.902	0.500	-	0.897	0.888
Lights	0	0	34	11	34	0	13	611	0	624	0	554	4	0	558	1216
% Lights	-	-	100.0	64.7	100.0	-	100.0	98.7	-	98.7	-	98.4	100.0	-	98.4	98.6
Other Vehicles	0	0	0	6	0	0	0	7	0	7	0	8	0	0	8	15
% Other Vehicles	-	-	0.0	35.3	0.0	-	0.0	1.1	-	1.1	-	1.4	0.0	-	1.4	1.2
Bicycles on Road	0	0	0	0	0	0	0	1	0	1	0	1	0	0	1	2
% Bicycles on Road	-	-	0.0	0.0	0.0	-	0.0	0.2	-	0.2	-	0.2	0.0	-	0.2	0.2



Turning Movement Peak Hour Data Plot (7:30)

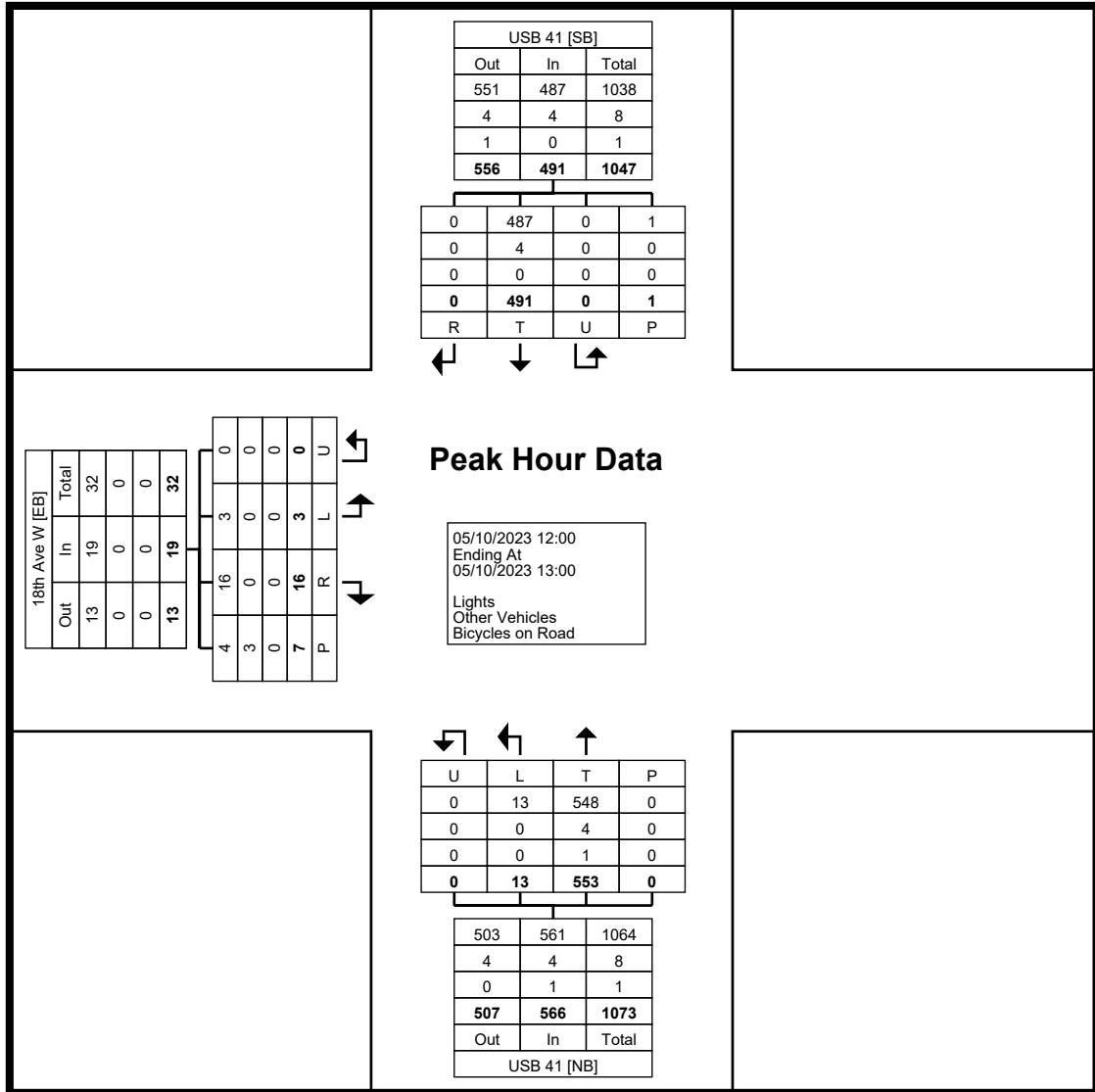
USB 41 & 18th Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 21\_USB 41 & 18th  
Ave W  
Site Code: 21  
Start Date: 05/10/2023  
Page No: 5

### Turning Movement Peak Hour Data (12:00)

Start Time	18th Ave W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
12:00	0	0	4	1	4	0	4	147	0	151	0	142	0	0	142	297
12:15	0	2	1	1	3	0	2	138	0	140	0	124	0	0	124	267
12:30	0	0	6	2	6	0	2	139	0	141	0	107	0	0	107	254
12:45	0	1	5	3	6	0	5	129	0	134	0	118	0	1	118	258
<b>Total</b>	<b>0</b>	<b>3</b>	<b>16</b>	<b>7</b>	<b>19</b>	<b>0</b>	<b>13</b>	<b>553</b>	<b>0</b>	<b>566</b>	<b>0</b>	<b>491</b>	<b>0</b>	<b>1</b>	<b>491</b>	<b>1076</b>
Approach %	0.0	15.8	84.2	-	-	0.0	2.3	97.7	-	-	0.0	100.0	0.0	-	-	-
Total %	0.0	0.3	1.5	-	1.8	0.0	1.2	51.4	-	52.6	0.0	45.6	0.0	-	45.6	-
PHF	0.000	0.375	0.667	-	0.792	0.000	0.650	0.940	-	0.937	0.000	0.864	0.000	-	0.864	0.906
Lights	0	3	16	4	19	0	13	548	0	561	0	487	0	1	487	1067
% Lights	-	100.0	100.0	57.1	100.0	-	100.0	99.1	-	99.1	-	99.2	-	100.0	99.2	99.2
Other Vehicles	0	0	0	3	0	0	0	4	0	4	0	4	0	0	4	8
% Other Vehicles	-	0.0	0.0	42.9	0.0	-	0.0	0.7	-	0.7	-	0.8	-	0.0	0.8	0.7
Bicycles on Road	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	1
% Bicycles on Road	-	0.0	0.0	0.0	0.0	-	0.0	0.2	-	0.2	-	0.0	-	0.0	0.0	0.1



Turning Movement Peak Hour Data Plot (12:00)



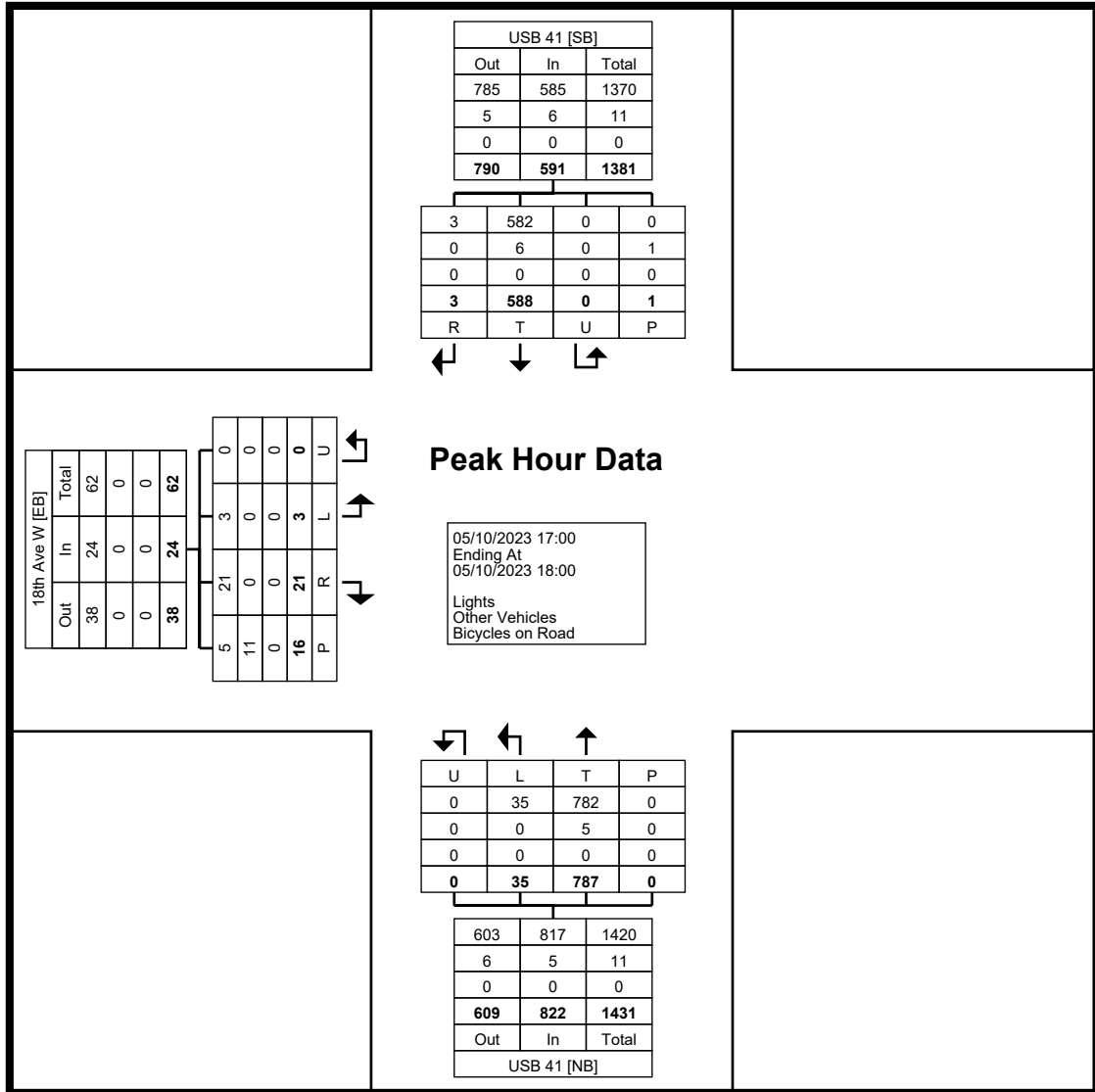
USB 41 & 18th Ave W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 21\_USB 41 & 18th  
Ave W  
Site Code: 21  
Start Date: 05/10/2023  
Page No: 7

### Turning Movement Peak Hour Data (17:00)

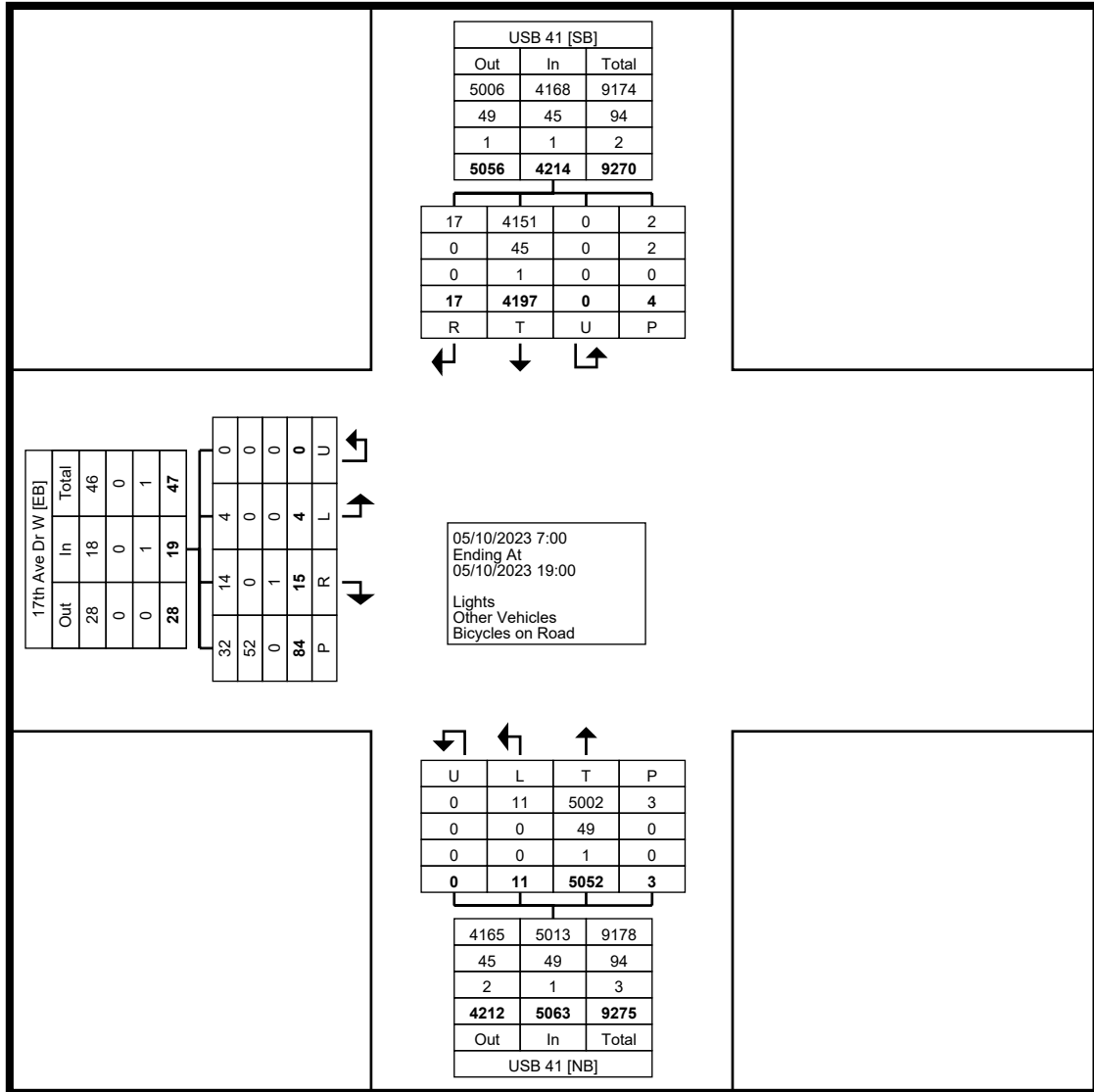
Start Time	18th Ave W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
17:00	0	2	10	4	12	0	9	215	0	224	0	177	1	0	178	414
17:15	0	0	4	4	4	0	10	203	0	213	0	149	1	0	150	367
17:30	0	1	3	3	4	0	11	190	0	201	0	132	0	1	132	337
17:45	0	0	4	5	4	0	5	179	0	184	0	130	1	0	131	319
<b>Total</b>	<b>0</b>	<b>3</b>	<b>21</b>	<b>16</b>	<b>24</b>	<b>0</b>	<b>35</b>	<b>787</b>	<b>0</b>	<b>822</b>	<b>0</b>	<b>588</b>	<b>3</b>	<b>1</b>	<b>591</b>	<b>1437</b>
Approach %	0.0	12.5	87.5	-	-	0.0	4.3	95.7	-	-	0.0	99.5	0.5	-	-	-
Total %	0.0	0.2	1.5	-	1.7	0.0	2.4	54.8	-	57.2	0.0	40.9	0.2	-	41.1	-
PHF	0.000	0.375	0.525	-	0.500	0.000	0.795	0.915	-	0.917	0.000	0.831	0.750	-	0.830	0.868
Lights	0	3	21	5	24	0	35	782	0	817	0	582	3	0	585	1426
% Lights	-	100.0	100.0	31.3	100.0	-	100.0	99.4	-	99.4	-	99.0	100.0	0.0	99.0	99.2
Other Vehicles	0	0	0	11	0	0	0	5	0	5	0	6	0	1	6	11
% Other Vehicles	-	0.0	0.0	68.8	0.0	-	0.0	0.6	-	0.6	-	1.0	0.0	100.0	1.0	0.8
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (17:00)

### Turning Movement Data

Start Time	17th Ave Dr W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
7:00	0	0	0	6	0	0	1	133	1	134	0	133	0	1	133	267
7:15	0	0	0	0	0	0	1	139	2	140	0	131	1	1	132	272
7:30	0	0	1	3	1	0	0	144	0	144	0	121	1	0	122	267
7:45	0	0	0	6	0	0	0	180	0	180	0	132	1	0	133	313
Hourly Total	0	0	1	15	1	0	2	596	3	598	0	517	3	2	520	1119
8:00	0	0	0	4	0	0	0	168	0	168	0	161	2	0	163	331
8:15	0	0	2	4	2	0	1	134	0	135	0	159	0	0	159	296
8:30	0	0	2	2	2	0	0	119	0	119	0	142	0	1	142	263
8:45	0	0	0	5	0	0	0	154	0	154	0	148	0	0	148	302
Hourly Total	0	0	4	15	4	0	1	575	0	576	0	610	2	1	612	1192
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	0	1	0	0	0	147	0	147	0	131	0	0	131	278
12:15	0	0	0	2	0	0	0	139	0	139	0	128	0	0	128	267
12:30	0	0	2	1	2	0	0	128	0	128	0	111	0	0	111	241
12:45	0	0	0	3	0	0	1	138	0	139	0	110	1	0	111	250
Hourly Total	0	0	2	7	2	0	1	552	0	553	0	480	1	0	481	1036
13:00	0	0	0	1	0	0	0	121	0	121	0	115	2	0	117	238
13:15	0	0	1	1	1	0	0	190	0	190	0	123	2	0	125	316
13:30	0	0	2	5	2	0	1	128	0	129	0	113	2	0	115	246
13:45	0	0	1	2	1	0	0	123	0	123	0	127	1	0	128	252
Hourly Total	0	0	4	9	4	0	1	562	0	563	0	478	7	0	485	1052
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	0	1	1	1	0	1	162	0	163	0	146	0	0	146	310
15:15	0	0	0	0	0	0	0	155	0	155	0	134	0	0	134	289
15:30	0	0	0	1	0	0	1	174	0	175	0	147	0	0	147	322
15:45	0	0	0	2	0	0	0	192	0	192	0	129	0	0	129	321
Hourly Total	0	0	1	4	1	0	2	683	0	685	0	556	0	0	556	1242
16:00	0	0	0	4	0	0	1	197	0	198	0	144	2	1	146	344
16:15	0	0	0	1	0	0	0	202	0	202	0	109	0	0	109	311
16:30	0	0	0	2	0	0	0	168	0	168	0	139	0	0	139	307
16:45	0	0	0	5	0	0	1	162	0	163	0	140	0	0	140	303
Hourly Total	0	0	0	12	0	0	2	729	0	731	0	532	2	1	534	1265
17:00	0	1	0	3	1	0	1	204	0	205	0	188	0	0	188	394
17:15	0	0	0	3	0	0	0	212	0	212	0	143	0	0	143	355
17:30	0	1	1	5	2	0	0	182	0	182	0	141	0	0	141	325
17:45	0	0	0	4	0	0	0	169	0	169	0	125	1	0	126	295
Hourly Total	0	2	1	15	3	0	1	767	0	768	0	597	1	0	598	1369
18:00	0	0	1	3	1	0	0	166	0	166	0	120	0	0	120	287
18:15	0	0	0	0	0	0	0	143	0	143	0	106	1	0	107	250
18:30	0	0	0	1	0	0	1	142	0	143	0	99	0	0	99	242
18:45	0	2	1	3	3	0	0	137	0	137	0	102	0	0	102	242
Hourly Total	0	2	2	7	4	0	1	588	0	589	0	427	1	0	428	1021
Grand Total	0	4	15	84	19	0	11	5052	3	5063	0	4197	17	4	4214	9296
Approach %	0.0	21.1	78.9	-	-	0.0	0.2	99.8	-	-	0.0	99.6	0.4	-	-	-
Total %	0.0	0.0	0.2	-	0.2	0.0	0.1	54.3	-	54.5	0.0	45.1	0.2	-	45.3	-
Lights	0	4	14	32	18	0	11	5002	3	5013	0	4151	17	2	4168	9199
% Lights	-	100.0	93.3	38.1	94.7	-	100.0	99.0	100.0	99.0	-	98.9	100.0	50.0	98.9	99.0
Other Vehicles	0	0	0	52	0	0	0	49	0	49	0	45	0	2	45	94
% Other Vehicles	-	0.0	0.0	61.9	0.0	-	0.0	1.0	0.0	1.0	-	1.1	0.0	50.0	1.1	1.0
Bicycles on Road	0	0	1	0	1	0	0	1	0	1	0	1	0	0	1	3
% Bicycles on Road	-	0.0	6.7	0.0	5.3	-	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0



Turning Movement Data Plot

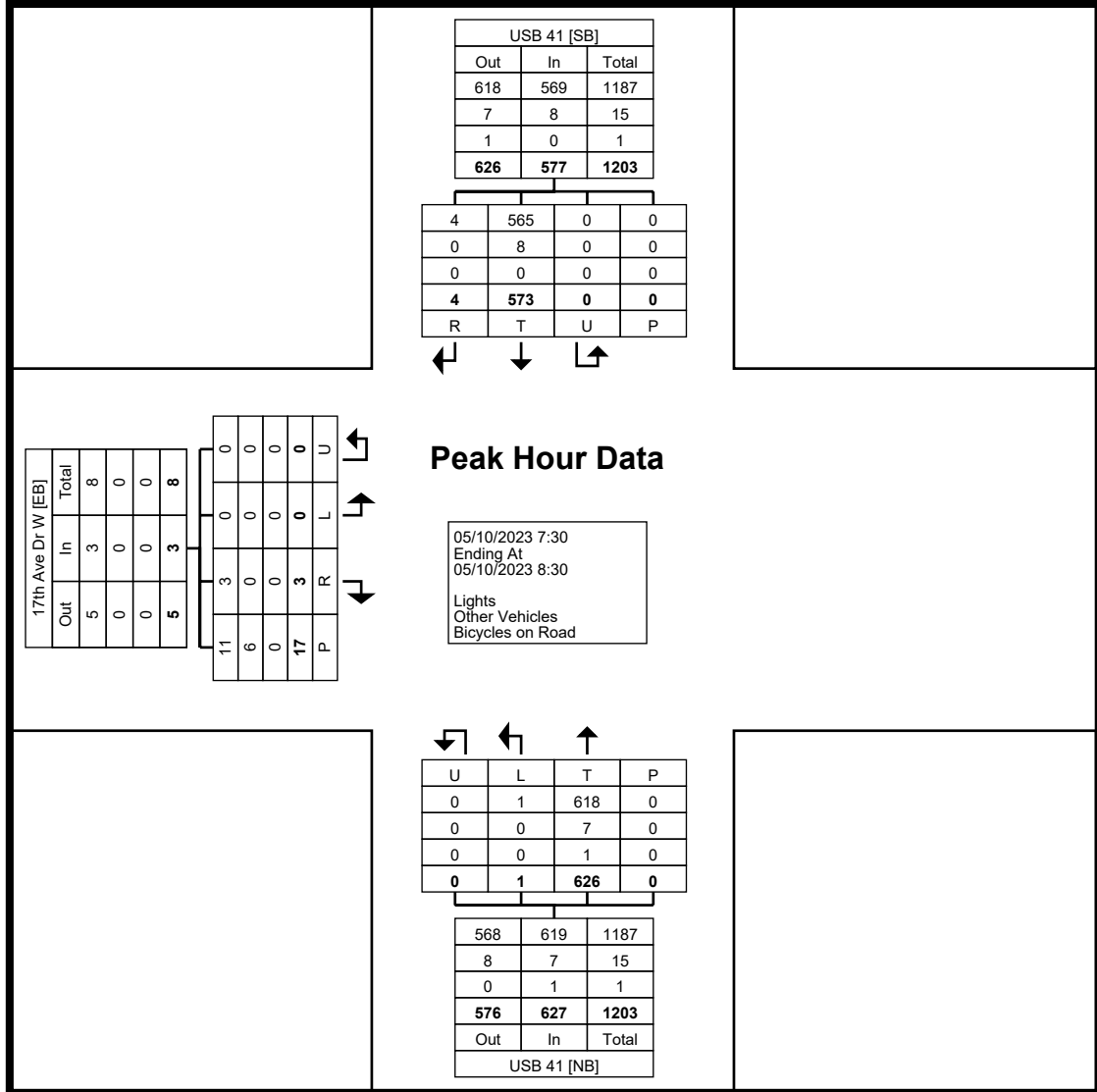
USB 41 & 17th Ave Dr W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 20\_USB 41 & 17th  
Ave Dr W  
Site Code: 20  
Start Date: 05/10/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:30)

Start Time	17th Ave Dr W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
7:30	0	0	1	3	1	0	0	144	0	144	0	121	1	0	122	267
7:45	0	0	0	6	0	0	0	180	0	180	0	132	1	0	133	313
8:00	0	0	0	4	0	0	0	168	0	168	0	161	2	0	163	331
8:15	0	0	2	4	2	0	1	134	0	135	0	159	0	0	159	296
<b>Total</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>17</b>	<b>3</b>	<b>0</b>	<b>1</b>	<b>626</b>	<b>0</b>	<b>627</b>	<b>0</b>	<b>573</b>	<b>4</b>	<b>0</b>	<b>577</b>	<b>1207</b>
<b>Approach %</b>	<b>0.0</b>	<b>0.0</b>	<b>100.0</b>	<b>-</b>	<b>-</b>	<b>0.0</b>	<b>0.2</b>	<b>99.8</b>	<b>-</b>	<b>-</b>	<b>0.0</b>	<b>99.3</b>	<b>0.7</b>	<b>-</b>	<b>-</b>	<b>-</b>
<b>Total %</b>	<b>0.0</b>	<b>0.0</b>	<b>0.2</b>	<b>-</b>	<b>0.2</b>	<b>0.0</b>	<b>0.1</b>	<b>51.9</b>	<b>-</b>	<b>51.9</b>	<b>0.0</b>	<b>47.5</b>	<b>0.3</b>	<b>-</b>	<b>47.8</b>	<b>-</b>
<b>PHF</b>	<b>0.000</b>	<b>0.000</b>	<b>0.375</b>	<b>-</b>	<b>0.375</b>	<b>0.000</b>	<b>0.250</b>	<b>0.869</b>	<b>-</b>	<b>0.871</b>	<b>0.000</b>	<b>0.890</b>	<b>0.500</b>	<b>-</b>	<b>0.885</b>	<b>0.912</b>
<b>Lights</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>11</b>	<b>3</b>	<b>0</b>	<b>1</b>	<b>618</b>	<b>0</b>	<b>619</b>	<b>0</b>	<b>565</b>	<b>4</b>	<b>0</b>	<b>569</b>	<b>1191</b>
<b>% Lights</b>	<b>-</b>	<b>-</b>	<b>100.0</b>	<b>64.7</b>	<b>100.0</b>	<b>-</b>	<b>100.0</b>	<b>98.7</b>	<b>-</b>	<b>98.7</b>	<b>-</b>	<b>98.6</b>	<b>100.0</b>	<b>-</b>	<b>98.6</b>	<b>98.7</b>
<b>Other Vehicles</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>6</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>7</b>	<b>0</b>	<b>7</b>	<b>0</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>8</b>	<b>15</b>
<b>% Other Vehicles</b>	<b>-</b>	<b>-</b>	<b>0.0</b>	<b>35.3</b>	<b>0.0</b>	<b>-</b>	<b>0.0</b>	<b>1.1</b>	<b>-</b>	<b>1.1</b>	<b>-</b>	<b>1.4</b>	<b>0.0</b>	<b>-</b>	<b>1.4</b>	<b>1.2</b>
<b>Bicycles on Road</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>
<b>% Bicycles on Road</b>	<b>-</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>	<b>0.0</b>	<b>-</b>	<b>0.0</b>	<b>0.2</b>	<b>-</b>	<b>0.2</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>	<b>-</b>	<b>0.0</b>	<b>0.1</b>



Turning Movement Peak Hour Data Plot (7:30)

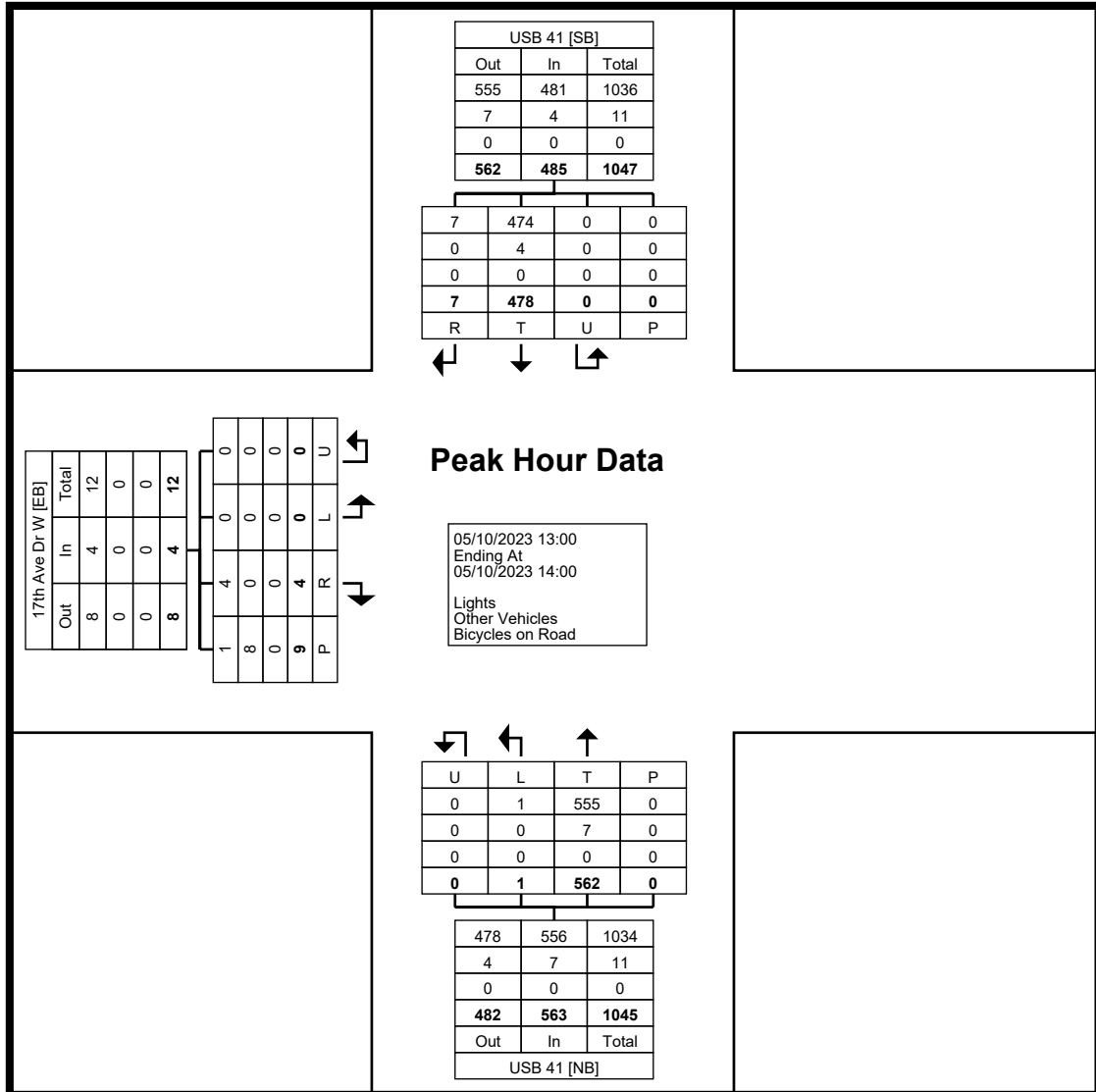
USB 41 & 17th Ave Dr W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 20\_USB 41 & 17th  
Ave Dr W  
Site Code: 20  
Start Date: 05/10/2023  
Page No: 5

### Turning Movement Peak Hour Data (13:00)

Start Time	17th Ave Dr W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
13:00	0	0	0	1	0	0	0	121	0	121	0	115	2	0	117	238
13:15	0	0	1	1	1	0	0	190	0	190	0	123	2	0	125	316
13:30	0	0	2	5	2	0	1	128	0	129	0	113	2	0	115	246
13:45	0	0	1	2	1	0	0	123	0	123	0	127	1	0	128	252
<b>Total</b>	<b>0</b>	<b>0</b>	<b>4</b>	<b>9</b>	<b>4</b>	<b>0</b>	<b>1</b>	<b>562</b>	<b>0</b>	<b>563</b>	<b>0</b>	<b>478</b>	<b>7</b>	<b>0</b>	<b>485</b>	<b>1052</b>
Approach %	0.0	0.0	100.0	-	-	0.0	0.2	99.8	-	-	0.0	98.6	1.4	-	-	-
Total %	0.0	0.0	0.4	-	0.4	0.0	0.1	53.4	-	53.5	0.0	45.4	0.7	-	46.1	-
PHF	0.000	0.000	0.500	-	0.500	0.000	0.250	0.739	-	0.741	0.000	0.941	0.875	-	0.947	0.832
Lights	0	0	4	1	4	0	1	555	0	556	0	474	7	0	481	1041
% Lights	-	-	100.0	11.1	100.0	-	100.0	98.8	-	98.8	-	99.2	100.0	-	99.2	99.0
Other Vehicles	0	0	0	8	0	0	0	7	0	7	0	4	0	0	4	11
% Other Vehicles	-	-	0.0	88.9	0.0	-	0.0	1.2	-	1.2	-	0.8	0.0	-	0.8	1.0
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	-	0.0	0.0	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.0



Turning Movement Peak Hour Data Plot (13:00)



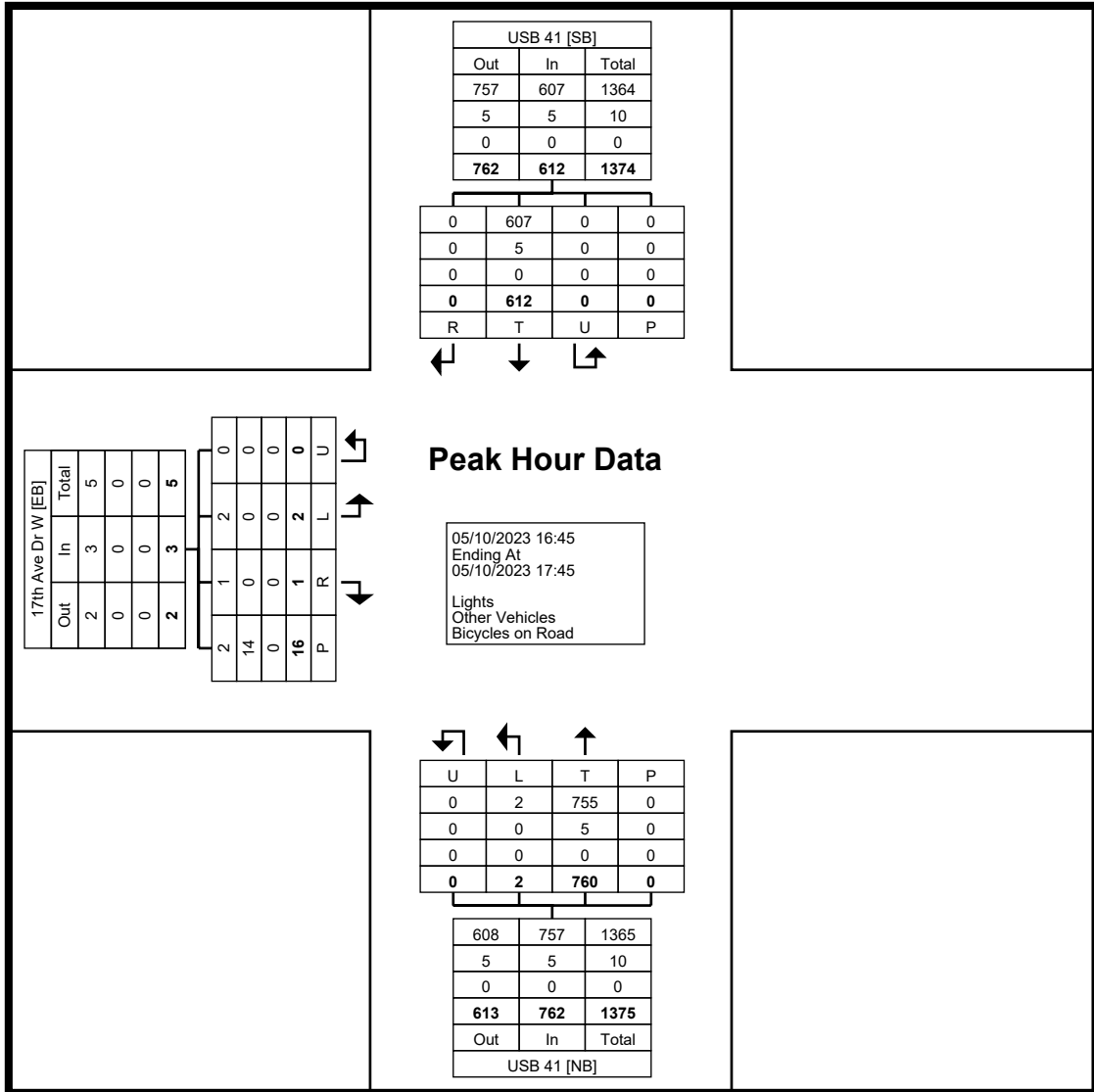
USB 41 & 17th Ave Dr W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 20\_USB 41 & 17th  
Ave Dr W  
Site Code: 20  
Start Date: 05/10/2023  
Page No: 7

### Turning Movement Peak Hour Data (16:45)

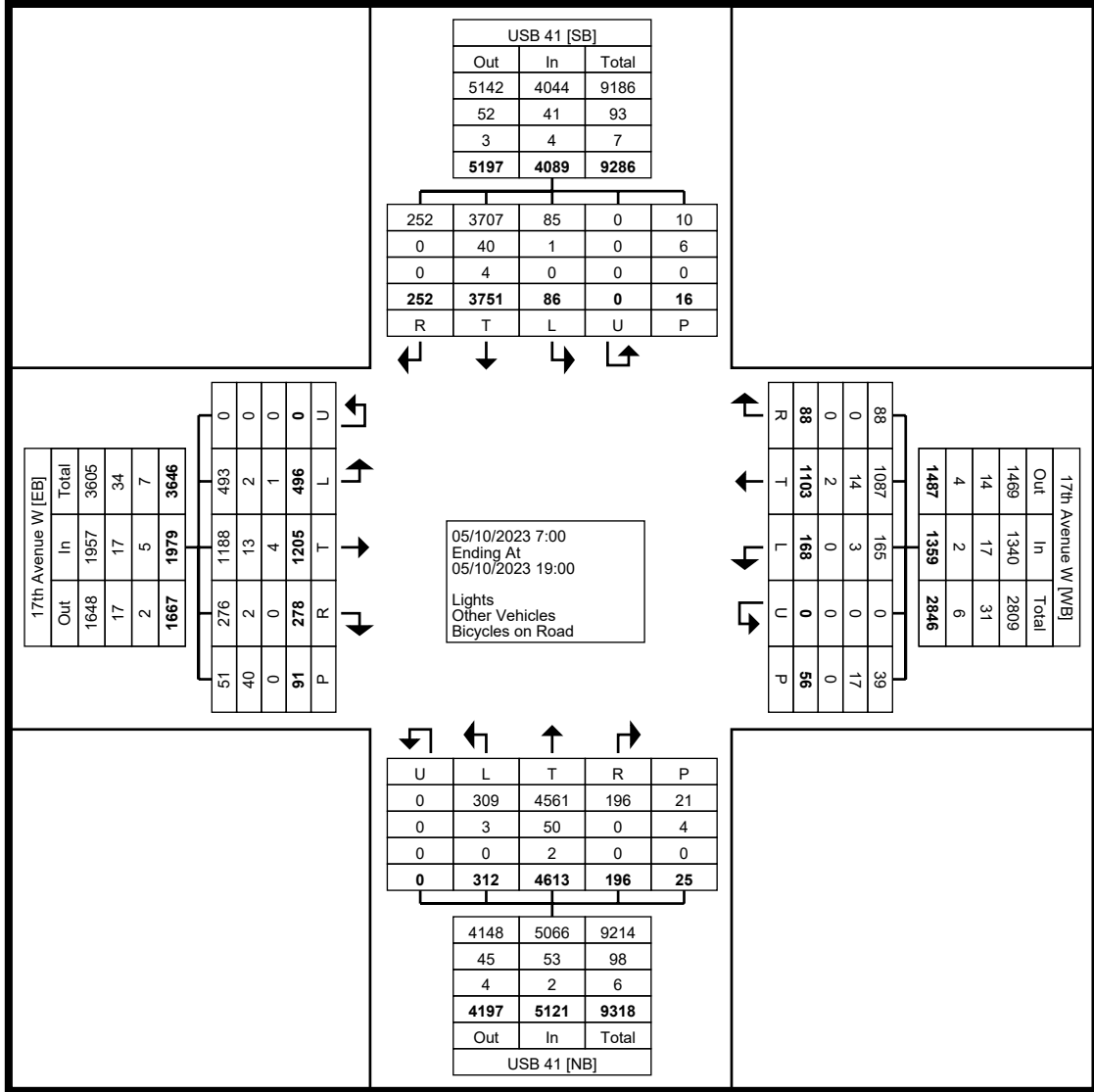
Start Time	17th Ave Dr W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
16:45	0	0	0	5	0	0	1	162	0	163	0	140	0	0	140	303
17:00	0	1	0	3	1	0	1	204	0	205	0	188	0	0	188	394
17:15	0	0	0	3	0	0	0	212	0	212	0	143	0	0	143	355
17:30	0	1	1	5	2	0	0	182	0	182	0	141	0	0	141	325
<b>Total</b>	<b>0</b>	<b>2</b>	<b>1</b>	<b>16</b>	<b>3</b>	<b>0</b>	<b>2</b>	<b>760</b>	<b>0</b>	<b>762</b>	<b>0</b>	<b>612</b>	<b>0</b>	<b>0</b>	<b>612</b>	<b>1377</b>
Approach %	0.0	66.7	33.3	-	-	0.0	0.3	99.7	-	-	0.0	100.0	0.0	-	-	-
Total %	0.0	0.1	0.1	-	0.2	0.0	0.1	55.2	-	55.3	0.0	44.4	0.0	-	44.4	-
PHF	0.000	0.500	0.250	-	0.375	0.000	0.500	0.896	-	0.899	0.000	0.814	0.000	-	0.814	0.874
Lights	0	2	1	2	3	0	2	755	0	757	0	607	0	0	607	1367
% Lights	-	100.0	100.0	12.5	100.0	-	100.0	99.3	-	99.3	-	99.2	-	-	99.2	99.3
Other Vehicles	0	0	0	14	0	0	0	5	0	5	0	5	0	0	5	10
% Other Vehicles	-	0.0	0.0	87.5	0.0	-	0.0	0.7	-	0.7	-	0.8	-	-	0.8	0.7
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	-	0.0	0.0	-	0.0	-	0.0	-	-	0.0	0.0



Turning Movement Peak Hour Data Plot (16:45)

### Turning Movement Data

Start Time	17th Avenue W Eastbound						17th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	8	27	9	4	44	0	4	16	1	2	21	0	9	105	4	0	118	0	2	108	7	1	117	300
7:15	0	14	41	13	0	68	0	4	47	5	1	56	0	14	129	3	2	146	0	2	113	9	0	124	394
7:30	0	7	54	11	3	72	0	3	45	3	3	51	0	9	125	5	0	139	0	1	116	19	0	136	398
7:45	0	16	69	10	4	95	0	4	52	4	5	60	0	17	149	5	0	171	0	3	113	24	1	140	466
Hourly Total	0	45	191	43	11	279	0	15	160	13	11	188	0	49	508	17	2	574	0	8	450	59	2	517	1558
8:00	0	20	71	6	4	97	0	3	60	5	2	68	0	10	142	11	2	163	0	7	155	16	0	178	506
8:15	0	14	40	10	3	64	0	5	54	3	2	62	0	13	140	5	4	158	0	4	146	7	1	157	441
8:30	0	8	39	14	5	61	0	6	31	1	3	38	0	7	115	3	0	125	0	1	126	6	0	133	357
8:45	0	11	28	10	3	49	0	6	22	3	1	31	0	9	124	10	2	143	0	3	131	4	5	138	361
Hourly Total	0	53	178	40	15	271	0	20	167	12	8	199	0	39	521	29	8	589	0	15	558	33	6	606	1665
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	13	29	4	1	46	0	6	18	0	6	24	0	9	122	6	3	137	0	4	121	7	0	132	339
12:15	0	12	25	7	0	44	0	4	20	2	3	26	0	7	137	9	0	153	0	1	118	4	0	123	346
12:30	0	19	30	7	1	56	0	2	28	2	0	32	0	4	129	5	1	138	0	1	104	8	0	113	339
12:45	0	8	33	7	3	48	0	3	19	2	1	24	0	2	110	4	0	116	0	1	100	6	1	107	295
Hourly Total	0	52	117	25	5	194	0	15	85	6	10	106	0	22	498	24	4	544	0	7	443	25	1	475	1319
13:00	0	15	28	6	1	49	0	5	23	2	2	30	0	9	133	6	0	148	0	3	107	5	0	115	342
13:15	0	7	25	9	1	41	0	8	24	0	0	32	0	7	163	9	1	179	0	2	108	8	1	118	370
13:30	0	12	17	5	4	34	0	7	24	4	1	35	0	7	124	3	0	134	0	3	105	7	0	115	318
13:45	0	11	35	4	0	50	0	2	30	0	4	32	0	8	109	4	0	121	0	1	117	7	0	125	328
Hourly Total	0	45	105	24	6	174	0	22	101	6	7	129	0	31	529	22	1	582	0	9	437	27	1	473	1358
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	16	39	11	1	66	0	8	34	5	2	47	0	12	144	5	1	161	0	4	127	5	0	136	410
15:15	0	21	48	10	2	79	0	8	37	1	0	46	0	12	151	6	1	169	0	4	113	10	0	127	421
15:30	0	18	41	11	1	70	0	8	42	3	0	53	0	7	167	8	1	182	0	3	127	8	1	138	443
15:45	0	28	50	6	6	84	0	6	40	4	0	50	0	11	174	8	0	193	0	3	118	10	0	131	458
Hourly Total	0	83	178	38	10	299	0	30	153	13	2	196	0	42	636	27	3	705	0	14	485	33	1	532	1732
16:00	0	23	40	17	7	80	0	4	48	1	2	53	0	15	158	8	0	181	0	3	120	10	0	133	447
16:15	0	22	36	7	1	65	0	5	31	4	3	40	0	10	186	7	0	203	0	3	96	3	0	102	410
16:30	0	25	53	9	3	87	0	6	45	3	3	54	0	16	163	4	0	183	0	2	124	9	0	135	459
16:45	0	27	44	4	5	75	0	6	33	7	0	46	0	14	145	3	0	162	0	4	130	6	1	140	423
Hourly Total	0	97	173	37	16	307	0	21	157	15	8	193	0	55	652	22	0	729	0	12	470	28	1	510	1739
17:00	0	31	51	10	1	92	0	5	46	4	2	55	0	9	173	10	1	192	0	1	177	7	2	185	524
17:15	0	19	44	14	7	77	0	6	39	4	0	49	0	12	193	3	1	208	0	6	123	7	1	136	470
17:30	0	12	44	13	5	69	0	5	51	1	2	57	0	11	191	11	4	213	0	6	121	6	0	133	472
17:45	0	16	30	6	6	52	0	9	48	2	0	59	0	13	163	8	0	184	0	2	110	7	0	119	414
Hourly Total	0	78	169	43	19	290	0	25	184	11	4	220	0	45	720	32	6	797	0	15	531	27	3	573	1880
18:00	0	11	30	8	4	49	0	5	36	2	3	43	0	11	147	6	0	164	0	2	108	5	0	115	371
18:15	0	11	18	7	0	36	0	3	26	7	1	36	0	9	132	12	1	153	0	0	96	4	0	100	325
18:30	0	17	25	8	3	50	0	4	25	3	0	32	0	5	142	3	0	150	0	1	86	6	0	93	325
18:45	0	4	21	5	2	30	0	8	9	0	2	17	0	4	128	2	0	134	0	3	87	5	1	95	276
Hourly Total	0	43	94	28	9	165	0	20	96	12	6	128	0	29	549	23	1	601	0	6	377	20	1	403	1297
Grand Total	0	496	1205	278	91	1979	0	168	1103	88	56	1359	0	312	4613	196	25	5121	0	86	3751	252	16	4089	12548
Approach %	0.0	25.1	60.9	14.0	-	-	0.0	12.4	81.2	6.5	-	-	0.0	6.1	90.1	3.8	-	-	0.0	2.1	91.7	6.2	-	-	-
Total %	0.0	4.0	9.6	2.2	-	15.8	0.0	1.3	8.8	0.7	-	10.8	0.0	2.5	36.8	1.6	-	40.8	0.0	0.7	29.9	2.0	-	32.6	-
Lights	0	493	1188	276	51	1957	0	165	1087	88	39	1340	0	309	4561	196	21	5066	0	85	3707	252	10	4044	12407
% Lights	-	99.4	98.6	99.3	56.0	98.9	-	98.2	98.5	100.0	69.6	98.6	-	99.0	98.9	100.0	84.0	98.9	-	98.8	98.8	100.0	62.5	98.9	98.9
Other Vehicles	0	2	13	2	40	17	0	3	14	0	17	17	0	3	50	0	4	53	0	1	40	0	6	41	128
% Other Vehicles	-	0.4	1.1	0.7	44.0	0.9	-	1.8	1.3	0.0	30.4	1.3	-	1.0	1.1	0.0	16.0	1.0	-	1.2	1.1	0.0	37.5	1.0	1.0
Bicycles on Road	0	1	4	0	0	5	0	0	2	0	0	2	0	0	2	0	0	2	0	0	4	0	0	4	13
% Bicycles on Road	-	0.2	0.3	0.0	0.0	0.3	-	0.0	0.2	0.0	0.0	0.1	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.1	0.0	0.0	0.1	0.1



Turning Movement Data Plot

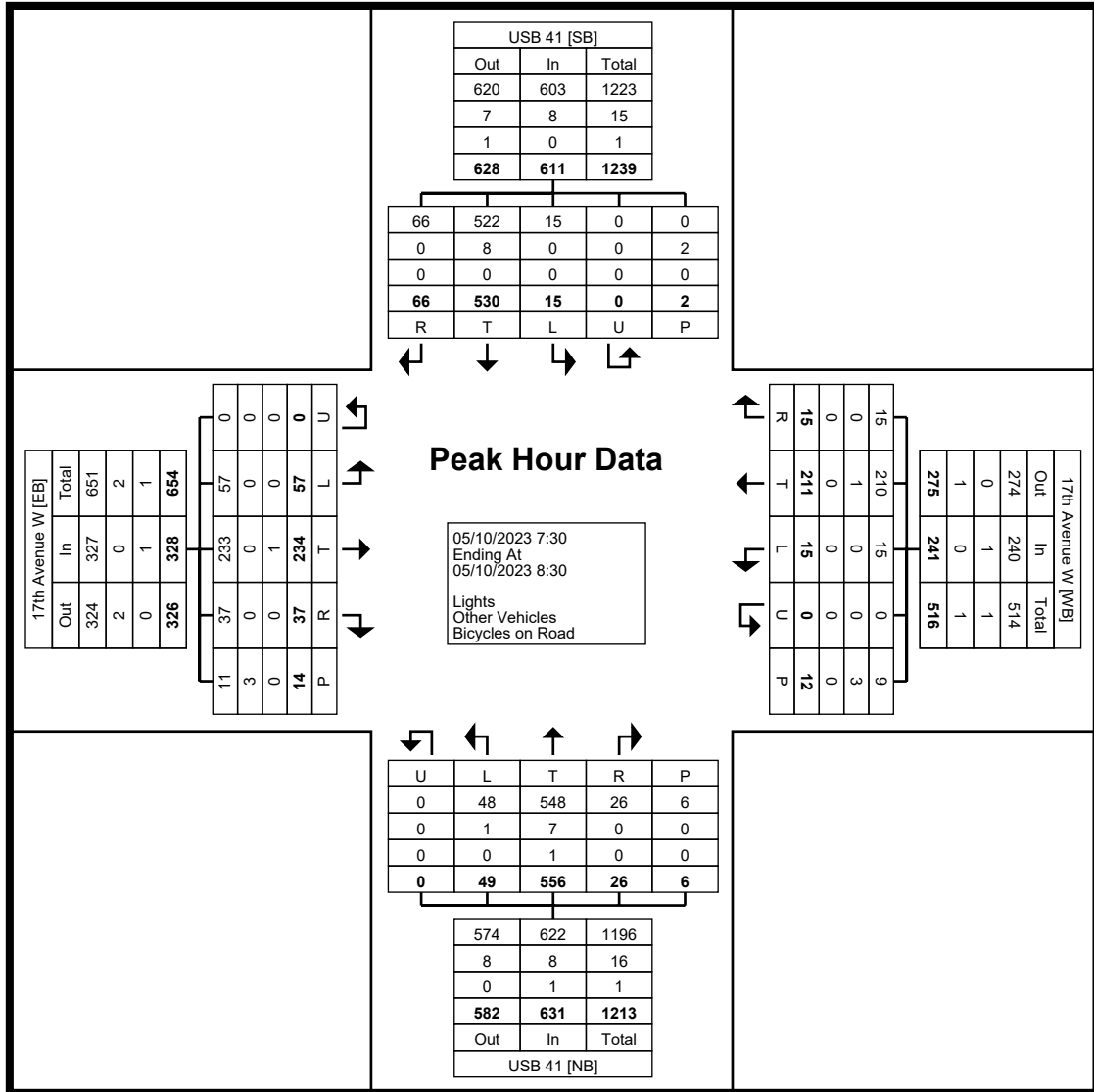
USB 41 at 17th Avenue W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 19\_USB 41 at  
17th Avenue W  
Site Code: 19  
Start Date: 05/10/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:30)

Start Time	17th Avenue W Eastbound						17th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:30	0	7	54	11	3	72	0	3	45	3	3	51	0	9	125	5	0	139	0	1	116	19	0	136	398
7:45	0	16	69	10	4	95	0	4	52	4	5	60	0	17	149	5	0	171	0	3	113	24	1	140	466
8:00	0	20	71	6	4	97	0	3	60	5	2	68	0	10	142	11	2	163	0	7	155	16	0	178	506
8:15	0	14	40	10	3	64	0	5	54	3	2	62	0	13	140	5	4	158	0	4	146	7	1	157	441
<b>Total</b>	0	57	234	37	14	328	0	15	211	15	12	241	0	49	556	26	6	631	0	15	530	66	2	611	1811
Approach %	0.0	17.4	71.3	11.3	-	-	0.0	6.2	87.6	6.2	-	-	0.0	7.8	88.1	4.1	-	-	0.0	2.5	86.7	10.8	-	-	-
Total %	0.0	3.1	12.9	2.0	-	18.1	0.0	0.8	11.7	0.8	-	13.3	0.0	2.7	30.7	1.4	-	34.8	0.0	0.8	29.3	3.6	-	33.7	-
PHF	0.000	0.713	0.824	0.841	-	0.845	0.000	0.750	0.879	0.750	-	0.886	0.000	0.721	0.933	0.591	-	0.923	0.000	0.536	0.855	0.688	-	0.858	0.895
Lights	0	57	233	37	11	327	0	15	210	15	9	240	0	48	548	26	6	622	0	15	522	66	0	603	1792
% Lights	-	100.0	99.6	100.0	78.6	99.7	-	100.0	99.5	100.0	75.0	99.6	-	98.0	98.6	100.0	100.0	98.6	-	100.0	98.5	100.0	0.0	98.7	99.0
Other Vehicles	0	0	0	0	3	0	0	0	1	0	3	1	0	1	7	0	0	8	0	0	8	0	2	8	17
% Other Vehicles	-	0.0	0.0	0.0	21.4	0.0	-	0.0	0.5	0.0	25.0	0.4	-	2.0	1.3	0.0	0.0	1.3	-	0.0	1.5	0.0	100.0	1.3	0.9
Bicycles on Road	0	0	1	0	0	1	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2
% Bicycles on Road	-	0.0	0.4	0.0	0.0	0.3	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.2	0.0	0.0	0.2	-	0.0	0.0	0.0	0.0	0.0	0.1



Turning Movement Peak Hour Data Plot (7:30)

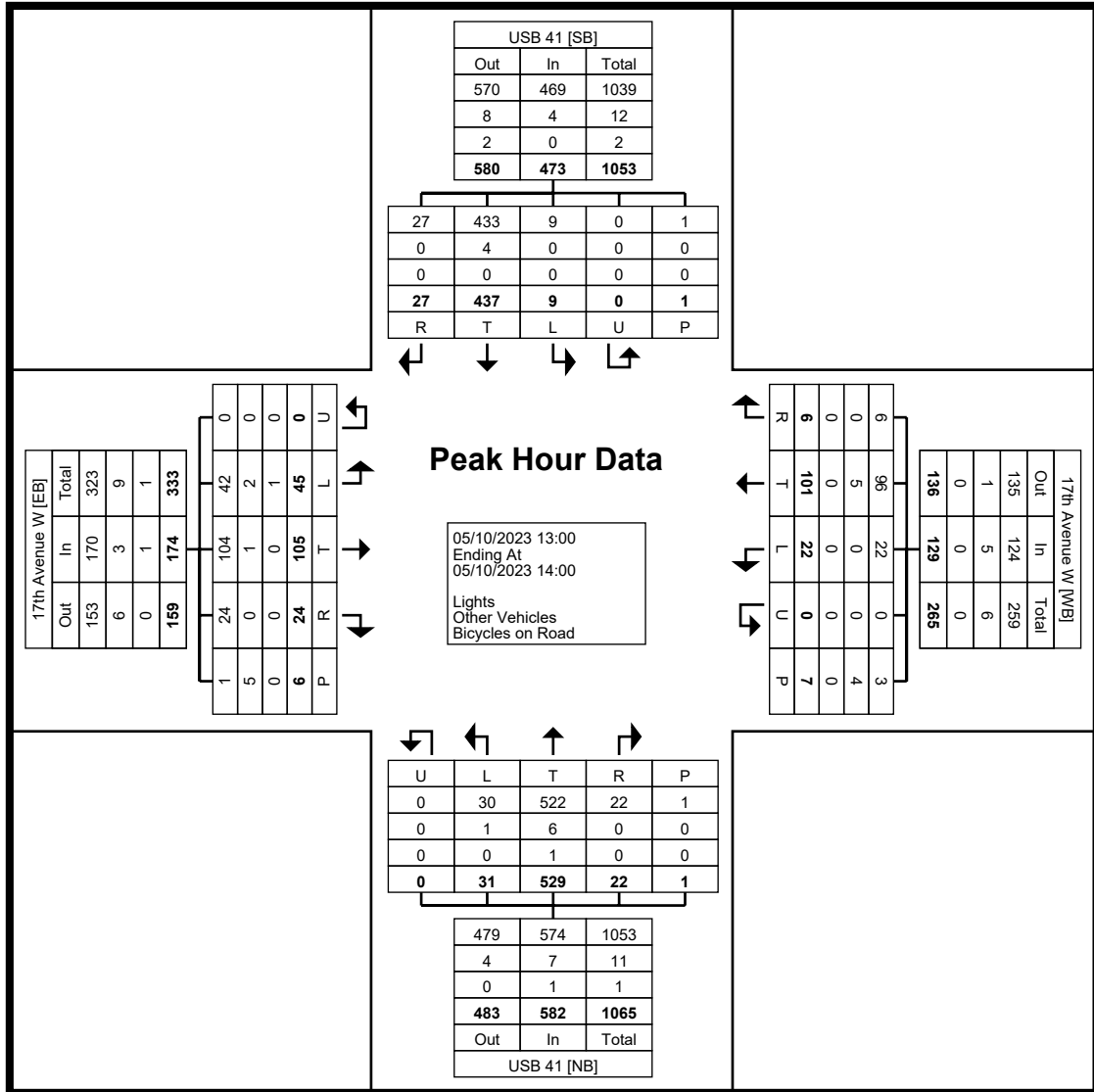
USB 41 at 17th Avenue W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 19\_USB 41 at  
17th Avenue W  
Site Code: 19  
Start Date: 05/10/2023  
Page No: 5

### Turning Movement Peak Hour Data (13:00)

Start Time	17th Avenue W Eastbound						17th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
13:00	0	15	28	6	1	49	0	5	23	2	2	30	0	9	133	6	0	148	0	3	107	5	0	115	342
13:15	0	7	25	9	1	41	0	8	24	0	0	32	0	7	163	9	1	179	0	2	108	8	1	118	370
13:30	0	12	17	5	4	34	0	7	24	4	1	35	0	7	124	3	0	134	0	3	105	7	0	115	318
13:45	0	11	35	4	0	50	0	2	30	0	4	32	0	8	109	4	0	121	0	1	117	7	0	125	328
<b>Total</b>	<b>0</b>	<b>45</b>	<b>105</b>	<b>24</b>	<b>6</b>	<b>174</b>	<b>0</b>	<b>22</b>	<b>101</b>	<b>6</b>	<b>7</b>	<b>129</b>	<b>0</b>	<b>31</b>	<b>529</b>	<b>22</b>	<b>1</b>	<b>582</b>	<b>0</b>	<b>9</b>	<b>437</b>	<b>27</b>	<b>1</b>	<b>473</b>	<b>1358</b>
Approach %	0.0	25.9	60.3	13.8	-	-	0.0	17.1	78.3	4.7	-	-	0.0	5.3	90.9	3.8	-	-	0.0	1.9	92.4	5.7	-	-	-
Total %	0.0	3.3	7.7	1.8	-	12.8	0.0	1.6	7.4	0.4	-	9.5	0.0	2.3	39.0	1.6	-	42.9	0.0	0.7	32.2	2.0	-	34.8	-
PHF	0.000	0.750	0.750	0.667	-	0.870	0.000	0.688	0.842	0.375	-	0.921	0.000	0.861	0.811	0.611	-	0.813	0.000	0.750	0.934	0.844	-	0.946	0.918
Lights	0	42	104	24	1	170	0	22	96	6	3	124	0	30	522	22	1	574	0	9	433	27	1	469	1337
% Lights	-	93.3	99.0	100.0	16.7	97.7	-	100.0	95.0	100.0	42.9	96.1	-	96.8	98.7	100.0	100.0	98.6	-	100.0	99.1	100.0	100.0	99.2	98.5
Other Vehicles	0	2	1	0	5	3	0	0	5	0	4	5	0	1	6	0	0	7	0	0	4	0	0	4	19
% Other Vehicles	-	4.4	1.0	0.0	83.3	1.7	-	0.0	5.0	0.0	57.1	3.9	-	3.2	1.1	0.0	0.0	1.2	-	0.0	0.9	0.0	0.0	0.8	1.4
Bicycles on Road	0	1	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2
% Bicycles on Road	-	2.2	0.0	0.0	0.0	0.6	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.2	0.0	0.0	0.2	-	0.0	0.0	0.0	0.0	0.0	0.1



Turning Movement Peak Hour Data Plot (13:00)



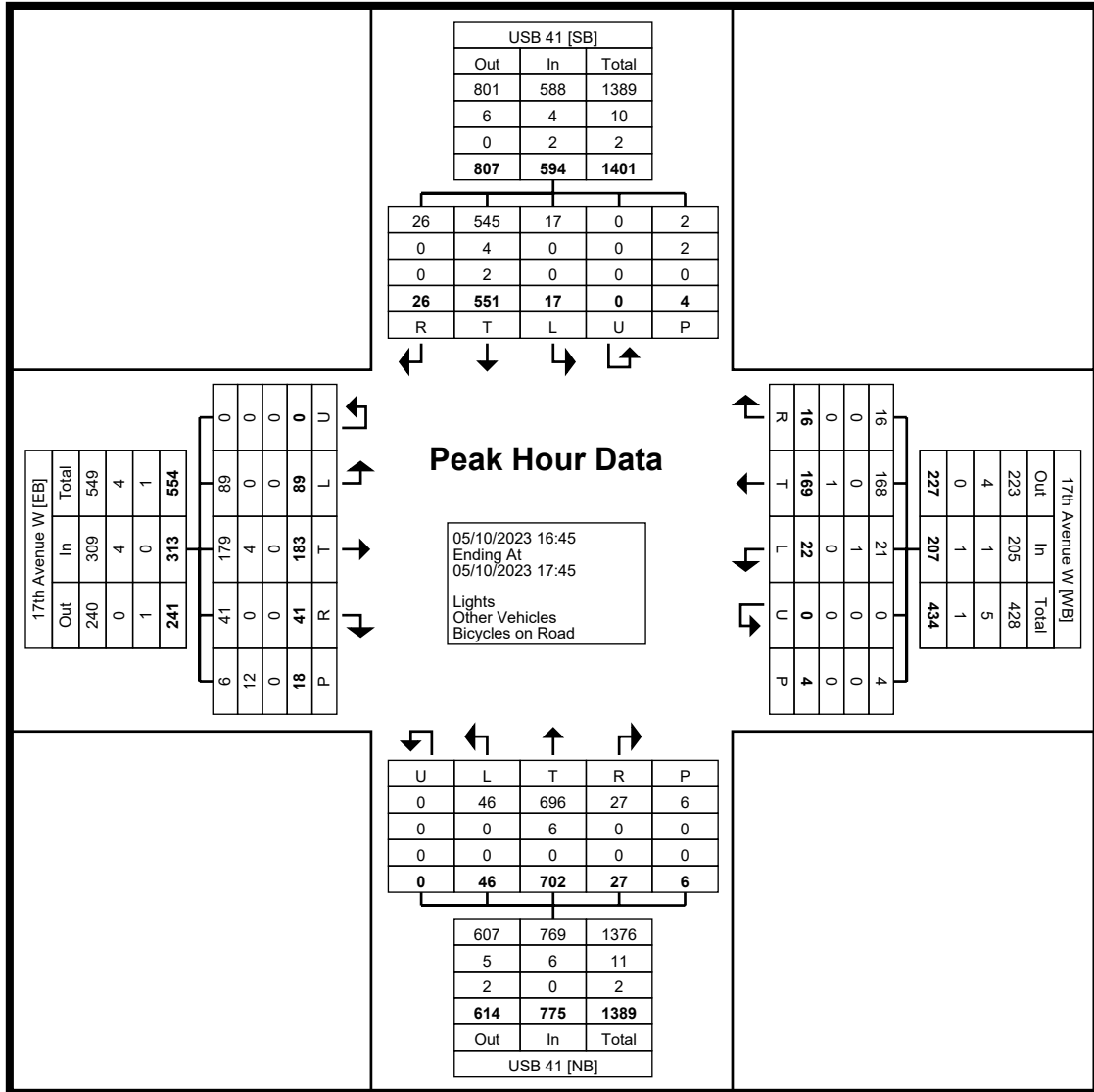
USB 41 at 17th Avenue W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 19\_USB 41 at  
17th Avenue W  
Site Code: 19  
Start Date: 05/10/2023  
Page No: 7

### Turning Movement Peak Hour Data (16:45)

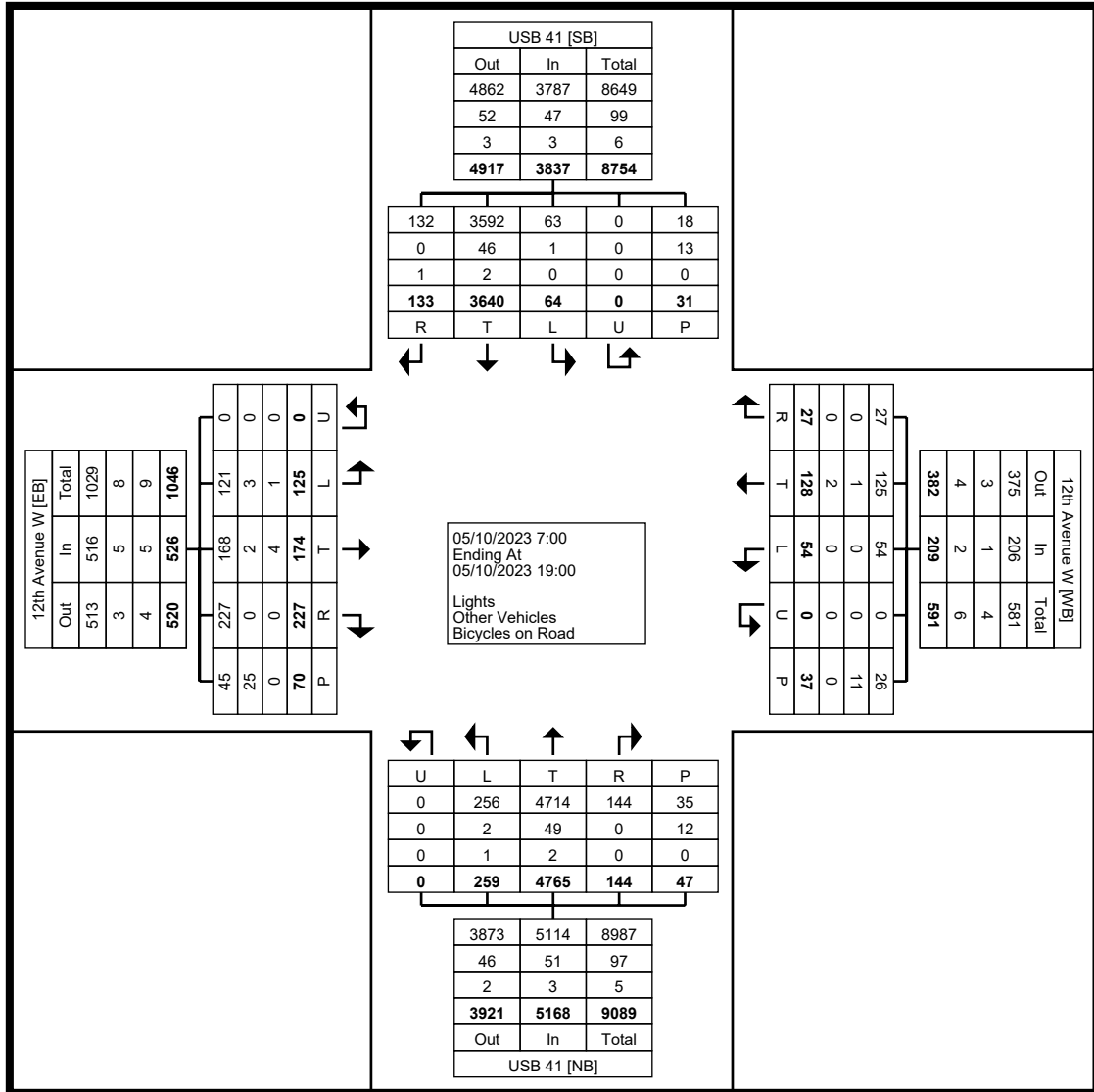
Start Time	17th Avenue W Eastbound						17th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:45	0	27	44	4	5	75	0	6	33	7	0	46	0	14	145	3	0	162	0	4	130	6	1	140	423
17:00	0	31	51	10	1	92	0	5	46	4	2	55	0	9	173	10	1	192	0	1	177	7	2	185	524
17:15	0	19	44	14	7	77	0	6	39	4	0	49	0	12	193	3	1	208	0	6	123	7	1	136	470
17:30	0	12	44	13	5	69	0	5	51	1	2	57	0	11	191	11	4	213	0	6	121	6	0	133	472
<b>Total</b>	0	89	183	41	18	313	0	22	169	16	4	207	0	46	702	27	6	775	0	17	551	26	4	594	1889
Approach %	0.0	28.4	58.5	13.1	-	-	0.0	10.6	81.6	7.7	-	-	0.0	5.9	90.6	3.5	-	-	0.0	2.9	92.8	4.4	-	-	-
Total %	0.0	4.7	9.7	2.2	-	16.6	0.0	1.2	8.9	0.8	-	11.0	0.0	2.4	37.2	1.4	-	41.0	0.0	0.9	29.2	1.4	-	31.4	-
PHF	0.000	0.718	0.897	0.732	-	0.851	0.000	0.917	0.828	0.571	-	0.908	0.000	0.821	0.909	0.614	-	0.910	0.000	0.708	0.778	0.929	-	0.803	0.901
Lights	0	89	179	41	6	309	0	21	168	16	4	205	0	46	696	27	6	769	0	17	545	26	2	588	1871
% Lights	-	100.0	97.8	100.0	33.3	98.7	-	95.5	99.4	100.0	100.0	99.0	-	100.0	99.1	100.0	100.0	99.2	-	100.0	98.9	100.0	50.0	99.0	99.0
Other Vehicles	0	0	4	0	12	4	0	1	0	0	0	1	0	0	6	0	0	6	0	0	4	0	2	4	15
% Other Vehicles	-	0.0	2.2	0.0	66.7	1.3	-	4.5	0.0	0.0	0.0	0.5	-	0.0	0.9	0.0	0.0	0.8	-	0.0	0.7	0.0	50.0	0.7	0.8
Bicycles on Road	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	2	0	0	2	3
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.6	0.0	0.0	0.5	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.4	0.0	0.0	0.3	0.2



Turning Movement Peak Hour Data Plot (16:45)

### Turning Movement Data

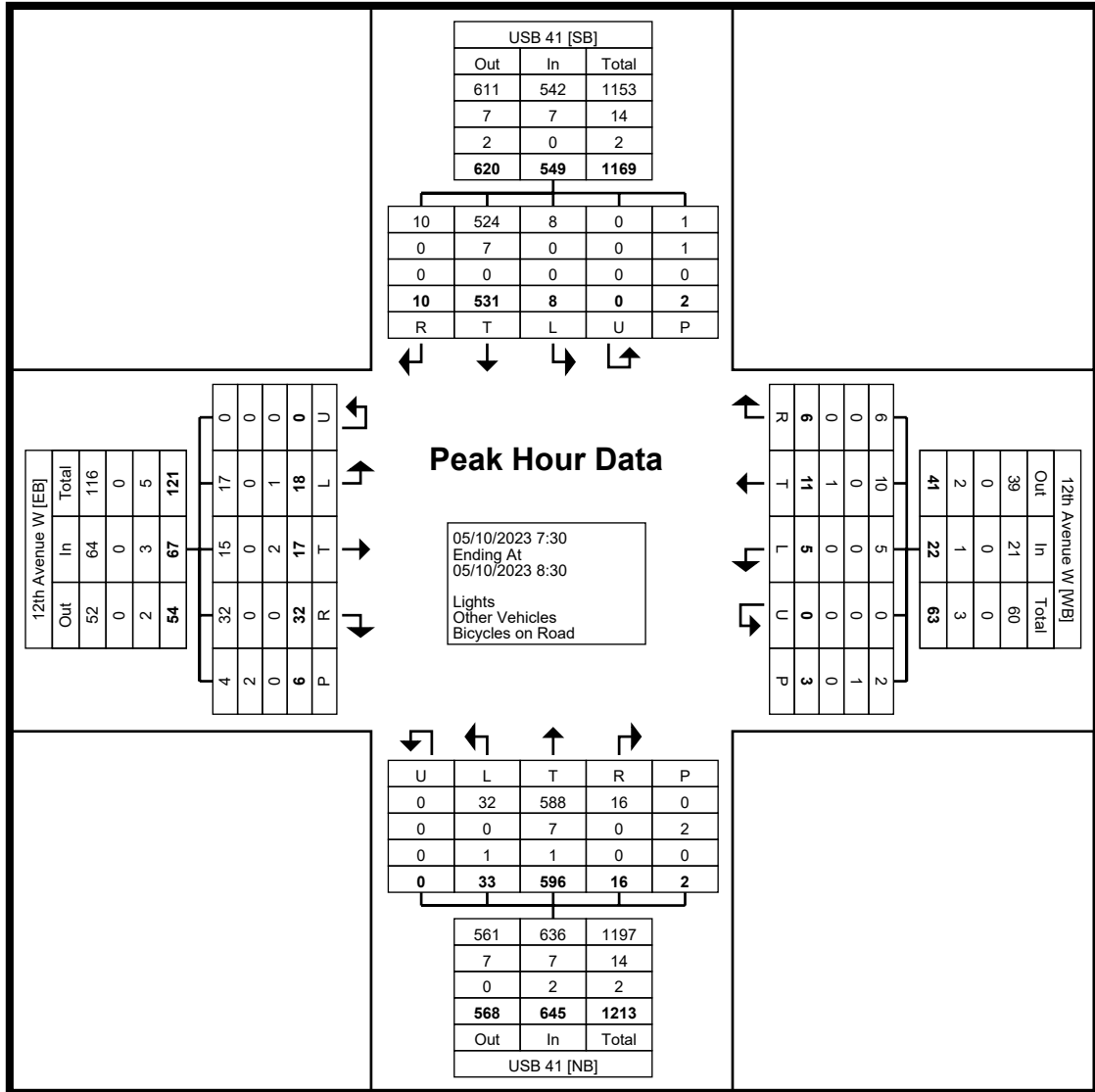
Start Time	12th Avenue W Eastbound						12th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	7	4	10	4	21	0	1	8	0	0	9	0	11	103	2	1	116	0	1	86	4	2	91	237
7:15	0	5	12	11	4	28	0	0	5	0	2	5	0	8	134	3	4	145	0	2	111	2	1	115	293
7:30	0	6	6	4	1	16	0	1	3	1	1	5	0	7	134	1	0	142	0	0	113	1	0	114	277
7:45	0	4	2	13	2	19	0	2	2	2	2	6	0	8	159	7	1	174	0	2	131	4	0	137	336
Hourly Total	0	22	24	38	11	84	0	4	18	3	5	25	0	34	530	13	6	577	0	5	441	11	3	457	1143
8:00	0	5	5	8	3	18	0	2	0	1	0	3	0	10	152	5	0	167	0	1	146	1	2	148	336
8:15	0	3	4	7	0	14	0	0	6	2	0	8	0	8	151	3	1	162	0	5	141	4	0	150	334
8:30	0	5	6	10	1	21	0	0	2	6	0	8	0	5	104	5	3	114	0	4	121	3	0	128	271
8:45	0	4	7	7	2	18	0	1	1	1	1	3	0	5	139	0	0	144	0	0	126	1	1	127	292
Hourly Total	0	17	22	32	6	71	0	3	9	10	1	22	0	28	546	13	4	587	0	10	534	9	3	553	1233
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	1	4	2	1	7	0	6	2	0	2	8	0	3	135	4	0	142	0	2	117	3	2	122	279
12:15	0	2	1	5	2	8	0	0	3	1	3	4	0	8	124	5	1	137	0	5	112	4	1	121	270
12:30	0	2	5	4	2	11	0	2	1	2	0	5	0	9	130	7	1	146	0	3	97	5	1	105	267
12:45	0	4	8	5	2	17	0	0	4	0	2	4	0	7	121	1	1	129	0	1	99	5	0	105	255
Hourly Total	0	9	18	16	7	43	0	8	10	3	7	21	0	27	510	17	3	554	0	11	425	17	4	453	1071
13:00	0	4	3	3	2	10	0	2	3	0	1	5	0	11	134	6	1	151	0	2	107	10	0	119	285
13:15	0	6	6	5	0	17	0	1	1	0	0	2	0	4	163	7	0	174	0	2	114	4	0	120	313
13:30	0	4	4	2	2	10	0	1	2	1	1	4	0	6	142	1	1	149	0	5	113	1	0	119	282
13:45	0	2	1	5	3	8	0	0	1	0	1	1	0	10	102	7	0	119	0	1	114	2	3	117	245
Hourly Total	0	16	14	15	7	45	0	4	7	1	3	12	0	31	541	21	2	593	0	10	448	17	3	475	1125
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	3	8	8	2	19	0	4	5	0	3	9	0	7	147	5	0	159	0	2	118	3	2	123	310
15:15	0	8	6	9	0	23	0	0	6	1	0	7	0	7	150	4	0	161	0	1	122	6	0	129	320
15:30	0	4	7	6	3	17	0	5	14	1	0	20	0	8	169	4	1	181	0	1	130	4	0	135	353
15:45	0	7	5	6	6	18	0	2	4	1	0	7	0	8	193	4	1	205	0	0	120	6	1	126	356
Hourly Total	0	22	26	29	11	77	0	11	29	3	3	43	0	30	659	17	2	706	0	4	490	19	3	513	1339
16:00	0	4	6	4	4	14	0	4	9	2	1	15	0	8	174	3	1	185	0	0	118	2	1	120	334
16:15	0	2	5	3	3	10	0	1	5	1	4	7	0	8	199	9	5	216	0	2	96	8	5	106	339
16:30	0	4	6	8	0	18	0	1	2	1	2	4	0	10	172	4	0	186	0	3	116	8	1	127	335
16:45	0	5	4	12	3	21	0	0	4	0	1	4	0	15	155	8	3	178	0	3	135	5	2	143	346
Hourly Total	0	15	21	27	10	63	0	6	20	4	8	30	0	41	700	24	9	765	0	8	465	23	9	496	1354
17:00	0	6	13	7	1	26	0	4	7	0	0	11	0	10	202	10	5	222	0	1	154	7	0	162	421
17:15	0	5	4	10	2	19	0	7	5	0	0	12	0	8	197	7	2	212	0	4	115	5	3	124	367
17:30	0	4	9	9	4	22	0	0	6	0	4	6	0	6	177	6	3	189	0	4	120	4	3	128	345
17:45	0	2	5	10	4	17	0	5	2	2	1	9	0	8	157	5	3	170	0	1	98	5	0	104	300
Hourly Total	0	17	31	36	11	84	0	16	20	2	5	38	0	32	733	28	13	793	0	10	487	21	6	518	1433
18:00	0	1	7	8	3	16	0	0	4	1	2	5	0	10	140	1	2	151	0	3	94	8	0	105	277
18:15	0	1	4	9	1	14	0	1	3	0	0	4	0	10	149	3	3	162	0	0	96	2	0	98	278
18:30	0	1	2	7	1	10	0	1	3	0	1	4	0	10	140	2	2	152	0	0	80	5	0	85	251
18:45	0	4	5	10	2	19	0	0	5	0	2	5	0	6	117	5	1	128	0	3	80	1	0	84	236
Hourly Total	0	7	18	34	7	59	0	2	15	1	5	18	0	36	546	11	8	593	0	6	350	16	0	372	1042
Grand Total	0	125	174	227	70	526	0	54	128	27	37	209	0	259	4765	144	47	5168	0	64	3640	133	31	3837	9740
Approach %	0.0	23.8	33.1	43.2	-	-	0.0	25.8	61.2	12.9	-	-	0.0	5.0	92.2	2.8	-	-	0.0	1.7	94.9	3.5	-	-	-
Total %	0.0	1.3	1.8	2.3	-	5.4	0.0	0.6	1.3	0.3	-	2.1	0.0	2.7	48.9	1.5	-	53.1	0.0	0.7	37.4	1.4	-	39.4	-
Lights	0	121	168	227	45	516	0	54	125	27	26	206	0	256	4714	144	35	5114	0	63	3592	132	18	3787	9623
% Lights	-	96.8	96.6	100.0	64.3	98.1	-	100.0	97.7	100.0	70.3	98.6	-	98.8	98.9	100.0	74.5	99.0	-	98.4	98.7	99.2	58.1	98.7	98.8
Other Vehicles	0	3	2	0	25	5	0	0	1	0	11	1	0	2	49	0	12	51	0	1	46	0	13	47	104
% Other Vehicles	-	2.4	1.1	0.0	35.7	1.0	-	0.0	0.8	0.0	29.7	0.5	-	0.8	1.0	0.0	25.5	1.0	-	1.6	1.3	0.0	41.9	1.2	1.1
Bicycles on Road	0	1	4	0	0	5	0	0	2	0	0	2	0	1	2	0	0	3	0	0	2	1	0	3	13
% Bicycles on Road	-	0.8	2.3	0.0	0.0	1.0	-	0.0	1.6	0.0	0.0	1.0	-	0.4	0.0	0.0	0.0	0.1	-	0.0	0.1	0.8	0.0	0.1	0.1



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:30)

Start Time	12th Avenue W Eastbound						12th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:30	0	6	6	4	1	16	0	1	3	1	1	5	0	7	134	1	0	142	0	0	113	1	0	114	277
7:45	0	4	2	13	2	19	0	2	2	2	2	6	0	8	159	7	1	174	0	2	131	4	0	137	336
8:00	0	5	5	8	3	18	0	2	0	1	0	3	0	10	152	5	0	167	0	1	146	1	2	148	336
8:15	0	3	4	7	0	14	0	0	6	2	0	8	0	8	151	3	1	162	0	5	141	4	0	150	334
<b>Total</b>	0	18	17	32	6	67	0	5	11	6	3	22	0	33	596	16	2	645	0	8	531	10	2	549	1283
Approach %	0.0	26.9	25.4	47.8	-	-	0.0	22.7	50.0	27.3	-	-	0.0	5.1	92.4	2.5	-	-	0.0	1.5	96.7	1.8	-	-	-
Total %	0.0	1.4	1.3	2.5	-	5.2	0.0	0.4	0.9	0.5	-	1.7	0.0	2.6	46.5	1.2	-	50.3	0.0	0.6	41.4	0.8	-	42.8	-
PHF	0.000	0.750	0.708	0.615	-	0.882	0.000	0.625	0.458	0.750	-	0.688	0.000	0.825	0.937	0.571	-	0.927	0.000	0.400	0.909	0.625	-	0.915	0.955
Lights	0	17	15	32	4	64	0	5	10	6	2	21	0	32	588	16	0	636	0	8	524	10	1	542	1263
% Lights	-	94.4	88.2	100.0	66.7	95.5	-	100.0	90.9	100.0	66.7	95.5	-	97.0	98.7	100.0	0.0	98.6	-	100.0	98.7	100.0	50.0	98.7	98.4
Other Vehicles	0	0	0	0	2	0	0	0	0	0	1	0	0	0	7	0	2	7	0	0	7	0	1	7	14
% Other Vehicles	-	0.0	0.0	0.0	33.3	0.0	-	0.0	0.0	0.0	33.3	0.0	-	0.0	1.2	0.0	100.0	1.1	-	0.0	1.3	0.0	50.0	1.3	1.1
Bicycles on Road	0	1	2	0	0	3	0	0	1	0	0	1	0	1	1	0	0	2	0	0	0	0	0	0	6
% Bicycles on Road	-	5.6	11.8	0.0	0.0	4.5	-	0.0	9.1	0.0	0.0	4.5	-	3.0	0.2	0.0	0.0	0.3	-	0.0	0.0	0.0	0.0	0.0	0.5



Turning Movement Peak Hour Data Plot (7:30)

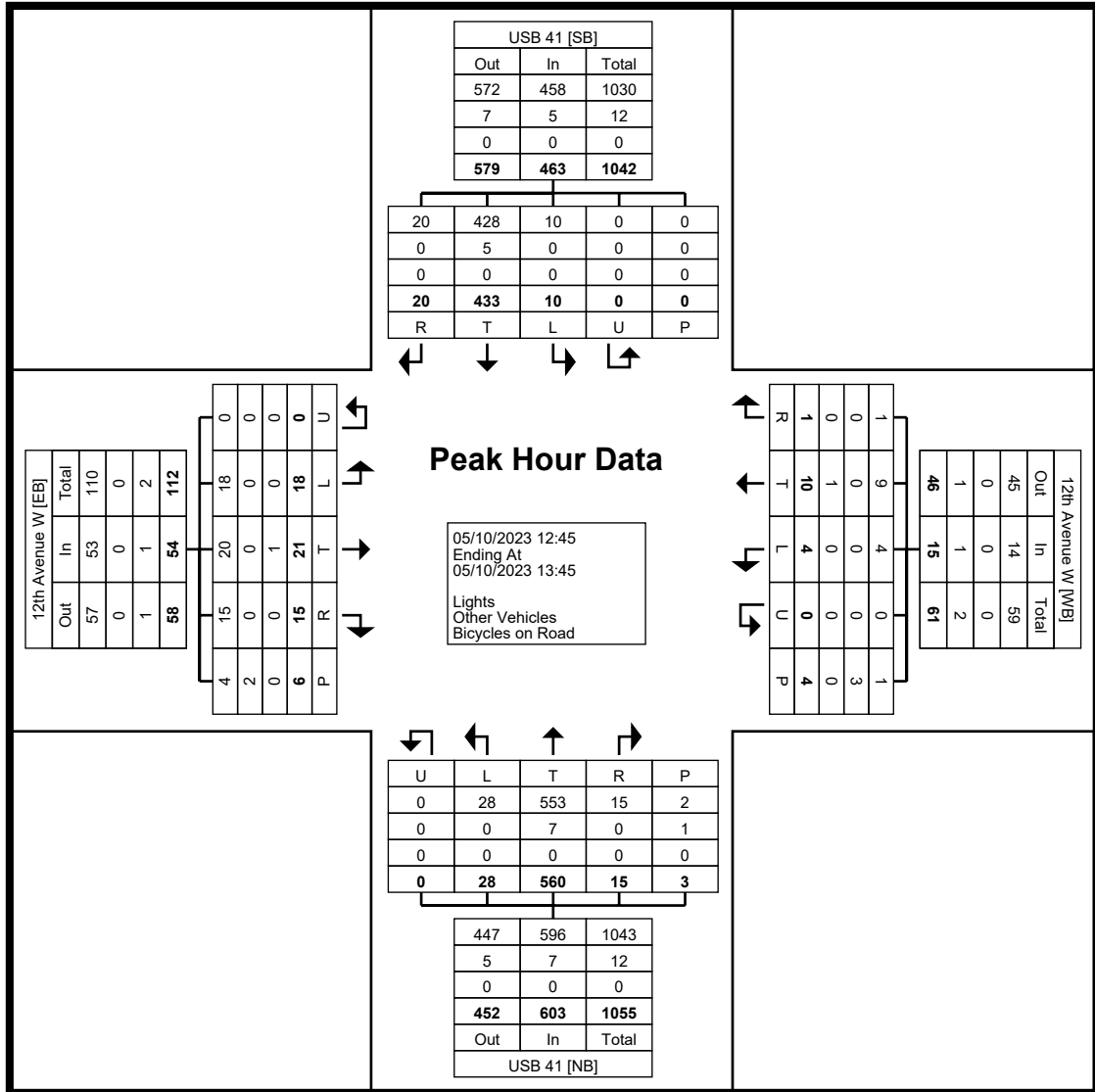
USB 41 at 12th Avenue W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 18\_USB 41 at  
12th Avenue W  
Site Code: 18  
Start Date: 05/10/2023  
Page No: 5

### Turning Movement Peak Hour Data (12:45)

Start Time	12th Avenue W Eastbound						12th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:45	0	4	8	5	2	17	0	0	4	0	2	4	0	7	121	1	1	129	0	1	99	5	0	105	255
13:00	0	4	3	3	2	10	0	2	3	0	1	5	0	11	134	6	1	151	0	2	107	10	0	119	285
13:15	0	6	6	5	0	17	0	1	1	0	0	2	0	4	163	7	0	174	0	2	114	4	0	120	313
13:30	0	4	4	2	2	10	0	1	2	1	1	4	0	6	142	1	1	149	0	5	113	1	0	119	282
<b>Total</b>	0	18	21	15	6	54	0	4	10	1	4	15	0	28	560	15	3	603	0	10	433	20	0	463	1135
Approach %	0.0	33.3	38.9	27.8	-	-	0.0	26.7	66.7	6.7	-	-	0.0	4.6	92.9	2.5	-	-	0.0	2.2	93.5	4.3	-	-	-
Total %	0.0	1.6	1.9	1.3	-	4.8	0.0	0.4	0.9	0.1	-	1.3	0.0	2.5	49.3	1.3	-	53.1	0.0	0.9	38.1	1.8	-	40.8	-
PHF	0.000	0.750	0.656	0.750	-	0.794	0.000	0.500	0.625	0.250	-	0.750	0.000	0.636	0.859	0.536	-	0.866	0.000	0.500	0.950	0.500	-	0.965	0.907
Lights	0	18	20	15	4	53	0	4	9	1	1	14	0	28	553	15	2	596	0	10	428	20	0	458	1121
% Lights	-	100.0	95.2	100.0	66.7	98.1	-	100.0	90.0	100.0	25.0	93.3	-	100.0	98.8	100.0	66.7	98.8	-	100.0	98.8	100.0	-	98.9	98.8
Other Vehicles	0	0	0	0	2	0	0	0	0	0	3	0	0	0	7	0	1	7	0	0	5	0	0	5	12
% Other Vehicles	-	0.0	0.0	0.0	33.3	0.0	-	0.0	0.0	0.0	75.0	0.0	-	0.0	1.3	0.0	33.3	1.2	-	0.0	1.2	0.0	-	1.1	1.1
Bicycles on Road	0	0	1	0	0	1	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2
% Bicycles on Road	-	0.0	4.8	0.0	0.0	1.9	-	0.0	10.0	0.0	0.0	6.7	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	-	0.0	0.2



Turning Movement Peak Hour Data Plot (12:45)



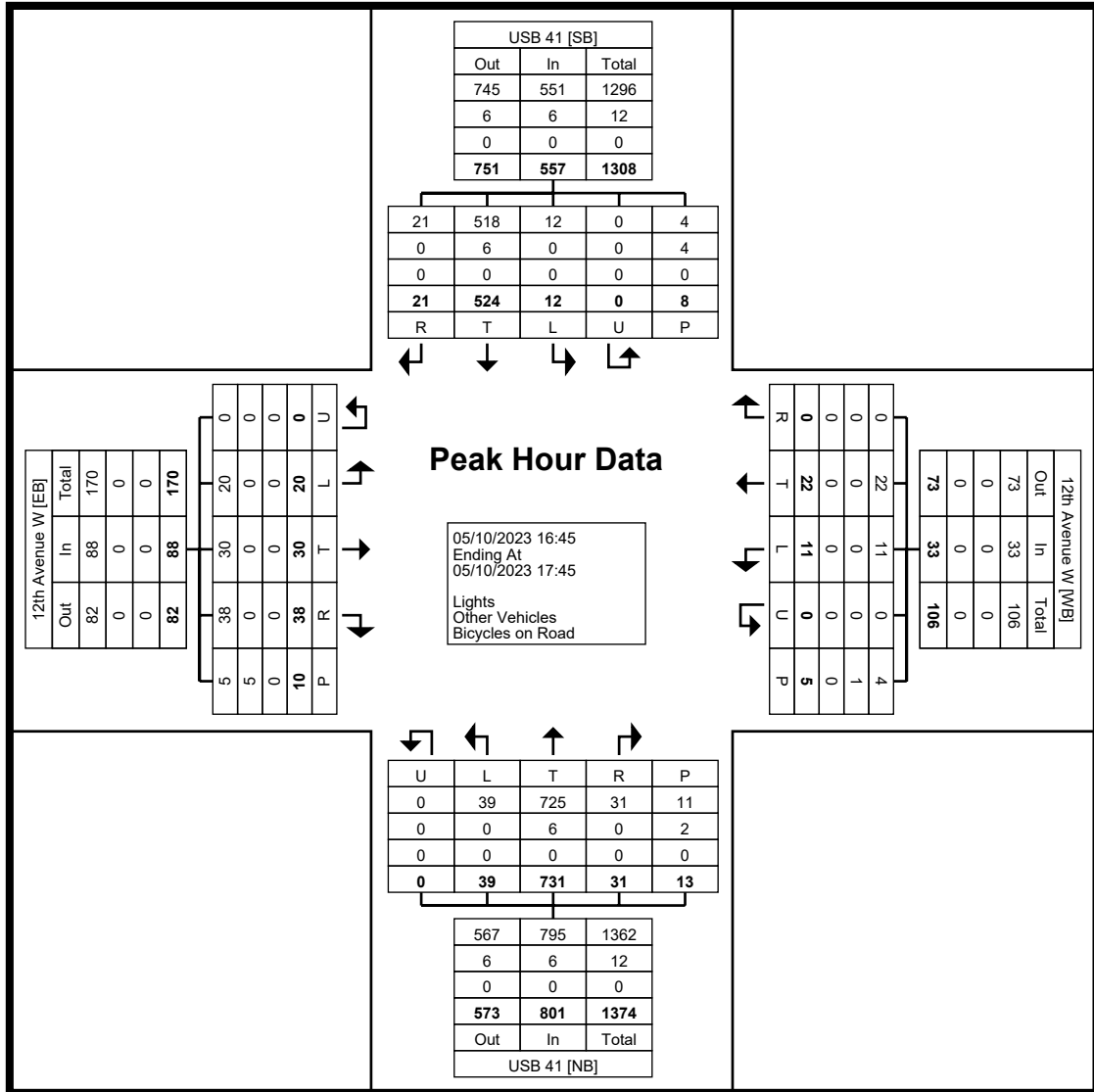
USB 41 at 12th Avenue W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 18\_USB 41 at  
12th Avenue W  
Site Code: 18  
Start Date: 05/10/2023  
Page No: 7

### Turning Movement Peak Hour Data (16:45)

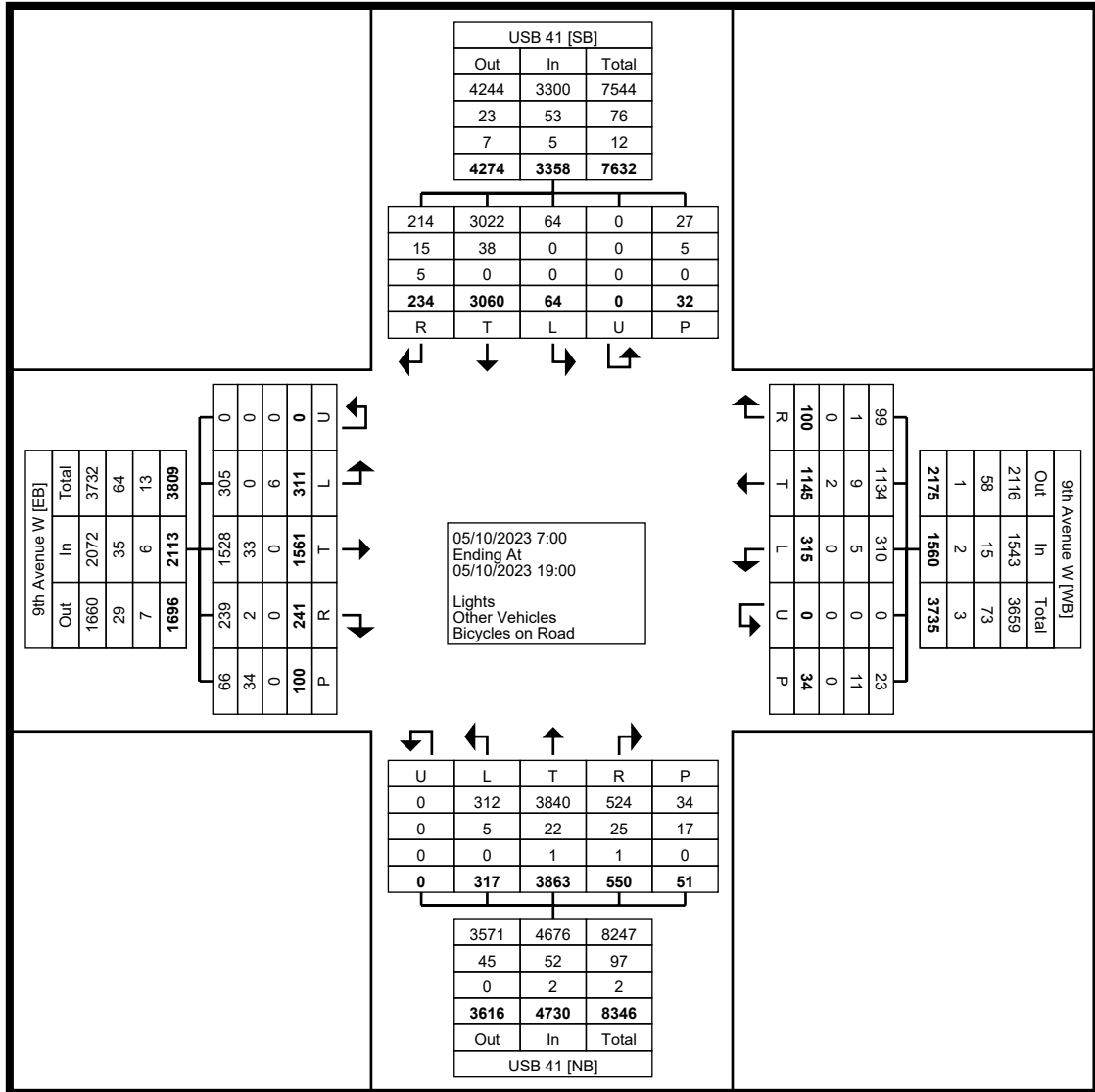
Start Time	12th Avenue W Eastbound						12th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:45	0	5	4	12	3	21	0	0	4	0	1	4	0	15	155	8	3	178	0	3	135	5	2	143	346
17:00	0	6	13	7	1	26	0	4	7	0	0	11	0	10	202	10	5	222	0	1	154	7	0	162	421
17:15	0	5	4	10	2	19	0	7	5	0	0	12	0	8	197	7	2	212	0	4	115	5	3	124	367
17:30	0	4	9	9	4	22	0	0	6	0	4	6	0	6	177	6	3	189	0	4	120	4	3	128	345
<b>Total</b>	0	20	30	38	10	88	0	11	22	0	5	33	0	39	731	31	13	801	0	12	524	21	8	557	1479
Approach %	0.0	22.7	34.1	43.2	-	-	0.0	33.3	66.7	0.0	-	-	0.0	4.9	91.3	3.9	-	-	0.0	2.2	94.1	3.8	-	-	-
Total %	0.0	1.4	2.0	2.6	-	5.9	0.0	0.7	1.5	0.0	-	2.2	0.0	2.6	49.4	2.1	-	54.2	0.0	0.8	35.4	1.4	-	37.7	-
PHF	0.000	0.833	0.577	0.792	-	0.846	0.000	0.393	0.786	0.000	-	0.688	0.000	0.650	0.905	0.775	-	0.902	0.000	0.750	0.851	0.750	-	0.860	0.878
Lights	0	20	30	38	5	88	0	11	22	0	4	33	0	39	725	31	11	795	0	12	518	21	4	551	1467
% Lights	-	100.0	100.0	100.0	50.0	100.0	-	100.0	100.0	-	80.0	100.0	-	100.0	99.2	100.0	84.6	99.3	-	100.0	98.9	100.0	50.0	98.9	99.2
Other Vehicles	0	0	0	0	5	0	0	0	0	0	1	0	0	0	6	0	2	6	0	0	6	0	4	6	12
% Other Vehicles	-	0.0	0.0	0.0	50.0	0.0	-	0.0	0.0	-	20.0	0.0	-	0.0	0.8	0.0	15.4	0.7	-	0.0	1.1	0.0	50.0	1.1	0.8
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	-	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (16:45)

### Turning Movement Data

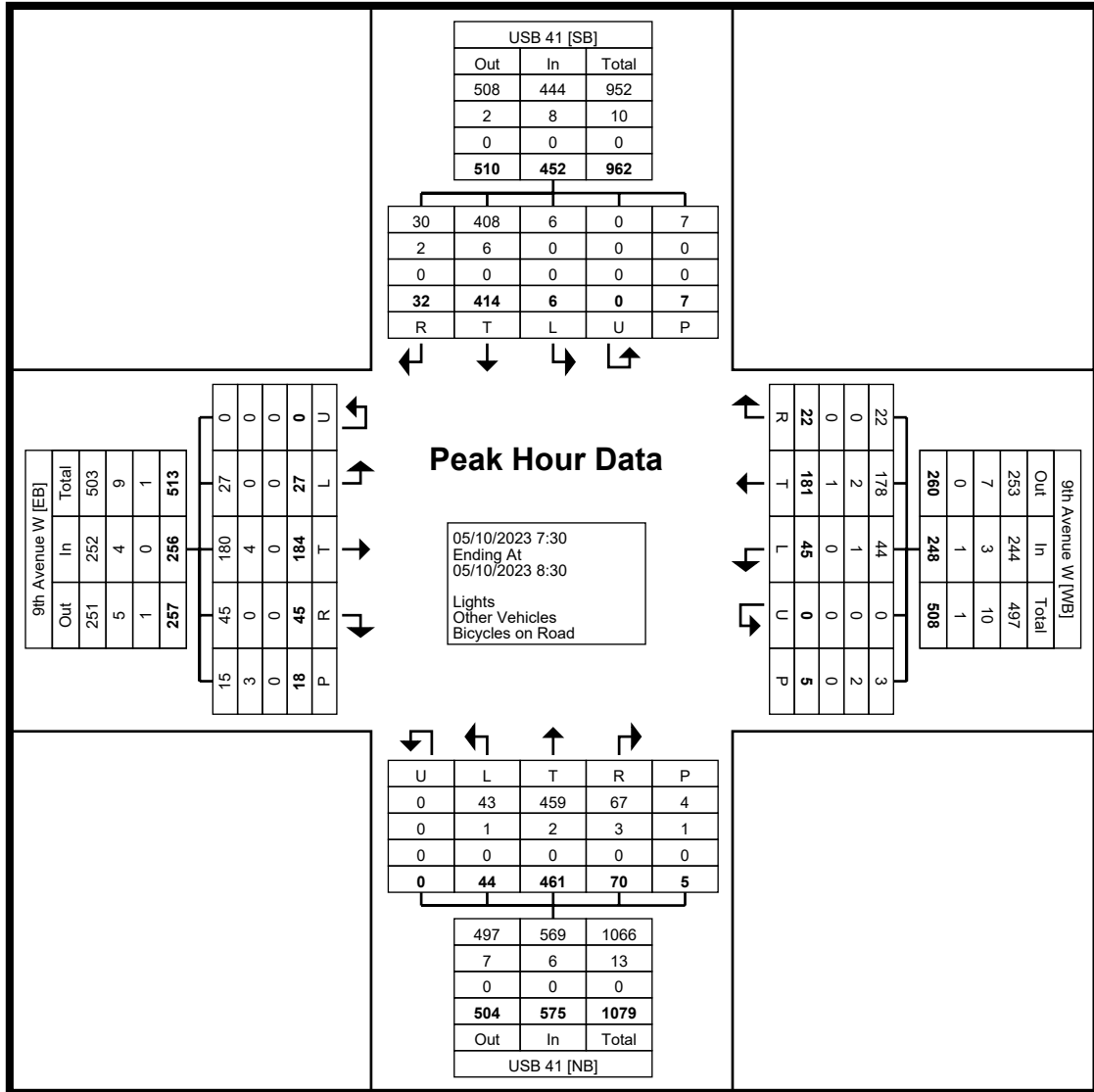
Start Time	9th Avenue W Eastbound						9th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	6	40	7	4	53	0	4	41	0	0	45	0	16	91	6	2	113	0	2	77	9	4	88	299
7:15	0	5	40	8	4	53	0	6	52	3	1	61	0	12	114	9	2	135	0	1	96	12	1	109	358
7:30	0	9	45	8	7	62	0	8	48	7	2	63	0	9	115	11	2	135	0	2	90	10	1	102	362
7:45	0	5	42	12	2	59	0	12	50	4	1	66	0	10	129	19	1	158	0	2	104	7	0	113	396
Hourly Total	0	25	167	35	17	227	0	30	191	14	4	235	0	47	449	45	7	541	0	7	367	38	6	412	1415
8:00	0	6	43	14	1	63	0	9	36	6	0	51	0	14	99	18	1	131	0	2	109	10	1	121	366
8:15	0	7	54	11	8	72	0	16	47	5	2	68	0	11	118	22	1	151	0	0	111	5	5	116	407
8:30	0	6	44	4	1	54	0	9	37	4	0	50	0	9	90	14	2	113	0	2	113	5	3	120	337
8:45	0	5	43	16	2	64	0	8	33	5	1	46	0	10	107	16	1	133	0	2	99	6	0	107	350
Hourly Total	0	24	184	45	12	253	0	42	153	20	3	215	0	44	414	70	5	528	0	6	432	26	9	464	1460
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	11	42	8	1	61	0	14	36	2	2	52	0	10	102	11	1	123	0	3	106	9	1	118	354
12:15	0	7	47	6	3	60	0	11	21	2	1	34	0	10	121	9	2	140	0	1	88	4	0	93	327
12:30	0	6	41	8	1	55	0	10	22	3	1	35	0	6	104	15	2	125	0	1	75	5	1	81	296
12:45	0	10	30	9	4	49	0	16	27	1	3	44	0	13	98	12	3	123	0	1	85	2	4	88	304
Hourly Total	0	34	160	31	9	225	0	51	106	8	7	165	0	39	425	47	8	511	0	6	354	20	6	380	1281
13:00	0	10	39	8	3	57	0	10	24	2	0	36	0	8	115	9	0	132	0	6	96	6	1	108	333
13:15	0	7	54	9	1	70	0	11	24	1	0	36	0	9	137	14	0	160	0	0	99	6	1	105	371
13:30	0	4	37	6	4	47	0	12	25	2	1	39	0	13	123	13	0	149	0	4	83	6	0	93	328
13:45	0	5	28	7	7	40	0	8	40	4	0	52	0	10	83	10	1	103	0	1	103	6	0	110	305
Hourly Total	0	26	158	30	15	214	0	41	113	9	1	163	0	40	458	46	1	544	0	11	381	24	2	416	1337
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	7	60	11	1	78	0	6	41	4	3	51	0	11	127	17	1	155	0	1	105	8	0	114	398
15:15	0	10	53	8	1	71	0	8	50	5	1	63	0	5	111	26	2	142	0	5	96	4	1	105	381
15:30	0	13	59	4	2	76	0	14	36	6	0	56	0	8	147	16	2	171	0	0	89	5	0	94	397
15:45	0	23	60	8	3	91	0	13	40	4	4	57	0	4	156	30	2	190	0	1	98	4	1	103	441
Hourly Total	0	53	232	31	7	316	0	41	167	19	8	227	0	28	541	89	7	658	0	7	388	21	2	416	1617
16:00	0	13	44	3	3	60	0	15	33	6	0	54	0	13	144	19	1	176	0	2	94	11	3	107	397
16:15	0	15	77	8	1	100	0	12	37	2	3	51	0	10	150	32	3	192	0	2	82	6	1	90	433
16:30	0	19	70	8	3	97	0	11	27	2	1	40	0	11	130	34	0	175	0	1	112	12	1	125	437
16:45	0	17	73	5	4	95	0	10	44	4	0	58	0	6	135	23	0	164	0	6	125	10	0	141	458
Hourly Total	0	64	264	24	11	352	0	48	141	14	4	203	0	40	559	108	4	707	0	11	413	39	5	463	1725
17:00	0	13	65	5	4	83	0	11	51	3	3	65	0	14	149	19	3	182	0	2	138	13	0	153	483
17:15	0	19	78	1	6	98	0	10	46	7	0	63	0	11	146	32	4	189	0	1	96	8	1	105	455
17:30	0	15	75	3	3	93	0	9	41	0	0	50	0	11	156	28	3	195	0	4	100	7	0	111	449
17:45	0	14	49	11	8	74	0	12	38	3	1	53	0	12	130	12	3	154	0	4	85	10	0	99	380
Hourly Total	0	61	267	20	21	348	0	42	176	13	4	231	0	48	581	91	13	720	0	11	419	38	1	468	1767
18:00	0	7	43	6	0	56	0	3	31	0	2	34	0	8	113	16	2	137	0	0	83	9	0	92	319
18:15	0	9	33	6	1	48	0	5	34	0	0	39	0	12	108	13	1	133	0	3	87	3	0	93	313
18:30	0	3	27	5	4	35	0	6	17	1	0	24	0	7	109	20	1	136	0	2	73	7	0	82	277
18:45	0	5	26	8	3	39	0	6	16	2	1	24	0	4	106	5	2	115	0	0	63	9	1	72	250
Hourly Total	0	24	129	25	8	178	0	20	98	3	3	121	0	31	436	54	6	521	0	5	306	28	1	339	1159
Grand Total	0	311	1561	241	100	2113	0	315	1145	100	34	1560	0	317	3863	550	51	4730	0	64	3060	234	32	3358	11761
Approach %	0.0	14.7	73.9	11.4	-	-	0.0	20.2	73.4	6.4	-	-	0.0	6.7	81.7	11.6	-	-	0.0	1.9	91.1	7.0	-	-	-
Total %	0.0	2.6	13.3	2.0	-	18.0	0.0	2.7	9.7	0.9	-	13.3	0.0	2.7	32.8	4.7	-	40.2	0.0	0.5	26.0	2.0	-	28.6	-
Lights	0	305	1528	239	66	2072	0	310	1134	99	23	1543	0	312	3840	524	34	4676	0	64	3022	214	27	3300	11591
% Lights	-	98.1	97.9	99.2	66.0	98.1	-	98.4	99.0	99.0	67.6	98.9	-	98.4	99.4	95.3	66.7	98.9	-	100.0	98.8	91.5	84.4	98.3	98.6
Other Vehicles	0	0	33	2	34	35	0	5	9	1	11	15	0	5	22	25	17	52	0	0	38	15	5	53	155
% Other Vehicles	-	0.0	2.1	0.8	34.0	1.7	-	1.6	0.8	1.0	32.4	1.0	-	1.6	0.6	4.5	33.3	1.1	-	0.0	1.2	6.4	15.6	1.6	1.3
Bicycles on Road	0	6	0	0	0	6	0	0	2	0	0	2	0	0	1	1	0	2	0	0	0	5	0	5	15
% Bicycles on Road	-	1.9	0.0	0.0	0.0	0.3	-	0.0	0.2	0.0	0.0	0.1	-	0.0	0.0	0.2	0.0	0.0	-	0.0	0.0	2.1	0.0	0.1	0.1



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:30)

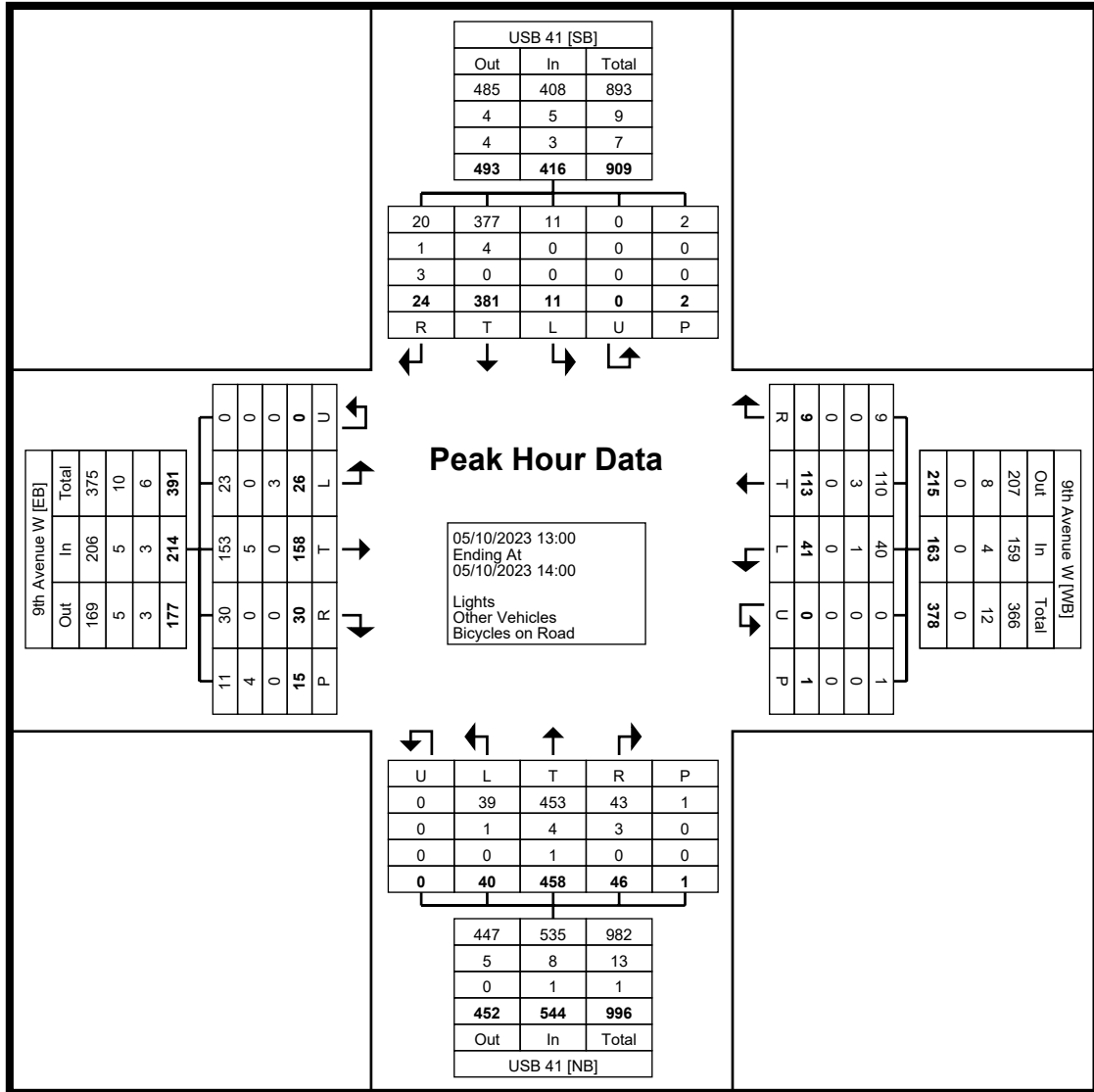
Start Time	9th Avenue W Eastbound						9th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:30	0	9	45	8	7	62	0	8	48	7	2	63	0	9	115	11	2	135	0	2	90	10	1	102	362
7:45	0	5	42	12	2	59	0	12	50	4	1	66	0	10	129	19	1	158	0	2	104	7	0	113	396
8:00	0	6	43	14	1	63	0	9	36	6	0	51	0	14	99	18	1	131	0	2	109	10	1	121	366
8:15	0	7	54	11	8	72	0	16	47	5	2	68	0	11	118	22	1	151	0	0	111	5	5	116	407
<b>Total</b>	0	27	184	45	18	256	0	45	181	22	5	248	0	44	461	70	5	575	0	6	414	32	7	452	1531
Approach %	0.0	10.5	71.9	17.6	-	-	0.0	18.1	73.0	8.9	-	-	0.0	7.7	80.2	12.2	-	-	0.0	1.3	91.6	7.1	-	-	-
Total %	0.0	1.8	12.0	2.9	-	16.7	0.0	2.9	11.8	1.4	-	16.2	0.0	2.9	30.1	4.6	-	37.6	0.0	0.4	27.0	2.1	-	29.5	-
PHF	0.000	0.750	0.852	0.804	-	0.889	0.000	0.703	0.905	0.786	-	0.912	0.000	0.786	0.893	0.795	-	0.910	0.000	0.750	0.932	0.800	-	0.934	0.940
Lights	0	27	180	45	15	252	0	44	178	22	3	244	0	43	459	67	4	569	0	6	408	30	7	444	1509
% Lights	-	100.0	97.8	100.0	83.3	98.4	-	97.8	98.3	100.0	60.0	98.4	-	97.7	99.6	95.7	80.0	99.0	-	100.0	98.6	93.8	100.0	98.2	98.6
Other Vehicles	0	0	4	0	3	4	0	1	2	0	2	3	0	1	2	3	1	6	0	0	6	2	0	8	21
% Other Vehicles	-	0.0	2.2	0.0	16.7	1.6	-	2.2	1.1	0.0	40.0	1.2	-	2.3	0.4	4.3	20.0	1.0	-	0.0	1.4	6.3	0.0	1.8	1.4
Bicycles on Road	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.6	0.0	0.0	0.4	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.1



Turning Movement Peak Hour Data Plot (7:30)

### Turning Movement Peak Hour Data (13:00)

Start Time	9th Avenue W Eastbound						9th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
13:00	0	10	39	8	3	57	0	10	24	2	0	36	0	8	115	9	0	132	0	6	96	6	1	108	333
13:15	0	7	54	9	1	70	0	11	24	1	0	36	0	9	137	14	0	160	0	0	99	6	1	105	371
13:30	0	4	37	6	4	47	0	12	25	2	1	39	0	13	123	13	0	149	0	4	83	6	0	93	328
13:45	0	5	28	7	7	40	0	8	40	4	0	52	0	10	83	10	1	103	0	1	103	6	0	110	305
<b>Total</b>	<b>0</b>	<b>26</b>	<b>158</b>	<b>30</b>	<b>15</b>	<b>214</b>	<b>0</b>	<b>41</b>	<b>113</b>	<b>9</b>	<b>1</b>	<b>163</b>	<b>0</b>	<b>40</b>	<b>458</b>	<b>46</b>	<b>1</b>	<b>544</b>	<b>0</b>	<b>11</b>	<b>381</b>	<b>24</b>	<b>2</b>	<b>416</b>	<b>1337</b>
Approach %	0.0	12.1	73.8	14.0	-	-	0.0	25.2	69.3	5.5	-	-	0.0	7.4	84.2	8.5	-	-	0.0	2.6	91.6	5.8	-	-	-
Total %	0.0	1.9	11.8	2.2	-	16.0	0.0	3.1	8.5	0.7	-	12.2	0.0	3.0	34.3	3.4	-	40.7	0.0	0.8	28.5	1.8	-	31.1	-
PHF	0.000	0.650	0.731	0.833	-	0.764	0.000	0.854	0.706	0.563	-	0.784	0.000	0.769	0.836	0.821	-	0.850	0.000	0.458	0.925	1.000	-	0.945	0.901
Lights	0	23	153	30	11	206	0	40	110	9	1	159	0	39	453	43	1	535	0	11	377	20	2	408	1308
% Lights	-	88.5	96.8	100.0	73.3	96.3	-	97.6	97.3	100.0	100.0	97.5	-	97.5	98.9	93.5	100.0	98.3	-	100.0	99.0	83.3	100.0	98.1	97.8
Other Vehicles	0	0	5	0	4	5	0	1	3	0	0	4	0	1	4	3	0	8	0	0	4	1	0	5	22
% Other Vehicles	-	0.0	3.2	0.0	26.7	2.3	-	2.4	2.7	0.0	0.0	2.5	-	2.5	0.9	6.5	0.0	1.5	-	0.0	1.0	4.2	0.0	1.2	1.6
Bicycles on Road	0	3	0	0	0	3	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	3	0	3	7
% Bicycles on Road	-	11.5	0.0	0.0	0.0	1.4	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.2	0.0	0.0	0.2	-	0.0	0.0	12.5	0.0	0.7	0.5

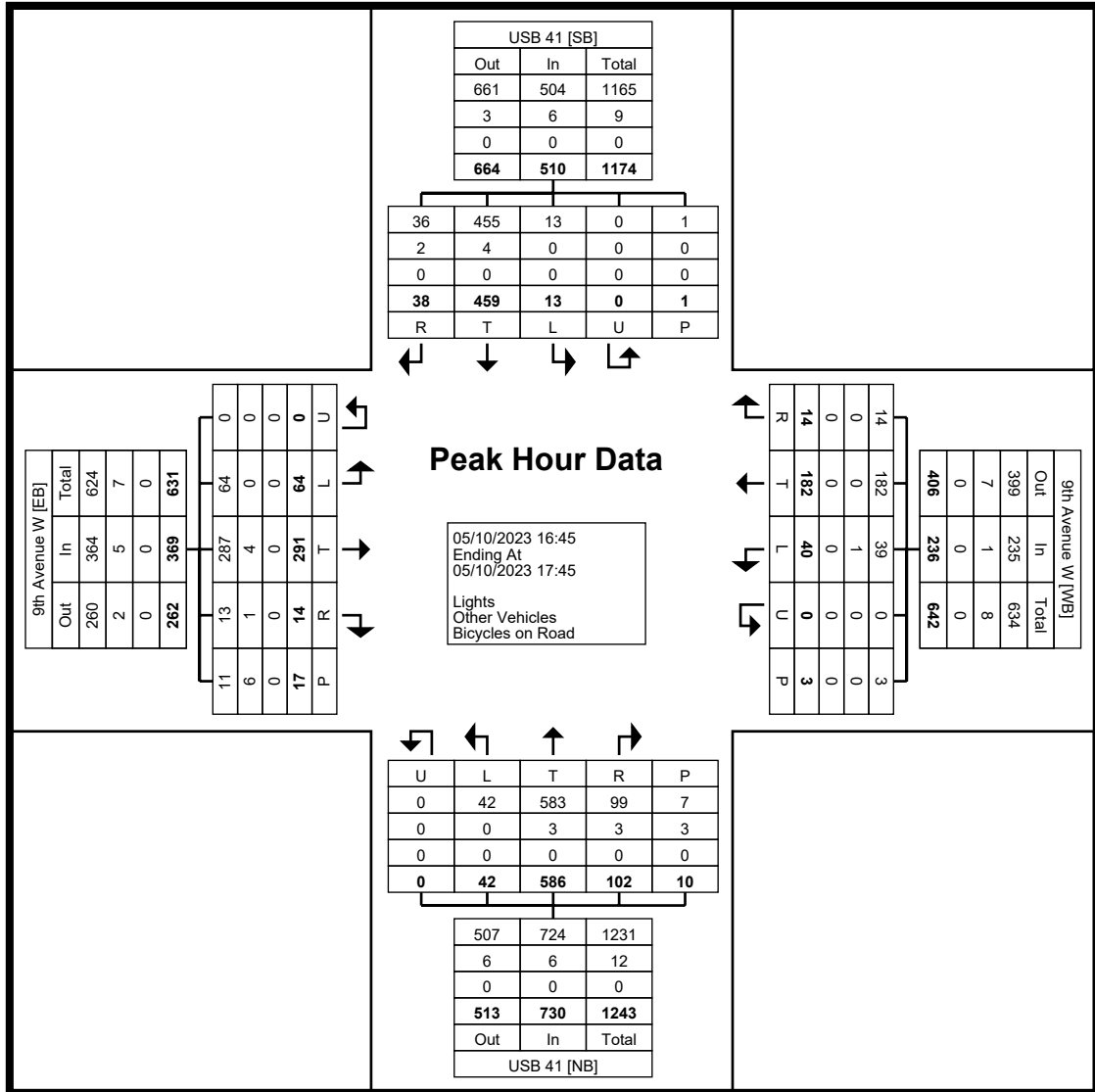


Turning Movement Peak Hour Data Plot (13:00)



### Turning Movement Peak Hour Data (16:45)

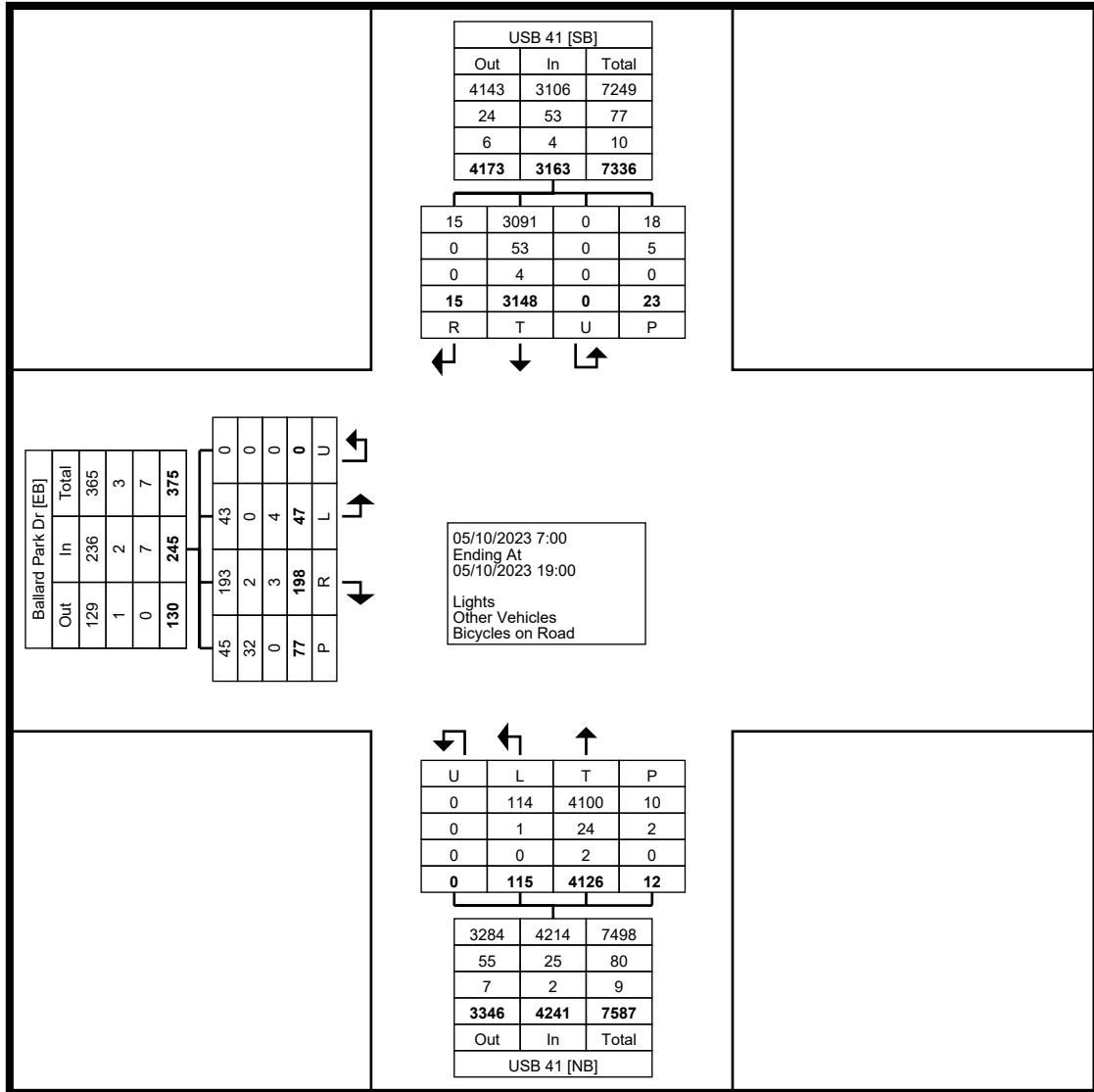
Start Time	9th Avenue W Eastbound						9th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:45	0	17	73	5	4	95	0	10	44	4	0	58	0	6	135	23	0	164	0	6	125	10	0	141	458
17:00	0	13	65	5	4	83	0	11	51	3	3	65	0	14	149	19	3	182	0	2	138	13	0	153	483
17:15	0	19	78	1	6	98	0	10	46	7	0	63	0	11	146	32	4	189	0	1	96	8	1	105	455
17:30	0	15	75	3	3	93	0	9	41	0	0	50	0	11	156	28	3	195	0	4	100	7	0	111	449
<b>Total</b>	0	64	291	14	17	369	0	40	182	14	3	236	0	42	586	102	10	730	0	13	459	38	1	510	1845
Approach %	0.0	17.3	78.9	3.8	-	-	0.0	16.9	77.1	5.9	-	-	0.0	5.8	80.3	14.0	-	-	0.0	2.5	90.0	7.5	-	-	-
Total %	0.0	3.5	15.8	0.8	-	20.0	0.0	2.2	9.9	0.8	-	12.8	0.0	2.3	31.8	5.5	-	39.6	0.0	0.7	24.9	2.1	-	27.6	-
PHF	0.000	0.842	0.933	0.700	-	0.941	0.000	0.909	0.892	0.500	-	0.908	0.000	0.750	0.939	0.797	-	0.936	0.000	0.542	0.832	0.731	-	0.833	0.955
Lights	0	64	287	13	11	364	0	39	182	14	3	235	0	42	583	99	7	724	0	13	455	36	1	504	1827
% Lights	-	100.0	98.6	92.9	64.7	98.6	-	97.5	100.0	100.0	100.0	99.6	-	100.0	99.5	97.1	70.0	99.2	-	100.0	99.1	94.7	100.0	98.8	99.0
Other Vehicles	0	0	4	1	6	5	0	1	0	0	0	1	0	0	3	3	3	6	0	0	4	2	0	6	18
% Other Vehicles	-	0.0	1.4	7.1	35.3	1.4	-	2.5	0.0	0.0	0.0	0.4	-	0.0	0.5	2.9	30.0	0.8	-	0.0	0.9	5.3	0.0	1.2	1.0
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (16:45)

### Turning Movement Data

Start Time	Ballard Park Dr Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
7:00	0	2	5	3	7	0	0	90	0	90	0	91	0	0	91	188
7:15	0	3	5	1	8	0	2	126	0	128	0	94	0	3	94	230
7:30	0	2	9	4	11	0	4	123	2	127	0	115	0	0	115	253
7:45	0	1	5	1	6	0	1	136	0	137	0	96	1	0	97	240
Hourly Total	0	8	24	9	32	0	7	475	2	482	0	396	1	3	397	911
8:00	0	2	4	1	6	0	3	111	0	114	0	120	1	1	121	241
8:15	0	2	3	1	5	0	6	122	0	128	0	116	2	0	118	251
8:30	0	0	4	0	4	0	4	98	0	102	0	105	0	1	105	211
8:45	0	1	14	1	15	0	5	110	1	115	0	97	0	1	97	227
Hourly Total	0	5	25	3	30	0	18	441	1	459	0	438	3	3	441	930
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	4	3	4	0	4	121	0	125	0	103	0	0	103	232
12:15	0	2	5	3	7	0	3	115	0	118	0	95	0	2	95	220
12:30	0	3	3	3	6	0	2	115	0	117	0	79	1	1	80	203
12:45	0	4	5	3	9	0	4	107	0	111	0	77	0	0	77	197
Hourly Total	0	9	17	12	26	0	13	458	0	471	0	354	1	3	355	852
13:00	0	3	13	4	16	0	1	120	0	121	0	95	0	0	95	232
13:15	0	0	4	3	4	0	4	148	0	152	0	91	0	1	91	247
13:30	0	1	6	4	7	0	1	119	0	120	0	98	0	2	98	225
13:45	0	1	5	4	6	0	5	86	3	91	0	97	0	0	97	194
Hourly Total	0	5	28	15	33	0	11	473	3	484	0	381	0	3	381	898
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	2	4	2	6	0	8	123	0	131	0	102	0	0	102	239
15:15	0	1	16	1	17	0	6	131	0	137	0	93	2	0	95	249
15:30	0	3	2	4	5	0	1	163	1	164	0	106	0	4	106	275
15:45	0	3	2	2	5	0	7	178	0	185	0	87	1	0	88	278
Hourly Total	0	9	24	9	33	0	22	595	1	617	0	388	3	4	391	1041
16:00	0	1	2	0	3	0	4	151	2	155	0	106	0	0	106	264
16:15	0	0	4	0	4	0	2	170	0	172	0	88	0	0	88	264
16:30	0	1	7	5	8	0	2	149	1	151	0	127	2	3	129	288
16:45	0	1	17	3	18	0	6	138	0	144	0	112	0	0	112	274
Hourly Total	0	3	30	8	33	0	14	608	3	622	0	433	2	3	435	1090
17:00	0	3	6	3	9	0	3	166	1	169	0	152	1	2	153	331
17:15	0	1	2	3	3	0	6	174	0	180	0	98	1	0	99	282
17:30	0	3	13	4	16	0	3	148	0	151	0	106	0	1	106	273
17:45	0	0	9	6	9	0	8	142	0	150	0	79	2	0	81	240
Hourly Total	0	7	30	16	37	0	20	630	1	650	0	435	4	3	439	1126
18:00	0	0	4	0	4	0	2	117	0	119	0	96	1	0	97	220
18:15	0	0	9	1	9	0	3	116	1	119	0	77	0	1	77	205
18:30	0	0	2	2	2	0	3	107	0	110	0	85	0	0	85	197
18:45	0	1	5	2	6	0	2	106	0	108	0	65	0	0	65	179
Hourly Total	0	1	20	5	21	0	10	446	1	456	0	323	1	1	324	801
Grand Total	0	47	198	77	245	0	115	4126	12	4241	0	3148	15	23	3163	7649
Approach %	0.0	19.2	80.8	-	-	0.0	2.7	97.3	-	-	0.0	99.5	0.5	-	-	-
Total %	0.0	0.6	2.6	-	3.2	0.0	1.5	53.9	-	55.4	0.0	41.2	0.2	-	41.4	-
Lights	0	43	193	45	236	0	114	4100	10	4214	0	3091	15	18	3106	7556
% Lights	-	91.5	97.5	58.4	96.3	-	99.1	99.4	83.3	99.4	-	98.2	100.0	78.3	98.2	98.8
Other Vehicles	0	0	2	32	2	0	1	24	2	25	0	53	0	5	53	80
% Other Vehicles	-	0.0	1.0	41.6	0.8	-	0.9	0.6	16.7	0.6	-	1.7	0.0	21.7	1.7	1.0
Bicycles on Road	0	4	3	0	7	0	0	2	0	2	0	4	0	0	4	13
% Bicycles on Road	-	8.5	1.5	0.0	2.9	-	0.0	0.0	0.0	0.0	-	0.1	0.0	0.0	0.1	0.2



Turning Movement Data Plot

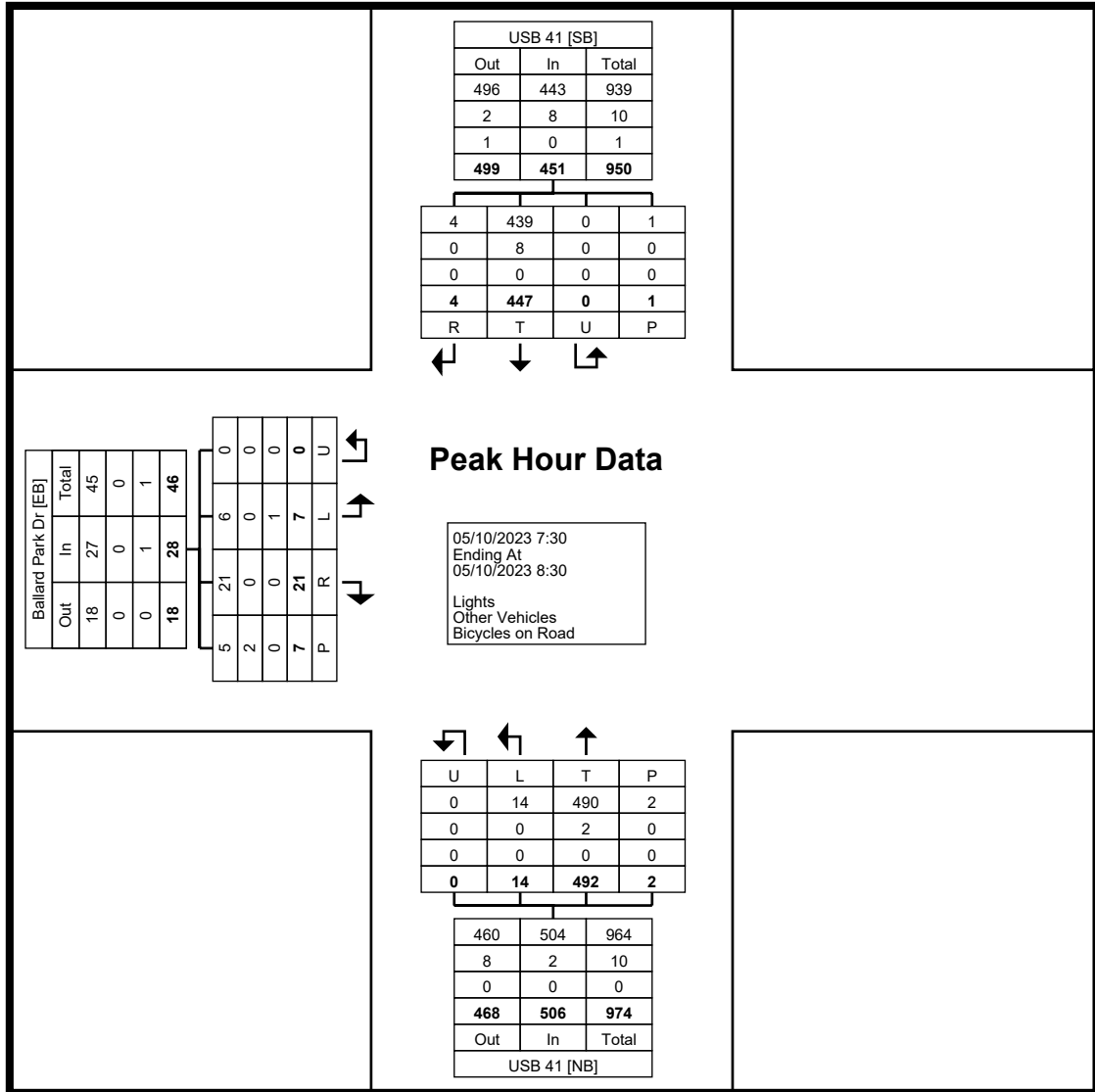
USB 41 & Ballard Park Dr  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 16\_USB 41 &  
Ballard Park Dr  
Site Code: 16  
Start Date: 05/10/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:30)

Start Time	Ballard Park Dr Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
7:30	0	2	9	4	11	0	4	123	2	127	0	115	0	0	115	253
7:45	0	1	5	1	6	0	1	136	0	137	0	96	1	0	97	240
8:00	0	2	4	1	6	0	3	111	0	114	0	120	1	1	121	241
8:15	0	2	3	1	5	0	6	122	0	128	0	116	2	0	118	251
<b>Total</b>	<b>0</b>	<b>7</b>	<b>21</b>	<b>7</b>	<b>28</b>	<b>0</b>	<b>14</b>	<b>492</b>	<b>2</b>	<b>506</b>	<b>0</b>	<b>447</b>	<b>4</b>	<b>1</b>	<b>451</b>	<b>985</b>
Approach %	0.0	25.0	75.0	-	-	0.0	2.8	97.2	-	-	0.0	99.1	0.9	-	-	-
Total %	0.0	0.7	2.1	-	2.8	0.0	1.4	49.9	-	51.4	0.0	45.4	0.4	-	45.8	-
PHF	0.000	0.875	0.583	-	0.636	0.000	0.583	0.904	-	0.923	0.000	0.931	0.500	-	0.932	0.973
Lights	0	6	21	5	27	0	14	490	2	504	0	439	4	1	443	974
% Lights	-	85.7	100.0	71.4	96.4	-	100.0	99.6	100.0	99.6	-	98.2	100.0	100.0	98.2	98.9
Other Vehicles	0	0	0	2	0	0	0	2	0	2	0	8	0	0	8	10
% Other Vehicles	-	0.0	0.0	28.6	0.0	-	0.0	0.4	0.0	0.4	-	1.8	0.0	0.0	1.8	1.0
Bicycles on Road	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	1
% Bicycles on Road	-	14.3	0.0	0.0	3.6	-	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.1



Turning Movement Peak Hour Data Plot (7:30)

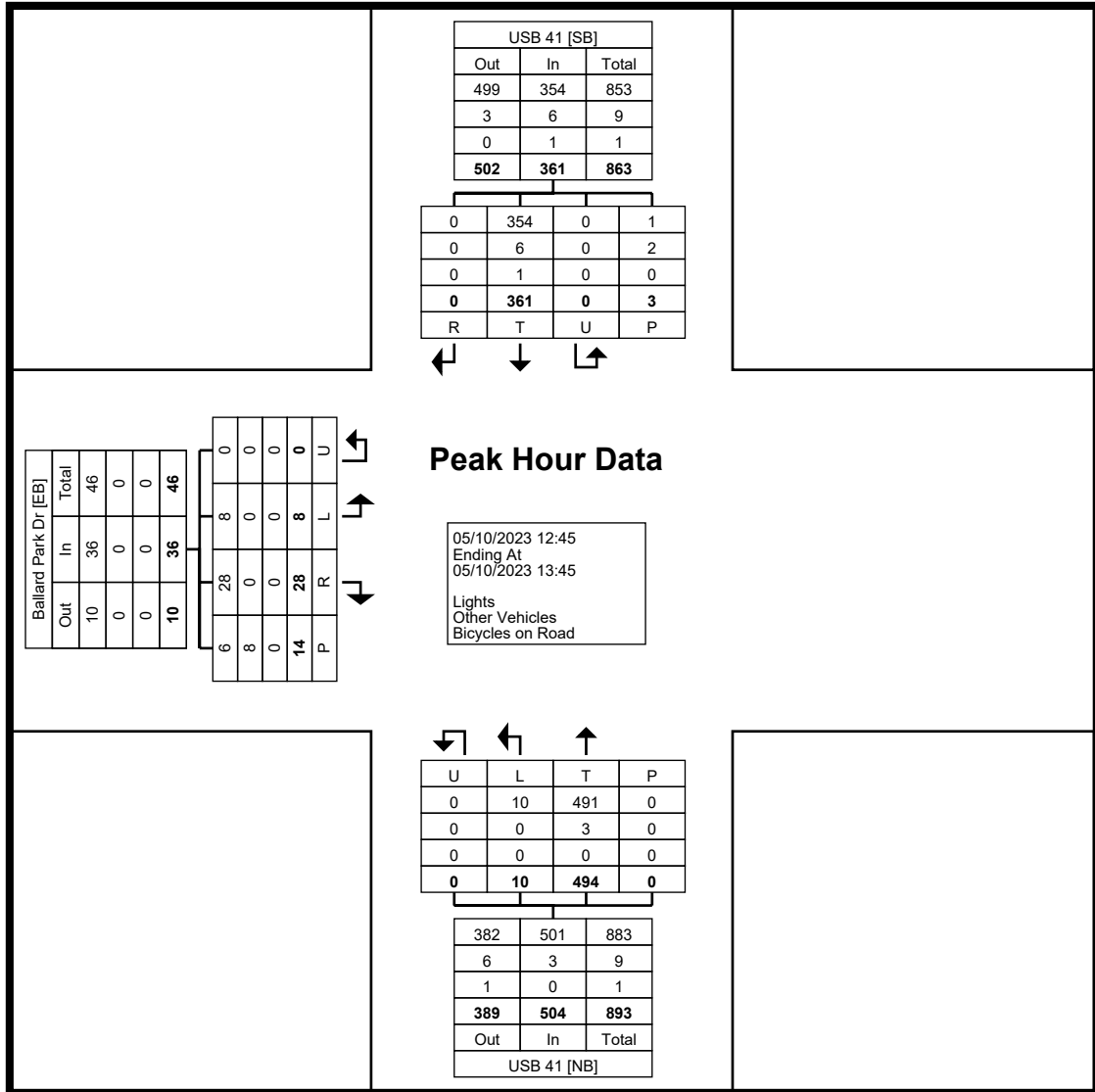
USB 41 & Ballard Park Dr  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 16\_USB 41 &  
Ballard Park Dr  
Site Code: 16  
Start Date: 05/10/2023  
Page No: 5

### Turning Movement Peak Hour Data (12:45)

Start Time	Ballard Park Dr Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
12:45	0	4	5	3	9	0	4	107	0	111	0	77	0	0	77	197
13:00	0	3	13	4	16	0	1	120	0	121	0	95	0	0	95	232
13:15	0	0	4	3	4	0	4	148	0	152	0	91	0	1	91	247
13:30	0	1	6	4	7	0	1	119	0	120	0	98	0	2	98	225
<b>Total</b>	<b>0</b>	<b>8</b>	<b>28</b>	<b>14</b>	<b>36</b>	<b>0</b>	<b>10</b>	<b>494</b>	<b>0</b>	<b>504</b>	<b>0</b>	<b>361</b>	<b>0</b>	<b>3</b>	<b>361</b>	<b>901</b>
<b>Approach %</b>	<b>0.0</b>	<b>22.2</b>	<b>77.8</b>	<b>-</b>	<b>-</b>	<b>0.0</b>	<b>2.0</b>	<b>98.0</b>	<b>-</b>	<b>-</b>	<b>0.0</b>	<b>100.0</b>	<b>0.0</b>	<b>-</b>	<b>-</b>	<b>-</b>
<b>Total %</b>	<b>0.0</b>	<b>0.9</b>	<b>3.1</b>	<b>-</b>	<b>4.0</b>	<b>0.0</b>	<b>1.1</b>	<b>54.8</b>	<b>-</b>	<b>55.9</b>	<b>0.0</b>	<b>40.1</b>	<b>0.0</b>	<b>-</b>	<b>40.1</b>	<b>-</b>
<b>PHF</b>	<b>0.000</b>	<b>0.500</b>	<b>0.538</b>	<b>-</b>	<b>0.563</b>	<b>0.000</b>	<b>0.625</b>	<b>0.834</b>	<b>-</b>	<b>0.829</b>	<b>0.000</b>	<b>0.921</b>	<b>0.000</b>	<b>-</b>	<b>0.921</b>	<b>0.912</b>
<b>Lights</b>	<b>0</b>	<b>8</b>	<b>28</b>	<b>6</b>	<b>36</b>	<b>0</b>	<b>10</b>	<b>491</b>	<b>0</b>	<b>501</b>	<b>0</b>	<b>354</b>	<b>0</b>	<b>1</b>	<b>354</b>	<b>891</b>
<b>% Lights</b>	<b>-</b>	<b>100.0</b>	<b>100.0</b>	<b>42.9</b>	<b>100.0</b>	<b>-</b>	<b>100.0</b>	<b>99.4</b>	<b>-</b>	<b>99.4</b>	<b>-</b>	<b>98.1</b>	<b>-</b>	<b>33.3</b>	<b>98.1</b>	<b>98.9</b>
<b>Other Vehicles</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>6</b>	<b>0</b>	<b>2</b>	<b>6</b>	<b>9</b>
<b>% Other Vehicles</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>	<b>57.1</b>	<b>0.0</b>	<b>-</b>	<b>0.0</b>	<b>0.6</b>	<b>-</b>	<b>0.6</b>	<b>-</b>	<b>1.7</b>	<b>-</b>	<b>66.7</b>	<b>1.7</b>	<b>1.0</b>
<b>Bicycles on Road</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>1</b>
<b>% Bicycles on Road</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>	<b>0.0</b>	<b>0.0</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>	<b>-</b>	<b>0.0</b>	<b>-</b>	<b>0.3</b>	<b>-</b>	<b>0.0</b>	<b>0.3</b>	<b>0.1</b>



Turning Movement Peak Hour Data Plot (12:45)



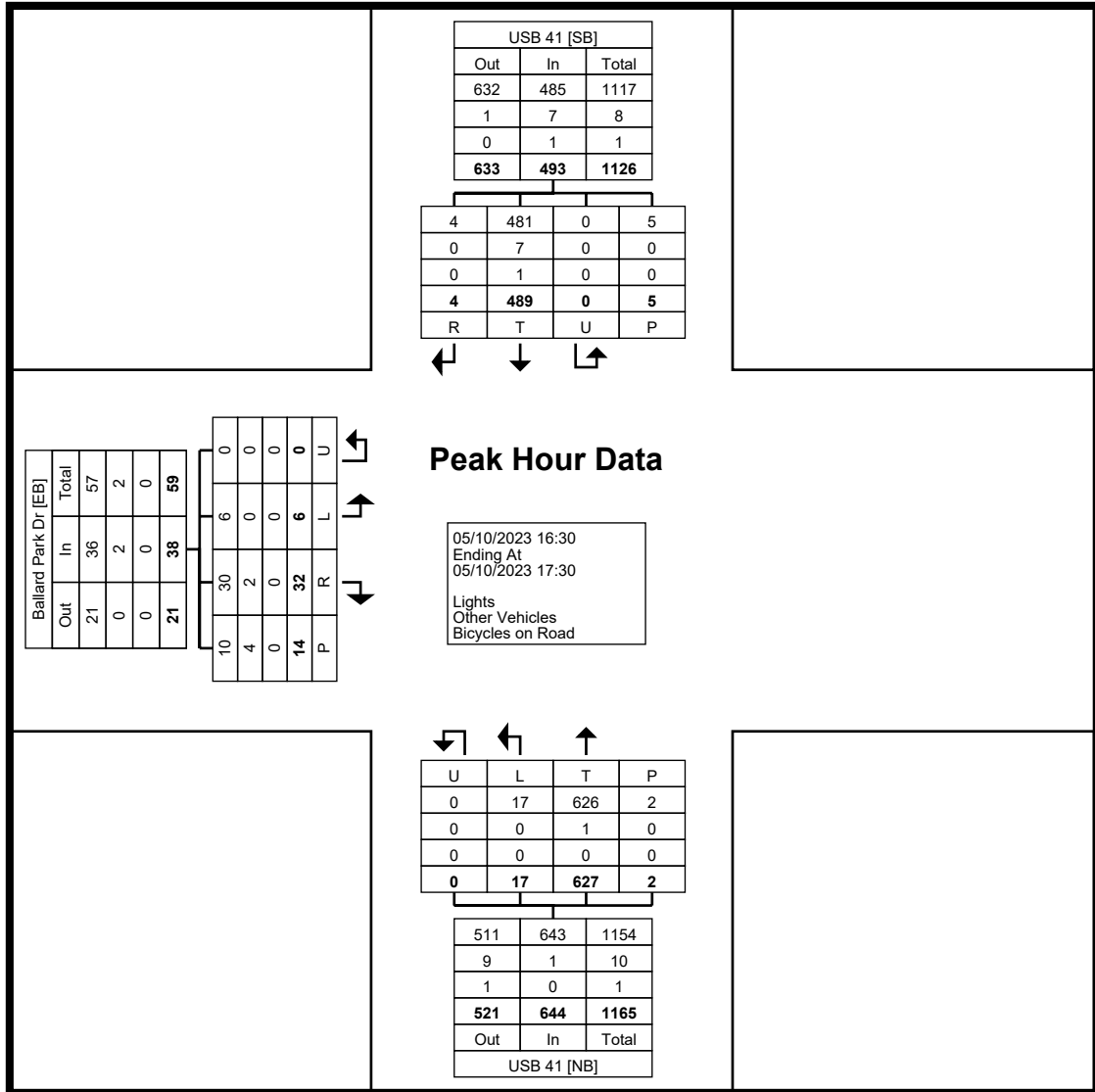
USB 41 & Ballard Park Dr  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 16\_USB 41 &  
Ballard Park Dr  
Site Code: 16  
Start Date: 05/10/2023  
Page No: 7

### Turning Movement Peak Hour Data (16:30)

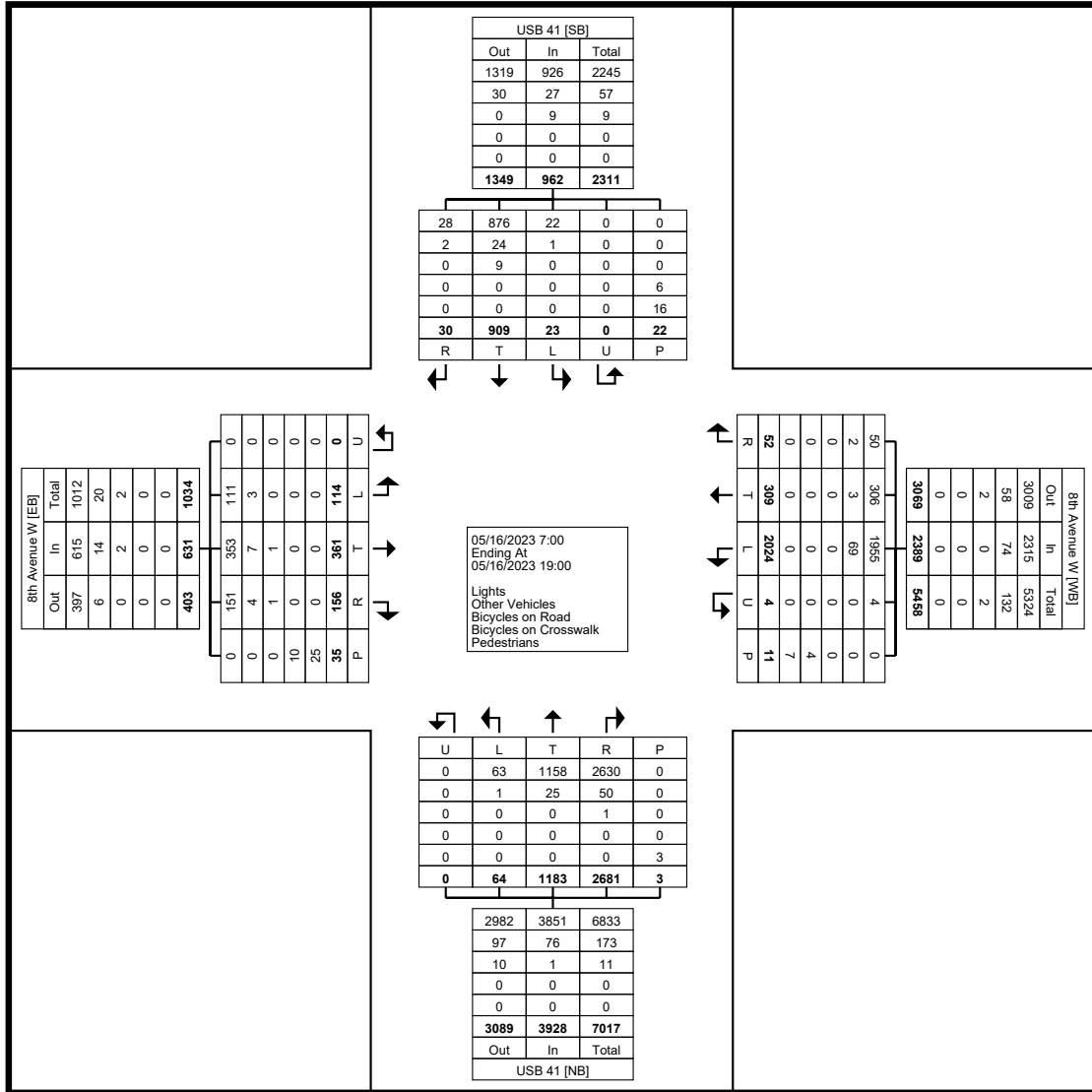
Start Time	Ballard Park Dr Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
16:30	0	1	7	5	8	0	2	149	1	151	0	127	2	3	129	288
16:45	0	1	17	3	18	0	6	138	0	144	0	112	0	0	112	274
17:00	0	3	6	3	9	0	3	166	1	169	0	152	1	2	153	331
17:15	0	1	2	3	3	0	6	174	0	180	0	98	1	0	99	282
<b>Total</b>	<b>0</b>	<b>6</b>	<b>32</b>	<b>14</b>	<b>38</b>	<b>0</b>	<b>17</b>	<b>627</b>	<b>2</b>	<b>644</b>	<b>0</b>	<b>489</b>	<b>4</b>	<b>5</b>	<b>493</b>	<b>1175</b>
Approach %	0.0	15.8	84.2	-	-	0.0	2.6	97.4	-	-	0.0	99.2	0.8	-	-	-
Total %	0.0	0.5	2.7	-	3.2	0.0	1.4	53.4	-	54.8	0.0	41.6	0.3	-	42.0	-
PHF	0.000	0.500	0.471	-	0.528	0.000	0.708	0.901	-	0.894	0.000	0.804	0.500	-	0.806	0.887
Lights	0	6	30	10	36	0	17	626	2	643	0	481	4	5	485	1164
% Lights	-	100.0	93.8	71.4	94.7	-	100.0	99.8	100.0	99.8	-	98.4	100.0	100.0	98.4	99.1
Other Vehicles	0	0	2	4	2	0	0	1	0	1	0	7	0	0	7	10
% Other Vehicles	-	0.0	6.3	28.6	5.3	-	0.0	0.2	0.0	0.2	-	1.4	0.0	0.0	1.4	0.9
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
% Bicycles on Road	-	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	-	0.2	0.0	0.0	0.2	0.1



Turning Movement Peak Hour Data Plot (16:30)

### Turning Movement Data

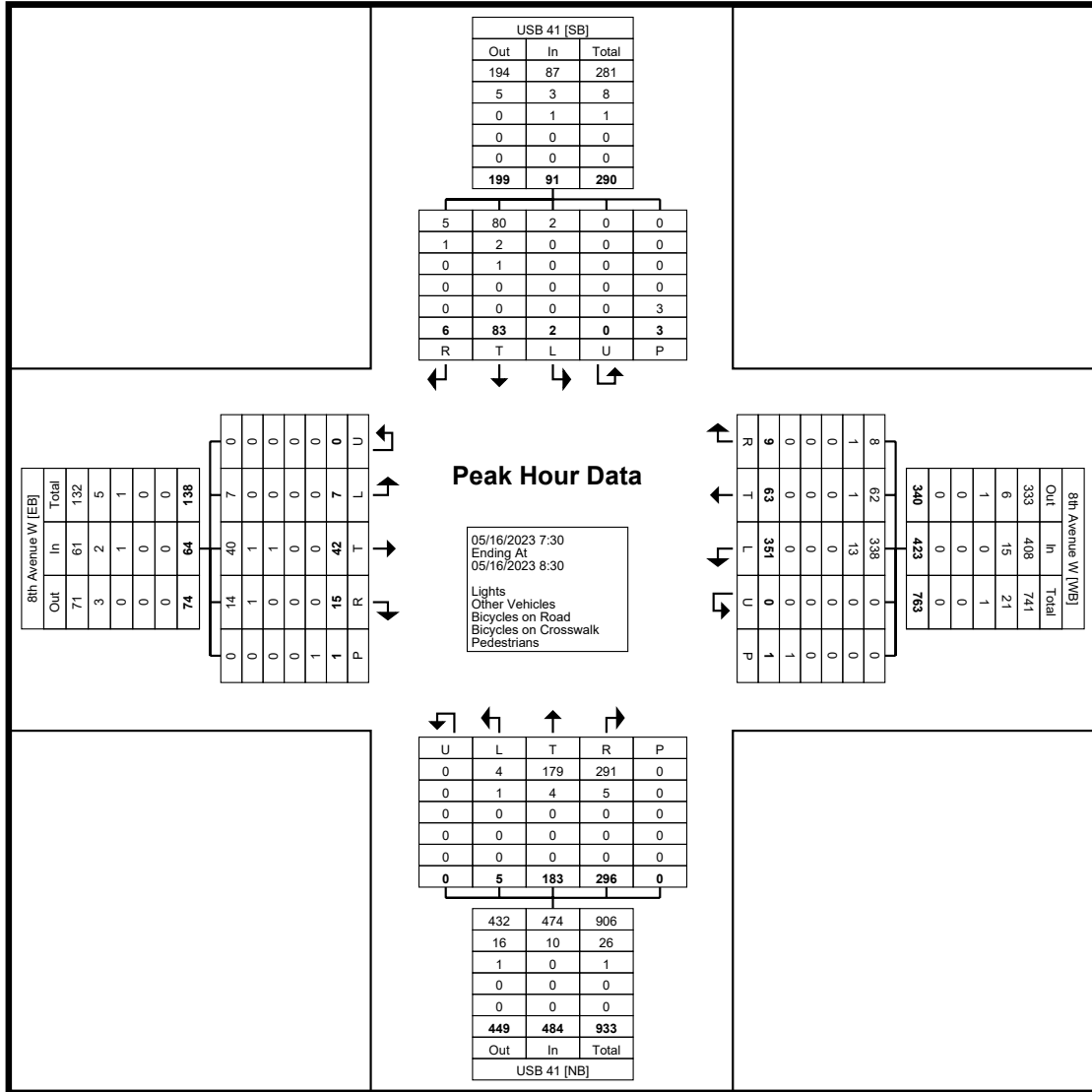
Start Time	8th Avenue W Eastbound						8th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	3	6	4	2	13	0	72	10	0	0	82	0	0	33	66	0	99	0	2	17	0	2	19	213
7:15	0	2	10	6	1	18	0	62	21	3	0	86	0	1	37	77	0	115	0	0	18	1	3	19	238
7:30	0	0	13	2	0	15	0	85	13	2	0	100	0	2	45	73	0	120	0	0	18	1	0	19	254
7:45	0	0	9	9	0	18	0	90	18	4	0	112	0	1	53	70	0	124	0	1	27	1	2	29	283
Hourly Total	0	5	38	21	3	64	0	309	62	9	0	380	0	4	168	286	0	458	0	3	80	3	7	86	988
8:00	0	5	13	2	1	20	0	88	12	2	1	102	0	2	35	83	0	120	0	1	19	3	1	23	265
8:15	0	2	7	2	0	11	0	88	20	1	0	109	0	0	50	70	0	120	0	0	19	1	0	20	260
8:30	0	2	6	1	0	9	0	68	25	1	1	94	0	3	38	50	0	91	0	2	27	0	3	29	223
8:45	0	2	1	3	3	6	0	95	24	5	1	124	0	0	45	57	0	102	0	0	25	1	1	26	258
Hourly Total	0	11	27	8	4	46	0	339	81	9	3	429	0	5	168	260	0	433	0	3	90	5	5	98	1006
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	4	4	7	1	15	1	68	5	4	0	78	0	4	35	63	0	102	0	0	40	2	0	42	237
12:15	0	1	9	5	1	15	0	53	6	1	0	60	0	1	31	70	0	102	0	1	34	1	0	36	213
12:30	0	3	5	7	0	15	0	55	4	1	0	60	0	1	39	80	0	120	0	3	22	1	0	26	221
12:45	0	7	11	4	2	22	1	49	5	3	0	58	0	1	48	92	0	141	0	0	33	2	0	35	256
Hourly Total	0	15	29	23	4	67	2	225	20	9	0	256	0	7	153	305	0	465	0	4	129	6	0	139	927
13:00	0	4	9	8	0	21	1	44	7	0	1	52	0	2	40	75	3	117	0	0	34	1	0	35	225
13:15	0	6	7	7	1	20	0	49	4	0	2	53	0	1	33	53	0	87	0	0	29	2	0	31	191
13:30	0	1	11	5	0	17	1	44	6	0	0	51	0	1	33	76	0	110	0	1	21	0	1	22	200
13:45	0	7	12	2	1	21	0	46	5	3	1	54	0	2	21	63	0	86	0	2	35	2	1	39	200
Hourly Total	0	18	39	22	2	79	2	183	22	3	4	210	0	6	127	267	3	400	0	3	119	5	2	127	816
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	4	13	7	1	24	0	58	8	3	0	69	0	3	45	84	0	132	0	0	31	0	0	31	256
15:15	0	1	17	4	0	22	0	80	9	3	0	92	0	2	41	98	0	141	0	0	34	1	0	35	290
15:30	0	3	14	4	0	21	0	56	9	2	0	67	0	1	44	105	0	150	0	1	35	1	0	37	275
15:45	0	3	12	12	0	27	0	67	5	0	0	72	0	2	24	99	0	125	0	1	33	1	1	35	259
Hourly Total	0	11	56	27	1	94	0	261	31	8	0	300	0	8	154	386	0	548	0	2	133	3	1	138	1080
16:00	0	4	27	5	3	36	0	49	8	3	0	60	0	3	43	128	0	174	0	0	32	1	2	33	303
16:15	0	8	24	10	1	42	0	54	9	2	0	65	0	0	34	111	0	145	0	0	32	2	1	34	286
16:30	0	5	16	8	3	29	0	70	11	2	0	83	0	2	41	112	0	155	0	0	35	2	0	37	304
16:45	0	4	24	6	5	34	0	84	8	2	1	94	0	3	44	95	0	142	0	0	48	1	0	49	319
Hourly Total	0	21	91	29	12	141	0	257	36	9	1	302	0	8	162	446	0	616	0	0	147	6	3	153	1212
17:00	0	11	31	6	2	48	0	79	25	3	0	107	0	4	33	94	0	131	0	4	50	1	1	55	341
17:15	0	6	17	8	2	31	0	53	7	1	0	61	0	6	42	123	0	171	0	1	27	0	1	28	291
17:30	0	5	19	4	0	28	0	70	9	0	0	79	0	4	32	103	0	139	0	0	33	1	0	34	280
17:45	0	4	4	2	1	10	0	60	6	0	1	66	0	3	30	102	0	135	0	2	23	0	0	25	236
Hourly Total	0	26	71	20	5	117	0	262	47	4	1	313	0	17	137	422	0	576	0	7	133	2	2	142	1148
18:00	0	6	8	1	0	15	0	49	1	0	1	50	0	3	31	94	0	128	0	0	27	0	0	27	220
18:15	0	0	0	3	0	3	0	46	2	0	0	48	0	1	31	76	0	108	0	0	15	0	0	15	174
18:30	0	1	1	0	2	2	0	52	3	0	1	55	0	3	28	76	0	107	0	1	18	0	1	19	183
18:45	0	0	1	2	2	3	0	41	4	1	0	46	0	2	24	63	0	89	0	0	18	0	1	18	156
Hourly Total	0	7	10	6	4	23	0	188	10	1	2	199	0	9	114	309	0	432	0	1	78	0	2	79	733
Grand Total	0	114	361	156	35	631	4	2024	309	52	11	2389	0	64	1183	2681	3	3928	0	23	909	30	22	962	7910
Approach %	0.0	18.1	57.2	24.7	-	-	0.2	84.7	12.9	2.2	-	-	0.0	1.6	30.1	68.3	-	-	0.0	2.4	94.5	3.1	-	-	-
Total %	0.0	1.4	4.6	2.0	-	8.0	0.1	25.6	3.9	0.7	-	30.2	0.0	0.8	15.0	33.9	-	49.7	0.0	0.3	11.5	0.4	-	12.2	-
Lights	0	111	353	151	-	615	4	1955	306	50	-	2315	0	63	1158	2630	-	3851	0	22	876	28	-	926	7707
% Lights	-	97.4	97.8	96.8	-	97.5	100.0	96.6	99.0	96.2	-	96.9	-	98.4	97.9	98.1	-	98.0	-	95.7	96.4	93.3	-	96.3	97.4
Other Vehicles	0	3	7	4	-	14	0	69	3	2	-	74	0	1	25	50	-	76	0	1	24	2	-	27	191
% Other Vehicles	-	2.6	1.9	2.6	-	2.2	0.0	3.4	1.0	3.8	-	3.1	-	1.6	2.1	1.9	-	1.9	-	4.3	2.6	6.7	-	2.8	2.4
Bicycles on Road	0	0	1	1	-	2	0	0	0	0	-	0	0	0	0	1	-	1	0	0	9	0	-	9	12
% Bicycles on Road	-	0.0	0.3	0.6	-	0.3	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	1.0	0.0	-	0.9	0.2
Bicycles on Crosswalk	-	-	-	-	10	-	-	-	-	-	4	-	-	-	-	0	-	-	-	-	-	-	6	-	-
% Bicycles on Crosswalk	-	-	-	-	28.6	-	-	-	-	-	36.4	-	-	-	-	0.0	-	-	-	-	-	-	27.3	-	-
Pedestrians	-	-	-	-	25	-	-	-	-	-	7	-	-	-	-	3	-	-	-	-	-	-	16	-	-
% Pedestrians	-	-	-	-	71.4	-	-	-	-	-	63.6	-	-	-	-	100.0	-	-	-	-	-	-	72.7	-	-



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:30)

Start Time	8th Avenue W Eastbound						8th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:30	0	0	13	2	0	15	0	85	13	2	0	100	0	2	45	73	0	120	0	0	18	1	0	19	254
7:45	0	0	9	9	0	18	0	90	18	4	0	112	0	1	53	70	0	124	0	1	27	1	2	29	283
8:00	0	5	13	2	1	20	0	88	12	2	1	102	0	2	35	83	0	120	0	1	19	3	1	23	265
8:15	0	2	7	2	0	11	0	88	20	1	0	109	0	0	50	70	0	120	0	0	19	1	0	20	260
<b>Total</b>	<b>0</b>	<b>7</b>	<b>42</b>	<b>15</b>	<b>1</b>	<b>64</b>	<b>0</b>	<b>351</b>	<b>63</b>	<b>9</b>	<b>1</b>	<b>423</b>	<b>0</b>	<b>5</b>	<b>183</b>	<b>296</b>	<b>0</b>	<b>484</b>	<b>0</b>	<b>2</b>	<b>83</b>	<b>6</b>	<b>3</b>	<b>91</b>	<b>1062</b>
Approach %	0.0	10.9	65.6	23.4	-	-	0.0	83.0	14.9	2.1	-	-	0.0	1.0	37.8	61.2	-	-	0.0	2.2	91.2	6.6	-	-	-
Total %	0.0	0.7	4.0	1.4	-	6.0	0.0	33.1	5.9	0.8	-	39.8	0.0	0.5	17.2	27.9	-	45.6	0.0	0.2	7.8	0.6	-	8.6	-
PHF	0.000	0.350	0.808	0.417	-	0.800	0.000	0.975	0.788	0.563	-	0.944	0.000	0.625	0.863	0.892	-	0.976	0.000	0.500	0.769	0.500	-	0.784	0.938
Lights	0	7	40	14	-	61	0	338	62	8	-	408	0	4	179	291	-	474	0	2	80	5	-	87	1030
% Lights	-	100.0	95.2	93.3	-	95.3	-	96.3	98.4	88.9	-	96.5	-	80.0	97.8	98.3	-	97.9	-	100.0	96.4	83.3	-	95.6	97.0
Other Vehicles	0	0	1	1	-	2	0	13	1	1	-	15	0	1	4	5	-	10	0	0	2	1	-	3	30
% Other Vehicles	-	0.0	2.4	6.7	-	3.1	-	3.7	1.6	11.1	-	3.5	-	20.0	2.2	1.7	-	2.1	-	0.0	2.4	16.7	-	3.3	2.8
Bicycles on Road	0	0	1	0	-	1	0	0	0	0	-	0	0	0	0	0	-	0	0	0	1	0	-	1	2
% Bicycles on Road	-	0.0	2.4	0.0	-	1.6	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	1.2	0.0	-	1.1	0.2
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	0.0	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	3	-	-
% Pedestrians	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



Turning Movement Peak Hour Data Plot (7:30)

### Turning Movement Peak Hour Data (12:00)

Start Time	8th Avenue W Eastbound						8th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	4	4	7	1	15	1	68	5	4	0	78	0	4	35	63	0	102	0	0	40	2	0	42	237
12:15	0	1	9	5	1	15	0	53	6	1	0	60	0	1	31	70	0	102	0	1	34	1	0	36	213
12:30	0	3	5	7	0	15	0	55	4	1	0	60	0	1	39	80	0	120	0	3	22	1	0	26	221
12:45	0	7	11	4	2	22	1	49	5	3	0	58	0	1	48	92	0	141	0	0	33	2	0	35	256
<b>Total</b>	0	15	29	23	4	67	2	225	20	9	0	256	0	7	153	305	0	465	0	4	129	6	0	139	927
Approach %	0.0	22.4	43.3	34.3	-	-	0.8	87.9	7.8	3.5	-	-	0.0	1.5	32.9	65.6	-	-	0.0	2.9	92.8	4.3	-	-	-
Total %	0.0	1.6	3.1	2.5	-	7.2	0.2	24.3	2.2	1.0	-	27.6	0.0	0.8	16.5	32.9	-	50.2	0.0	0.4	13.9	0.6	-	15.0	-
PHF	0.000	0.536	0.659	0.821	-	0.761	0.500	0.827	0.833	0.563	-	0.821	0.000	0.438	0.797	0.829	-	0.824	0.000	0.333	0.806	0.750	-	0.827	0.905
Lights	0	14	28	23	-	65	2	218	20	8	-	248	0	7	147	298	-	452	0	4	122	6	-	132	897
% Lights	-	93.3	96.6	100.0	-	97.0	100.0	96.9	100.0	88.9	-	96.9	-	100.0	96.1	97.7	-	97.2	-	100.0	94.6	100.0	-	95.0	96.8
Other Vehicles	0	1	1	0	-	2	0	7	0	1	-	8	0	0	6	6	-	12	0	0	7	0	-	7	29
% Other Vehicles	-	6.7	3.4	0.0	-	3.0	0.0	3.1	0.0	11.1	-	3.1	-	0.0	3.9	2.0	-	2.6	-	0.0	5.4	0.0	-	5.0	3.1
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	1	-	1	0	0	0	0	-	0	1
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.3	-	0.2	-	0.0	0.0	0.0	-	0.0	0.1
Bicycles on Crosswalk	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	25.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	3	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	75.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-





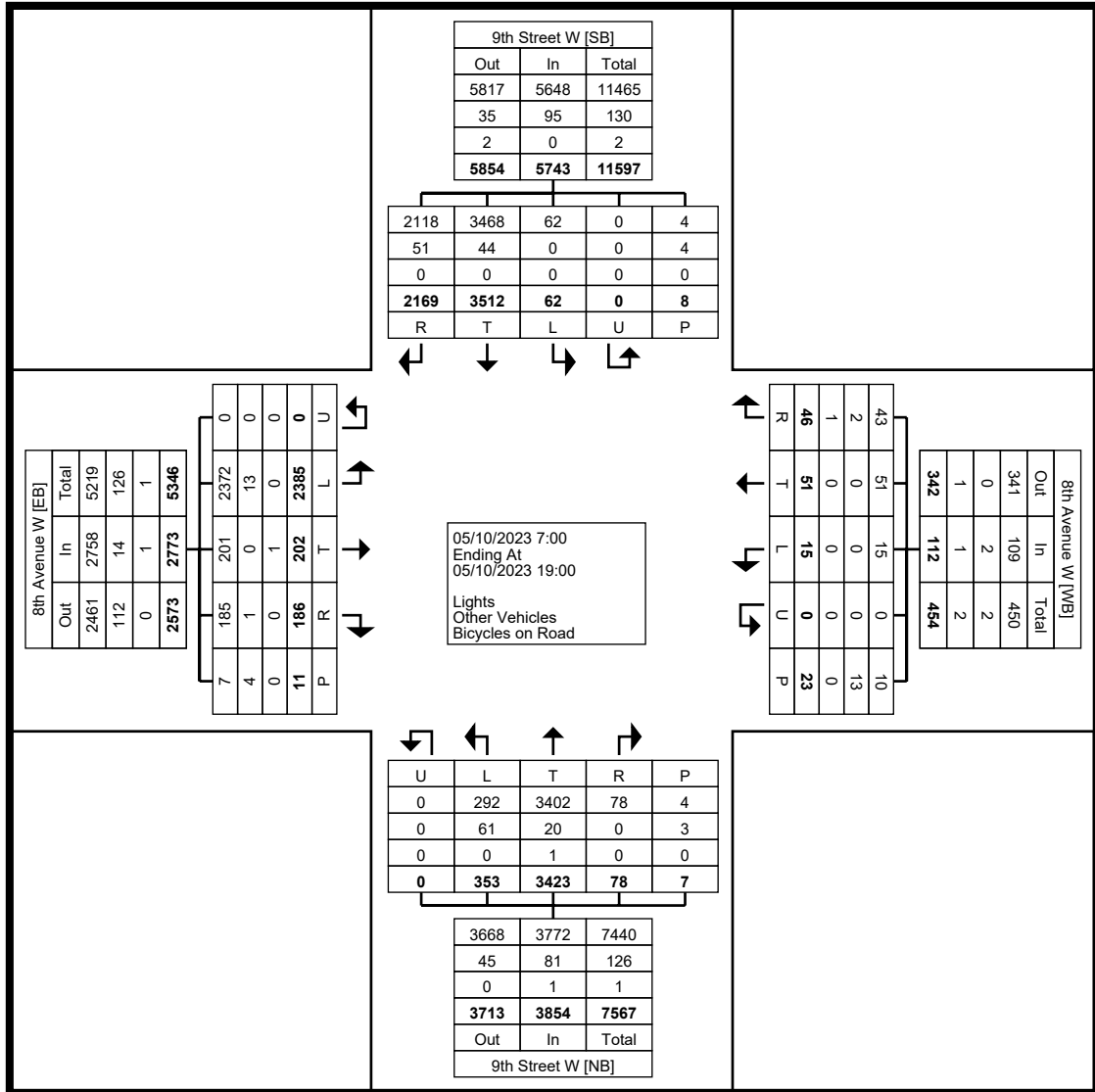
### Turning Movement Peak Hour Data (16:30)

Start Time	8th Avenue W Eastbound						8th Avenue W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:30	0	5	16	8	3	29	0	70	11	2	0	83	0	2	41	112	0	155	0	0	35	2	0	37	304
16:45	0	4	24	6	5	34	0	84	8	2	1	94	0	3	44	95	0	142	0	0	48	1	0	49	319
17:00	0	11	31	6	2	48	0	79	25	3	0	107	0	4	33	94	0	131	0	4	50	1	1	55	341
17:15	0	6	17	8	2	31	0	53	7	1	0	61	0	6	42	123	0	171	0	1	27	0	1	28	291
<b>Total</b>	0	26	88	28	12	142	0	286	51	8	1	345	0	15	160	424	0	599	0	5	160	4	2	169	1255
Approach %	0.0	18.3	62.0	19.7	-	-	0.0	82.9	14.8	2.3	-	-	0.0	2.5	26.7	70.8	-	-	0.0	3.0	94.7	2.4	-	-	-
Total %	0.0	2.1	7.0	2.2	-	11.3	0.0	22.8	4.1	0.6	-	27.5	0.0	1.2	12.7	33.8	-	47.7	0.0	0.4	12.7	0.3	-	13.5	-
PHF	0.000	0.591	0.710	0.875	-	0.740	0.000	0.851	0.510	0.667	-	0.806	0.000	0.625	0.909	0.862	-	0.876	0.000	0.313	0.800	0.500	-	0.768	0.920
Lights	0	25	86	27	-	138	0	280	51	8	-	339	0	15	157	416	-	588	0	5	157	4	-	166	1231
% Lights	-	96.2	97.7	96.4	-	97.2	-	97.9	100.0	100.0	-	98.3	-	100.0	98.1	98.1	-	98.2	-	100.0	98.1	100.0	-	98.2	98.1
Other Vehicles	0	1	2	1	-	4	0	6	0	0	-	6	0	0	3	8	-	11	0	0	2	0	-	2	23
% Other Vehicles	-	3.8	2.3	3.6	-	2.8	-	2.1	0.0	0.0	-	1.7	-	0.0	1.9	1.9	-	1.8	-	0.0	1.3	0.0	-	1.2	1.8
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	1	0	-	1	1
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.6	0.0	-	0.6	0.1
Bicycles on Crosswalk	-	-	-	-	6	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	1	-	-
% Bicycles on Crosswalk	-	-	-	-	50.0	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	50.0	-	-
Pedestrians	-	-	-	-	6	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	50.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	50.0	-	-



### Turning Movement Data

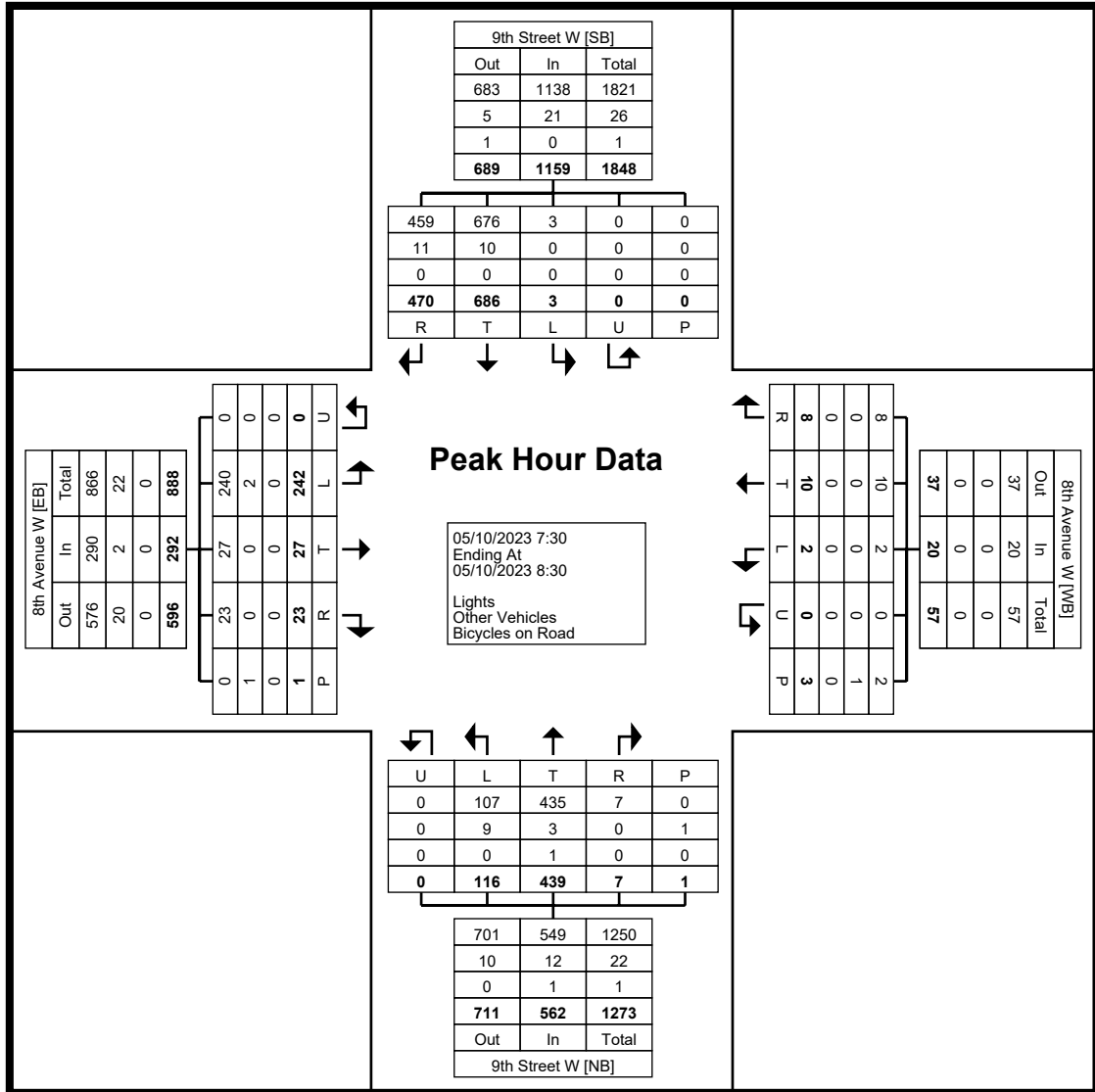
Start Time	8th Avenue W Eastbound						8th Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	44	7	2	0	53	0	0	1	1	1	2	0	17	93	0	0	110	0	0	180	116	0	296	461
7:15	0	65	9	3	1	77	0	0	1	2	1	3	0	26	73	3	0	102	0	2	176	95	1	273	455
7:30	0	62	8	7	0	77	0	1	2	5	0	8	0	31	94	3	1	128	0	3	173	115	0	291	504
7:45	0	50	9	5	0	64	0	0	6	0	1	6	0	31	128	2	0	161	0	0	193	111	0	304	535
Hourly Total	0	221	33	17	1	271	0	1	10	8	3	19	0	105	388	8	1	501	0	5	722	437	1	1164	1955
8:00	0	64	6	3	1	73	0	1	2	1	0	4	0	31	112	1	0	144	0	0	174	115	0	289	510
8:15	0	66	4	8	0	78	0	0	0	2	2	2	0	23	105	1	0	129	0	0	146	129	0	275	484
8:30	0	39	1	3	0	43	0	0	1	0	0	1	0	11	98	0	0	109	0	3	142	97	0	242	395
8:45	0	48	7	9	0	64	0	2	5	0	1	7	0	25	77	3	0	105	0	1	113	78	1	192	368
Hourly Total	0	217	18	23	1	258	0	3	8	3	3	14	0	90	392	5	0	487	0	4	575	419	1	998	1757
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	76	10	8	0	94	0	1	2	3	3	6	0	10	77	6	0	93	0	2	80	59	1	141	334
12:15	0	68	5	10	1	83	0	1	1	3	1	5	0	8	98	5	0	111	0	6	78	53	1	137	336
12:30	0	67	6	3	1	76	0	1	1	2	0	4	0	12	98	3	0	113	0	2	83	55	0	140	333
12:45	0	58	3	6	0	67	0	1	3	5	0	9	0	12	91	2	0	105	0	2	75	47	0	124	305
Hourly Total	0	269	24	27	2	320	0	4	7	13	4	24	0	42	364	16	0	422	0	12	316	214	2	542	1308
13:00	0	73	6	7	0	86	0	1	0	3	1	4	0	11	89	3	0	103	0	4	92	43	1	139	332
13:15	0	82	6	2	0	90	0	3	1	2	0	6	0	6	89	6	0	101	0	2	83	51	1	136	333
13:30	0	59	7	3	1	69	0	0	2	3	1	5	0	9	87	6	0	102	0	3	90	51	0	144	320
13:45	0	56	3	6	0	65	0	0	1	1	0	2	0	7	83	0	1	90	0	1	74	56	1	131	288
Hourly Total	0	270	22	18	1	310	0	4	4	9	2	17	0	33	348	15	1	396	0	10	339	201	3	550	1273
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	88	4	7	1	99	0	0	2	3	1	5	0	6	109	2	0	117	0	1	83	51	0	135	356
15:15	0	65	9	4	0	78	0	0	1	0	1	1	0	6	130	3	0	139	0	1	123	66	0	190	408
15:30	0	102	6	9	1	117	0	1	1	2	0	4	0	5	133	7	0	145	0	1	98	56	0	155	421
15:45	0	88	16	1	0	105	0	0	2	1	0	3	0	5	115	4	0	124	0	2	91	53	0	146	378
Hourly Total	0	343	35	21	2	399	0	1	6	6	2	13	0	22	487	16	0	525	0	5	395	226	0	626	1563
16:00	0	79	8	10	0	97	0	0	6	0	1	6	0	5	131	0	1	136	0	3	93	68	0	164	403
16:15	0	118	8	5	0	131	0	1	1	0	1	2	0	5	139	1	1	145	0	1	77	65	0	143	421
16:30	0	95	11	9	1	115	0	0	1	3	2	4	0	1	139	4	1	144	0	7	101	53	0	161	424
16:45	0	94	7	6	0	107	0	0	1	1	1	2	0	7	140	2	1	149	0	2	79	51	0	132	390
Hourly Total	0	386	34	30	1	450	0	1	9	4	5	14	0	18	549	7	4	574	0	13	350	237	0	600	1638
17:00	0	109	16	17	0	142	0	0	0	0	0	0	0	5	125	0	0	130	0	5	150	65	0	220	492
17:15	0	109	2	6	1	117	0	0	2	0	1	2	0	7	121	1	0	129	0	0	110	52	0	162	410
17:30	0	94	9	7	0	110	0	1	1	0	0	2	0	4	134	2	0	140	0	3	122	50	0	175	427
17:45	0	99	4	4	0	107	0	0	2	0	0	2	0	4	126	2	0	132	0	0	105	68	0	173	414
Hourly Total	0	411	31	34	1	476	0	1	5	0	1	6	0	20	506	5	0	531	0	8	487	235	0	730	1743
18:00	0	68	2	9	1	79	0	0	0	0	1	0	0	2	123	3	1	128	0	1	91	50	0	142	349
18:15	0	74	2	1	0	77	0	0	0	1	0	1	0	9	82	2	0	93	0	0	93	57	1	150	321
18:30	0	64	0	3	1	67	0	0	2	2	1	4	0	8	97	0	0	105	0	3	73	45	0	121	297
18:45	0	62	1	3	0	66	0	0	0	0	1	0	0	4	87	1	0	92	0	1	71	48	0	120	278
Hourly Total	0	268	5	16	2	289	0	0	2	3	3	5	0	23	389	6	1	418	0	5	328	200	1	533	1245
Grand Total	0	2385	202	186	11	2773	0	15	51	46	23	112	0	353	3423	78	7	3854	0	62	3512	2169	8	5743	12482
Approach %	0.0	86.0	7.3	6.7	-	-	0.0	13.4	45.5	41.1	-	-	0.0	9.2	88.8	2.0	-	-	0.0	1.1	61.2	37.8	-	-	-
Total %	0.0	19.1	1.6	1.5	-	22.2	0.0	0.1	0.4	0.4	-	0.9	0.0	2.8	27.4	0.6	-	30.9	0.0	0.5	28.1	17.4	-	46.0	-
Lights	0	2372	201	185	7	2758	0	15	51	43	10	109	0	292	3402	78	4	3772	0	62	3468	2118	4	5648	12287
% Lights	-	99.5	99.5	99.5	63.6	99.5	-	100.0	100.0	93.5	43.5	97.3	-	82.7	99.4	100.0	57.1	97.9	-	100.0	98.7	97.6	50.0	98.3	98.4
Other Vehicles	0	13	0	1	4	14	0	0	0	2	13	2	0	61	20	0	3	81	0	0	44	51	4	95	192
% Other Vehicles	-	0.5	0.0	0.5	36.4	0.5	-	0.0	0.0	4.3	56.5	1.8	-	17.3	0.6	0.0	42.9	2.1	-	0.0	1.3	2.4	50.0	1.7	1.5
Bicycles on Road	0	0	1	0	0	1	0	0	0	1	0	1	0	0	1	0	0	1	0	0	0	0	0	0	3
% Bicycles on Road	-	0.0	0.5	0.0	0.0	0.0	-	0.0	0.0	2.2	0.0	0.9	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:30)

Start Time	8th Avenue W Eastbound						8th Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:30	0	62	8	7	0	77	0	1	2	5	0	8	0	31	94	3	1	128	0	3	173	115	0	291	504
7:45	0	50	9	5	0	64	0	0	6	0	1	6	0	31	128	2	0	161	0	0	193	111	0	304	535
8:00	0	64	6	3	1	73	0	1	2	1	0	4	0	31	112	1	0	144	0	0	174	115	0	289	510
8:15	0	66	4	8	0	78	0	0	0	2	2	2	0	23	105	1	0	129	0	0	146	129	0	275	484
<b>Total</b>	0	242	27	23	1	292	0	2	10	8	3	20	0	116	439	7	1	562	0	3	686	470	0	1159	2033
Approach %	0.0	82.9	9.2	7.9	-	-	0.0	10.0	50.0	40.0	-	-	0.0	20.6	78.1	1.2	-	-	0.0	0.3	59.2	40.6	-	-	-
Total %	0.0	11.9	1.3	1.1	-	14.4	0.0	0.1	0.5	0.4	-	1.0	0.0	5.7	21.6	0.3	-	27.6	0.0	0.1	33.7	23.1	-	57.0	-
PHF	0.000	0.917	0.750	0.719	-	0.936	0.000	0.500	0.417	0.400	-	0.625	0.000	0.935	0.857	0.583	-	0.873	0.000	0.250	0.889	0.911	-	0.953	0.950
Lights	0	240	27	23	0	290	0	2	10	8	2	20	0	107	435	7	0	549	0	3	676	459	0	1138	1997
% Lights	-	99.2	100.0	100.0	0.0	99.3	-	100.0	100.0	100.0	66.7	100.0	-	92.2	99.1	100.0	0.0	97.7	-	100.0	98.5	97.7	-	98.2	98.2
Other Vehicles	0	2	0	0	1	2	0	0	0	0	1	0	0	9	3	0	1	12	0	0	10	11	0	21	35
% Other Vehicles	-	0.8	0.0	0.0	100.0	0.7	-	0.0	0.0	0.0	33.3	0.0	-	7.8	0.7	0.0	100.0	2.1	-	0.0	1.5	2.3	-	1.8	1.7
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.2	0.0	0.0	0.2	-	0.0	0.0	0.0	-	0.0	0.0



Turning Movement Peak Hour Data Plot (7:30)

### Turning Movement Peak Hour Data (12:00)

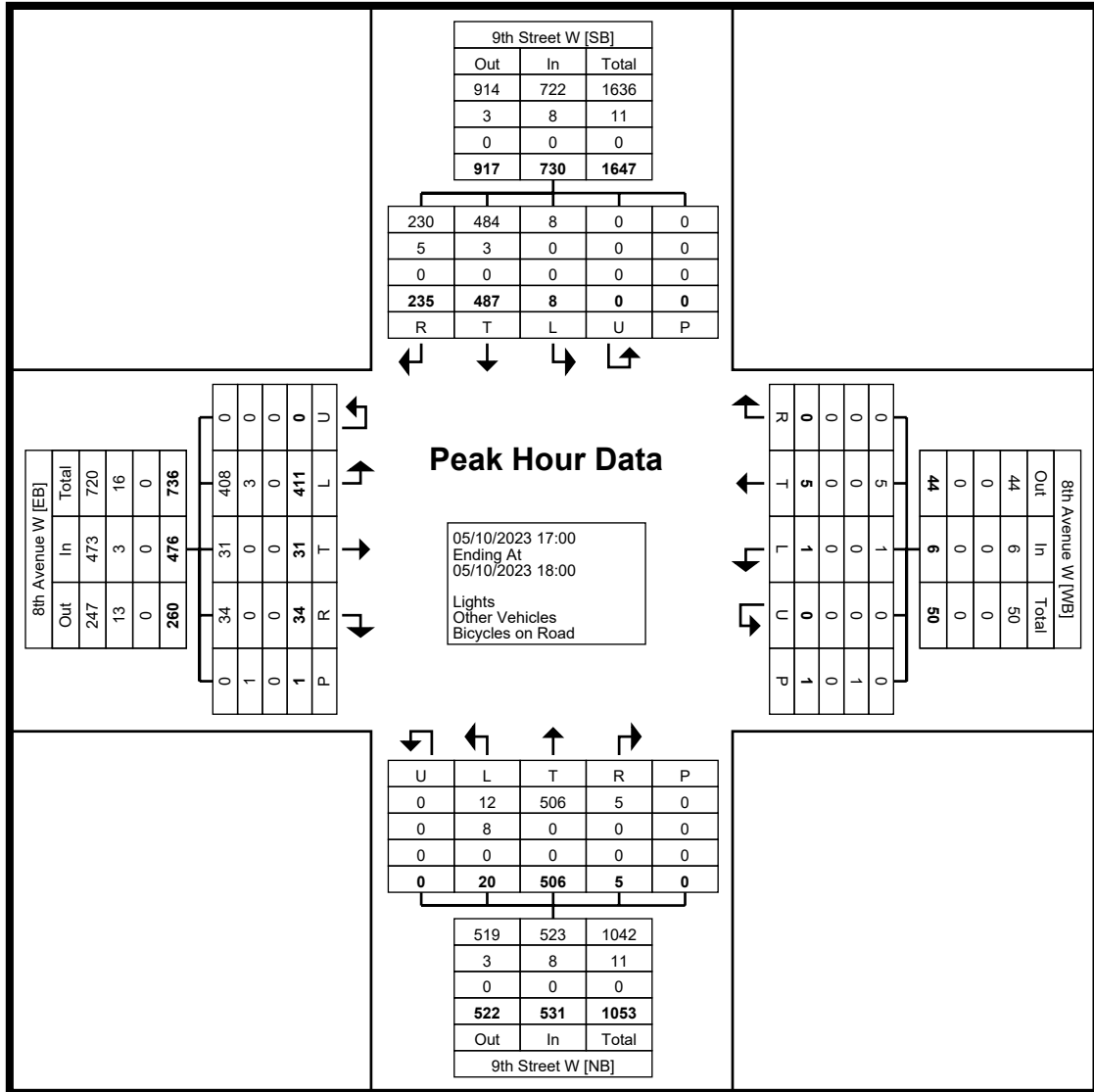
Start Time	8th Avenue W Eastbound						8th Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	76	10	8	0	94	0	1	2	3	3	6	0	10	77	6	0	93	0	2	80	59	1	141	334
12:15	0	68	5	10	1	83	0	1	1	3	1	5	0	8	98	5	0	111	0	6	78	53	1	137	336
12:30	0	67	6	3	1	76	0	1	1	2	0	4	0	12	98	3	0	113	0	2	83	55	0	140	333
12:45	0	58	3	6	0	67	0	1	3	5	0	9	0	12	91	2	0	105	0	2	75	47	0	124	305
<b>Total</b>	<b>0</b>	<b>269</b>	<b>24</b>	<b>27</b>	<b>2</b>	<b>320</b>	<b>0</b>	<b>4</b>	<b>7</b>	<b>13</b>	<b>4</b>	<b>24</b>	<b>0</b>	<b>42</b>	<b>364</b>	<b>16</b>	<b>0</b>	<b>422</b>	<b>0</b>	<b>12</b>	<b>316</b>	<b>214</b>	<b>2</b>	<b>542</b>	<b>1308</b>
Approach %	0.0	84.1	7.5	8.4	-	-	0.0	16.7	29.2	54.2	-	-	0.0	10.0	86.3	3.8	-	-	0.0	2.2	58.3	39.5	-	-	-
Total %	0.0	20.6	1.8	2.1	-	24.5	0.0	0.3	0.5	1.0	-	1.8	0.0	3.2	27.8	1.2	-	32.3	0.0	0.9	24.2	16.4	-	41.4	-
PHF	0.000	0.885	0.600	0.675	-	0.851	0.000	1.000	0.583	0.650	-	0.667	0.000	0.875	0.929	0.667	-	0.934	0.000	0.500	0.952	0.907	-	0.961	0.973
Lights	0	268	23	27	2	318	0	4	7	12	2	23	0	36	360	16	0	412	0	12	312	206	0	530	1283
% Lights	-	99.6	95.8	100.0	100.0	99.4	-	100.0	100.0	92.3	50.0	95.8	-	85.7	98.9	100.0	-	97.6	-	100.0	98.7	96.3	0.0	97.8	98.1
Other Vehicles	0	1	0	0	0	1	0	0	0	0	2	0	0	6	4	0	0	10	0	0	4	8	2	12	23
% Other Vehicles	-	0.4	0.0	0.0	0.0	0.3	-	0.0	0.0	0.0	50.0	0.0	-	14.3	1.1	0.0	-	2.4	-	0.0	1.3	3.7	100.0	2.2	1.8
Bicycles on Road	0	0	1	0	0	1	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2
% Bicycles on Road	-	0.0	4.2	0.0	0.0	0.3	-	0.0	0.0	7.7	0.0	4.2	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	0.0	0.0	0.2





### Turning Movement Peak Hour Data (17:00)

Start Time	8th Avenue W Eastbound						8th Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
17:00	0	109	16	17	0	142	0	0	0	0	0	0	0	5	125	0	0	130	0	5	150	65	0	220	492
17:15	0	109	2	6	1	117	0	0	2	0	1	2	0	7	121	1	0	129	0	0	110	52	0	162	410
17:30	0	94	9	7	0	110	0	1	1	0	0	2	0	4	134	2	0	140	0	3	122	50	0	175	427
17:45	0	99	4	4	0	107	0	0	2	0	0	2	0	4	126	2	0	132	0	0	105	68	0	173	414
<b>Total</b>	<b>0</b>	<b>411</b>	<b>31</b>	<b>34</b>	<b>1</b>	<b>476</b>	<b>0</b>	<b>1</b>	<b>5</b>	<b>0</b>	<b>1</b>	<b>6</b>	<b>0</b>	<b>20</b>	<b>506</b>	<b>5</b>	<b>0</b>	<b>531</b>	<b>0</b>	<b>8</b>	<b>487</b>	<b>235</b>	<b>0</b>	<b>730</b>	<b>1743</b>
Approach %	0.0	86.3	6.5	7.1	-	-	0.0	16.7	83.3	0.0	-	-	0.0	3.8	95.3	0.9	-	-	0.0	1.1	66.7	32.2	-	-	-
Total %	0.0	23.6	1.8	2.0	-	27.3	0.0	0.1	0.3	0.0	-	0.3	0.0	1.1	29.0	0.3	-	30.5	0.0	0.5	27.9	13.5	-	41.9	-
PHF	0.000	0.943	0.484	0.500	-	0.838	0.000	0.250	0.625	0.000	-	0.750	0.000	0.714	0.944	0.625	-	0.948	0.000	0.400	0.812	0.864	-	0.830	0.886
Lights	0	408	31	34	0	473	0	1	5	0	0	6	0	12	506	5	0	523	0	8	484	230	0	722	1724
% Lights	-	99.3	100.0	100.0	0.0	99.4	-	100.0	100.0	-	0.0	100.0	-	60.0	100.0	100.0	-	98.5	-	100.0	99.4	97.9	-	98.9	98.9
Other Vehicles	0	3	0	0	1	3	0	0	0	0	1	0	0	8	0	0	0	8	0	0	3	5	0	8	19
% Other Vehicles	-	0.7	0.0	0.0	100.0	0.6	-	0.0	0.0	-	100.0	0.0	-	40.0	0.0	0.0	-	1.5	-	0.0	0.6	2.1	-	1.1	1.1
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	-	0.0	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0



Turning Movement Peak Hour Data Plot (17:00)





### Turning Movement Peak Hour Data (7:15)

Start Time	Driveway Eastbound						7th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15	0	0	0	0	1	0	0	0	0	0	0	0	0	0	136	2	0	138	0	1	289	0	0	290	428
7:30	0	0	0	0	0	0	0	0	0	1	0	1	0	0	163	1	0	164	0	2	302	1	0	305	470
7:45	0	0	0	0	0	0	0	0	0	2	1	2	0	0	179	0	0	179	0	1	274	2	0	277	458
8:00	0	0	0	0	0	0	0	0	0	1	0	1	0	0	171	0	0	171	0	1	275	1	0	277	449
<b>Total</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>4</b>	<b>1</b>	<b>4</b>	<b>0</b>	<b>0</b>	<b>649</b>	<b>3</b>	<b>0</b>	<b>652</b>	<b>0</b>	<b>5</b>	<b>1140</b>	<b>4</b>	<b>0</b>	<b>1149</b>	<b>1805</b>
Approach %	0.0	0.0	0.0	0.0	-	-	0.0	0.0	0.0	100.0	-	-	0.0	0.0	99.5	0.5	-	-	0.0	0.4	99.2	0.3	-	-	-
Total %	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.2	-	0.2	0.0	0.0	36.0	0.2	-	36.1	0.0	0.3	63.2	0.2	-	63.7	-
PHF	0.000	0.000	0.000	0.000	-	0.000	0.000	0.000	0.000	0.500	-	0.500	0.000	0.000	0.906	0.375	-	0.911	0.000	0.625	0.944	0.500	-	0.942	0.960
Lights	0	0	0	0	-	0	0	0	0	3	-	3	0	0	628	3	-	631	0	5	1107	4	-	1116	1750
% Lights	-	-	-	-	-	-	-	-	-	75.0	-	75.0	-	-	96.8	100.0	-	96.8	-	100.0	97.1	100.0	-	97.1	97.0
Other Vehicles	0	0	0	0	-	0	0	0	0	1	-	1	0	0	19	0	-	19	0	0	33	0	-	33	53
% Other Vehicles	-	-	-	-	-	-	-	-	-	25.0	-	25.0	-	-	2.9	0.0	-	2.9	-	0.0	2.9	0.0	-	2.9	2.9
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	2	0	-	2	0	0	0	0	-	0	2
% Bicycles on Road	-	-	-	-	-	-	-	-	-	0.0	-	0.0	-	-	0.3	0.0	-	0.3	-	0.0	0.0	0.0	-	0.0	0.1
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	0.0	-	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	100.0	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-



### Turning Movement Peak Hour Data (12:00)

Start Time	Driveway Eastbound						7th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	0	0	1	0	1	0	0	0	4	0	4	0	0	160	2	0	162	0	1	148	0	0	149	316
12:15	0	1	0	1	1	2	0	2	0	0	0	2	0	0	172	0	0	172	0	2	123	1	0	126	302
12:30	0	0	0	0	1	0	0	0	0	0	0	0	0	0	159	1	0	160	0	1	133	0	0	134	294
12:45	0	0	0	0	0	0	0	1	0	3	0	4	0	1	155	2	0	158	0	1	122	0	0	123	285
<b>Total</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>2</b>	<b>2</b>	<b>3</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>7</b>	<b>0</b>	<b>10</b>	<b>0</b>	<b>1</b>	<b>646</b>	<b>5</b>	<b>0</b>	<b>652</b>	<b>0</b>	<b>5</b>	<b>526</b>	<b>1</b>	<b>0</b>	<b>532</b>	<b>1197</b>
Approach %	0.0	33.3	0.0	66.7	-	-	0.0	30.0	0.0	70.0	-	-	0.0	0.2	99.1	0.8	-	-	0.0	0.9	98.9	0.2	-	-	-
Total %	0.0	0.1	0.0	0.2	-	0.3	0.0	0.3	0.0	0.6	-	0.8	0.0	0.1	54.0	0.4	-	54.5	0.0	0.4	43.9	0.1	-	44.4	-
PHF	0.000	0.250	0.000	0.500	-	0.375	0.000	0.375	0.000	0.438	-	0.625	0.000	0.250	0.939	0.625	-	0.948	0.000	0.625	0.889	0.250	-	0.893	0.947
Lights	0	0	0	2	-	2	0	3	0	7	-	10	0	1	630	4	-	635	0	5	507	1	-	513	1160
% Lights	-	0.0	-	100.0	-	66.7	-	100.0	-	100.0	-	100.0	-	100.0	97.5	80.0	-	97.4	-	100.0	96.4	100.0	-	96.4	96.9
Other Vehicles	0	1	0	0	-	1	0	0	0	0	-	0	0	0	14	1	-	15	0	0	18	0	-	18	34
% Other Vehicles	-	100.0	-	0.0	-	33.3	-	0.0	-	0.0	-	0.0	-	0.0	2.2	20.0	-	2.3	-	0.0	3.4	0.0	-	3.4	2.8
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	2	0	-	2	0	0	1	0	-	1	3
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.3	0.0	-	0.3	-	0.0	0.2	0.0	-	0.2	0.3
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-





### Turning Movement Peak Hour Data (17:00)

Start Time	Driveway Eastbound						7th Ave W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
17:00	0	0	0	1	0	1	0	0	0	7	0	7	0	0	220	2	0	222	0	1	210	2	0	213	443
17:15	0	0	0	0	2	0	0	0	0	3	0	3	0	0	239	0	0	239	0	0	175	0	0	175	417
17:30	0	0	0	0	0	0	0	0	0	7	0	7	0	0	233	0	0	233	0	0	174	0	0	174	414
17:45	0	0	0	0	0	0	0	0	0	3	0	3	0	0	214	0	0	214	0	0	172	0	0	172	389
<b>Total</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>2</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>20</b>	<b>0</b>	<b>20</b>	<b>0</b>	<b>0</b>	<b>906</b>	<b>2</b>	<b>0</b>	<b>908</b>	<b>0</b>	<b>1</b>	<b>731</b>	<b>2</b>	<b>0</b>	<b>734</b>	<b>1663</b>
Approach %	0.0	0.0	0.0	100.0	-	-	0.0	0.0	0.0	100.0	-	-	0.0	0.0	99.8	0.2	-	-	0.0	0.1	99.6	0.3	-	-	-
Total %	0.0	0.0	0.0	0.1	-	0.1	0.0	0.0	0.0	1.2	-	1.2	0.0	0.0	54.5	0.1	-	54.6	0.0	0.1	44.0	0.1	-	44.1	-
PHF	0.000	0.000	0.000	0.250	-	0.250	0.000	0.000	0.000	0.714	-	0.714	0.000	0.000	0.948	0.250	-	0.950	0.000	0.250	0.870	0.250	-	0.862	0.938
Lights	0	0	0	1	-	1	0	0	0	20	-	20	0	0	899	2	-	901	0	1	715	2	-	718	1640
% Lights	-	-	-	100.0	-	100.0	-	-	-	100.0	-	100.0	-	-	99.2	100.0	-	99.2	-	100.0	97.8	100.0	-	97.8	98.6
Other Vehicles	0	0	0	0	-	0	0	0	0	0	-	0	0	0	7	0	-	7	0	0	16	0	-	16	23
% Other Vehicles	-	-	-	0.0	-	0.0	-	-	-	0.0	-	0.0	-	-	0.8	0.0	-	0.8	-	0.0	2.2	0.0	-	2.2	1.4
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	-	-	0.0	-	0.0	-	-	-	0.0	-	0.0	-	-	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-



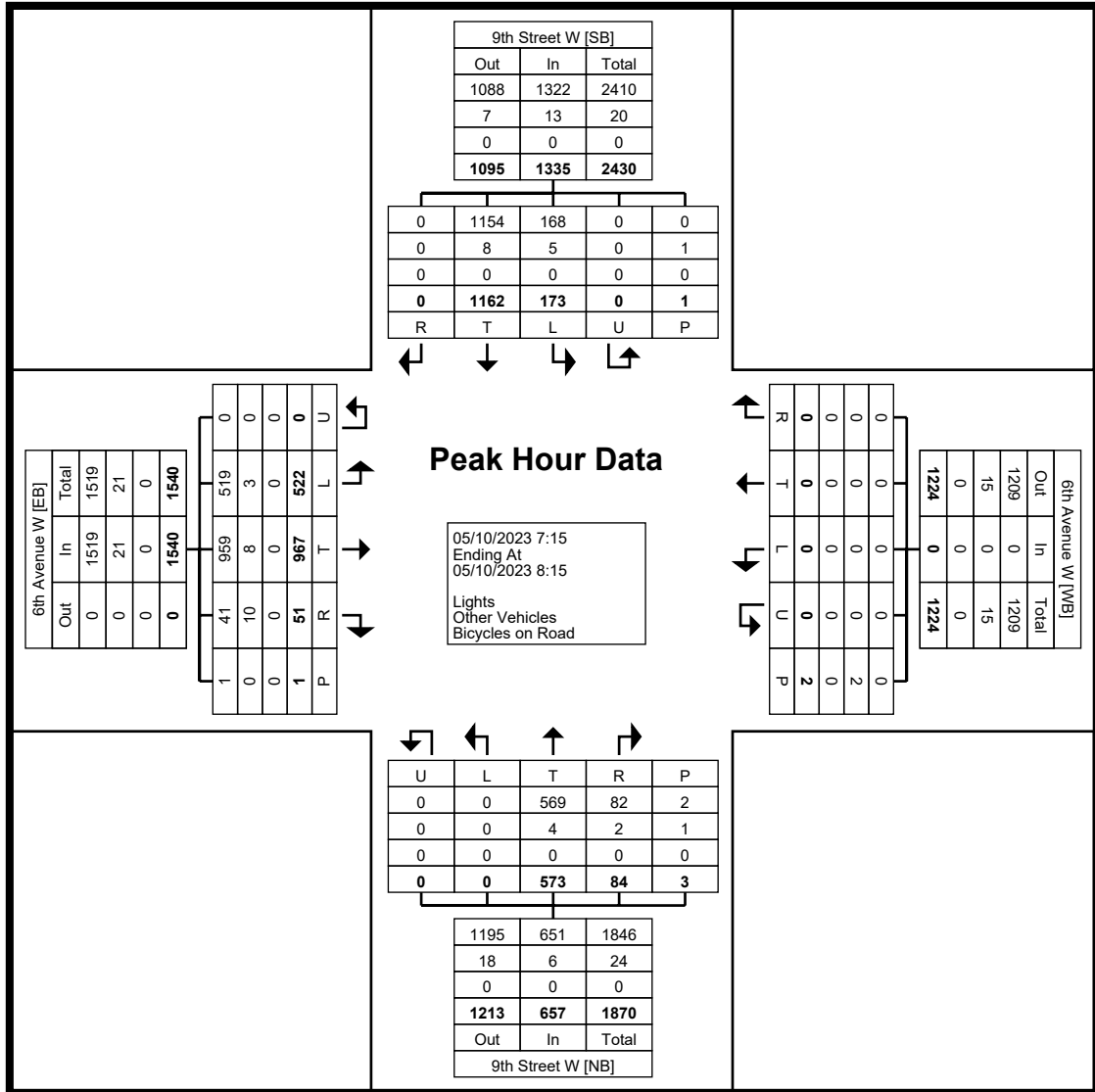
### Turning Movement Data

Start Time	6th Avenue W Eastbound						6th Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	107	222	7	0	336	0	0	0	0	1	0	0	0	109	17	0	126	0	44	246	0	0	290	752
7:15	0	108	243	5	0	356	0	0	0	0	1	0	0	0	120	23	1	143	0	59	303	0	0	362	861
7:30	0	137	257	13	1	407	0	0	0	0	0	0	0	0	140	23	1	163	0	40	271	0	0	311	881
7:45	0	149	247	17	0	413	0	0	0	0	0	0	0	0	154	14	0	168	0	32	273	0	1	305	886
Hourly Total	0	501	969	42	1	1512	0	0	0	0	2	0	0	0	523	77	2	600	0	175	1093	0	1	1268	3380
8:00	0	128	220	16	0	364	0	0	0	0	1	0	0	0	159	24	1	183	0	42	315	0	0	357	904
8:15	0	127	245	12	0	384	0	0	0	0	2	0	0	0	151	16	0	167	0	44	234	0	0	278	829
8:30	0	110	196	6	0	312	0	0	0	0	0	0	0	0	110	21	0	131	0	40	251	0	0	291	734
8:45	0	115	187	15	1	317	0	0	0	0	1	0	0	0	130	13	1	143	0	28	202	0	0	230	690
Hourly Total	0	480	848	49	1	1377	0	0	0	0	4	0	0	0	550	74	2	624	0	154	1002	0	0	1156	3157
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	134	202	10	0	346	0	0	0	0	2	0	0	0	124	25	0	149	0	35	128	0	1	163	658
12:15	0	114	193	10	1	317	0	0	0	0	0	0	0	0	148	34	0	182	0	37	138	0	2	175	674
12:30	0	114	229	8	2	351	0	0	0	0	0	0	0	0	119	23	2	142	0	35	116	0	2	151	644
12:45	0	111	182	5	0	298	0	0	0	0	0	0	0	0	152	27	0	179	0	32	135	0	0	167	644
Hourly Total	0	473	806	33	3	1312	0	0	0	0	2	0	0	0	543	109	2	652	0	139	517	0	5	656	2620
13:00	0	96	230	12	0	338	0	0	0	0	1	0	0	0	131	19	0	150	0	36	116	0	0	152	640
13:15	0	130	232	11	1	373	0	0	0	0	0	0	0	0	154	20	1	174	0	31	134	0	1	165	712
13:30	0	132	241	14	1	387	0	0	0	0	1	0	0	0	133	12	0	145	1	30	121	0	0	152	684
13:45	0	135	213	12	0	360	0	0	0	0	0	0	0	0	133	13	1	146	0	33	121	0	0	154	660
Hourly Total	0	493	916	49	2	1458	0	0	0	0	2	0	0	0	551	64	2	615	1	130	492	0	1	623	2696
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	157	310	11	1	478	0	0	0	0	0	0	0	0	169	25	0	194	0	33	133	0	2	166	838
15:15	0	171	296	13	0	480	0	0	0	0	3	0	0	0	162	14	0	176	0	24	150	0	0	174	830
15:30	0	152	260	9	0	421	0	0	0	0	0	0	0	0	217	14	0	231	0	36	162	0	0	198	850
15:45	0	142	282	10	0	434	0	0	0	0	0	0	0	0	220	7	0	227	0	32	143	0	0	175	836
Hourly Total	0	622	1148	43	1	1813	0	0	0	0	3	0	0	0	768	60	0	828	0	125	588	0	2	713	3354
16:00	0	163	309	11	0	483	0	0	0	0	1	0	0	0	196	9	0	205	0	32	136	0	0	168	856
16:15	0	151	296	10	0	457	0	0	0	0	0	0	0	0	213	11	0	224	0	34	130	0	1	164	845
16:30	0	120	272	10	0	402	0	0	0	0	3	0	0	0	262	9	0	271	0	35	167	0	0	202	875
16:45	0	146	257	11	1	414	0	0	0	0	2	0	0	0	222	12	0	234	0	34	124	0	0	158	806
Hourly Total	0	580	1134	42	1	1756	0	0	0	0	6	0	0	0	893	41	0	934	0	135	557	0	1	692	3382
17:00	0	151	325	12	0	488	0	0	0	0	0	0	0	0	200	11	0	211	0	36	185	0	0	221	920
17:15	0	129	307	13	2	449	0	0	0	0	1	0	0	0	234	5	0	239	0	26	164	0	1	190	878
17:30	0	131	245	7	0	383	0	0	0	0	0	0	0	0	244	8	0	252	0	27	173	0	0	200	835
17:45	0	148	256	14	0	418	0	0	0	0	0	0	0	0	210	8	0	218	0	35	158	0	0	193	829
Hourly Total	0	559	1133	46	2	1738	0	0	0	0	1	0	0	0	888	32	0	920	0	124	680	0	1	804	3462
18:00	0	126	191	12	0	329	0	0	0	0	1	0	0	0	185	12	0	197	0	28	145	0	0	173	699
18:15	0	125	173	9	0	307	0	0	0	0	0	0	0	0	154	19	1	173	0	31	119	0	0	150	630
18:30	0	112	180	10	0	302	0	0	0	0	0	0	0	0	141	14	0	155	0	27	132	0	0	159	616
18:45	0	107	155	5	0	267	0	0	0	0	0	0	0	0	140	11	0	151	0	14	103	0	0	117	535
Hourly Total	0	470	699	36	0	1205	0	0	0	0	1	0	0	0	620	56	1	676	0	100	499	0	0	599	2480
Grand Total	0	4178	7653	340	11	12171	0	0	0	0	21	0	0	0	5336	513	9	5849	1	1082	5428	0	11	6511	24531
Approach %	0.0	34.3	62.9	2.8	-	-	0.0	0.0	0.0	0.0	-	-	0.0	0.0	91.2	8.8	-	-	0.0	16.6	83.4	0.0	-	-	-
Total %	0.0	17.0	31.2	1.4	-	49.6	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	21.8	2.1	-	23.8	0.0	4.4	22.1	0.0	-	26.5	-
Lights	0	4142	7576	278	5	11996	0	0	0	0	10	0	0	0	5312	507	3	5819	1	1072	5392	0	4	6465	24280
% Lights	-	99.1	99.0	81.8	45.5	98.6	-	-	-	-	47.6	-	-	-	99.6	98.8	33.3	99.5	100.0	99.1	99.3	-	36.4	99.3	99.0
Other Vehicles	0	34	77	62	6	173	0	0	0	0	11	0	0	0	24	6	6	30	0	10	36	0	7	46	249
% Other Vehicles	-	0.8	1.0	18.2	54.5	1.4	-	-	-	-	52.4	-	-	-	0.4	1.2	66.7	0.5	0.0	0.9	0.7	-	63.6	0.7	1.0
Bicycles on Road	0	2	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	-	-	-	0.0	-	-	-	0.0	0.0	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0



### Turning Movement Peak Hour Data (7:15)

Start Time	6th Avenue W Eastbound						6th Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15	0	108	243	5	0	356	0	0	0	0	1	0	0	0	120	23	1	143	0	59	303	0	0	362	861
7:30	0	137	257	13	1	407	0	0	0	0	0	0	0	0	140	23	1	163	0	40	271	0	0	311	881
7:45	0	149	247	17	0	413	0	0	0	0	0	0	0	0	154	14	0	168	0	32	273	0	1	305	886
8:00	0	128	220	16	0	364	0	0	0	0	1	0	0	0	159	24	1	183	0	42	315	0	0	357	904
<b>Total</b>	<b>0</b>	<b>522</b>	<b>967</b>	<b>51</b>	<b>1</b>	<b>1540</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>573</b>	<b>84</b>	<b>3</b>	<b>657</b>	<b>0</b>	<b>173</b>	<b>1162</b>	<b>0</b>	<b>1</b>	<b>1335</b>	<b>3532</b>
Approach %	0.0	33.9	62.8	3.3	-	-	0.0	0.0	0.0	0.0	-	-	0.0	0.0	87.2	12.8	-	-	0.0	13.0	87.0	0.0	-	-	-
Total %	0.0	14.8	27.4	1.4	-	43.6	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	16.2	2.4	-	18.6	0.0	4.9	32.9	0.0	-	37.8	-
PHF	0.000	0.876	0.941	0.750	-	0.932	0.000	0.000	0.000	0.000	-	0.000	0.000	0.000	0.901	0.875	-	0.898	0.000	0.733	0.922	0.000	-	0.922	0.977
Lights	0	519	959	41	1	1519	0	0	0	0	0	0	0	0	569	82	2	651	0	168	1154	0	0	1322	3492
% Lights	-	99.4	99.2	80.4	100.0	98.6	-	-	-	-	0.0	-	-	-	99.3	97.6	66.7	99.1	-	97.1	99.3	-	0.0	99.0	98.9
Other Vehicles	0	3	8	10	0	21	0	0	0	0	2	0	0	0	4	2	1	6	0	5	8	0	1	13	40
% Other Vehicles	-	0.6	0.8	19.6	0.0	1.4	-	-	-	-	100.0	-	-	-	0.7	2.4	33.3	0.9	-	2.9	0.7	-	100.0	1.0	1.1
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	-	-	-	0.0	-	-	-	0.0	0.0	0.0	0.0	-	0.0	0.0	-	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (7:15)

### Turning Movement Peak Hour Data (13:00)

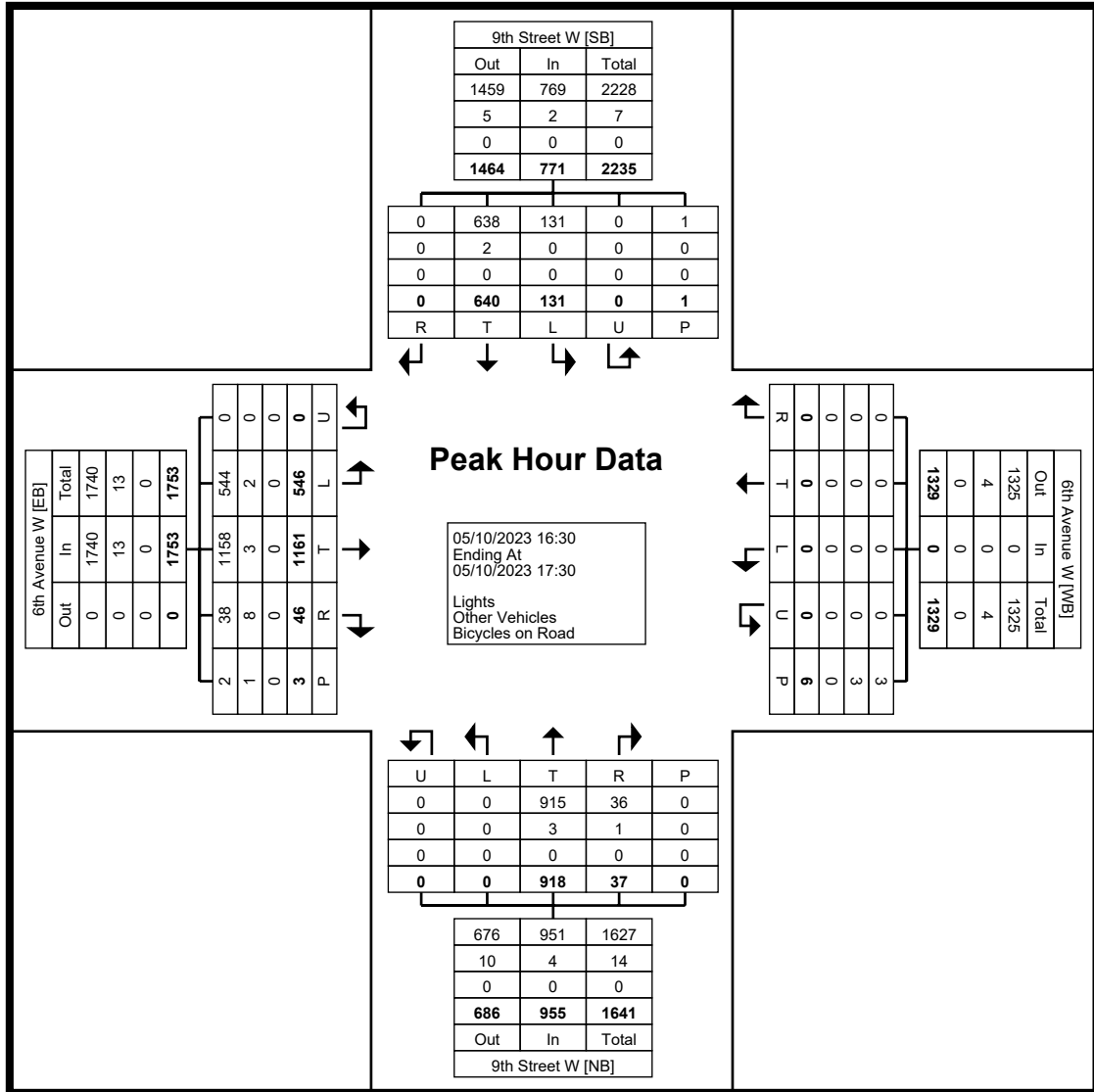
Start Time	6th Avenue W Eastbound						6th Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
13:00	0	96	230	12	0	338	0	0	0	0	1	0	0	0	131	19	0	150	0	36	116	0	0	152	640
13:15	0	130	232	11	1	373	0	0	0	0	0	0	0	0	154	20	1	174	0	31	134	0	1	165	712
13:30	0	132	241	14	1	387	0	0	0	0	1	0	0	0	133	12	0	145	1	30	121	0	0	152	684
13:45	0	135	213	12	0	360	0	0	0	0	0	0	0	0	133	13	1	146	0	33	121	0	0	154	660
<b>Total</b>	<b>0</b>	<b>493</b>	<b>916</b>	<b>49</b>	<b>2</b>	<b>1458</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>551</b>	<b>64</b>	<b>2</b>	<b>615</b>	<b>1</b>	<b>130</b>	<b>492</b>	<b>0</b>	<b>1</b>	<b>623</b>	<b>2696</b>
Approach %	0.0	33.8	62.8	3.4	-	-	0.0	0.0	0.0	0.0	-	-	0.0	0.0	89.6	10.4	-	-	0.2	20.9	79.0	0.0	-	-	-
Total %	0.0	18.3	34.0	1.8	-	54.1	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	20.4	2.4	-	22.8	0.0	4.8	18.2	0.0	-	23.1	-
PHF	0.000	0.913	0.950	0.875	-	0.942	0.000	0.000	0.000	0.000	-	0.000	0.000	0.000	0.894	0.800	-	0.884	0.250	0.903	0.918	0.000	-	0.944	0.947
Lights	0	489	896	42	0	1427	0	0	0	0	2	0	0	0	547	64	0	611	1	128	489	0	0	618	2656
% Lights	-	99.2	97.8	85.7	0.0	97.9	-	-	-	-	100.0	-	-	-	99.3	100.0	0.0	99.3	100.0	98.5	99.4	-	0.0	99.2	98.5
Other Vehicles	0	4	20	7	2	31	0	0	0	0	0	0	0	0	4	0	2	4	0	2	3	0	1	5	40
% Other Vehicles	-	0.8	2.2	14.3	100.0	2.1	-	-	-	-	0.0	-	-	-	0.7	0.0	100.0	0.7	0.0	1.5	0.6	-	100.0	0.8	1.5
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	-	-	-	0.0	-	-	-	0.0	0.0	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0





### Turning Movement Peak Hour Data (16:30)

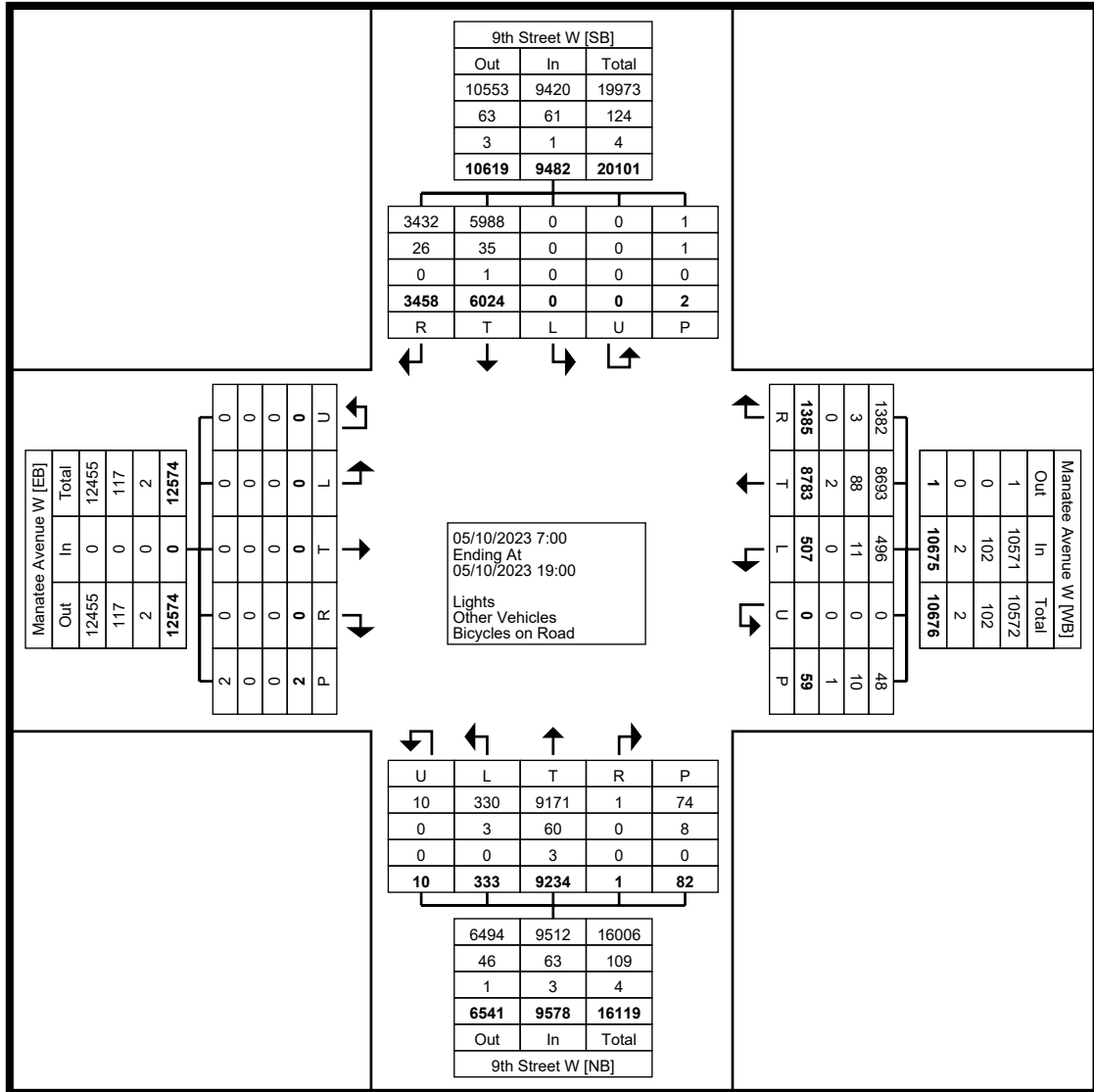
Start Time	6th Avenue W Eastbound						6th Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:30	0	120	272	10	0	402	0	0	0	0	3	0	0	0	262	9	0	271	0	35	167	0	0	202	875
16:45	0	146	257	11	1	414	0	0	0	0	2	0	0	0	222	12	0	234	0	34	124	0	0	158	806
17:00	0	151	325	12	0	488	0	0	0	0	0	0	0	0	200	11	0	211	0	36	185	0	0	221	920
17:15	0	129	307	13	2	449	0	0	0	0	1	0	0	0	234	5	0	239	0	26	164	0	1	190	878
<b>Total</b>	<b>0</b>	<b>546</b>	<b>1161</b>	<b>46</b>	<b>3</b>	<b>1753</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>6</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>918</b>	<b>37</b>	<b>0</b>	<b>955</b>	<b>0</b>	<b>131</b>	<b>640</b>	<b>0</b>	<b>1</b>	<b>771</b>	<b>3479</b>
Approach %	0.0	31.1	66.2	2.6	-	-	0.0	0.0	0.0	0.0	-	-	0.0	0.0	96.1	3.9	-	-	0.0	17.0	83.0	0.0	-	-	-
Total %	0.0	15.7	33.4	1.3	-	50.4	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	26.4	1.1	-	27.5	0.0	3.8	18.4	0.0	-	22.2	-
PHF	0.000	0.904	0.893	0.885	-	0.898	0.000	0.000	0.000	0.000	-	0.000	0.000	0.000	0.876	0.771	-	0.881	0.000	0.910	0.865	0.000	-	0.872	0.945
Lights	0	544	1158	38	2	1740	0	0	0	0	3	0	0	0	915	36	0	951	0	131	638	0	1	769	3460
% Lights	-	99.6	99.7	82.6	66.7	99.3	-	-	-	-	50.0	-	-	-	99.7	97.3	-	99.6	-	100.0	99.7	-	100.0	99.7	99.5
Other Vehicles	0	2	3	8	1	13	0	0	0	0	3	0	0	0	3	1	0	4	0	0	2	0	0	2	19
% Other Vehicles	-	0.4	0.3	17.4	33.3	0.7	-	-	-	-	50.0	-	-	-	0.3	2.7	-	0.4	-	0.0	0.3	-	0.0	0.3	0.5
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	-	-	-	0.0	-	-	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (16:30)

### Turning Movement Data

Start Time	Manatee Avenue W Eastbound						Manatee Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	0	0	0	0	0	0	8	289	19	1	316	0	8	208	0	2	216	0	0	288	160	0	448	980
7:15	0	0	0	0	0	0	0	3	282	22	2	307	0	6	218	0	1	224	0	0	353	160	0	513	1044
7:30	0	0	0	0	0	0	0	7	299	14	3	320	0	8	268	0	0	276	0	0	320	132	0	452	1048
7:45	0	0	0	0	0	0	0	19	303	17	1	339	0	20	264	0	4	284	0	0	277	121	0	398	1021
Hourly Total	0	0	0	0	0	0	0	37	1173	72	7	1282	0	42	958	0	7	1000	0	0	1238	573	0	1811	4093
8:00	0	0	0	0	0	0	0	17	251	18	2	286	0	17	280	0	3	297	0	0	346	129	0	475	1058
8:15	0	0	0	0	0	0	0	12	262	31	2	305	1	14	266	0	6	281	0	0	279	128	0	407	993
8:30	0	0	0	0	0	0	0	19	295	26	1	340	0	17	199	0	11	216	0	0	260	110	0	370	926
8:45	0	0	0	0	0	0	0	14	243	27	1	284	2	17	234	0	2	253	0	0	218	137	0	355	892
Hourly Total	0	0	0	0	0	0	0	62	1051	102	6	1215	3	65	979	0	22	1047	0	0	1103	504	0	1607	3869
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	0	0	0	0	0	17	263	28	4	308	0	22	249	0	5	271	0	0	159	101	0	260	839
12:15	0	0	0	0	0	0	0	17	297	32	2	346	0	8	241	0	1	249	0	0	146	91	0	237	832
12:30	0	0	0	0	1	0	0	14	294	36	7	344	0	12	233	0	5	245	0	0	143	90	0	233	822
12:45	0	0	0	0	0	0	0	19	266	27	5	312	0	10	252	0	7	262	0	0	148	89	0	237	811
Hourly Total	0	0	0	0	1	0	0	67	1120	123	18	1310	0	52	975	0	18	1027	0	0	596	371	0	967	3304
13:00	0	0	0	0	0	0	0	18	286	24	2	328	1	17	199	0	0	217	0	0	127	84	0	211	756
13:15	0	0	0	0	0	0	0	16	250	21	2	287	2	13	281	0	1	296	0	0	159	95	0	254	837
13:30	0	0	0	0	0	0	0	17	271	31	0	319	0	11	244	0	1	255	0	0	126	94	0	220	794
13:45	0	0	0	0	0	0	0	18	267	27	0	312	0	18	264	0	1	282	0	0	149	94	0	243	837
Hourly Total	0	0	0	0	0	0	0	69	1074	103	4	1246	3	59	988	0	3	1050	0	0	561	367	0	928	3224
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	0	0	0	0	0	0	13	297	48	1	358	0	11	310	1	1	322	0	0	140	103	0	243	923
15:15	0	0	0	0	0	0	0	19	293	45	9	357	0	9	318	0	3	327	0	0	154	108	0	262	946
15:30	0	0	0	0	0	0	0	12	257	41	0	310	0	7	376	0	2	383	0	0	192	94	0	286	979
15:45	0	0	0	0	0	0	0	25	294	70	0	389	0	16	358	0	2	374	0	0	141	106	0	247	1010
Hourly Total	0	0	0	0	0	0	0	69	1141	204	10	1414	0	43	1362	1	8	1406	0	0	627	411	0	1038	3858
16:00	0	0	0	0	0	0	0	18	280	64	1	362	0	4	333	0	3	337	0	0	148	98	1	246	945
16:15	0	0	0	0	0	0	0	10	273	82	2	365	1	4	385	0	3	390	0	0	168	124	0	292	1047
16:30	0	0	0	0	0	0	0	15	246	58	2	319	0	5	380	0	1	385	0	0	166	114	0	280	984
16:45	0	0	0	0	1	0	0	22	295	79	4	396	0	5	359	0	2	364	0	0	136	98	0	234	994
Hourly Total	0	0	0	0	1	0	0	65	1094	283	9	1442	1	18	1457	0	9	1476	0	0	618	434	1	1052	3970
17:00	0	0	0	0	0	0	0	22	323	83	1	428	0	3	355	0	7	358	0	0	215	124	0	339	1125
17:15	0	0	0	0	0	0	0	19	254	103	2	376	0	8	371	0	1	379	0	0	168	119	1	287	1042
17:30	0	0	0	0	0	0	0	20	300	83	0	403	1	3	370	0	2	374	0	0	176	115	0	291	1068
17:45	0	0	0	0	0	0	0	18	287	71	0	376	0	12	333	0	1	345	0	0	180	97	0	277	998
Hourly Total	0	0	0	0	0	0	0	79	1164	340	3	1583	1	26	1429	0	11	1456	0	0	739	455	1	1194	4233
18:00	0	0	0	0	0	0	0	20	248	54	0	322	1	9	327	0	0	337	0	0	161	97	0	258	917
18:15	0	0	0	0	0	0	0	16	262	44	1	322	0	8	268	0	3	276	0	0	135	75	0	210	808
18:30	0	0	0	0	0	0	0	13	225	32	0	270	1	5	246	0	1	252	0	0	135	95	0	230	752
18:45	0	0	0	0	0	0	0	10	231	28	1	269	0	6	245	0	0	251	0	0	111	76	0	187	707
Hourly Total	0	0	0	0	0	0	0	59	966	158	2	1183	2	28	1086	0	4	1116	0	0	542	343	0	885	3184
Grand Total	0	0	0	0	2	0	0	507	8783	1385	59	10675	10	333	9234	1	82	9578	0	0	6024	3458	2	9482	29735
Approach %	0.0	0.0	0.0	0.0	-	-	0.0	4.7	82.3	13.0	-	-	0.1	3.5	96.4	0.0	-	-	0.0	0.0	63.5	36.5	-	-	-
Total %	0.0	0.0	0.0	0.0	-	0.0	0.0	1.7	29.5	4.7	-	35.9	0.0	1.1	31.1	0.0	-	32.2	0.0	0.0	20.3	11.6	-	31.9	-
Lights	0	0	0	0	2	0	0	496	8693	1382	48	10571	0	330	9171	1	74	9512	0	0	5988	3432	1	9420	29503
% Lights	-	-	-	-	100.0	-	-	97.8	99.0	99.8	81.4	99.0	100.0	99.1	99.3	100.0	90.2	99.3	-	-	99.4	99.2	50.0	99.3	99.2
Other Vehicles	0	0	0	0	0	0	0	11	88	3	10	102	0	3	60	0	8	63	0	0	35	26	1	61	226
% Other Vehicles	-	-	-	-	0.0	-	-	2.2	1.0	0.2	16.9	1.0	0.0	0.9	0.6	0.0	9.8	0.7	-	-	0.6	0.8	50.0	0.6	0.8
Bicycles on Road	0	0	0	0	0	0	0	0	2	0	1	2	0	0	3	0	0	3	0	0	1	0	0	1	6
% Bicycles on Road	-	-	-	-	0.0	-	-	0.0	0.0	0.0	1.7	0.0	0.0	0.0	0.0	0.0	0.0	0.0	-	-	0.0	0.0	0.0	0.0	0.0



Turning Movement Data Plot

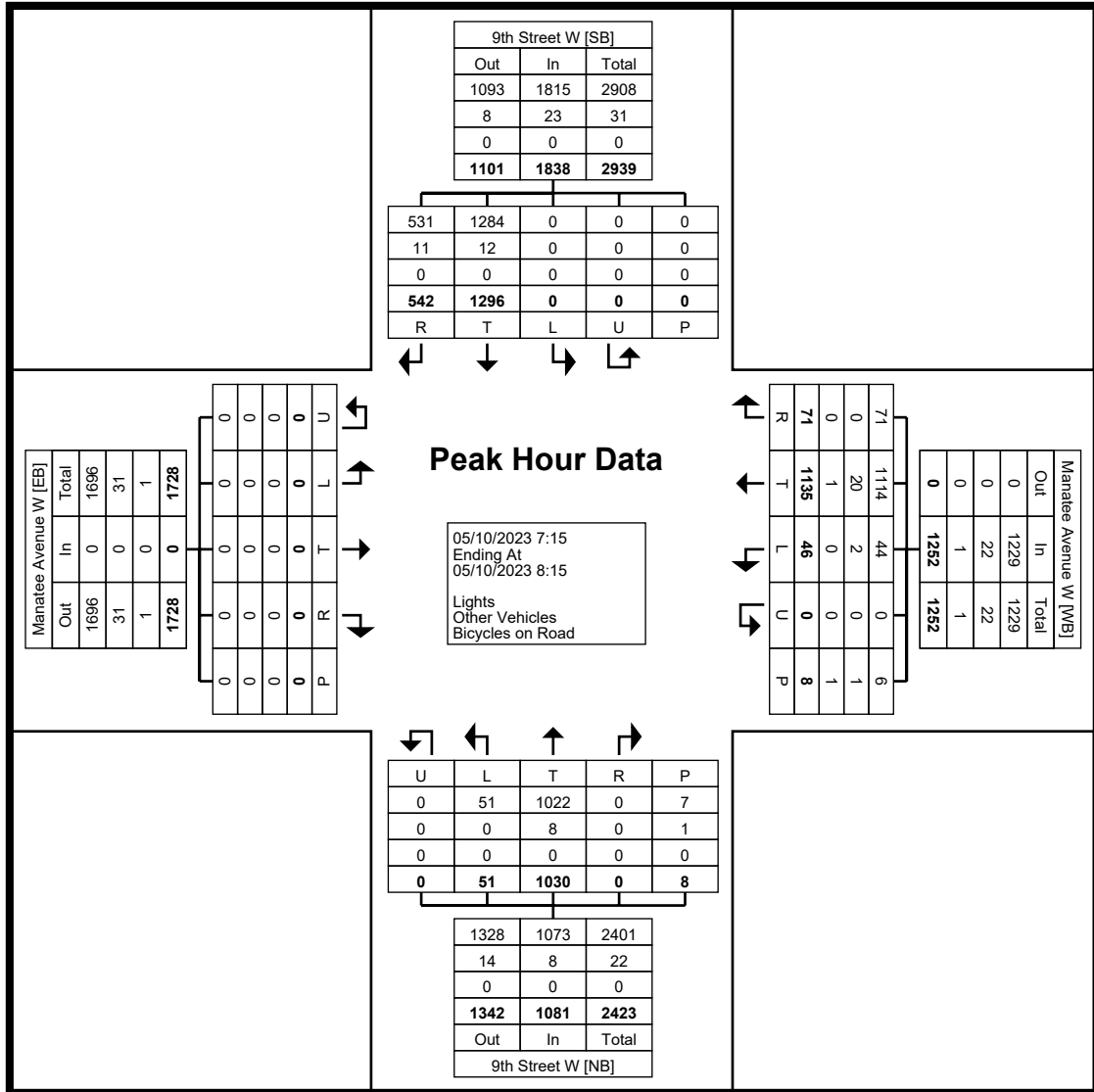
9th Street W and Manatee Avenue W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 11\_9th Street W and Manatee Avenue W  
Site Code: 11  
Start Date: 05/10/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:15)

Start Time	Manatee Avenue W Eastbound						Manatee Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15	0	0	0	0	0	0	0	3	282	22	2	307	0	6	218	0	1	224	0	0	353	160	0	513	1044
7:30	0	0	0	0	0	0	0	7	299	14	3	320	0	8	268	0	0	276	0	0	320	132	0	452	1048
7:45	0	0	0	0	0	0	0	19	303	17	1	339	0	20	264	0	4	284	0	0	277	121	0	398	1021
8:00	0	0	0	0	0	0	0	17	251	18	2	286	0	17	280	0	3	297	0	0	346	129	0	475	1058
<b>Total</b>	0	0	0	0	0	0	0	46	1135	71	8	1252	0	51	1030	0	8	1081	0	0	1296	542	0	1838	4171
Approach %	0.0	0.0	0.0	0.0	-	-	0.0	3.7	90.7	5.7	-	-	0.0	4.7	95.3	0.0	-	-	0.0	0.0	70.5	29.5	-	-	-
Total %	0.0	0.0	0.0	0.0	-	0.0	0.0	1.1	27.2	1.7	-	30.0	0.0	1.2	24.7	0.0	-	25.9	0.0	0.0	31.1	13.0	-	44.1	-
PHF	0.000	0.000	0.000	0.000	-	0.000	0.000	0.605	0.936	0.807	-	0.923	0.000	0.638	0.920	0.000	-	0.910	0.000	0.000	0.918	0.847	-	0.896	0.986
Lights	0	0	0	0	0	0	0	44	1114	71	6	1229	0	51	1022	0	7	1073	0	0	1284	531	0	1815	4117
% Lights	-	-	-	-	-	-	-	95.7	98.1	100.0	75.0	98.2	-	100.0	99.2	-	87.5	99.3	-	-	99.1	98.0	-	98.7	98.7
Other Vehicles	0	0	0	0	0	0	0	2	20	0	1	22	0	0	8	0	1	8	0	0	12	11	0	23	53
% Other Vehicles	-	-	-	-	-	-	-	4.3	1.8	0.0	12.5	1.8	-	0.0	0.8	-	12.5	0.7	-	-	0.9	2.0	-	1.3	1.3
Bicycles on Road	0	0	0	0	0	0	0	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	1
% Bicycles on Road	-	-	-	-	-	-	-	0.0	0.1	0.0	12.5	0.1	-	0.0	0.0	-	0.0	0.0	-	-	0.0	0.0	-	0.0	0.0



Turning Movement Peak Hour Data Plot (7:15)

### Turning Movement Peak Hour Data (12:00)

Start Time	Manatee Avenue W Eastbound						Manatee Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	0	0	0	0	0	0	17	263	28	4	308	0	22	249	0	5	271	0	0	159	101	0	260	839
12:15	0	0	0	0	0	0	0	17	297	32	2	346	0	8	241	0	1	249	0	0	146	91	0	237	832
12:30	0	0	0	0	1	0	0	14	294	36	7	344	0	12	233	0	5	245	0	0	143	90	0	233	822
12:45	0	0	0	0	0	0	0	19	266	27	5	312	0	10	252	0	7	262	0	0	148	89	0	237	811
<b>Total</b>	0	0	0	0	1	0	0	67	1120	123	18	1310	0	52	975	0	18	1027	0	0	596	371	0	967	3304
Approach %	0.0	0.0	0.0	0.0	-	-	0.0	5.1	85.5	9.4	-	-	0.0	5.1	94.9	0.0	-	-	0.0	0.0	61.6	38.4	-	-	-
Total %	0.0	0.0	0.0	0.0	-	0.0	0.0	2.0	33.9	3.7	-	39.6	0.0	1.6	29.5	0.0	-	31.1	0.0	0.0	18.0	11.2	-	29.3	-
PHF	0.000	0.000	0.000	0.000	-	0.000	0.000	0.882	0.943	0.854	-	0.947	0.000	0.591	0.967	0.000	-	0.947	0.000	0.000	0.937	0.918	-	0.930	0.985
Lights	0	0	0	0	1	0	0	66	1112	123	15	1301	0	50	961	0	17	1011	0	0	590	368	0	958	3270
% Lights	-	-	-	-	100.0	-	-	98.5	99.3	100.0	83.3	99.3	-	96.2	98.6	-	94.4	98.4	-	-	99.0	99.2	-	99.1	99.0
Other Vehicles	0	0	0	0	0	0	0	1	8	0	3	9	0	2	12	0	1	14	0	0	6	3	0	9	32
% Other Vehicles	-	-	-	-	0.0	-	-	1.5	0.7	0.0	16.7	0.7	-	3.8	1.2	-	5.6	1.4	-	-	1.0	0.8	-	0.9	1.0
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	2
% Bicycles on Road	-	-	-	-	0.0	-	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.2	-	0.0	0.2	-	-	0.0	0.0	-	0.0	0.1





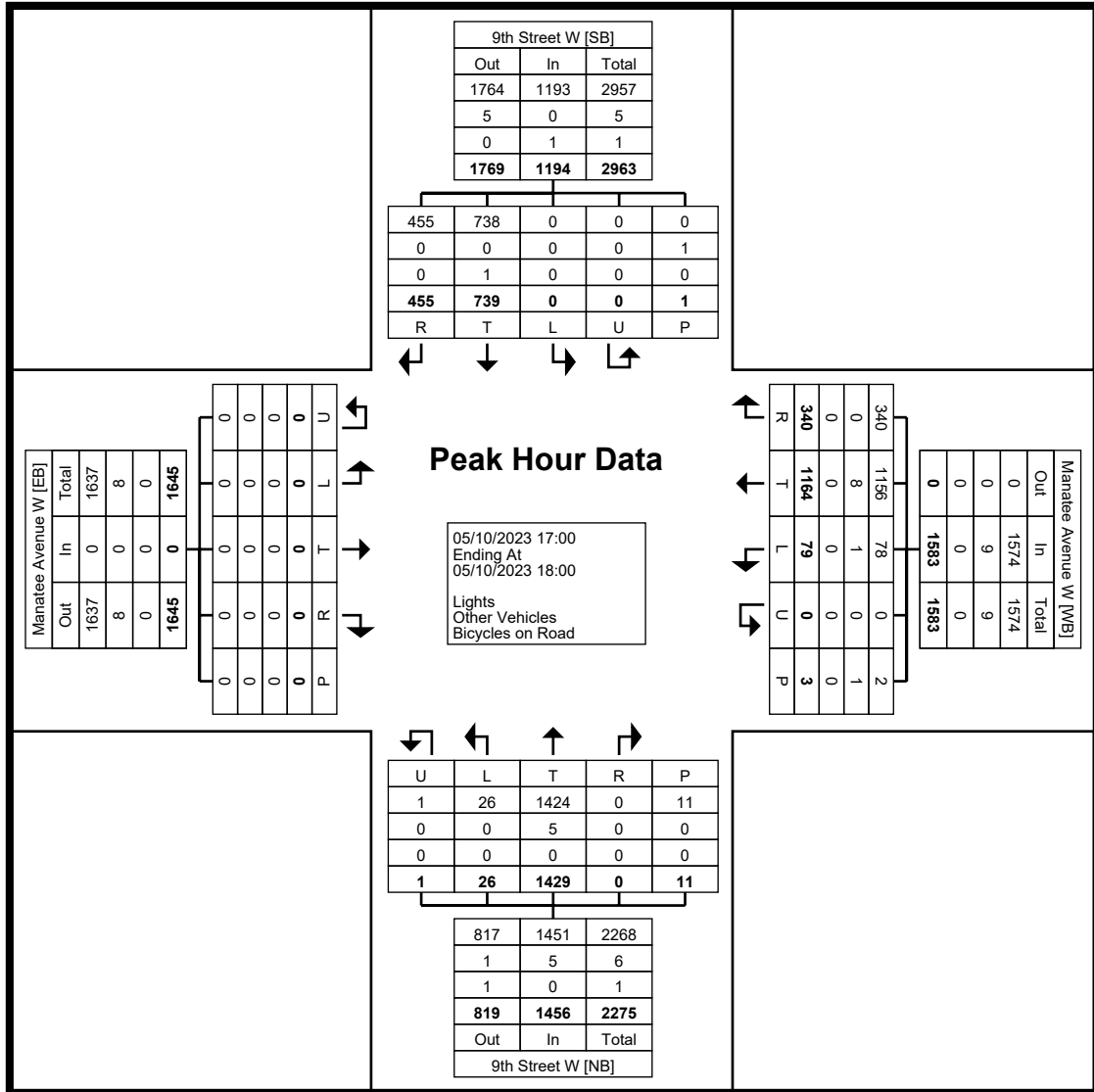
9th Street W and Manatee Avenue W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 11\_9th Street W and Manatee Avenue W  
Site Code: 11  
Start Date: 05/10/2023  
Page No: 7

### Turning Movement Peak Hour Data (17:00)

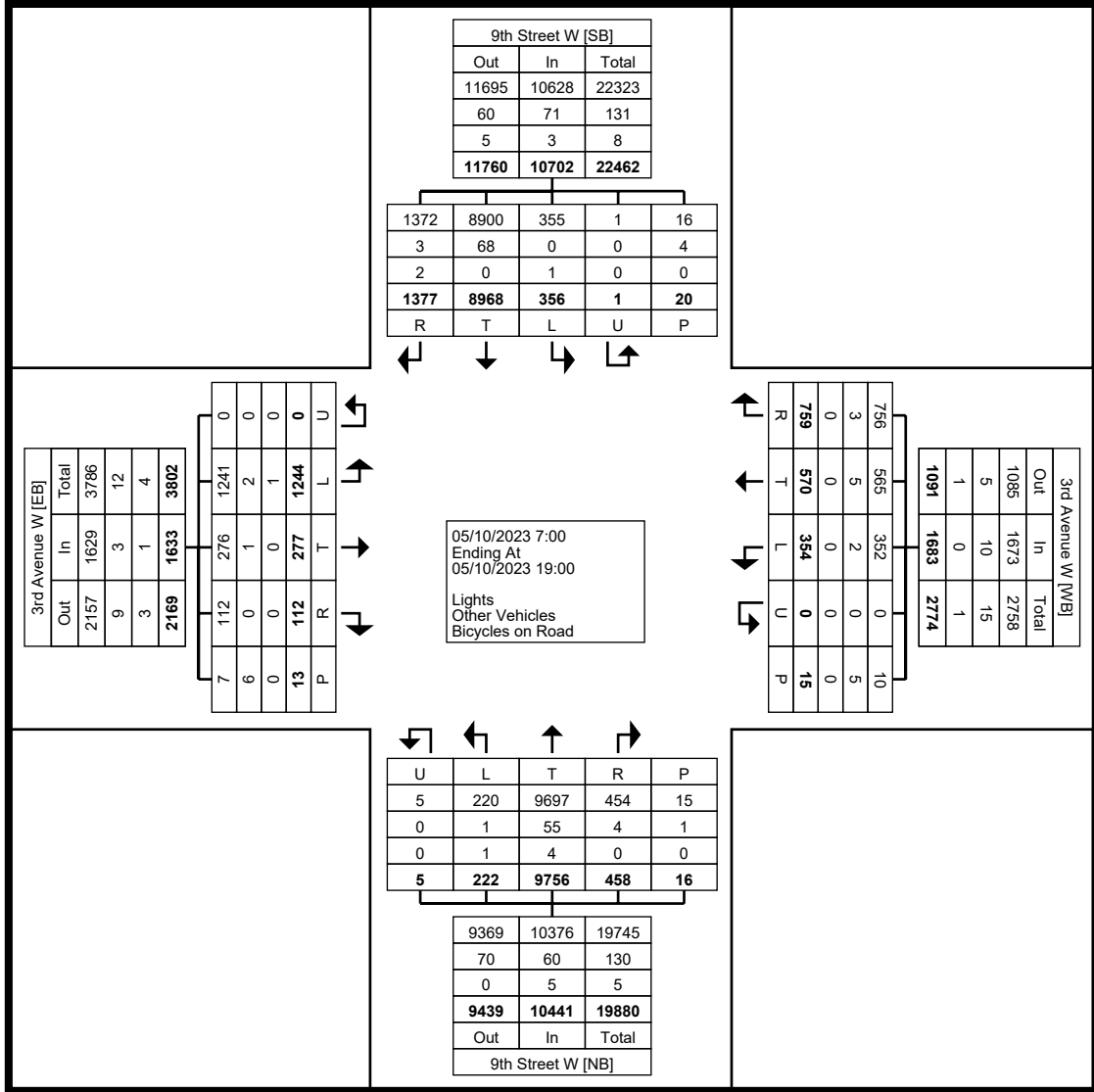
Start Time	Manatee Avenue W Eastbound						Manatee Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
17:00	0	0	0	0	0	0	0	22	323	83	1	428	0	3	355	0	7	358	0	0	215	124	0	339	1125
17:15	0	0	0	0	0	0	0	19	254	103	2	376	0	8	371	0	1	379	0	0	168	119	1	287	1042
17:30	0	0	0	0	0	0	0	20	300	83	0	403	1	3	370	0	2	374	0	0	176	115	0	291	1068
17:45	0	0	0	0	0	0	0	18	287	71	0	376	0	12	333	0	1	345	0	0	180	97	0	277	998
<b>Total</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>79</b>	<b>1164</b>	<b>340</b>	<b>3</b>	<b>1583</b>	<b>1</b>	<b>26</b>	<b>1429</b>	<b>0</b>	<b>11</b>	<b>1456</b>	<b>0</b>	<b>0</b>	<b>739</b>	<b>455</b>	<b>1</b>	<b>1194</b>	<b>4233</b>
Approach %	0.0	0.0	0.0	0.0	-	-	0.0	5.0	73.5	21.5	-	-	0.1	1.8	98.1	0.0	-	-	0.0	0.0	61.9	38.1	-	-	-
Total %	0.0	0.0	0.0	0.0	-	0.0	0.0	1.9	27.5	8.0	-	37.4	0.0	0.6	33.8	0.0	-	34.4	0.0	0.0	17.5	10.7	-	28.2	-
PHF	0.000	0.000	0.000	0.000	-	0.000	0.000	0.898	0.901	0.825	-	0.925	0.250	0.542	0.963	0.000	-	0.960	0.000	0.000	0.859	0.917	-	0.881	0.941
Lights	0	0	0	0	0	0	0	78	1156	340	2	1574	1	26	1424	0	11	1451	0	0	738	455	0	1193	4218
% Lights	-	-	-	-	-	-	-	98.7	99.3	100.0	66.7	99.4	100.0	100.0	99.7	-	100.0	99.7	-	-	99.9	100.0	0.0	99.9	99.6
Other Vehicles	0	0	0	0	0	0	0	1	8	0	1	9	0	0	5	0	0	5	0	0	0	0	1	0	14
% Other Vehicles	-	-	-	-	-	-	-	1.3	0.7	0.0	33.3	0.6	0.0	0.0	0.3	-	0.0	0.3	-	-	0.0	0.0	100.0	0.0	0.3
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
% Bicycles on Road	-	-	-	-	-	-	-	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	-	-	0.1	0.0	0.0	0.1	0.0



Turning Movement Peak Hour Data Plot (17:00)

### Turning Movement Data

Start Time	3rd Avenue W Eastbound						3rd Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	13	6	3	0	22	0	2	10	15	0	27	0	9	170	14	1	193	0	13	505	77	0	595	837
7:15	0	8	4	3	0	15	0	11	24	13	0	48	0	7	226	14	2	247	0	22	488	94	0	604	914
7:30	0	12	4	2	0	18	0	19	35	1	0	55	0	7	251	21	0	279	0	30	468	95	1	593	945
7:45	0	17	13	3	0	33	0	9	32	15	0	56	0	7	215	29	0	251	0	35	421	93	0	549	889
Hourly Total	0	50	27	11	0	88	0	41	101	44	0	186	0	30	862	78	3	970	0	100	1882	359	1	2341	3585
8:00	0	6	5	1	1	12	0	11	29	12	0	52	0	8	254	23	0	285	0	22	441	125	0	588	937
8:15	0	9	8	2	2	19	0	5	23	21	0	49	0	11	264	22	1	297	0	24	430	82	0	536	901
8:30	0	14	8	3	0	25	0	5	24	19	0	48	0	7	201	17	0	225	0	15	385	83	0	483	781
8:45	0	18	5	1	1	24	0	10	25	11	0	46	0	8	214	26	0	248	0	15	319	66	0	400	718
Hourly Total	0	47	26	7	4	80	0	31	101	63	0	195	0	34	933	88	1	1055	0	76	1575	356	0	2007	3337
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	17	3	3	0	23	0	16	22	24	0	62	1	5	243	16	0	265	1	8	228	21	1	258	608
12:15	0	18	6	4	0	28	0	8	12	14	0	34	0	10	258	12	0	280	0	12	222	30	0	264	606
12:30	0	24	9	5	0	38	0	12	14	27	1	53	0	10	223	18	1	251	0	11	234	28	0	273	615
12:45	0	24	6	4	0	34	0	13	12	15	1	40	0	6	258	12	1	276	0	6	189	39	1	234	584
Hourly Total	0	83	24	16	0	123	0	49	60	80	2	189	1	31	982	58	2	1072	1	37	873	118	2	1029	2413
13:00	0	35	5	3	0	43	0	12	23	13	1	48	0	13	192	15	0	220	0	11	204	23	0	238	549
13:15	0	28	6	6	0	40	0	10	12	9	0	31	0	6	249	23	0	278	0	9	219	29	0	257	606
13:30	0	16	7	4	0	27	0	8	9	16	2	33	0	7	233	19	0	259	0	10	214	31	0	255	574
13:45	0	26	15	4	1	45	0	14	17	16	0	47	0	11	282	17	2	310	0	10	195	25	0	230	632
Hourly Total	0	105	33	17	1	155	0	44	61	54	3	159	0	37	956	74	2	1067	0	40	832	108	0	980	2361
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	33	12	3	0	48	0	9	21	27	0	57	1	4	333	3	0	341	0	6	237	31	0	274	720
15:15	0	44	9	2	0	55	0	5	14	25	1	44	0	8	343	16	0	367	0	6	273	42	1	321	787
15:30	0	60	8	4	0	72	0	15	19	32	0	66	0	6	387	13	0	406	0	10	244	25	4	279	823
15:45	0	53	12	5	0	70	0	9	17	31	0	57	0	4	391	9	0	404	0	5	205	25	0	235	766
Hourly Total	0	190	41	14	0	245	0	38	71	115	1	224	1	22	1454	41	0	1518	0	27	959	123	5	1109	3096
16:00	0	81	9	7	1	97	0	16	23	36	0	75	0	4	394	4	0	402	0	7	241	25	2	273	847
16:15	0	54	13	6	0	73	0	11	12	26	0	49	0	5	440	10	0	455	0	7	248	24	0	279	856
16:30	0	81	16	2	3	99	0	16	16	50	2	82	0	8	436	5	3	449	0	6	248	29	0	283	913
16:45	0	99	8	1	1	108	0	17	27	48	0	92	0	10	401	7	0	418	0	9	206	20	1	235	853
Hourly Total	0	315	46	16	5	377	0	60	78	160	2	298	0	27	1671	26	3	1724	0	29	943	98	3	1070	3469
17:00	0	87	12	3	1	102	0	23	21	58	0	102	0	5	438	6	0	449	0	7	293	30	1	330	983
17:15	0	100	11	1	1	112	0	15	19	51	6	85	0	1	469	7	4	477	0	7	247	25	0	279	953
17:30	0	77	16	2	0	95	0	12	9	26	0	47	1	8	436	8	0	453	0	8	264	29	0	301	896
17:45	0	70	7	3	0	80	0	13	15	32	0	60	0	5	398	15	0	418	0	7	284	39	3	330	888
Hourly Total	0	334	46	9	2	389	0	63	64	167	6	294	1	19	1741	36	4	1797	0	29	1088	123	4	1240	3720
18:00	0	38	12	6	0	56	0	8	14	9	1	31	0	0	362	14	1	376	0	4	225	24	1	253	716
18:15	0	30	5	7	0	42	0	11	6	34	0	51	0	8	289	14	0	311	0	4	185	25	2	214	618
18:30	0	20	7	4	1	31	0	4	9	20	0	33	0	4	268	13	0	285	0	6	239	23	0	268	617
18:45	0	32	10	5	0	47	0	5	5	13	0	23	2	10	238	16	0	266	0	4	167	20	2	191	527
Hourly Total	0	120	34	22	1	176	0	28	34	76	1	138	2	22	1157	57	1	1238	0	18	816	92	5	926	2478
Grand Total	0	1244	277	112	13	1633	0	354	570	759	15	1683	5	222	9756	458	16	10441	1	356	8968	1377	20	10702	24459
Approach %	0.0	76.2	17.0	6.9	-	-	0.0	21.0	33.9	45.1	-	-	0.0	2.1	93.4	4.4	-	-	0.0	3.3	83.8	12.9	-	-	-
Total %	0.0	5.1	1.1	0.5	-	6.7	0.0	1.4	2.3	3.1	-	6.9	0.0	0.9	39.9	1.9	-	42.7	0.0	1.5	36.7	5.6	-	43.8	-
Lights	0	1241	276	112	7	1629	0	352	565	756	10	1673	5	220	9697	454	15	10376	1	355	8900	1372	16	10628	24306
% Lights	-	99.8	99.6	100.0	53.8	99.8	-	99.4	99.1	99.6	66.7	99.4	100.0	99.1	99.4	99.1	93.8	99.4	100.0	99.7	99.2	99.6	80.0	99.3	99.4
Other Vehicles	0	2	1	0	6	3	0	2	5	3	5	10	0	1	55	4	1	60	0	0	68	3	4	71	144
% Other Vehicles	-	0.2	0.4	0.0	46.2	0.2	-	0.6	0.9	0.4	33.3	0.6	0.0	0.5	0.6	0.9	6.3	0.6	0.0	0.0	0.8	0.2	20.0	0.7	0.6
Bicycles on Road	0	1	0	0	0	1	0	0	0	0	0	0	0	1	4	0	0	5	0	1	0	2	0	3	9
% Bicycles on Road	-	0.1	0.0	0.0	0.0	0.1	-	0.0	0.0	0.0	0.0	0.0	0.0	0.5	0.0	0.0	0.0	0.0	0.0	0.3	0.0	0.1	0.0	0.0	0.0



Turning Movement Data Plot

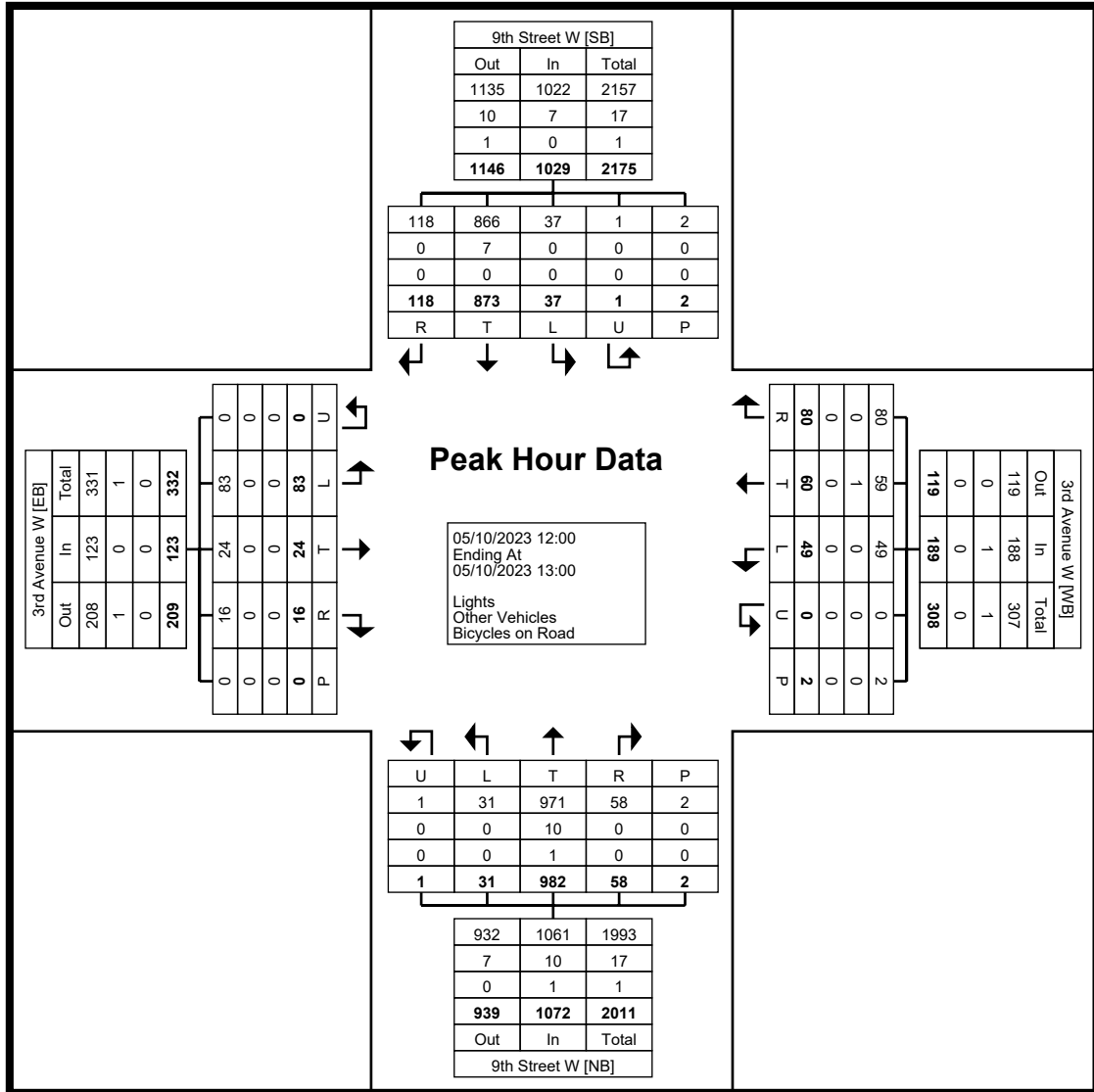
### Turning Movement Peak Hour Data (7:15)

Start Time	3rd Avenue W Eastbound						3rd Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15	0	8	4	3	0	15	0	11	24	13	0	48	0	7	226	14	2	247	0	22	488	94	0	604	914
7:30	0	12	4	2	0	18	0	19	35	1	0	55	0	7	251	21	0	279	0	30	468	95	1	593	945
7:45	0	17	13	3	0	33	0	9	32	15	0	56	0	7	215	29	0	251	0	35	421	93	0	549	889
8:00	0	6	5	1	1	12	0	11	29	12	0	52	0	8	254	23	0	285	0	22	441	125	0	588	937
<b>Total</b>	0	43	26	9	1	78	0	50	120	41	0	211	0	29	946	87	2	1062	0	109	1818	407	1	2334	3685
Approach %	0.0	55.1	33.3	11.5	-	-	0.0	23.7	56.9	19.4	-	-	0.0	2.7	89.1	8.2	-	-	0.0	4.7	77.9	17.4	-	-	-
Total %	0.0	1.2	0.7	0.2	-	2.1	0.0	1.4	3.3	1.1	-	5.7	0.0	0.8	25.7	2.4	-	28.8	0.0	3.0	49.3	11.0	-	63.3	-
PHF	0.000	0.632	0.500	0.750	-	0.591	0.000	0.658	0.857	0.683	-	0.942	0.000	0.906	0.931	0.750	-	0.932	0.000	0.779	0.931	0.814	-	0.966	0.975
Lights	0	43	26	9	0	78	0	48	118	41	0	207	0	28	940	85	2	1053	0	109	1794	406	1	2309	3647
% Lights	-	100.0	100.0	100.0	0.0	100.0	-	96.0	98.3	100.0	-	98.1	-	96.6	99.4	97.7	100.0	99.2	-	100.0	98.7	99.8	100.0	98.9	99.0
Other Vehicles	0	0	0	0	1	0	0	2	2	0	0	4	0	1	4	2	0	7	0	0	24	1	0	25	36
% Other Vehicles	-	0.0	0.0	0.0	100.0	0.0	-	4.0	1.7	0.0	-	1.9	-	3.4	0.4	2.3	0.0	0.7	-	0.0	1.3	0.2	0.0	1.1	1.0
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	2
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.2	0.0	0.0	0.2	-	0.0	0.0	0.0	0.0	0.0	0.1



### Turning Movement Peak Hour Data (12:00)

Start Time	3rd Avenue W Eastbound						3rd Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	17	3	3	0	23	0	16	22	24	0	62	1	5	243	16	0	265	1	8	228	21	1	258	608
12:15	0	18	6	4	0	28	0	8	12	14	0	34	0	10	258	12	0	280	0	12	222	30	0	264	606
12:30	0	24	9	5	0	38	0	12	14	27	1	53	0	10	223	18	1	251	0	11	234	28	0	273	615
12:45	0	24	6	4	0	34	0	13	12	15	1	40	0	6	258	12	1	276	0	6	189	39	1	234	584
<b>Total</b>	<b>0</b>	<b>83</b>	<b>24</b>	<b>16</b>	<b>0</b>	<b>123</b>	<b>0</b>	<b>49</b>	<b>60</b>	<b>80</b>	<b>2</b>	<b>189</b>	<b>1</b>	<b>31</b>	<b>982</b>	<b>58</b>	<b>2</b>	<b>1072</b>	<b>1</b>	<b>37</b>	<b>873</b>	<b>118</b>	<b>2</b>	<b>1029</b>	<b>2413</b>
Approach %	0.0	67.5	19.5	13.0	-	-	0.0	25.9	31.7	42.3	-	-	0.1	2.9	91.6	5.4	-	-	0.1	3.6	84.8	11.5	-	-	-
Total %	0.0	3.4	1.0	0.7	-	5.1	0.0	2.0	2.5	3.3	-	7.8	0.0	1.3	40.7	2.4	-	44.4	0.0	1.5	36.2	4.9	-	42.6	-
PHF	0.000	0.865	0.667	0.800	-	0.809	0.000	0.766	0.682	0.741	-	0.762	0.250	0.775	0.952	0.806	-	0.957	0.250	0.771	0.933	0.756	-	0.942	0.981
Lights	0	83	24	16	0	123	0	49	59	80	2	188	1	31	971	58	2	1061	1	37	866	118	2	1022	2394
% Lights	-	100.0	100.0	100.0	-	100.0	-	100.0	98.3	100.0	100.0	99.5	100.0	100.0	98.9	100.0	100.0	99.0	100.0	100.0	99.2	100.0	100.0	99.3	99.2
Other Vehicles	0	0	0	0	0	0	0	0	1	0	0	1	0	0	10	0	0	10	0	0	7	0	0	7	18
% Other Vehicles	-	0.0	0.0	0.0	-	0.0	-	0.0	1.7	0.0	0.0	0.5	0.0	0.0	1.0	0.0	0.0	0.9	0.0	0.0	0.8	0.0	0.0	0.7	0.7
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.0	0.0	0.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (12:00)



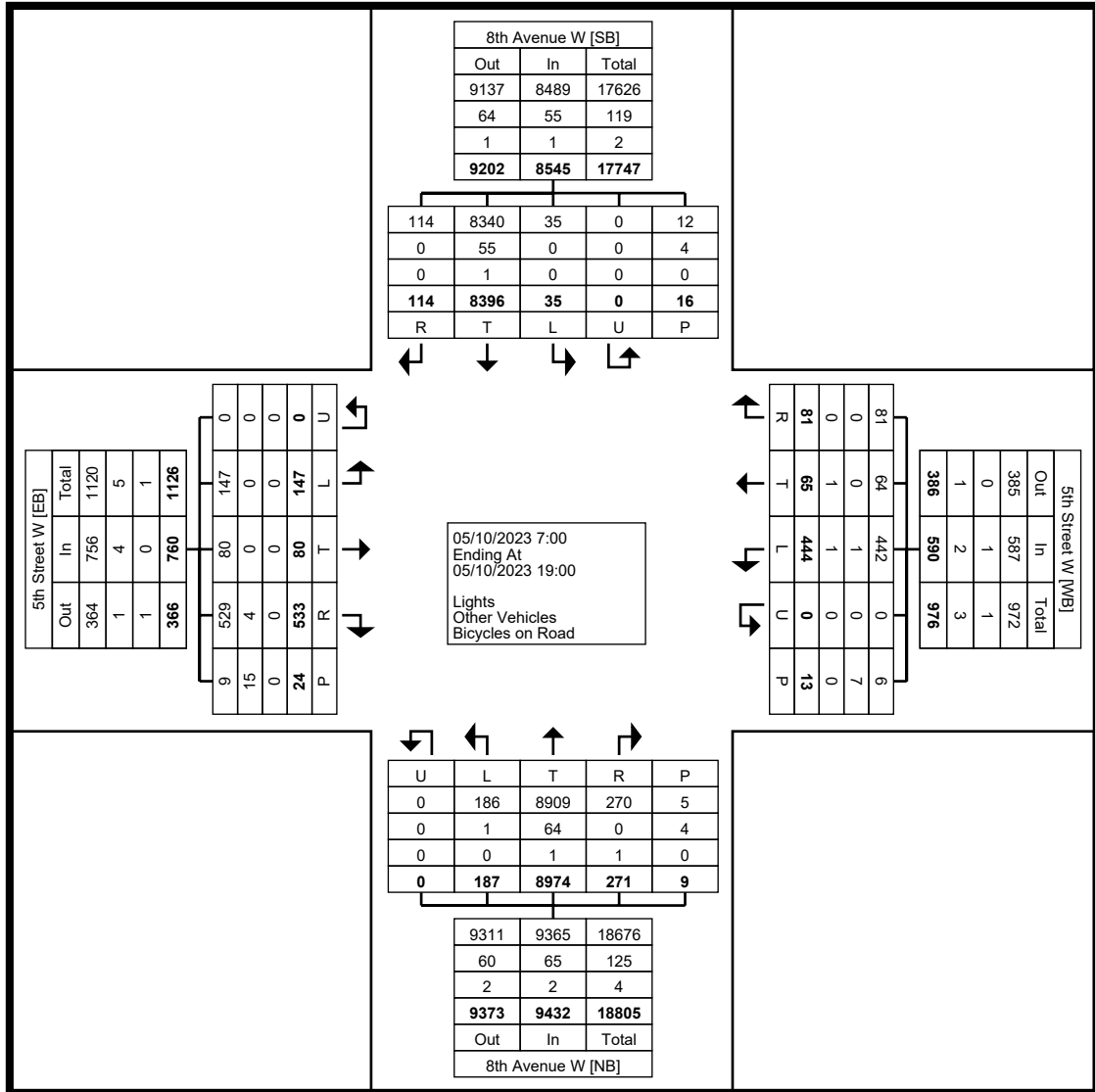
### Turning Movement Peak Hour Data (17:00)

Start Time	3rd Avenue W Eastbound						3rd Avenue W Westbound						9th Street W Northbound						9th Street W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
17:00	0	87	12	3	1	102	0	23	21	58	0	102	0	5	438	6	0	449	0	7	293	30	1	330	983
17:15	0	100	11	1	1	112	0	15	19	51	6	85	0	1	469	7	4	477	0	7	247	25	0	279	953
17:30	0	77	16	2	0	95	0	12	9	26	0	47	1	8	436	8	0	453	0	8	264	29	0	301	896
17:45	0	70	7	3	0	80	0	13	15	32	0	60	0	5	398	15	0	418	0	7	284	39	3	330	888
<b>Total</b>	0	334	46	9	2	389	0	63	64	167	6	294	1	19	1741	36	4	1797	0	29	1088	123	4	1240	3720
Approach %	0.0	85.9	11.8	2.3	-	-	0.0	21.4	21.8	56.8	-	-	0.1	1.1	96.9	2.0	-	-	0.0	2.3	87.7	9.9	-	-	-
Total %	0.0	9.0	1.2	0.2	-	10.5	0.0	1.7	1.7	4.5	-	7.9	0.0	0.5	46.8	1.0	-	48.3	0.0	0.8	29.2	3.3	-	33.3	-
PHF	0.000	0.835	0.719	0.750	-	0.868	0.000	0.685	0.762	0.720	-	0.721	0.250	0.594	0.928	0.600	-	0.942	0.000	0.906	0.928	0.788	-	0.939	0.946
Lights	0	333	46	9	1	388	0	63	63	167	5	293	1	19	1734	36	4	1790	0	29	1085	123	3	1237	3708
% Lights	-	99.7	100.0	100.0	50.0	99.7	-	100.0	98.4	100.0	83.3	99.7	100.0	100.0	99.6	100.0	100.0	99.6	-	100.0	99.7	100.0	75.0	99.8	99.7
Other Vehicles	0	1	0	0	1	1	0	0	1	0	1	1	0	0	7	0	0	7	0	0	3	0	1	3	12
% Other Vehicles	-	0.3	0.0	0.0	50.0	0.3	-	0.0	1.6	0.0	16.7	0.3	0.0	0.0	0.4	0.0	0.0	0.4	-	0.0	0.3	0.0	25.0	0.2	0.3
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



### Turning Movement Data

Start Time	5th Street W Eastbound						5th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	2	1	45	0	48	0	33	0	0	0	33	0	4	190	2	0	196	0	1	483	0	1	484	761
7:15	0	3	5	61	0	69	0	34	0	0	0	34	0	11	193	5	0	209	0	0	448	2	0	450	762
7:30	0	3	1	73	0	77	0	42	1	1	1	44	0	13	211	4	0	228	0	1	368	0	0	369	718
7:45	0	6	0	59	1	65	0	23	1	0	1	24	0	14	226	3	0	243	0	0	369	0	1	369	701
Hourly Total	0	14	7	238	1	259	0	132	2	1	2	135	0	42	820	14	0	876	0	2	1668	2	2	1672	2942
8:00	0	2	1	42	2	45	0	23	2	1	0	26	0	15	210	3	0	228	0	0	378	0	1	378	677
8:15	0	1	4	26	1	31	0	21	2	2	0	25	0	5	221	9	1	235	0	2	338	3	1	343	634
8:30	0	5	1	16	0	22	0	22	2	1	0	25	0	1	199	3	1	203	0	3	342	2	0	347	597
8:45	0	1	2	16	0	19	0	14	1	2	0	17	0	3	197	2	0	202	0	1	331	3	0	335	573
Hourly Total	0	9	8	100	3	117	0	80	7	6	0	93	0	24	827	17	2	868	0	6	1389	8	2	1403	2481
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	8	4	7	0	19	0	9	3	2	1	14	0	7	239	4	0	250	0	1	202	1	0	204	487
12:15	0	9	1	12	0	22	0	7	4	7	1	18	0	2	229	5	0	236	0	0	213	5	0	218	494
12:30	0	7	1	11	1	19	0	4	4	2	1	10	0	11	234	5	0	250	0	2	250	5	3	257	536
12:45	0	4	6	4	0	14	0	10	1	2	1	13	0	4	224	6	1	234	0	1	214	5	2	220	481
Hourly Total	0	28	12	34	1	74	0	30	12	13	4	55	0	24	926	20	1	970	0	4	879	16	5	899	1998
13:00	0	9	1	13	0	23	0	11	4	4	0	19	0	8	206	4	0	218	0	3	208	5	1	216	476
13:15	0	5	3	8	0	16	0	12	1	5	3	18	0	3	250	6	3	259	0	1	222	4	2	227	520
13:30	0	7	2	11	0	20	0	8	1	3	0	12	0	1	238	8	0	247	0	2	205	6	0	213	492
13:45	0	2	2	7	2	11	0	10	1	3	1	14	0	6	231	4	0	241	0	0	209	4	0	213	479
Hourly Total	0	23	8	39	2	70	0	41	7	15	4	63	0	18	925	22	3	965	0	6	844	19	3	869	1967
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	7	3	7	1	17	0	7	0	3	0	10	0	1	325	7	2	333	0	1	234	1	0	236	596
15:15	0	5	2	14	3	21	0	13	2	2	0	17	0	2	310	11	0	323	0	0	254	7	0	261	622
15:30	0	5	2	6	2	13	0	10	6	2	0	18	0	6	344	13	0	363	0	2	219	1	0	222	616
15:45	0	2	3	8	0	13	0	9	1	3	0	13	0	5	368	14	0	387	0	0	232	6	0	238	651
Hourly Total	0	19	10	35	6	64	0	39	9	10	0	58	0	14	1347	45	2	1406	0	3	939	15	0	957	2485
16:00	0	4	3	9	0	16	0	12	1	2	0	15	0	7	414	16	0	437	0	1	209	2	0	212	680
16:15	0	7	7	9	0	23	0	9	1	3	1	13	0	10	380	12	0	402	0	1	229	2	1	232	670
16:30	0	4	2	8	0	14	0	7	2	4	0	13	0	4	363	32	0	399	0	1	242	3	3	246	672
16:45	0	4	2	7	2	13	0	14	4	4	0	22	0	5	357	18	0	380	0	3	219	7	0	229	644
Hourly Total	0	19	14	33	2	66	0	42	8	13	1	63	0	26	1514	78	0	1618	0	6	899	14	4	919	2666
17:00	0	2	8	9	0	19	0	23	7	6	0	36	0	4	388	14	0	406	0	1	233	6	0	240	701
17:15	0	3	4	4	3	11	0	10	5	3	0	18	0	4	390	13	0	407	0	1	250	5	0	256	692
17:30	0	4	2	7	1	13	0	8	1	2	0	11	0	10	366	13	0	389	0	0	288	6	0	294	707
17:45	0	3	4	8	0	15	0	8	1	3	1	12	0	6	369	20	0	395	0	0	244	12	0	256	678
Hourly Total	0	12	18	28	4	58	0	49	14	14	1	77	0	24	1513	60	0	1597	0	2	1015	29	0	1046	2778
18:00	0	10	1	13	1	24	0	10	2	5	0	17	0	5	326	8	0	339	0	2	184	2	0	188	568
18:15	0	2	2	6	1	10	0	4	1	3	0	8	0	1	290	2	0	293	0	1	200	1	0	202	513
18:30	0	4	0	2	0	6	0	5	2	1	0	8	0	4	248	5	0	257	0	2	214	4	0	220	491
18:45	0	7	0	5	3	12	0	12	1	0	1	13	0	5	238	0	1	243	0	1	165	4	0	170	438
Hourly Total	0	23	3	26	5	52	0	31	6	9	1	46	0	15	1102	15	1	1132	0	6	763	11	0	780	2010
Grand Total	0	147	80	533	24	760	0	444	65	81	13	590	0	187	8974	271	9	9432	0	35	8396	114	16	8545	19327
Approach %	0.0	19.3	10.5	70.1	-	-	0.0	75.3	11.0	13.7	-	-	0.0	2.0	95.1	2.9	-	-	0.0	0.4	98.3	1.3	-	-	-
Total %	0.0	0.8	0.4	2.8	-	3.9	0.0	2.3	0.3	0.4	-	3.1	0.0	1.0	46.4	1.4	-	48.8	0.0	0.2	43.4	0.6	-	44.2	-
Lights	0	147	80	529	9	756	0	442	64	81	6	587	0	186	8909	270	5	9365	0	35	8340	114	12	8489	19197
% Lights	-	100.0	100.0	99.2	37.5	99.5	-	99.5	98.5	100.0	46.2	99.5	-	99.5	99.3	99.6	55.6	99.3	-	100.0	99.3	100.0	75.0	99.3	99.3
Other Vehicles	0	0	0	4	15	4	0	1	0	0	7	1	0	1	64	0	4	65	0	0	55	0	4	55	125
% Other Vehicles	-	0.0	0.0	0.8	62.5	0.5	-	0.2	0.0	0.0	53.8	0.2	-	0.5	0.7	0.0	44.4	0.7	-	0.0	0.7	0.0	25.0	0.6	0.6
Bicycles on Road	0	0	0	0	0	0	0	1	1	0	0	2	0	0	1	1	0	2	0	0	1	0	0	1	5
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.2	1.5	0.0	0.0	0.3	-	0.0	0.0	0.4	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Data Plot

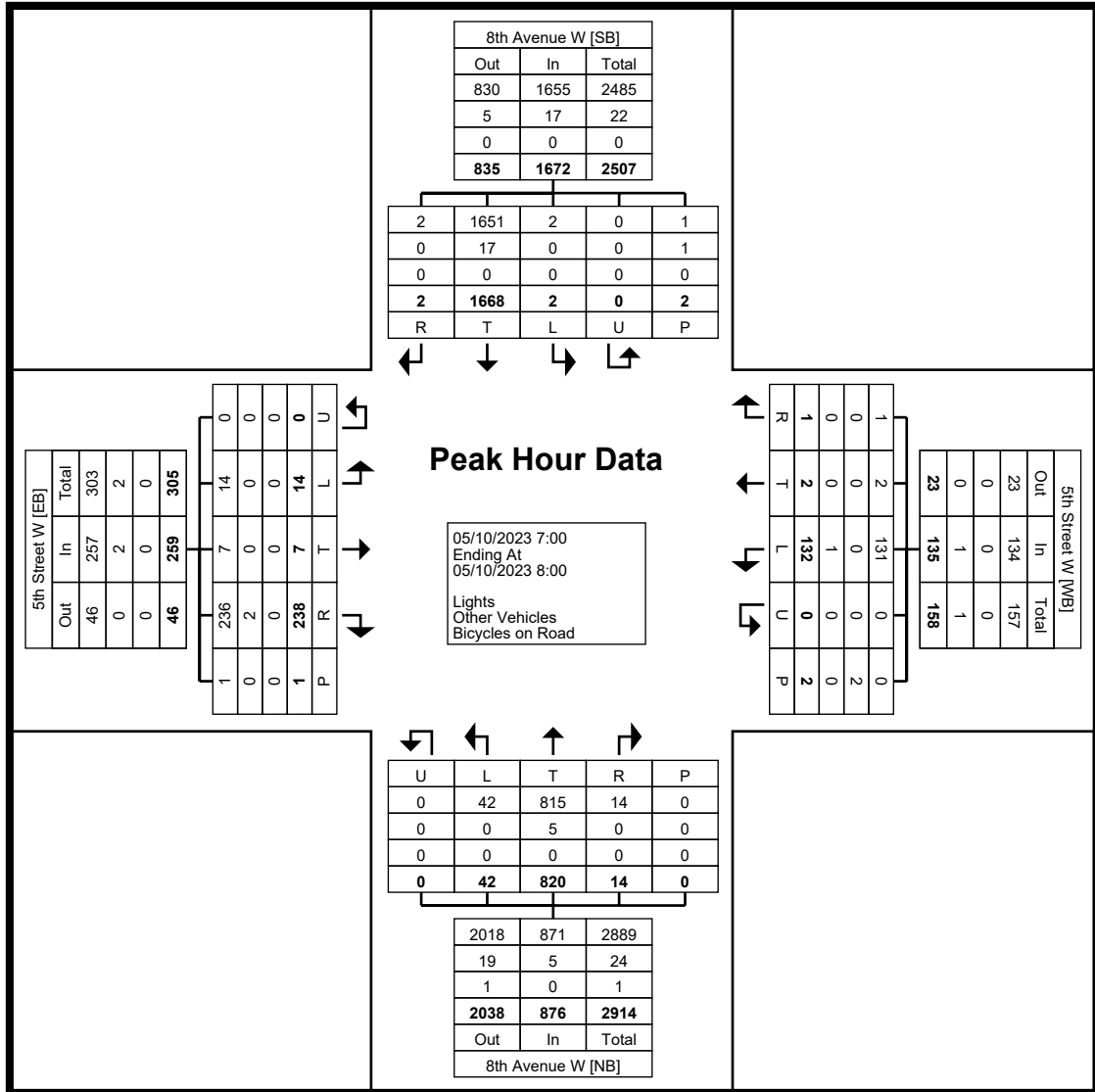
8th Avenue W and 5th Street W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 9\_8th Avenue W  
and 5th Street W  
Site Code: 9  
Start Date: 05/10/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:00)

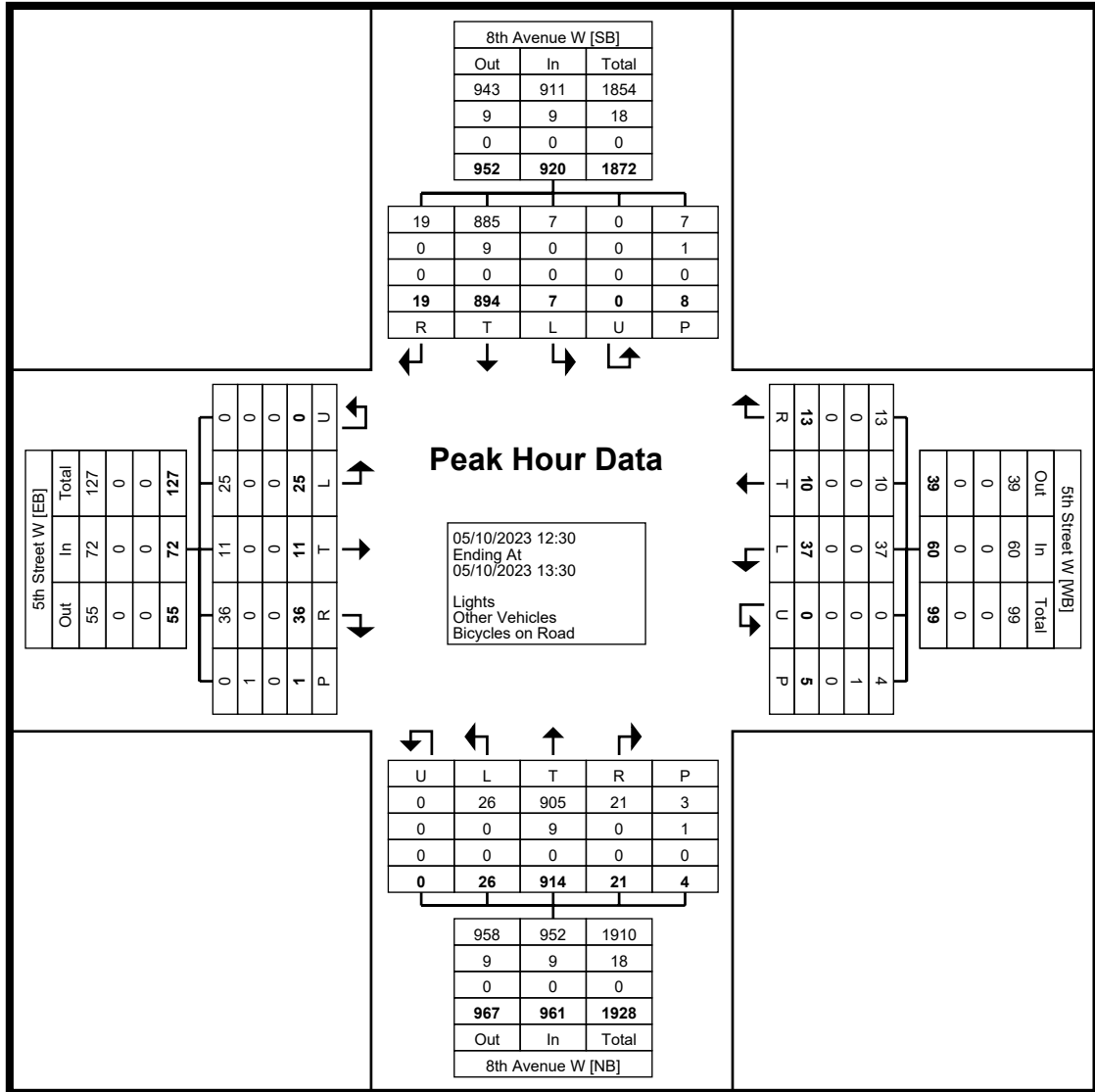
Start Time	5th Street W Eastbound						5th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	2	1	45	0	48	0	33	0	0	0	33	0	4	190	2	0	196	0	1	483	0	1	484	761
7:15	0	3	5	61	0	69	0	34	0	0	0	34	0	11	193	5	0	209	0	0	448	2	0	450	762
7:30	0	3	1	73	0	77	0	42	1	1	1	44	0	13	211	4	0	228	0	1	368	0	0	369	718
7:45	0	6	0	59	1	65	0	23	1	0	1	24	0	14	226	3	0	243	0	0	369	0	1	369	701
<b>Total</b>	0	14	7	238	1	259	0	132	2	1	2	135	0	42	820	14	0	876	0	2	1668	2	2	1672	2942
Approach %	0.0	5.4	2.7	91.9	-	-	0.0	97.8	1.5	0.7	-	-	0.0	4.8	93.6	1.6	-	-	0.0	0.1	99.8	0.1	-	-	-
Total %	0.0	0.5	0.2	8.1	-	8.8	0.0	4.5	0.1	0.0	-	4.6	0.0	1.4	27.9	0.5	-	29.8	0.0	0.1	56.7	0.1	-	56.8	-
PHF	0.000	0.583	0.350	0.815	-	0.841	0.000	0.786	0.500	0.250	-	0.767	0.000	0.750	0.907	0.700	-	0.901	0.000	0.500	0.863	0.250	-	0.864	0.965
Lights	0	14	7	236	1	257	0	131	2	1	0	134	0	42	815	14	0	871	0	2	1651	2	1	1655	2917
% Lights	-	100.0	100.0	99.2	100.0	99.2	-	99.2	100.0	100.0	0.0	99.3	-	100.0	99.4	100.0	-	99.4	-	100.0	99.0	100.0	50.0	99.0	99.2
Other Vehicles	0	0	0	2	0	2	0	0	0	0	2	0	0	0	5	0	0	5	0	0	17	0	1	17	24
% Other Vehicles	-	0.0	0.0	0.8	0.0	0.8	-	0.0	0.0	0.0	100.0	0.0	-	0.0	0.6	0.0	-	0.6	-	0.0	1.0	0.0	50.0	1.0	0.8
Bicycles on Road	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.8	0.0	0.0	0.0	0.7	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (7:00)

### Turning Movement Peak Hour Data (12:30)

Start Time	5th Street W Eastbound						5th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:30	0	7	1	11	1	19	0	4	4	2	1	10	0	11	234	5	0	250	0	2	250	5	3	257	536
12:45	0	4	6	4	0	14	0	10	1	2	1	13	0	4	224	6	1	234	0	1	214	5	2	220	481
13:00	0	9	1	13	0	23	0	11	4	4	0	19	0	8	206	4	0	218	0	3	208	5	1	216	476
13:15	0	5	3	8	0	16	0	12	1	5	3	18	0	3	250	6	3	259	0	1	222	4	2	227	520
<b>Total</b>	0	25	11	36	1	72	0	37	10	13	5	60	0	26	914	21	4	961	0	7	894	19	8	920	2013
Approach %	0.0	34.7	15.3	50.0	-	-	0.0	61.7	16.7	21.7	-	-	0.0	2.7	95.1	2.2	-	-	0.0	0.8	97.2	2.1	-	-	-
Total %	0.0	1.2	0.5	1.8	-	3.6	0.0	1.8	0.5	0.6	-	3.0	0.0	1.3	45.4	1.0	-	47.7	0.0	0.3	44.4	0.9	-	45.7	-
PHF	0.000	0.694	0.458	0.692	-	0.783	0.000	0.771	0.625	0.650	-	0.789	0.000	0.591	0.914	0.875	-	0.928	0.000	0.583	0.894	0.950	-	0.895	0.939
Lights	0	25	11	36	0	72	0	37	10	13	4	60	0	26	905	21	3	952	0	7	885	19	7	911	1995
% Lights	-	100.0	100.0	100.0	0.0	100.0	-	100.0	100.0	100.0	80.0	100.0	-	100.0	99.0	100.0	75.0	99.1	-	100.0	99.0	100.0	87.5	99.0	99.1
Other Vehicles	0	0	0	0	1	0	0	0	0	0	1	0	0	0	9	0	1	9	0	0	9	0	1	9	18
% Other Vehicles	-	0.0	0.0	0.0	100.0	0.0	-	0.0	0.0	0.0	20.0	0.0	-	0.0	1.0	0.0	25.0	0.9	-	0.0	1.0	0.0	12.5	1.0	0.9
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (12:30)



8th Avenue W and 5th Street W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 9\_8th Avenue W  
and 5th Street W  
Site Code: 9  
Start Date: 05/10/2023  
Page No: 7

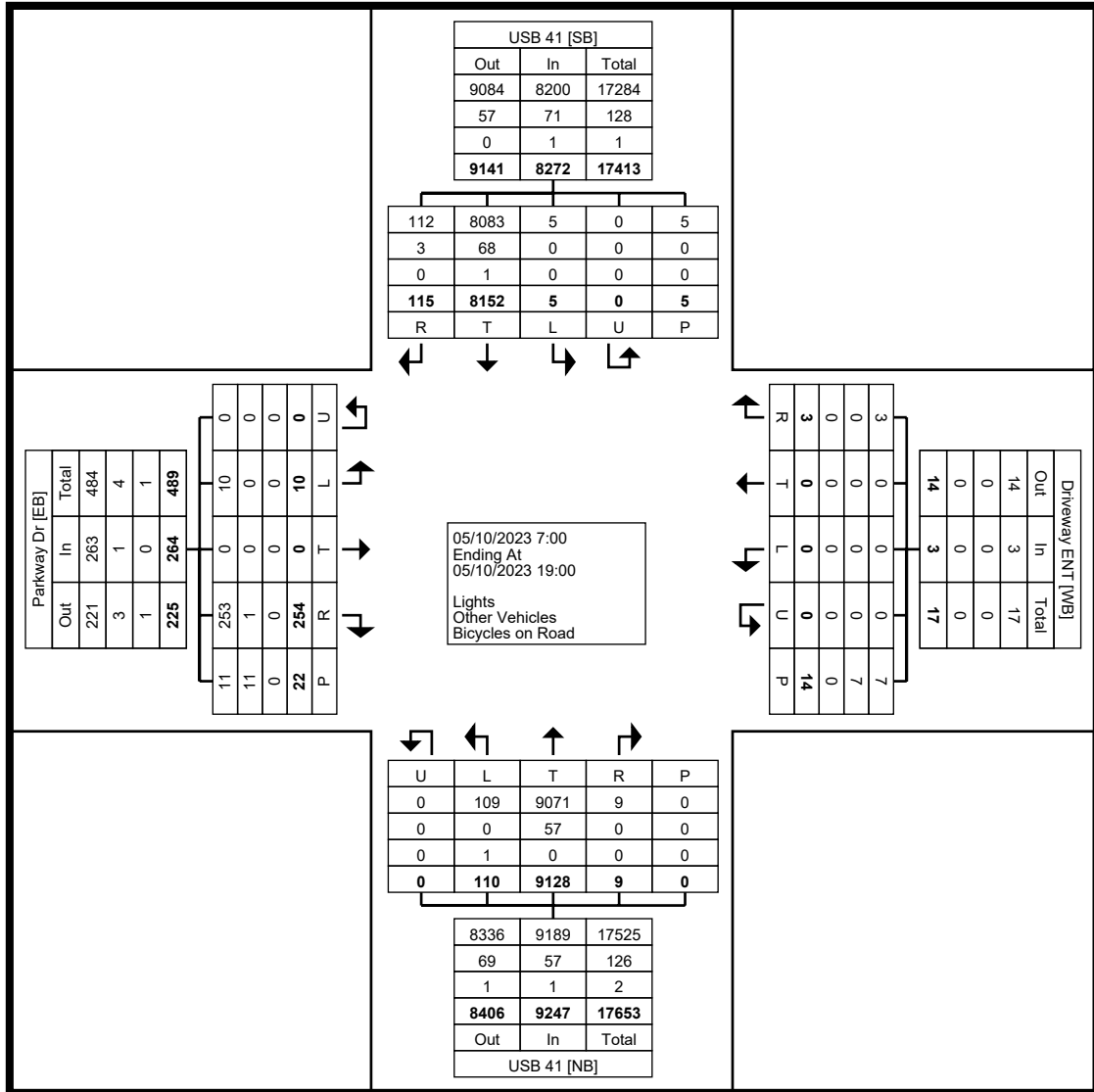
### Turning Movement Peak Hour Data (17:00)

Start Time	5th Street W Eastbound						5th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
17:00	0	2	8	9	0	19	0	23	7	6	0	36	0	4	388	14	0	406	0	1	233	6	0	240	701
17:15	0	3	4	4	3	11	0	10	5	3	0	18	0	4	390	13	0	407	0	1	250	5	0	256	692
17:30	0	4	2	7	1	13	0	8	1	2	0	11	0	10	366	13	0	389	0	0	288	6	0	294	707
17:45	0	3	4	8	0	15	0	8	1	3	1	12	0	6	369	20	0	395	0	0	244	12	0	256	678
<b>Total</b>	0	12	18	28	4	58	0	49	14	14	1	77	0	24	1513	60	0	1597	0	2	1015	29	0	1046	2778
Approach %	0.0	20.7	31.0	48.3	-	-	0.0	63.6	18.2	18.2	-	-	0.0	1.5	94.7	3.8	-	-	0.0	0.2	97.0	2.8	-	-	-
Total %	0.0	0.4	0.6	1.0	-	2.1	0.0	1.8	0.5	0.5	-	2.8	0.0	0.9	54.5	2.2	-	57.5	0.0	0.1	36.5	1.0	-	37.7	-
PHF	0.000	0.750	0.563	0.778	-	0.763	0.000	0.533	0.500	0.583	-	0.535	0.000	0.600	0.970	0.750	-	0.981	0.000	0.500	0.881	0.604	-	0.889	0.982
Lights	0	12	18	28	1	58	0	49	14	14	1	77	0	24	1503	60	0	1587	0	2	1014	29	0	1045	2767
% Lights	-	100.0	100.0	100.0	25.0	100.0	-	100.0	100.0	100.0	100.0	100.0	-	100.0	99.3	100.0	-	99.4	-	100.0	99.9	100.0	-	99.9	99.6
Other Vehicles	0	0	0	0	3	0	0	0	0	0	0	0	0	0	10	0	0	10	0	0	1	0	0	1	11
% Other Vehicles	-	0.0	0.0	0.0	75.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.7	0.0	-	0.6	-	0.0	0.1	0.0	-	0.1	0.4
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0



### Turning Movement Data

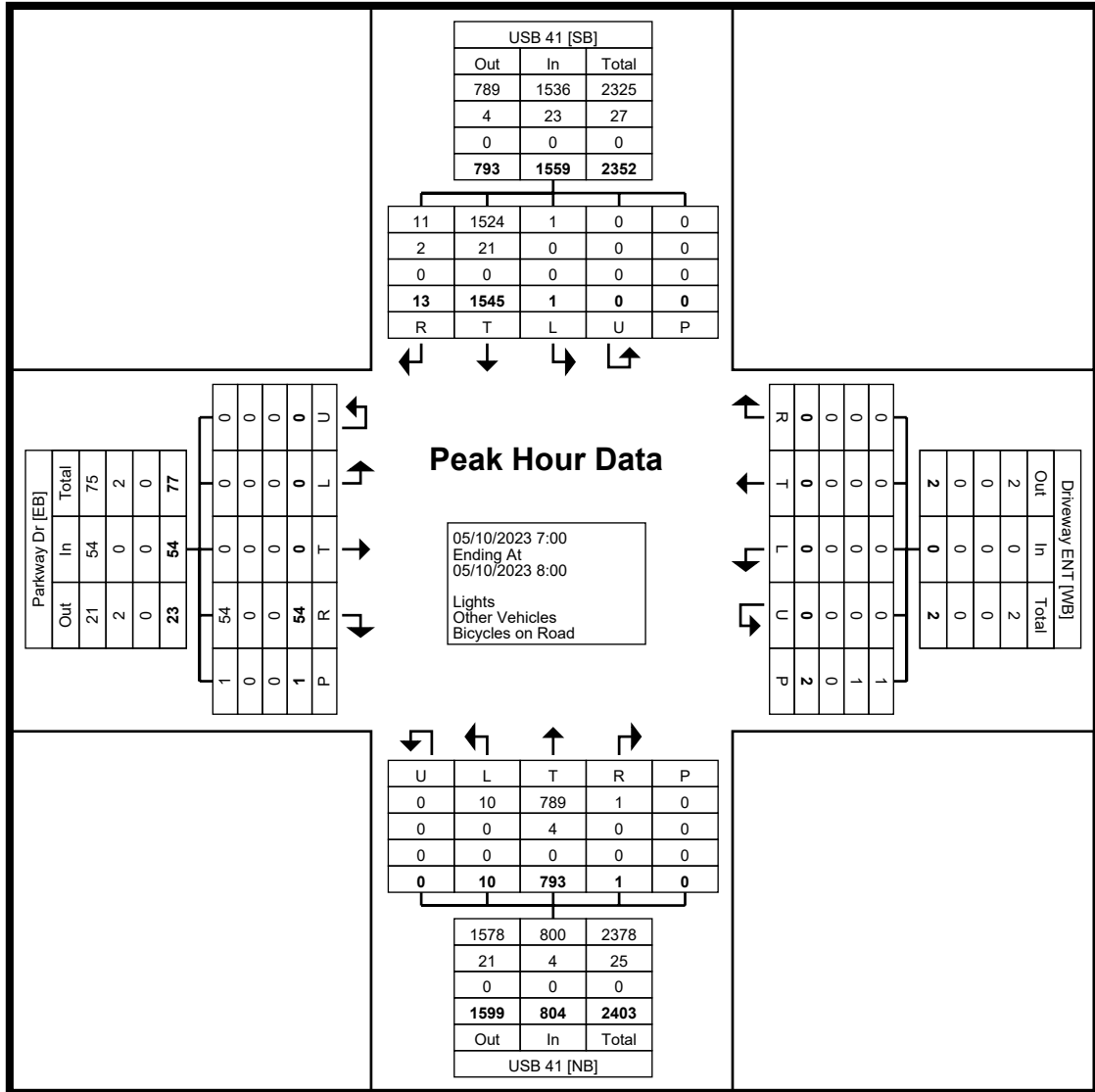
Start Time	Parkway Dr Eastbound						Driveway ENT Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total	
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total		
7:00	0	0	0	16	0	16	0	0	0	0	0	0	0	4	176	1	0	181	0	0	455	4	0	459	656	
7:15	0	0	0	13	0	13	0	0	0	0	0	0	0	4	182	0	0	186	0	0	419	5	0	424	623	
7:30	0	0	0	17	0	17	0	0	0	0	1	0	0	2	222	0	0	224	0	0	344	2	0	346	587	
7:45	0	0	0	8	1	8	0	0	0	0	1	0	0	0	213	0	0	213	0	1	327	2	0	330	551	
Hourly Total	0	0	0	54	1	54	0	0	0	0	2	0	0	10	793	1	0	804	0	1	1545	13	0	1559	2417	
8:00	0	0	0	13	2	13	0	0	0	0	1	0	0	2	211	0	0	213	0	0	349	5	0	354	580	
8:15	0	0	0	10	1	10	0	0	0	0	1	0	0	1	232	0	0	233	0	0	333	4	0	337	580	
8:30	0	0	0	12	0	12	0	0	0	0	0	0	0	5	193	0	0	198	0	0	330	4	0	334	544	
8:45	0	2	0	4	0	6	0	0	0	0	0	0	0	4	193	0	0	197	0	0	329	3	0	332	535	
Hourly Total	0	2	0	39	3	41	0	0	0	0	2	0	0	12	829	0	0	841	0	0	1341	16	0	1357	2239	
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	
12:00	0	2	0	11	1	13	0	0	0	0	1	0	0	6	256	0	0	262	0	0	201	4	0	205	480	
12:15	0	1	0	4	0	5	0	0	0	1	1	1	0	5	233	1	0	239	0	0	216	4	0	220	465	
12:30	0	1	0	8	2	9	0	0	0	0	1	0	0	1	249	0	0	250	0	2	241	5	0	248	507	
12:45	0	0	0	9	0	9	0	0	0	0	0	0	0	4	236	0	0	240	0	0	210	1	0	211	460	
Hourly Total	0	4	0	32	3	36	0	0	0	1	3	1	0	16	974	1	0	991	0	2	868	14	0	884	1912	
13:00	0	0	0	3	0	3	0	0	0	0	0	0	0	1	215	1	0	217	0	0	205	3	0	208	428	
13:15	0	0	0	8	0	8	0	0	0	0	0	0	0	1	255	1	0	257	0	0	222	8	0	230	495	
13:30	0	0	0	5	0	5	0	0	0	1	0	1	0	1	248	1	0	250	0	0	203	3	0	206	462	
13:45	0	0	0	5	0	5	0	0	0	0	1	0	0	3	263	0	0	266	0	2	201	1	0	204	475	
Hourly Total	0	0	0	21	0	21	0	0	0	1	1	1	0	6	981	3	0	990	0	2	831	15	0	848	1860	
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	
15:00	0	1	0	5	1	6	0	0	0	0	1	0	0	6	323	1	0	330	0	0	234	6	0	240	576	
15:15	0	0	0	10	2	10	0	0	0	0	0	0	0	6	326	1	0	333	0	0	256	5	0	261	604	
15:30	0	1	0	6	2	7	0	0	0	0	0	0	0	2	334	0	0	336	0	0	199	2	1	201	544	
15:45	0	0	0	7	0	7	0	0	0	0	0	0	0	2	385	0	0	387	0	0	245	5	1	250	644	
Hourly Total	0	2	0	28	5	30	0	0	0	0	1	0	0	16	1368	2	0	1386	0	0	934	18	2	952	2368	
16:00	0	0	0	4	0	4	0	0	0	0	1	0	0	8	398	0	0	406	0	0	204	3	0	207	617	
16:15	0	0	0	10	1	10	0	0	0	0	1	0	0	2	395	0	0	397	0	0	224	5	0	229	636	
16:30	0	0	0	4	0	4	0	0	0	0	1	0	0	1	380	0	0	381	0	0	240	1	0	241	626	
16:45	0	0	0	6	2	6	0	0	0	0	0	0	0	7	364	1	0	372	0	0	233	2	0	235	613	
Hourly Total	0	0	0	24	3	24	0	0	0	0	3	0	0	18	1537	1	0	1556	0	0	901	11	0	912	2492	
17:00	0	0	0	6	0	6	0	0	0	0	0	0	0	4	405	0	0	409	0	0	223	4	1	227	642	
17:15	0	0	0	13	3	13	0	0	0	0	0	0	0	4	389	0	0	393	0	0	237	1	0	238	644	
17:30	0	0	0	12	2	12	0	0	0	1	0	1	0	5	362	0	0	367	0	0	268	4	1	272	652	
17:45	0	0	0	9	0	9	0	0	0	0	0	0	0	7	367	1	0	375	0	0	247	4	0	251	635	
Hourly Total	0	0	0	40	5	40	0	0	0	1	0	1	0	20	1523	1	0	1544	0	0	975	13	2	988	2573	
18:00	0	1	0	4	0	5	0	0	0	0	1	0	0	2	357	0	0	359	0	0	190	6	0	196	560	
18:15	0	0	0	5	1	5	0	0	0	0	0	0	0	3	284	0	0	287	0	0	192	3	0	195	487	
18:30	0	1	0	3	0	4	0	0	0	0	0	0	0	4	258	0	0	262	0	0	208	2	0	210	476	
18:45	0	0	0	4	1	4	0	0	0	0	1	0	0	3	224	0	0	227	0	0	167	4	1	171	402	
Hourly Total	0	2	0	16	2	18	0	0	0	0	2	0	0	12	1123	0	0	1135	0	0	757	15	1	772	1925	
Grand Total	0	10	0	254	22	264	0	0	0	3	14	3	0	110	9128	9	0	9247	0	5	8152	115	5	8272	17786	
Approach %	0.0	3.8	0.0	96.2	-	-	0.0	0.0	0.0	100.0	-	-	0.0	1.2	98.7	0.1	-	-	0.0	0.1	98.5	1.4	-	-	-	
Total %	0.0	0.1	0.0	1.4	-	1.5	0.0	0.0	0.0	0.0	-	0.0	0.0	0.6	51.3	0.1	-	52.0	0.0	0.0	45.8	0.6	-	46.5	-	
Lights	0	10	0	253	11	263	0	0	0	3	7	3	0	109	9071	9	0	9189	0	5	8083	112	5	8200	17655	
% Lights	-	100.0	-	99.6	50.0	99.6	-	-	-	100.0	50.0	100.0	-	-	99.1	99.4	100.0	-	99.4	-	100.0	99.2	97.4	100.0	99.1	99.3
Other Vehicles	0	0	0	1	11	1	0	0	0	0	7	0	0	0	57	0	0	57	0	0	68	3	0	71	129	
% Other Vehicles	-	0.0	-	0.4	50.0	0.4	-	-	-	0.0	50.0	0.0	-	-	0.0	0.6	0.0	-	0.6	-	0.0	0.8	2.6	0.0	0.9	0.7
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	1	0	0	1	2	
% Bicycles on Road	-	0.0	-	0.0	0.0	0.0	-	-	-	0.0	0.0	0.0	-	-	0.9	0.0	0.0	-	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:00)

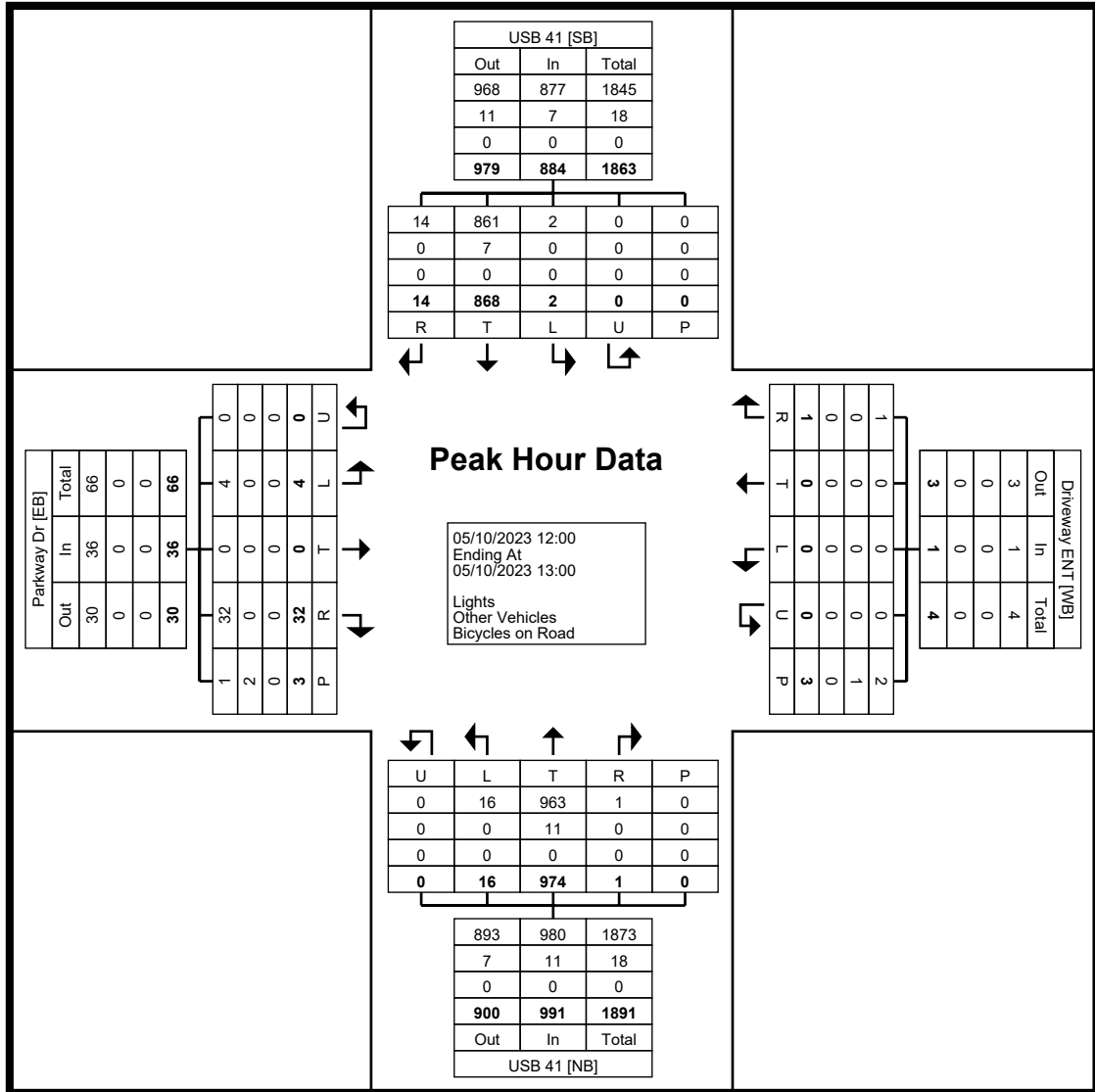
Start Time	Parkway Dr Eastbound						Driveway ENT Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	0	0	16	0	16	0	0	0	0	0	0	0	4	176	1	0	181	0	0	455	4	0	459	656
7:15	0	0	0	13	0	13	0	0	0	0	0	0	0	4	182	0	0	186	0	0	419	5	0	424	623
7:30	0	0	0	17	0	17	0	0	0	0	1	0	0	2	222	0	0	224	0	0	344	2	0	346	587
7:45	0	0	0	8	1	8	0	0	0	0	1	0	0	0	213	0	0	213	0	1	327	2	0	330	551
<b>Total</b>	0	0	0	54	1	54	0	0	0	0	2	0	0	10	793	1	0	804	0	1	1545	13	0	1559	2417
Approach %	0.0	0.0	0.0	100.0	-	-	0.0	0.0	0.0	0.0	-	-	0.0	1.2	98.6	0.1	-	-	0.0	0.1	99.1	0.8	-	-	-
Total %	0.0	0.0	0.0	2.2	-	2.2	0.0	0.0	0.0	0.0	-	0.0	0.0	0.4	32.8	0.0	-	33.3	0.0	0.0	63.9	0.5	-	64.5	-
PHF	0.000	0.000	0.000	0.794	-	0.794	0.000	0.000	0.000	0.000	-	0.000	0.000	0.625	0.893	0.250	-	0.897	0.000	0.250	0.849	0.650	-	0.849	0.921
Lights	0	0	0	54	1	54	0	0	0	0	1	0	0	10	789	1	0	800	0	1	1524	11	0	1536	2390
% Lights	-	-	-	100.0	100.0	100.0	-	-	-	-	50.0	-	-	100.0	99.5	100.0	-	99.5	-	100.0	98.6	84.6	-	98.5	98.9
Other Vehicles	0	0	0	0	0	0	0	0	0	0	1	0	0	0	4	0	0	4	0	0	21	2	0	23	27
% Other Vehicles	-	-	-	0.0	0.0	0.0	-	-	-	-	50.0	-	-	0.0	0.5	0.0	-	0.5	-	0.0	1.4	15.4	-	1.5	1.1
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	-	-	0.0	0.0	0.0	-	-	-	-	0.0	-	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0



Turning Movement Peak Hour Data Plot (7:00)

### Turning Movement Peak Hour Data (12:00)

Start Time	Parkway Dr Eastbound						Driveway ENT Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	2	0	11	1	13	0	0	0	0	1	0	0	6	256	0	0	262	0	0	201	4	0	205	480
12:15	0	1	0	4	0	5	0	0	0	1	1	1	0	5	233	1	0	239	0	0	216	4	0	220	465
12:30	0	1	0	8	2	9	0	0	0	0	1	0	0	1	249	0	0	250	0	2	241	5	0	248	507
12:45	0	0	0	9	0	9	0	0	0	0	0	0	0	4	236	0	0	240	0	0	210	1	0	211	460
<b>Total</b>	<b>0</b>	<b>4</b>	<b>0</b>	<b>32</b>	<b>3</b>	<b>36</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>3</b>	<b>1</b>	<b>0</b>	<b>16</b>	<b>974</b>	<b>1</b>	<b>0</b>	<b>991</b>	<b>0</b>	<b>2</b>	<b>868</b>	<b>14</b>	<b>0</b>	<b>884</b>	<b>1912</b>
Approach %	0.0	11.1	0.0	88.9	-	-	0.0	0.0	0.0	100.0	-	-	0.0	1.6	98.3	0.1	-	-	0.0	0.2	98.2	1.6	-	-	-
Total %	0.0	0.2	0.0	1.7	-	1.9	0.0	0.0	0.0	0.1	-	0.1	0.0	0.8	50.9	0.1	-	51.8	0.0	0.1	45.4	0.7	-	46.2	-
PHF	0.000	0.500	0.000	0.727	-	0.692	0.000	0.000	0.000	0.250	-	0.250	0.000	0.667	0.951	0.250	-	0.946	0.000	0.250	0.900	0.700	-	0.891	0.943
Lights	0	4	0	32	1	36	0	0	0	1	2	1	0	16	963	1	0	980	0	2	861	14	0	877	1894
% Lights	-	100.0	-	100.0	33.3	100.0	-	-	-	100.0	66.7	100.0	-	100.0	98.9	100.0	-	98.9	-	100.0	99.2	100.0	-	99.2	99.1
Other Vehicles	0	0	0	0	2	0	0	0	0	0	1	0	0	0	11	0	0	11	0	0	7	0	0	7	18
% Other Vehicles	-	0.0	-	0.0	66.7	0.0	-	-	-	0.0	33.3	0.0	-	0.0	1.1	0.0	-	1.1	-	0.0	0.8	0.0	-	0.8	0.9
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	-	0.0	0.0	0.0	-	-	-	0.0	0.0	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0

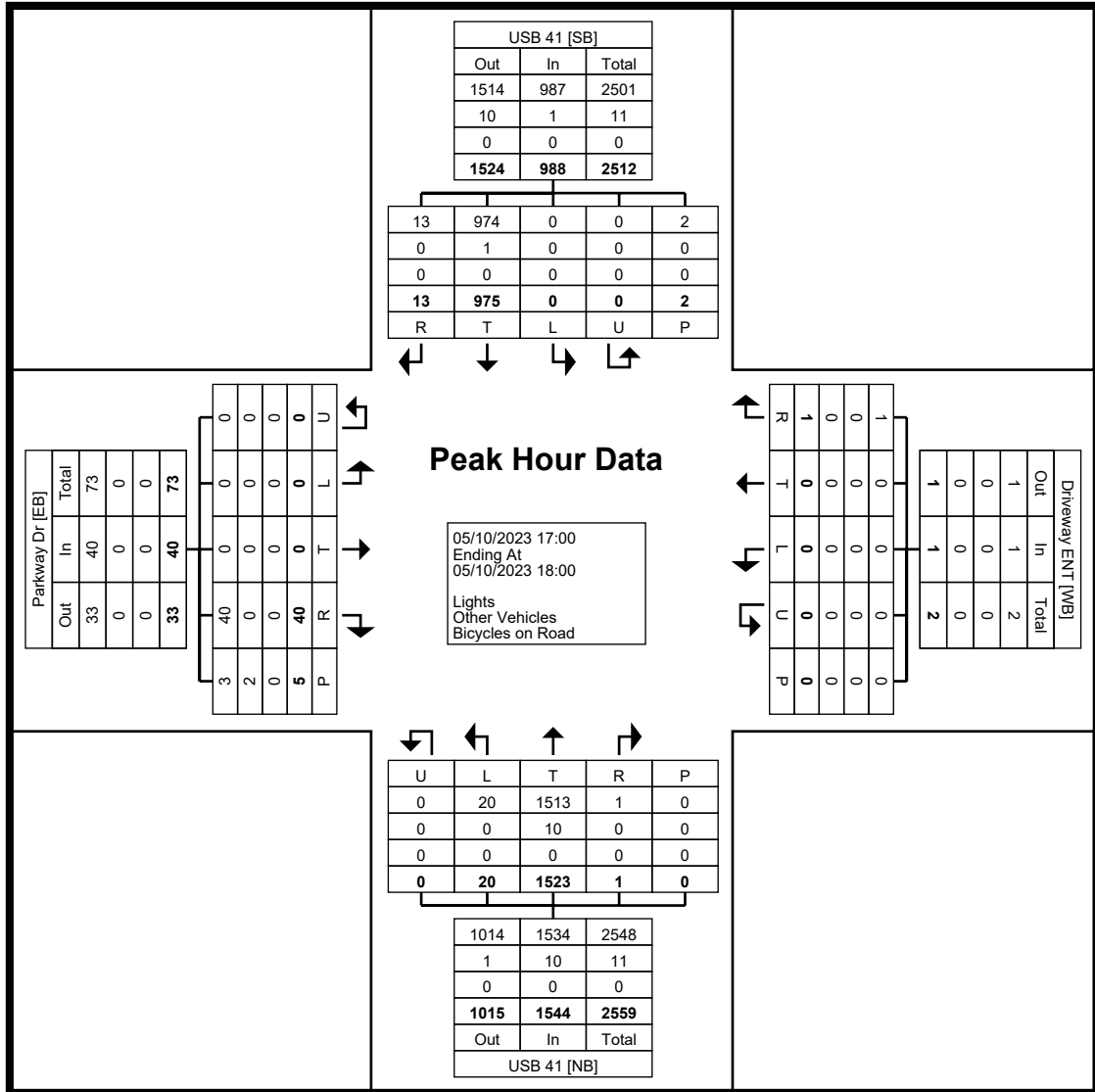


Turning Movement Peak Hour Data Plot (12:00)



### Turning Movement Peak Hour Data (17:00)

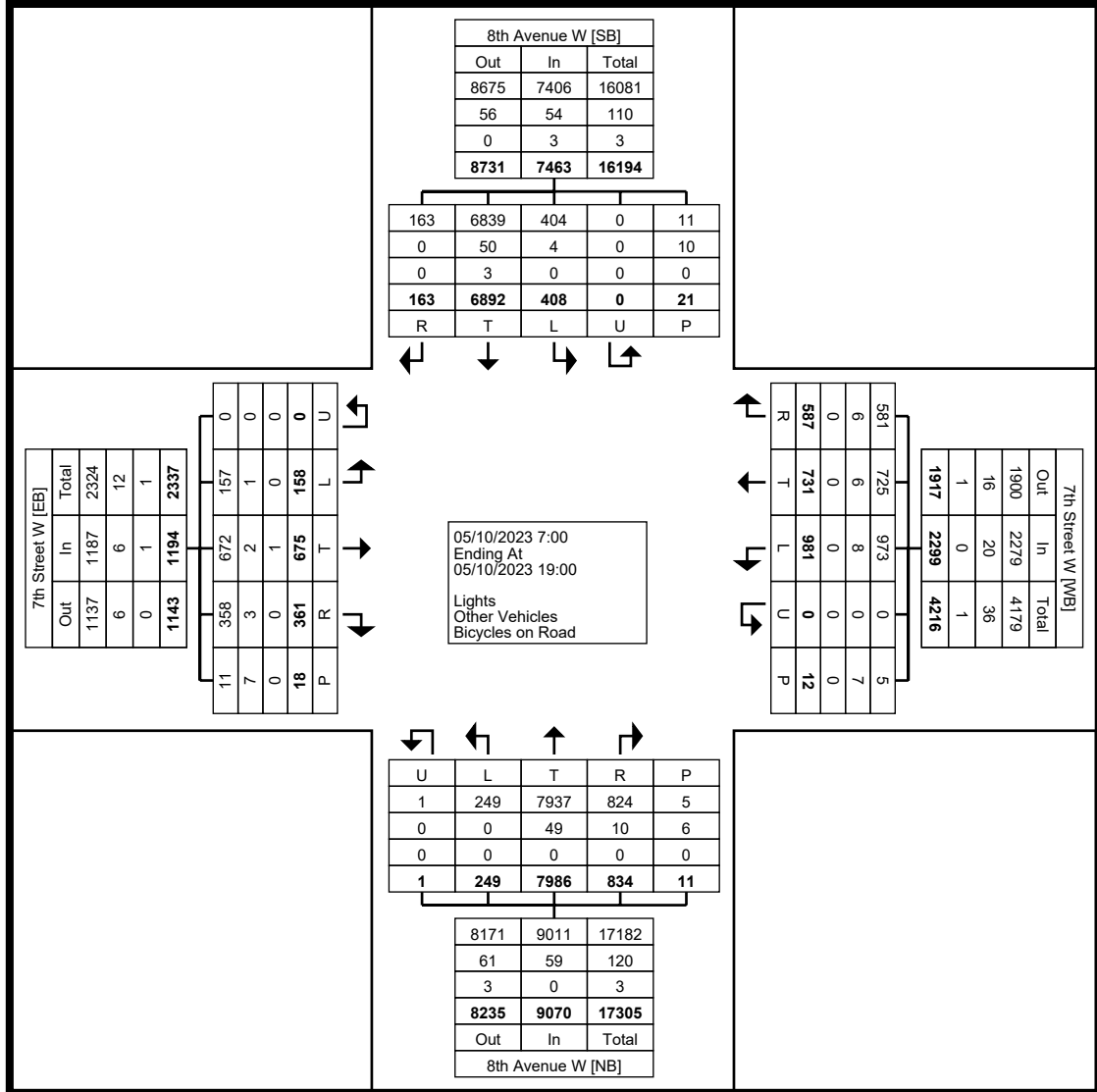
Start Time	Parkway Dr Eastbound						Driveway ENT Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
17:00	0	0	0	6	0	6	0	0	0	0	0	0	0	4	405	0	0	409	0	0	223	4	1	227	642
17:15	0	0	0	13	3	13	0	0	0	0	0	0	0	4	389	0	0	393	0	0	237	1	0	238	644
17:30	0	0	0	12	2	12	0	0	0	1	0	1	0	5	362	0	0	367	0	0	268	4	1	272	652
17:45	0	0	0	9	0	9	0	0	0	0	0	0	0	7	367	1	0	375	0	0	247	4	0	251	635
<b>Total</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>40</b>	<b>5</b>	<b>40</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>20</b>	<b>1523</b>	<b>1</b>	<b>0</b>	<b>1544</b>	<b>0</b>	<b>0</b>	<b>975</b>	<b>13</b>	<b>2</b>	<b>988</b>	<b>2573</b>
Approach %	0.0	0.0	0.0	100.0	-	-	0.0	0.0	0.0	100.0	-	-	0.0	1.3	98.6	0.1	-	-	0.0	0.0	98.7	1.3	-	-	-
Total %	0.0	0.0	0.0	1.6	-	1.6	0.0	0.0	0.0	0.0	-	0.0	0.0	0.8	59.2	0.0	-	60.0	0.0	0.0	37.9	0.5	-	38.4	-
PHF	0.000	0.000	0.000	0.769	-	0.769	0.000	0.000	0.000	0.250	-	0.250	0.000	0.714	0.940	0.250	-	0.944	0.000	0.000	0.910	0.813	-	0.908	0.987
Lights	0	0	0	40	3	40	0	0	0	1	0	1	0	20	1513	1	0	1534	0	0	974	13	2	987	2562
% Lights	-	-	-	100.0	60.0	100.0	-	-	-	100.0	-	100.0	-	100.0	99.3	100.0	-	99.4	-	-	99.9	100.0	100.0	99.9	99.6
Other Vehicles	0	0	0	0	2	0	0	0	0	0	0	0	0	0	10	0	0	10	0	0	1	0	0	1	11
% Other Vehicles	-	-	-	0.0	40.0	0.0	-	-	-	0.0	-	0.0	-	0.0	0.7	0.0	-	0.6	-	-	0.1	0.0	0.0	0.1	0.4
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	-	-	0.0	0.0	0.0	-	-	-	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	-	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (17:00)

### Turning Movement Data

Start Time	7th Street W Eastbound						7th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	5	18	16	0	39	0	60	12	2	0	74	0	5	141	10	0	156	0	1	382	2	0	385	654
7:15	0	9	18	38	0	65	0	80	21	1	0	102	0	4	180	11	0	195	0	0	309	5	0	314	676
7:30	0	7	25	44	1	76	0	73	30	5	1	108	0	4	187	20	0	211	0	3	243	2	0	248	643
7:45	0	1	22	35	1	58	0	52	19	3	1	74	0	6	179	22	0	207	0	5	231	2	0	238	577
Hourly Total	0	22	83	133	2	238	0	265	82	11	2	358	0	19	687	63	0	769	0	9	1165	11	0	1185	2550
8:00	0	2	24	26	1	52	0	57	14	7	1	78	0	4	195	11	0	210	0	1	263	5	2	269	609
8:15	0	4	20	14	0	38	0	60	17	7	0	84	0	6	199	31	1	236	0	2	264	7	0	273	631
8:30	0	3	17	10	0	30	0	28	10	12	0	50	0	3	164	20	0	187	0	14	290	8	1	312	579
8:45	0	7	19	11	0	37	0	23	10	9	0	42	0	5	172	16	1	193	0	11	294	4	0	309	581
Hourly Total	0	16	80	61	1	157	0	168	51	35	1	254	0	18	730	78	2	826	0	28	1111	24	3	1163	2400
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	12	22	7	0	41	0	11	22	13	1	46	0	8	229	20	1	257	0	12	186	2	0	200	544
12:15	0	9	20	9	0	38	0	25	16	23	1	64	0	8	200	26	0	234	0	20	184	10	0	214	550
12:30	0	8	11	10	1	29	0	21	16	30	1	67	0	12	197	35	0	244	0	18	208	5	0	231	571
12:45	0	6	23	6	0	35	0	25	25	29	0	79	0	15	199	26	1	240	0	16	190	8	0	214	568
Hourly Total	0	35	76	32	1	143	0	82	79	95	3	256	0	43	825	107	2	975	0	66	768	25	0	859	2233
13:00	0	6	16	7	0	29	0	19	18	17	0	54	0	5	193	19	0	217	0	16	169	5	1	190	490
13:15	0	0	22	5	1	27	0	21	20	18	0	59	0	6	225	20	0	251	0	17	215	8	4	240	577
13:30	0	5	27	6	0	38	0	23	13	25	1	61	0	4	207	23	1	234	0	24	173	4	0	201	534
13:45	0	3	19	13	0	35	0	11	20	26	0	57	1	6	243	27	0	277	0	16	179	4	3	199	568
Hourly Total	0	14	84	31	1	129	0	74	71	86	1	231	1	21	868	89	1	979	0	73	736	21	8	830	2169
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	7	18	7	0	32	0	23	26	20	1	69	0	7	272	42	0	321	0	14	201	4	1	219	641
15:15	0	4	24	13	3	41	0	19	27	31	0	77	0	12	271	30	0	313	0	16	235	5	0	256	687
15:30	0	6	29	4	0	39	0	18	28	15	0	61	0	5	281	44	2	330	0	15	179	1	2	195	625
15:45	0	3	22	10	0	35	0	27	31	18	0	76	0	17	330	42	0	389	0	8	207	8	0	223	723
Hourly Total	0	20	93	34	3	147	0	87	112	84	1	283	0	41	1154	158	2	1353	0	53	822	18	3	893	2676
16:00	0	1	24	7	0	32	0	22	28	23	1	73	0	15	348	27	0	390	0	16	183	5	0	204	699
16:15	0	4	21	3	2	28	0	19	28	22	1	69	0	7	338	46	2	391	0	20	202	7	0	229	717
16:30	0	3	19	8	0	30	0	31	43	24	1	98	0	7	330	38	1	375	0	23	204	5	0	232	735
16:45	0	3	22	4	1	29	0	30	35	18	0	83	0	7	323	24	0	354	0	20	197	4	1	221	687
Hourly Total	0	11	86	22	3	119	0	102	134	87	3	323	0	36	1339	135	3	1510	0	79	786	21	1	886	2838
17:00	0	6	32	2	0	40	0	31	34	24	0	89	0	14	359	35	0	408	0	10	195	8	1	213	750
17:15	0	4	31	4	2	39	0	28	32	24	0	84	0	11	343	31	0	385	0	16	201	4	0	221	729
17:30	0	4	25	7	1	36	0	44	26	20	0	90	0	10	335	27	0	372	0	8	225	7	1	240	738
17:45	0	6	31	11	0	48	0	29	39	26	0	94	0	11	321	26	0	358	0	12	207	7	1	226	726
Hourly Total	0	20	119	24	3	163	0	132	131	94	0	357	0	46	1358	119	0	1523	0	46	828	26	3	900	2943
18:00	0	6	10	6	1	22	0	24	25	22	0	71	0	9	330	24	1	363	0	15	166	5	0	186	642
18:15	0	3	16	8	1	27	0	16	17	23	0	56	0	8	254	26	0	288	0	11	172	4	3	187	558
18:30	0	4	12	4	1	20	0	18	11	18	0	47	0	2	234	21	0	257	0	10	189	5	0	204	528
18:45	0	7	16	6	1	29	0	13	18	32	1	63	0	6	207	14	0	227	0	18	149	3	0	170	489
Hourly Total	0	20	54	24	4	98	0	71	71	95	1	237	0	25	1025	85	1	1135	0	54	676	17	3	747	2217
Grand Total	0	158	675	361	18	1194	0	981	731	587	12	2299	1	249	7986	834	11	9070	0	408	6892	163	21	7463	20026
Approach %	0.0	13.2	56.5	30.2	-	-	0.0	42.7	31.8	25.5	-	-	0.0	2.7	88.0	9.2	-	-	0.0	5.5	92.3	2.2	-	-	-
Total %	0.0	0.8	3.4	1.8	-	6.0	0.0	4.9	3.7	2.9	-	11.5	0.0	1.2	39.9	4.2	-	45.3	0.0	2.0	34.4	0.8	-	37.3	-
Lights	0	157	672	358	11	1187	0	973	725	581	5	2279	1	249	7937	824	5	9011	0	404	6839	163	11	7406	19883
% Lights	-	99.4	99.6	99.2	61.1	99.4	-	99.2	99.2	99.0	41.7	99.1	100.0	100.0	99.4	98.8	45.5	99.3	-	99.0	99.2	100.0	52.4	99.2	99.3
Other Vehicles	0	1	2	3	7	6	0	8	6	6	7	20	0	0	49	10	6	59	0	4	50	0	10	54	139
% Other Vehicles	-	0.6	0.3	0.8	38.9	0.5	-	0.8	0.8	1.0	58.3	0.9	0.0	0.0	0.6	1.2	54.5	0.7	-	1.0	0.7	0.0	47.6	0.7	0.7
Bicycles on Road	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	3	4
% Bicycles on Road	-	0.0	0.1	0.0	0.0	0.1	-	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Data Plot

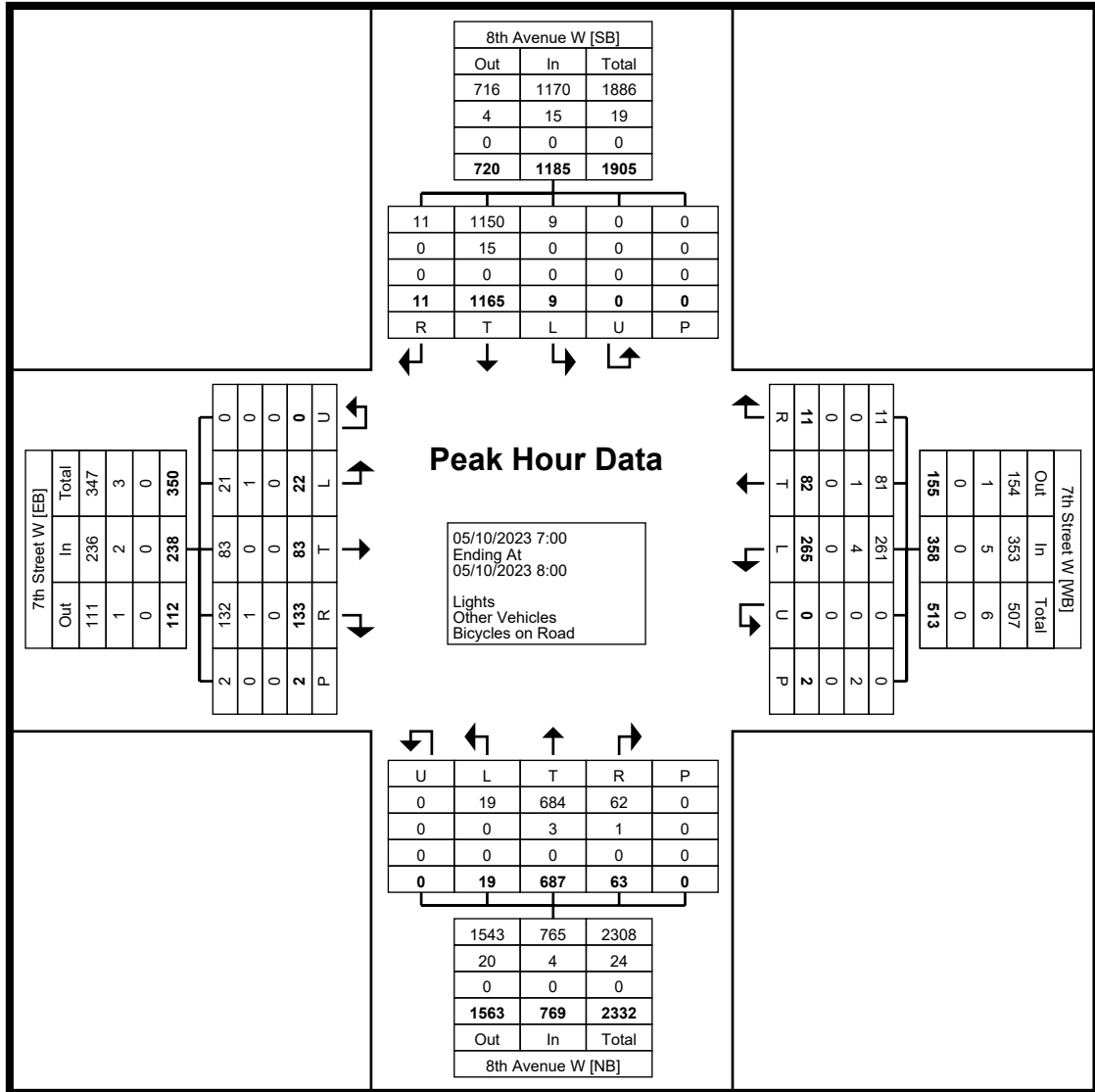
8th Avenue W and 7th Street W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 7\_8th Avenue W  
and 7th Street W  
Site Code: 7  
Start Date: 05/10/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:00)

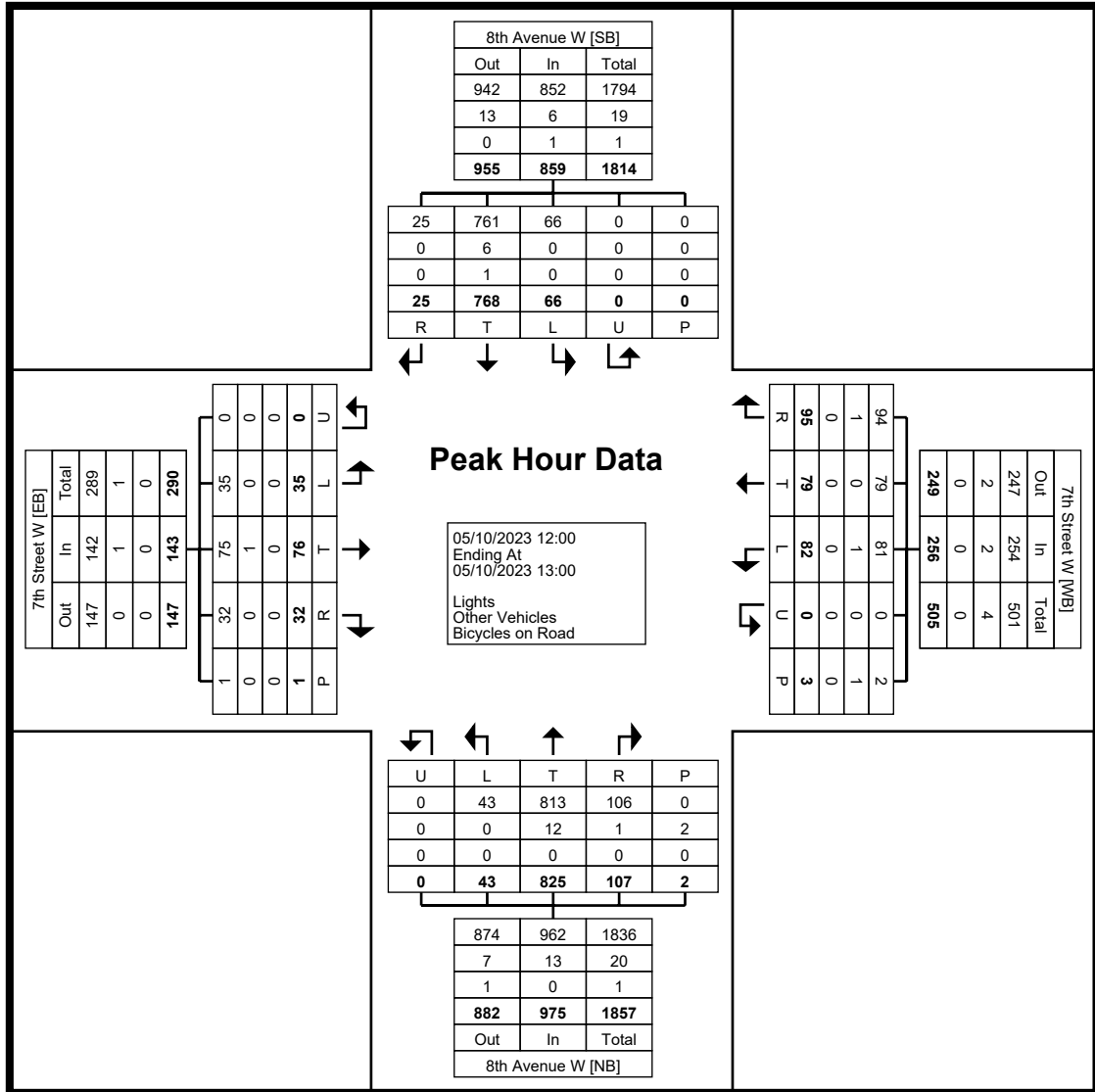
Start Time	7th Street W Eastbound						7th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	5	18	16	0	39	0	60	12	2	0	74	0	5	141	10	0	156	0	1	382	2	0	385	654
7:15	0	9	18	38	0	65	0	80	21	1	0	102	0	4	180	11	0	195	0	0	309	5	0	314	676
7:30	0	7	25	44	1	76	0	73	30	5	1	108	0	4	187	20	0	211	0	3	243	2	0	248	643
7:45	0	1	22	35	1	58	0	52	19	3	1	74	0	6	179	22	0	207	0	5	231	2	0	238	577
<b>Total</b>	<b>0</b>	<b>22</b>	<b>83</b>	<b>133</b>	<b>2</b>	<b>238</b>	<b>0</b>	<b>265</b>	<b>82</b>	<b>11</b>	<b>2</b>	<b>358</b>	<b>0</b>	<b>19</b>	<b>687</b>	<b>63</b>	<b>0</b>	<b>769</b>	<b>0</b>	<b>9</b>	<b>1165</b>	<b>11</b>	<b>0</b>	<b>1185</b>	<b>2550</b>
Approach %	0.0	9.2	34.9	55.9	-	-	0.0	74.0	22.9	3.1	-	-	0.0	2.5	89.3	8.2	-	-	0.0	0.8	98.3	0.9	-	-	-
Total %	0.0	0.9	3.3	5.2	-	9.3	0.0	10.4	3.2	0.4	-	14.0	0.0	0.7	26.9	2.5	-	30.2	0.0	0.4	45.7	0.4	-	46.5	-
PHF	0.000	0.611	0.830	0.756	-	0.783	0.000	0.828	0.683	0.550	-	0.829	0.000	0.792	0.918	0.716	-	0.911	0.000	0.450	0.762	0.550	-	0.769	0.943
Lights	0	21	83	132	2	236	0	261	81	11	0	353	0	19	684	62	0	765	0	9	1150	11	0	1170	2524
% Lights	-	95.5	100.0	99.2	100.0	99.2	-	98.5	98.8	100.0	0.0	98.6	-	100.0	99.6	98.4	-	99.5	-	100.0	98.7	100.0	-	98.7	99.0
Other Vehicles	0	1	0	1	0	2	0	4	1	0	2	5	0	0	3	1	0	4	0	0	15	0	0	15	26
% Other Vehicles	-	4.5	0.0	0.8	0.0	0.8	-	1.5	1.2	0.0	100.0	1.4	-	0.0	0.4	1.6	-	0.5	-	0.0	1.3	0.0	-	1.3	1.0
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0



Turning Movement Peak Hour Data Plot (7:00)

### Turning Movement Peak Hour Data (12:00)

Start Time	7th Street W Eastbound						7th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	12	22	7	0	41	0	11	22	13	1	46	0	8	229	20	1	257	0	12	186	2	0	200	544
12:15	0	9	20	9	0	38	0	25	16	23	1	64	0	8	200	26	0	234	0	20	184	10	0	214	550
12:30	0	8	11	10	1	29	0	21	16	30	1	67	0	12	197	35	0	244	0	18	208	5	0	231	571
12:45	0	6	23	6	0	35	0	25	25	29	0	79	0	15	199	26	1	240	0	16	190	8	0	214	568
<b>Total</b>	<b>0</b>	<b>35</b>	<b>76</b>	<b>32</b>	<b>1</b>	<b>143</b>	<b>0</b>	<b>82</b>	<b>79</b>	<b>95</b>	<b>3</b>	<b>256</b>	<b>0</b>	<b>43</b>	<b>825</b>	<b>107</b>	<b>2</b>	<b>975</b>	<b>0</b>	<b>66</b>	<b>768</b>	<b>25</b>	<b>0</b>	<b>859</b>	<b>2233</b>
Approach %	0.0	24.5	53.1	22.4	-	-	0.0	32.0	30.9	37.1	-	-	0.0	4.4	84.6	11.0	-	-	0.0	7.7	89.4	2.9	-	-	-
Total %	0.0	1.6	3.4	1.4	-	6.4	0.0	3.7	3.5	4.3	-	11.5	0.0	1.9	36.9	4.8	-	43.7	0.0	3.0	34.4	1.1	-	38.5	-
PHF	0.000	0.729	0.826	0.800	-	0.872	0.000	0.820	0.790	0.792	-	0.810	0.000	0.717	0.901	0.764	-	0.948	0.000	0.825	0.923	0.625	-	0.930	0.978
Lights	0	35	75	32	1	142	0	81	79	94	2	254	0	43	813	106	0	962	0	66	761	25	0	852	2210
% Lights	-	100.0	98.7	100.0	100.0	99.3	-	98.8	100.0	98.9	66.7	99.2	-	100.0	98.5	99.1	0.0	98.7	-	100.0	99.1	100.0	-	99.2	99.0
Other Vehicles	0	0	1	0	0	1	0	1	0	1	1	2	0	0	12	1	2	13	0	0	6	0	0	6	22
% Other Vehicles	-	0.0	1.3	0.0	0.0	0.7	-	1.2	0.0	1.1	33.3	0.8	-	0.0	1.5	0.9	100.0	1.3	-	0.0	0.8	0.0	-	0.7	1.0
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.1	0.0	-	0.1	0.0



Turning Movement Peak Hour Data Plot (12:00)



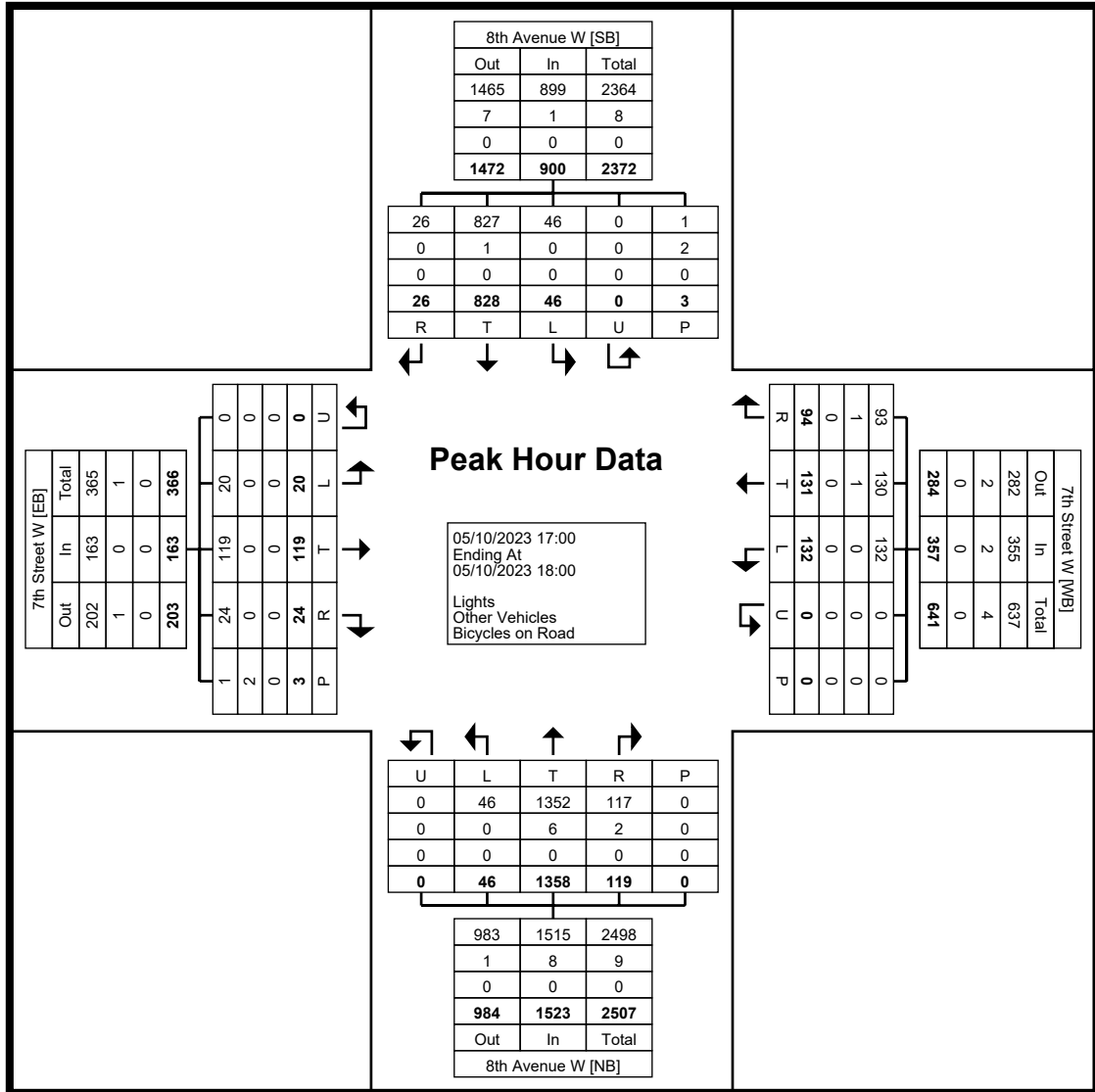
8th Avenue W and 7th Street W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 7\_8th Avenue W  
and 7th Street W  
Site Code: 7  
Start Date: 05/10/2023  
Page No: 7

### Turning Movement Peak Hour Data (17:00)

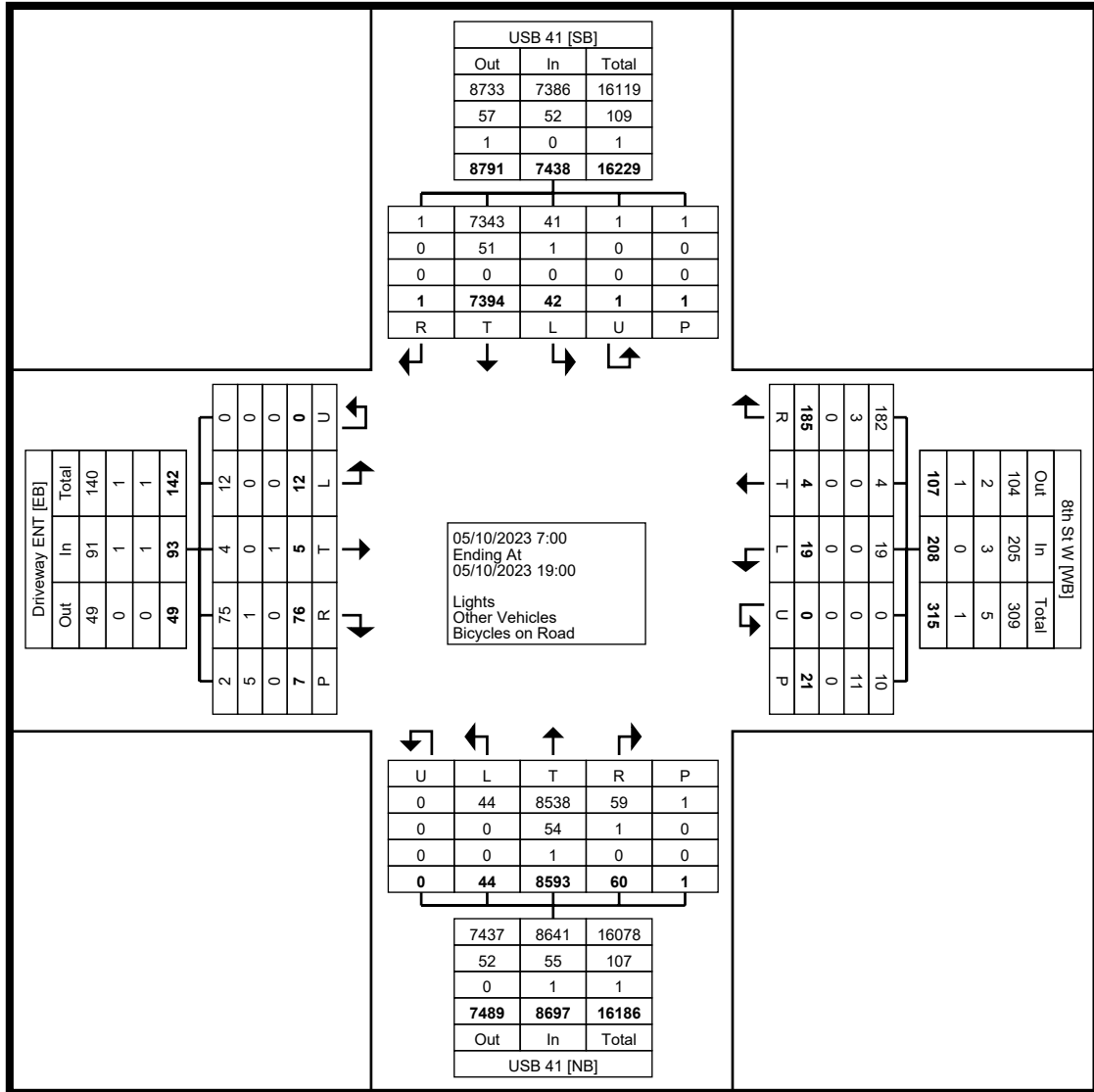
Start Time	7th Street W Eastbound						7th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
17:00	0	6	32	2	0	40	0	31	34	24	0	89	0	14	359	35	0	408	0	10	195	8	1	213	750
17:15	0	4	31	4	2	39	0	28	32	24	0	84	0	11	343	31	0	385	0	16	201	4	0	221	729
17:30	0	4	25	7	1	36	0	44	26	20	0	90	0	10	335	27	0	372	0	8	225	7	1	240	738
17:45	0	6	31	11	0	48	0	29	39	26	0	94	0	11	321	26	0	358	0	12	207	7	1	226	726
<b>Total</b>	0	20	119	24	3	163	0	132	131	94	0	357	0	46	1358	119	0	1523	0	46	828	26	3	900	2943
Approach %	0.0	12.3	73.0	14.7	-	-	0.0	37.0	36.7	26.3	-	-	0.0	3.0	89.2	7.8	-	-	0.0	5.1	92.0	2.9	-	-	-
Total %	0.0	0.7	4.0	0.8	-	5.5	0.0	4.5	4.5	3.2	-	12.1	0.0	1.6	46.1	4.0	-	51.7	0.0	1.6	28.1	0.9	-	30.6	-
PHF	0.000	0.833	0.930	0.545	-	0.849	0.000	0.750	0.840	0.904	-	0.949	0.000	0.821	0.946	0.850	-	0.933	0.000	0.719	0.920	0.813	-	0.938	0.981
Lights	0	20	119	24	1	163	0	132	130	93	0	355	0	46	1352	117	0	1515	0	46	827	26	1	899	2932
% Lights	-	100.0	100.0	100.0	33.3	100.0	-	100.0	99.2	98.9	-	99.4	-	100.0	99.6	98.3	-	99.5	-	100.0	99.9	100.0	33.3	99.9	99.6
Other Vehicles	0	0	0	0	2	0	0	0	1	1	0	2	0	0	6	2	0	8	0	0	1	0	2	1	11
% Other Vehicles	-	0.0	0.0	0.0	66.7	0.0	-	0.0	0.8	1.1	-	0.6	-	0.0	0.4	1.7	-	0.5	-	0.0	0.1	0.0	66.7	0.1	0.4
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (17:00)

### Turning Movement Data

Start Time	Driveway ENT Eastbound						8th St W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	0	0	0	0	0	0	0	0	2	0	2	0	0	153	1	0	154	0	0	376	0	0	376	532
7:15	0	0	0	0	0	0	0	0	0	0	0	0	0	1	183	3	0	187	0	0	322	0	1	322	509
7:30	0	0	0	0	0	0	0	0	0	1	1	1	0	0	202	2	0	204	0	0	256	0	0	256	461
7:45	0	0	0	0	0	0	0	0	0	0	3	0	0	0	187	2	0	189	0	2	258	0	0	260	449
Hourly Total	0	0	0	0	0	0	0	0	0	3	4	3	0	1	725	8	0	734	0	2	1212	0	1	1214	1951
8:00	0	0	0	0	0	0	0	0	0	1	1	1	0	0	208	0	0	208	0	1	272	0	0	273	482
8:15	0	0	0	0	0	0	0	0	0	2	0	2	0	0	206	1	0	207	0	3	256	0	0	259	468
8:30	0	1	0	1	0	2	0	1	0	3	0	4	0	0	177	1	0	178	0	0	321	0	0	321	505
8:45	0	0	0	1	0	1	0	0	0	5	0	5	0	0	195	2	0	197	0	0	312	0	0	312	515
Hourly Total	0	1	0	2	0	3	0	1	0	11	1	12	0	0	786	4	0	790	0	4	1161	0	0	1165	1970
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	1	4	0	5	0	1	0	12	0	13	0	6	241	1	0	248	0	2	187	0	0	189	455
12:15	0	0	0	7	0	7	0	4	0	6	0	10	0	5	228	1	0	234	0	2	202	0	0	204	455
12:30	0	0	0	5	0	5	0	0	0	12	2	12	0	2	230	1	0	233	0	4	243	0	0	247	497
12:45	0	2	0	2	0	4	0	0	0	4	1	4	0	3	228	3	0	234	0	2	190	0	0	192	434
Hourly Total	0	2	1	18	0	21	0	5	0	34	3	39	0	16	927	6	0	949	0	10	822	0	0	832	1841
13:00	0	1	0	4	0	5	0	0	0	6	1	6	0	1	214	1	0	216	0	1	207	0	0	208	435
13:15	0	0	1	4	0	5	0	3	0	5	4	8	0	1	219	0	0	220	0	2	213	0	0	215	448
13:30	0	1	0	5	0	6	0	2	2	7	0	11	0	3	252	3	0	258	0	1	200	0	0	201	476
13:45	0	0	0	4	0	4	0	1	0	5	0	6	0	0	250	2	0	252	0	2	201	0	0	203	465
Hourly Total	0	2	1	17	0	20	0	6	2	23	5	31	0	5	935	6	0	946	0	6	821	0	0	827	1824
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	0	0	1	0	1	0	1	0	9	1	10	0	0	297	4	0	301	0	2	220	0	0	222	534
15:15	0	0	0	7	0	7	0	0	1	12	0	13	0	0	306	1	0	307	0	2	243	0	0	245	572
15:30	0	0	0	1	0	1	0	1	0	4	0	5	0	3	300	5	0	308	0	3	200	0	0	203	517
15:45	0	1	0	2	0	3	0	1	1	8	0	10	0	0	332	7	1	339	0	0	219	0	0	219	571
Hourly Total	0	1	0	11	0	12	0	3	2	33	1	38	0	3	1235	17	1	1255	0	7	882	0	0	889	2194
16:00	0	0	1	1	0	2	0	0	0	9	1	9	0	2	367	3	0	372	0	1	201	0	0	202	585
16:15	0	1	0	2	0	3	0	1	0	9	1	10	0	1	362	0	0	363	0	4	217	0	0	221	597
16:30	0	0	0	6	0	6	0	0	0	8	1	8	0	1	343	1	0	345	0	1	232	0	0	233	592
16:45	0	1	0	2	1	3	0	0	0	5	2	5	0	0	348	3	0	351	0	0	229	0	0	229	588
Hourly Total	0	2	1	11	1	14	0	1	0	31	5	32	0	4	1420	7	0	1431	0	6	879	0	0	885	2362
17:00	0	0	0	2	0	2	0	0	0	10	0	10	0	1	382	3	0	386	0	0	202	0	0	202	600
17:15	0	1	0	3	1	4	0	0	0	12	0	12	0	0	366	0	0	366	0	2	228	0	0	230	612
17:30	0	1	2	1	0	4	0	0	0	10	1	10	0	2	354	2	0	358	0	1	227	0	0	228	600
17:45	0	0	0	2	1	2	0	1	0	5	0	6	0	0	346	2	0	348	1	1	219	0	0	221	577
Hourly Total	0	2	2	8	2	12	0	1	0	37	1	38	0	3	1448	7	0	1458	1	4	876	0	0	881	2389
18:00	0	0	0	2	0	2	0	0	0	3	0	3	0	4	348	3	0	355	0	0	187	0	0	187	547
18:15	0	0	0	0	1	0	0	1	0	3	0	4	0	1	274	1	0	276	0	0	187	0	0	187	467
18:30	0	2	0	3	2	5	0	0	0	6	0	6	0	1	245	0	0	246	0	1	202	0	0	203	460
18:45	0	0	0	4	1	4	0	1	0	1	1	2	0	6	250	1	0	257	0	2	165	1	0	168	431
Hourly Total	0	2	0	9	4	11	0	2	0	13	1	15	0	12	1117	5	0	1134	0	3	741	1	0	745	1905
Grand Total	0	12	5	76	7	93	0	19	4	185	21	208	0	44	8593	60	1	8697	1	42	7394	1	1	7438	16436
Approach %	0.0	12.9	5.4	81.7	-	-	0.0	9.1	1.9	88.9	-	-	0.0	0.5	98.8	0.7	-	-	0.0	0.6	99.4	0.0	-	-	-
Total %	0.0	0.1	0.0	0.5	-	0.6	0.0	0.1	0.0	1.1	-	1.3	0.0	0.3	52.3	0.4	-	52.9	0.0	0.3	45.0	0.0	-	45.3	-
Lights	0	12	4	75	2	91	0	19	4	182	10	205	0	44	8538	59	1	8641	1	41	7343	1	1	7386	16323
% Lights	-	100.0	80.0	98.7	28.6	97.8	-	100.0	100.0	98.4	47.6	98.6	-	100.0	99.4	98.3	100.0	99.4	100.0	97.6	99.3	100.0	100.0	99.3	99.3
Other Vehicles	0	0	0	1	5	1	0	0	0	3	11	3	0	0	54	1	0	55	0	1	51	0	0	52	111
% Other Vehicles	-	0.0	0.0	1.3	71.4	1.1	-	0.0	0.0	1.6	52.4	1.4	-	0.0	0.6	1.7	0.0	0.6	0.0	2.4	0.7	0.0	0.0	0.7	0.7
Bicycles on Road	0	0	1	0	0	1	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2
% Bicycles on Road	-	0.0	20.0	0.0	0.0	1.1	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Data Plot

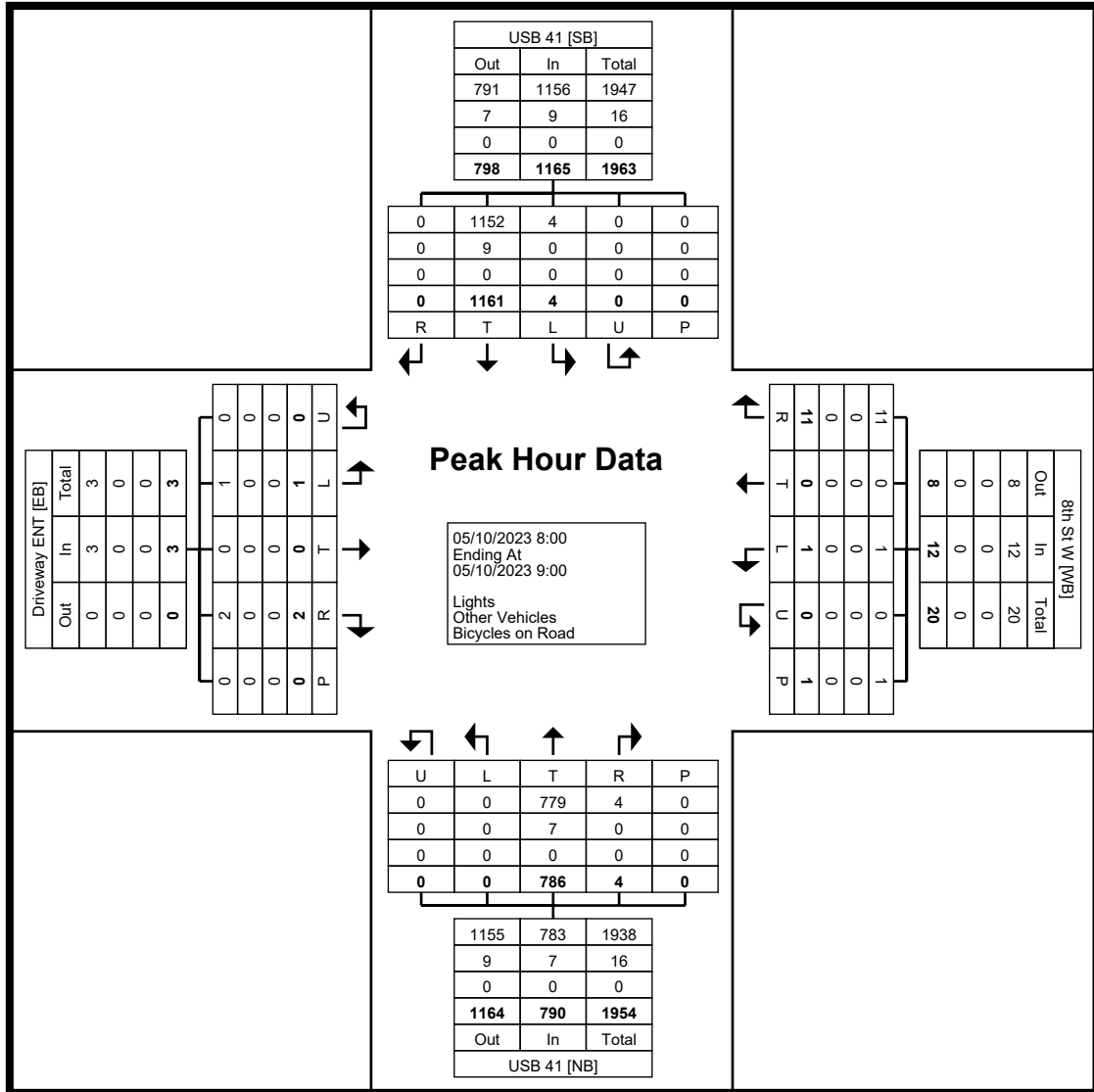
USB 41 & 8th St W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 6\_USB 41 & 8th St W  
Site Code: 6  
Start Date: 05/10/2023  
Page No: 3

### Turning Movement Peak Hour Data (8:00)

Start Time	Driveway ENT Eastbound						8th St W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
8:00	0	0	0	0	0	0	0	0	0	1	1	1	0	0	208	0	0	208	0	1	272	0	0	273	482
8:15	0	0	0	0	0	0	0	0	0	2	0	2	0	0	206	1	0	207	0	3	256	0	0	259	468
8:30	0	1	0	1	0	2	0	1	0	3	0	4	0	0	177	1	0	178	0	0	321	0	0	321	505
8:45	0	0	0	1	0	1	0	0	0	5	0	5	0	0	195	2	0	197	0	0	312	0	0	312	515
<b>Total</b>	0	1	0	2	0	3	0	1	0	11	1	12	0	0	786	4	0	790	0	4	1161	0	0	1165	1970
Approach %	0.0	33.3	0.0	66.7	-	-	0.0	8.3	0.0	91.7	-	-	0.0	0.0	99.5	0.5	-	-	0.0	0.3	99.7	0.0	-	-	-
Total %	0.0	0.1	0.0	0.1	-	0.2	0.0	0.1	0.0	0.6	-	0.6	0.0	0.0	39.9	0.2	-	40.1	0.0	0.2	58.9	0.0	-	59.1	-
PHF	0.000	0.250	0.000	0.500	-	0.375	0.000	0.250	0.000	0.550	-	0.600	0.000	0.000	0.945	0.500	-	0.950	0.000	0.333	0.904	0.000	-	0.907	0.956
Lights	0	1	0	2	0	3	0	1	0	11	1	12	0	0	779	4	0	783	0	4	1152	0	0	1156	1954
% Lights	-	100.0	-	100.0	-	100.0	-	100.0	-	100.0	100.0	100.0	-	-	99.1	100.0	-	99.1	-	100.0	99.2	-	-	99.2	99.2
Other Vehicles	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	0	0	7	0	0	9	0	0	9	16
% Other Vehicles	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	-	-	0.9	0.0	-	0.9	-	0.0	0.8	-	-	0.8	0.8
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	-	-	0.0	0.0	-	0.0	-	0.0	0.0	-	-	0.0	0.0



Turning Movement Peak Hour Data Plot (8:00)

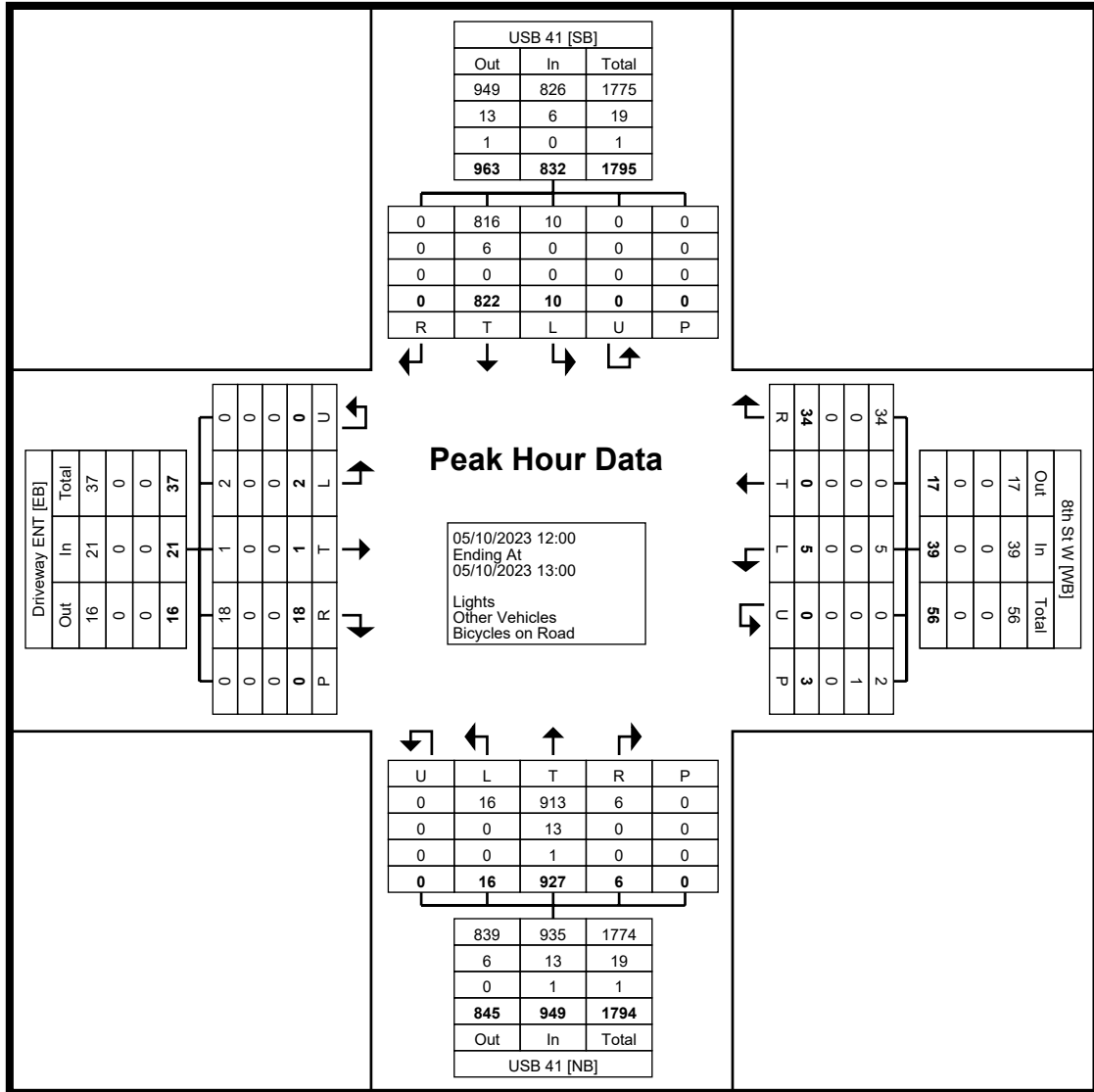
USB 41 & 8th St W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 6\_USB 41 & 8th St  
W  
Site Code: 6  
Start Date: 05/10/2023  
Page No: 5

### Turning Movement Peak Hour Data (12:00)

Start Time	Driveway ENT Eastbound						8th St W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	0	1	4	0	5	0	1	0	12	0	13	0	6	241	1	0	248	0	2	187	0	0	189	455
12:15	0	0	0	7	0	7	0	4	0	6	0	10	0	5	228	1	0	234	0	2	202	0	0	204	455
12:30	0	0	0	5	0	5	0	0	0	12	2	12	0	2	230	1	0	233	0	4	243	0	0	247	497
12:45	0	2	0	2	0	4	0	0	0	4	1	4	0	3	228	3	0	234	0	2	190	0	0	192	434
<b>Total</b>	<b>0</b>	<b>2</b>	<b>1</b>	<b>18</b>	<b>0</b>	<b>21</b>	<b>0</b>	<b>5</b>	<b>0</b>	<b>34</b>	<b>3</b>	<b>39</b>	<b>0</b>	<b>16</b>	<b>927</b>	<b>6</b>	<b>0</b>	<b>949</b>	<b>0</b>	<b>10</b>	<b>822</b>	<b>0</b>	<b>0</b>	<b>832</b>	<b>1841</b>
Approach %	0.0	9.5	4.8	85.7	-	-	0.0	12.8	0.0	87.2	-	-	0.0	1.7	97.7	0.6	-	-	0.0	1.2	98.8	0.0	-	-	-
Total %	0.0	0.1	0.1	1.0	-	1.1	0.0	0.3	0.0	1.8	-	2.1	0.0	0.9	50.4	0.3	-	51.5	0.0	0.5	44.6	0.0	-	45.2	-
PHF	0.000	0.250	0.250	0.643	-	0.750	0.000	0.313	0.000	0.708	-	0.750	0.000	0.667	0.962	0.500	-	0.957	0.000	0.625	0.846	0.000	-	0.842	0.926
Lights	0	2	1	18	0	21	0	5	0	34	2	39	0	16	913	6	0	935	0	10	816	0	0	826	1821
% Lights	-	100.0	100.0	100.0	-	100.0	-	100.0	-	100.0	66.7	100.0	-	100.0	98.5	100.0	-	98.5	-	100.0	99.3	-	-	99.3	98.9
Other Vehicles	0	0	0	0	0	0	0	0	0	0	1	0	0	0	13	0	0	13	0	0	6	0	0	6	19
% Other Vehicles	-	0.0	0.0	0.0	-	0.0	-	0.0	-	0.0	33.3	0.0	-	0.0	1.4	0.0	-	1.4	-	0.0	0.7	-	-	0.7	1.0
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.1	0.0	-	0.1	-	0.0	0.0	-	-	0.0	0.1

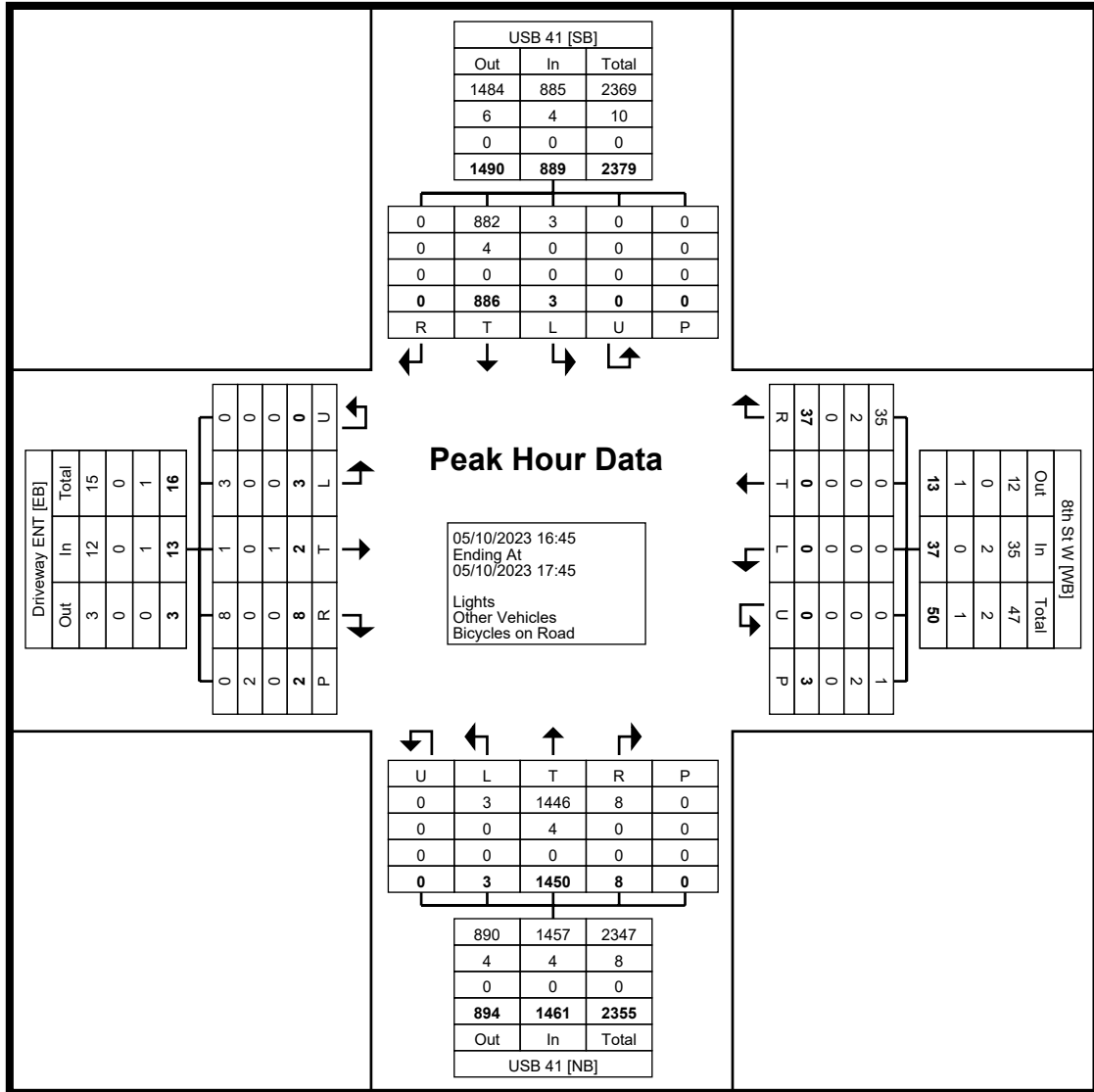


Turning Movement Peak Hour Data Plot (12:00)



### Turning Movement Peak Hour Data (16:45)

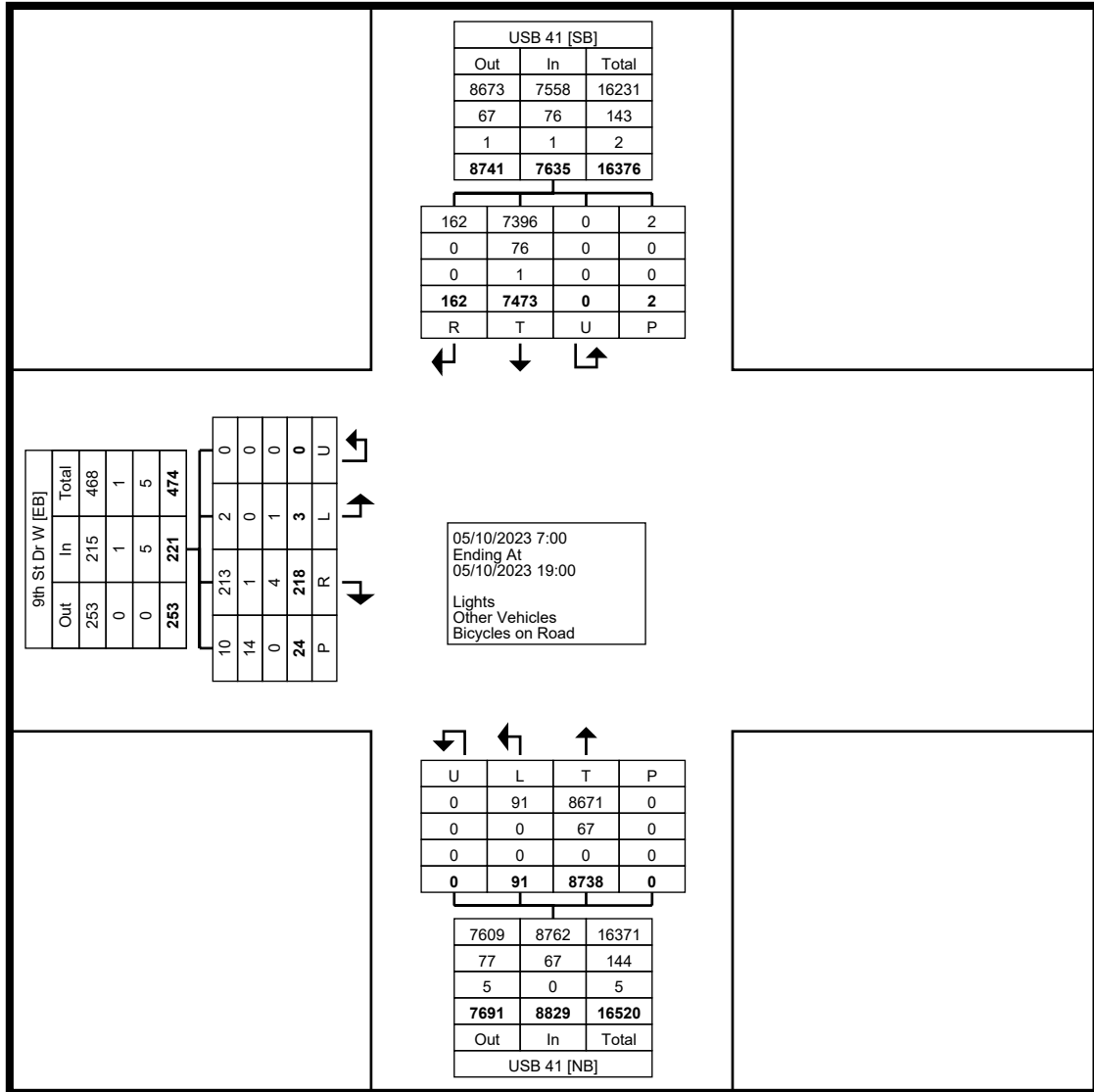
Start Time	Driveway ENT Eastbound						8th St W Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:45	0	1	0	2	1	3	0	0	0	5	2	5	0	0	348	3	0	351	0	0	229	0	0	229	588
17:00	0	0	0	2	0	2	0	0	0	10	0	10	0	1	382	3	0	386	0	0	202	0	0	202	600
17:15	0	1	0	3	1	4	0	0	0	12	0	12	0	0	366	0	0	366	0	2	228	0	0	230	612
17:30	0	1	2	1	0	4	0	0	0	10	1	10	0	2	354	2	0	358	0	1	227	0	0	228	600
<b>Total</b>	<b>0</b>	<b>3</b>	<b>2</b>	<b>8</b>	<b>2</b>	<b>13</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>37</b>	<b>3</b>	<b>37</b>	<b>0</b>	<b>3</b>	<b>1450</b>	<b>8</b>	<b>0</b>	<b>1461</b>	<b>0</b>	<b>3</b>	<b>886</b>	<b>0</b>	<b>0</b>	<b>889</b>	<b>2400</b>
Approach %	0.0	23.1	15.4	61.5	-	-	0.0	0.0	0.0	100.0	-	-	0.0	0.2	99.2	0.5	-	-	0.0	0.3	99.7	0.0	-	-	-
Total %	0.0	0.1	0.1	0.3	-	0.5	0.0	0.0	0.0	1.5	-	1.5	0.0	0.1	60.4	0.3	-	60.9	0.0	0.1	36.9	0.0	-	37.0	-
PHF	0.000	0.750	0.250	0.667	-	0.813	0.000	0.000	0.000	0.771	-	0.771	0.000	0.375	0.949	0.667	-	0.946	0.000	0.375	0.967	0.000	-	0.966	0.980
Lights	0	3	1	8	0	12	0	0	0	35	1	35	0	3	1446	8	0	1457	0	3	882	0	0	885	2389
% Lights	-	100.0	50.0	100.0	0.0	92.3	-	-	-	94.6	33.3	94.6	-	100.0	99.7	100.0	-	99.7	-	100.0	99.5	-	-	99.6	99.5
Other Vehicles	0	0	0	0	2	0	0	0	0	2	2	2	0	0	4	0	0	4	0	0	4	0	0	4	10
% Other Vehicles	-	0.0	0.0	0.0	100.0	0.0	-	-	-	5.4	66.7	5.4	-	0.0	0.3	0.0	-	0.3	-	0.0	0.5	-	-	0.4	0.4
Bicycles on Road	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
% Bicycles on Road	-	0.0	50.0	0.0	0.0	7.7	-	-	-	0.0	0.0	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	-	-	0.0	0.0



Turning Movement Peak Hour Data Plot (16:45)

### Turning Movement Data

Start Time	9th St Dr W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
7:00	0	0	8	1	8	0	4	149	0	153	0	360	6	0	366	527
7:15	0	0	8	0	8	0	3	166	0	169	0	345	3	0	348	525
7:30	0	0	5	0	5	0	3	203	0	206	0	255	2	0	257	468
7:45	0	1	5	0	6	0	3	192	0	195	0	238	4	0	242	443
Hourly Total	0	1	26	1	27	0	13	710	0	723	0	1198	15	0	1213	1963
8:00	0	0	9	1	9	0	1	199	0	200	0	247	3	0	250	459
8:15	0	1	10	0	11	0	2	214	0	216	0	264	3	0	267	494
8:30	0	0	6	0	6	0	2	177	0	179	0	317	6	0	323	508
8:45	0	0	8	1	8	0	1	180	0	181	0	328	5	0	333	522
Hourly Total	0	1	33	2	34	0	6	770	0	776	0	1156	17	0	1173	1983
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	6	0	6	0	8	229	0	237	0	196	7	0	203	446
12:15	0	0	6	3	6	0	4	252	0	256	0	229	3	0	232	494
12:30	0	0	9	2	9	0	3	230	0	233	0	229	9	0	238	480
12:45	0	0	7	0	7	0	1	231	0	232	0	211	2	0	213	452
Hourly Total	0	0	28	5	28	0	16	942	0	958	0	865	21	0	886	1872
13:00	0	0	4	0	4	0	3	211	0	214	0	200	3	0	203	421
13:15	0	0	8	0	8	0	2	240	0	242	0	237	3	0	240	490
13:30	0	0	8	0	8	0	3	267	0	270	0	192	1	0	193	471
13:45	0	0	3	0	3	0	3	240	0	243	0	194	5	0	199	445
Hourly Total	0	0	23	0	23	0	11	958	0	969	0	823	12	0	835	1827
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	0	5	1	5	0	2	292	0	294	0	223	9	0	232	531
15:15	0	0	7	2	7	0	4	312	0	316	0	249	6	0	255	578
15:30	0	0	5	0	5	0	1	291	0	292	0	197	6	0	203	500
15:45	0	0	3	0	3	0	1	354	0	355	0	237	11	0	248	606
Hourly Total	0	0	20	3	20	0	8	1249	0	1257	0	906	32	0	938	2215
16:00	0	0	7	1	7	0	2	386	0	388	0	206	3	0	209	604
16:15	0	0	7	2	7	0	4	357	0	361	0	222	7	0	229	597
16:30	0	0	11	0	11	0	9	345	0	354	0	229	11	0	240	605
16:45	0	0	8	2	8	0	5	347	0	352	0	205	4	0	209	569
Hourly Total	0	0	33	5	33	0	20	1435	0	1455	0	862	25	0	887	2375
17:00	0	0	9	1	9	0	3	379	0	382	0	206	5	0	211	602
17:15	0	1	8	2	9	0	3	385	0	388	0	234	2	2	236	633
17:30	0	0	10	4	10	0	2	369	0	371	0	235	4	0	239	620
17:45	0	0	6	0	6	0	2	356	0	358	0	224	8	0	232	596
Hourly Total	0	1	33	7	34	0	10	1489	0	1499	0	899	19	2	918	2451
18:00	0	0	8	0	8	0	3	373	0	376	0	199	7	0	206	590
18:15	0	0	7	0	7	0	2	303	0	305	0	189	6	0	195	507
18:30	0	0	4	1	4	0	2	270	0	272	0	194	5	0	199	475
18:45	0	0	3	0	3	0	0	239	0	239	0	182	3	0	185	427
Hourly Total	0	0	22	1	22	0	7	1185	0	1192	0	764	21	0	785	1999
Grand Total	0	3	218	24	221	0	91	8738	0	8829	0	7473	162	2	7635	16685
Approach %	0.0	1.4	98.6	-	-	0.0	1.0	99.0	-	-	0.0	97.9	2.1	-	-	-
Total %	0.0	0.0	1.3	-	1.3	0.0	0.5	52.4	-	52.9	0.0	44.8	1.0	-	45.8	-
Lights	0	2	213	10	215	0	91	8671	0	8762	0	7396	162	2	7558	16535
% Lights	-	66.7	97.7	41.7	97.3	-	100.0	99.2	-	99.2	-	99.0	100.0	100.0	99.0	99.1
Other Vehicles	0	0	1	14	1	0	0	67	0	67	0	76	0	0	76	144
% Other Vehicles	-	0.0	0.5	58.3	0.5	-	0.0	0.8	-	0.8	-	1.0	0.0	0.0	1.0	0.9
Bicycles on Road	0	1	4	0	5	0	0	0	0	0	0	1	0	0	1	6
% Bicycles on Road	-	33.3	1.8	0.0	2.3	-	0.0	0.0	-	0.0	-	0.0	0.0	0.0	0.0	0.0



Turning Movement Data Plot

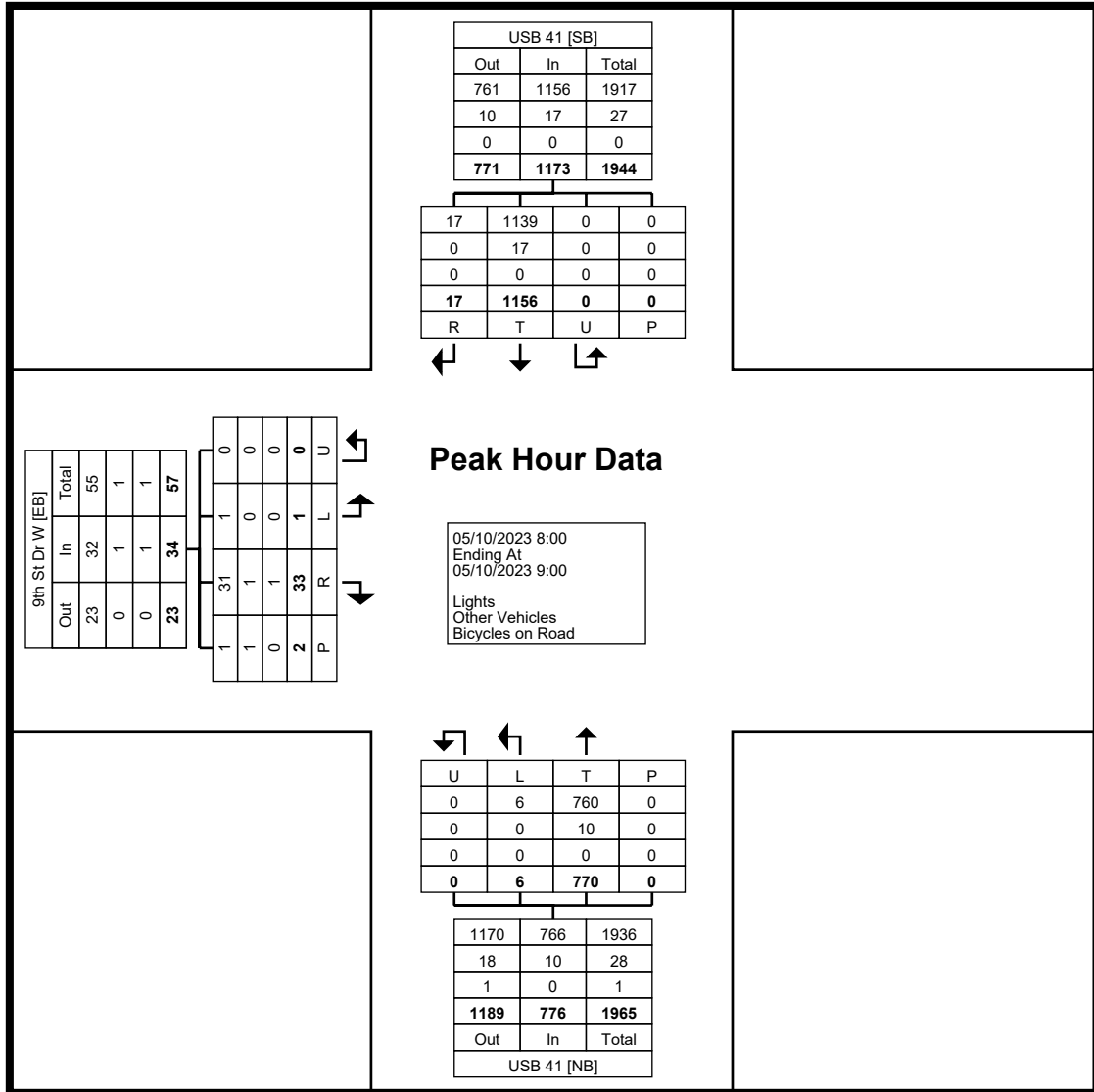
USB 41 & 9th St Dr W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 5\_USB 41 & 9th St  
Dr W  
Site Code: 5  
Start Date: 05/10/2023  
Page No: 3

### Turning Movement Peak Hour Data (8:00)

Start Time	9th St Dr W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
8:00	0	0	9	1	9	0	1	199	0	200	0	247	3	0	250	459
8:15	0	1	10	0	11	0	2	214	0	216	0	264	3	0	267	494
8:30	0	0	6	0	6	0	2	177	0	179	0	317	6	0	323	508
8:45	0	0	8	1	8	0	1	180	0	181	0	328	5	0	333	522
<b>Total</b>	<b>0</b>	<b>1</b>	<b>33</b>	<b>2</b>	<b>34</b>	<b>0</b>	<b>6</b>	<b>770</b>	<b>0</b>	<b>776</b>	<b>0</b>	<b>1156</b>	<b>17</b>	<b>0</b>	<b>1173</b>	<b>1983</b>
Approach %	0.0	2.9	97.1	-	-	0.0	0.8	99.2	-	-	0.0	98.6	1.4	-	-	-
Total %	0.0	0.1	1.7	-	1.7	0.0	0.3	38.8	-	39.1	0.0	58.3	0.9	-	59.2	-
PHF	0.000	0.250	0.825	-	0.773	0.000	0.750	0.900	-	0.898	0.000	0.881	0.708	-	0.881	0.950
Lights	0	1	31	1	32	0	6	760	0	766	0	1139	17	0	1156	1954
% Lights	-	100.0	93.9	50.0	94.1	-	100.0	98.7	-	98.7	-	98.5	100.0	-	98.6	98.5
Other Vehicles	0	0	1	1	1	0	0	10	0	10	0	17	0	0	17	28
% Other Vehicles	-	0.0	3.0	50.0	2.9	-	0.0	1.3	-	1.3	-	1.5	0.0	-	1.4	1.4
Bicycles on Road	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1
% Bicycles on Road	-	0.0	3.0	0.0	2.9	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.1



Turning Movement Peak Hour Data Plot (8:00)

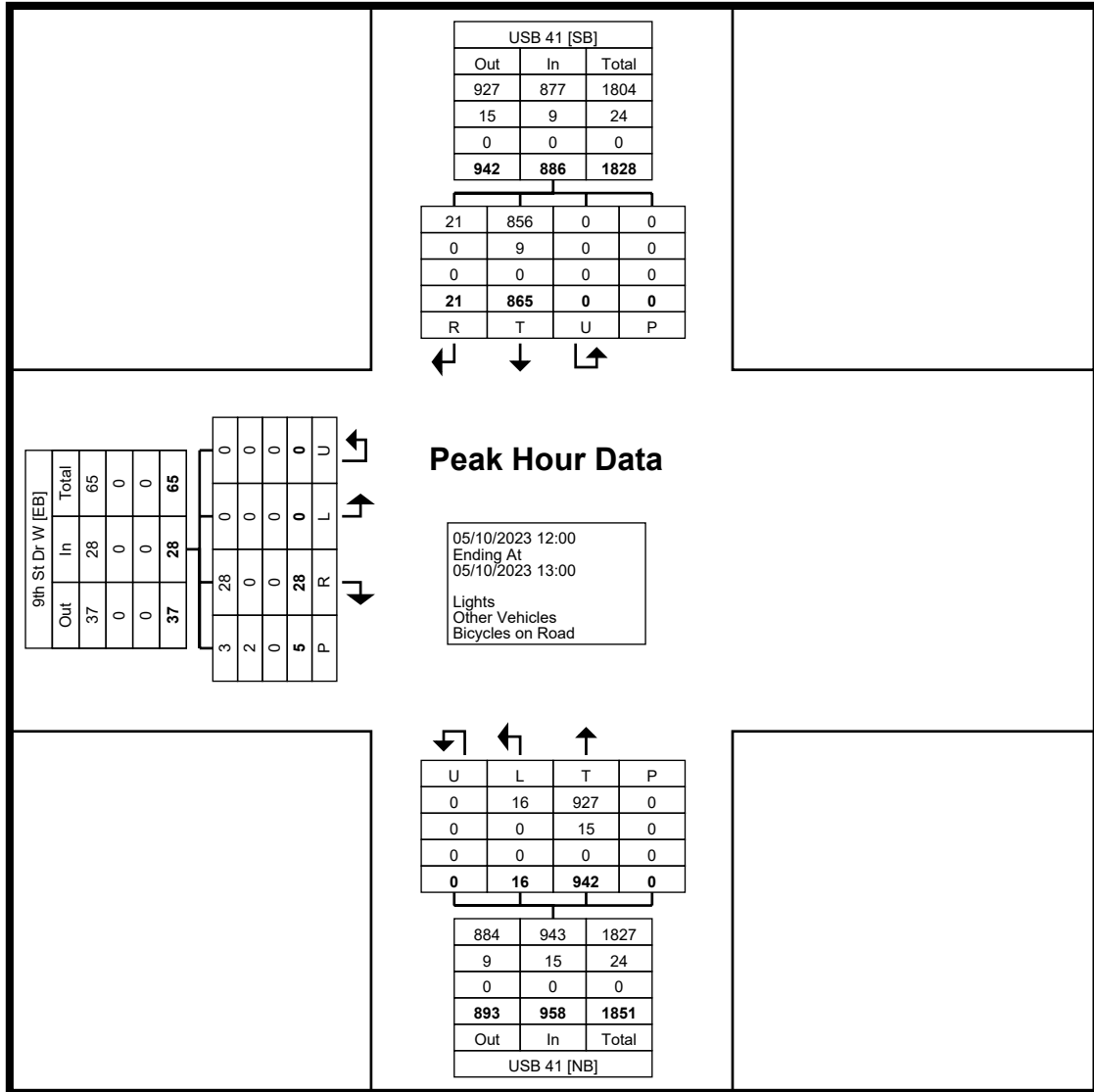
USB 41 & 9th St Dr W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 5\_USB 41 & 9th St  
Dr W  
Site Code: 5  
Start Date: 05/10/2023  
Page No: 5

### Turning Movement Peak Hour Data (12:00)

Start Time	9th St Dr W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
12:00	0	0	6	0	6	0	8	229	0	237	0	196	7	0	203	446
12:15	0	0	6	3	6	0	4	252	0	256	0	229	3	0	232	494
12:30	0	0	9	2	9	0	3	230	0	233	0	229	9	0	238	480
12:45	0	0	7	0	7	0	1	231	0	232	0	211	2	0	213	452
<b>Total</b>	<b>0</b>	<b>0</b>	<b>28</b>	<b>5</b>	<b>28</b>	<b>0</b>	<b>16</b>	<b>942</b>	<b>0</b>	<b>958</b>	<b>0</b>	<b>865</b>	<b>21</b>	<b>0</b>	<b>886</b>	<b>1872</b>
Approach %	0.0	0.0	100.0	-	-	0.0	1.7	98.3	-	-	0.0	97.6	2.4	-	-	-
Total %	0.0	0.0	1.5	-	1.5	0.0	0.9	50.3	-	51.2	0.0	46.2	1.1	-	47.3	-
PHF	0.000	0.000	0.778	-	0.778	0.000	0.500	0.935	-	0.936	0.000	0.944	0.583	-	0.931	0.947
Lights	0	0	28	3	28	0	16	927	0	943	0	856	21	0	877	1848
% Lights	-	-	100.0	60.0	100.0	-	100.0	98.4	-	98.4	-	99.0	100.0	-	99.0	98.7
Other Vehicles	0	0	0	2	0	0	0	15	0	15	0	9	0	0	9	24
% Other Vehicles	-	-	0.0	40.0	0.0	-	0.0	1.6	-	1.6	-	1.0	0.0	-	1.0	1.3
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	-	0.0	0.0	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.0

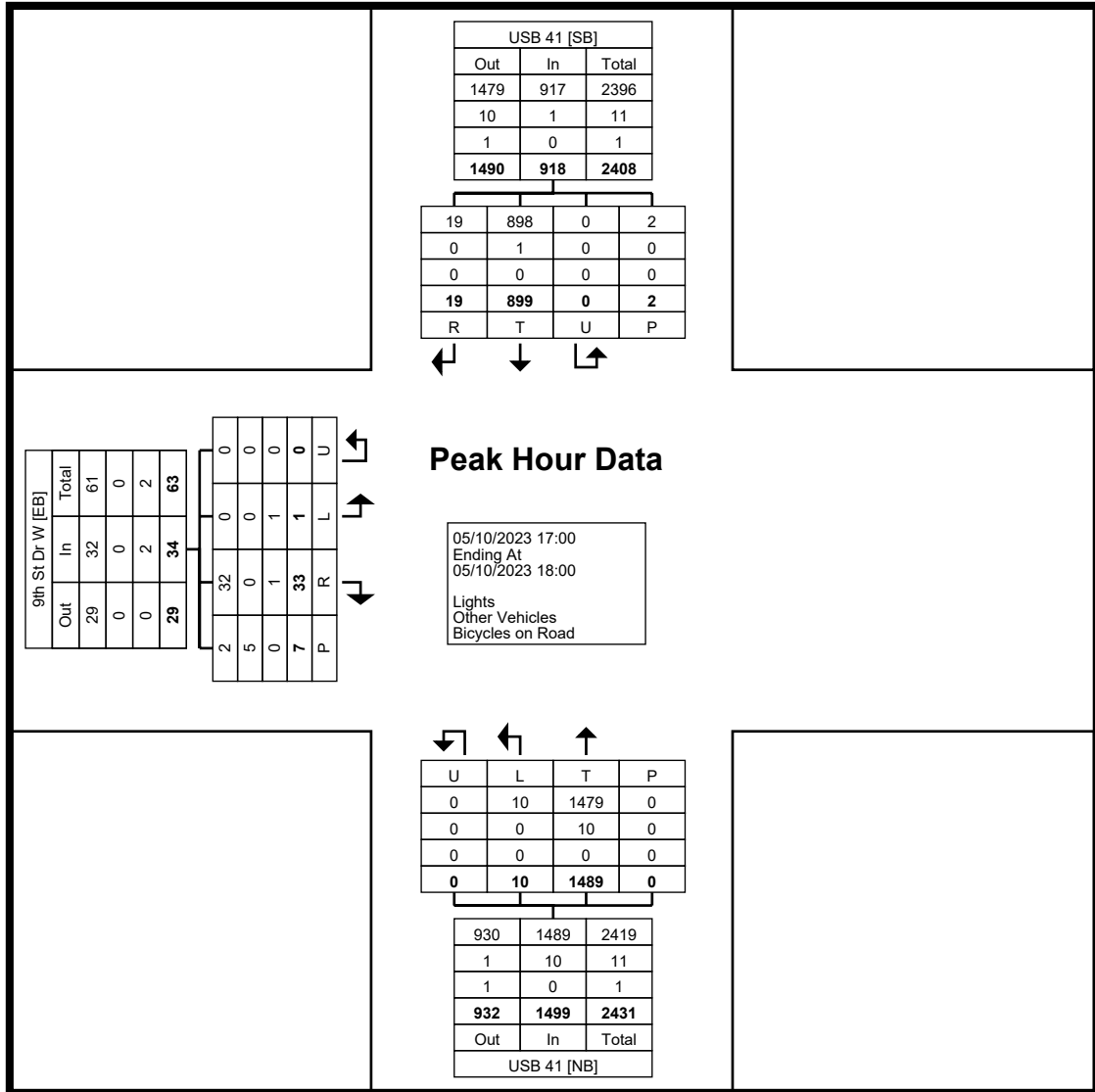


Turning Movement Peak Hour Data Plot (12:00)



### Turning Movement Peak Hour Data (17:00)

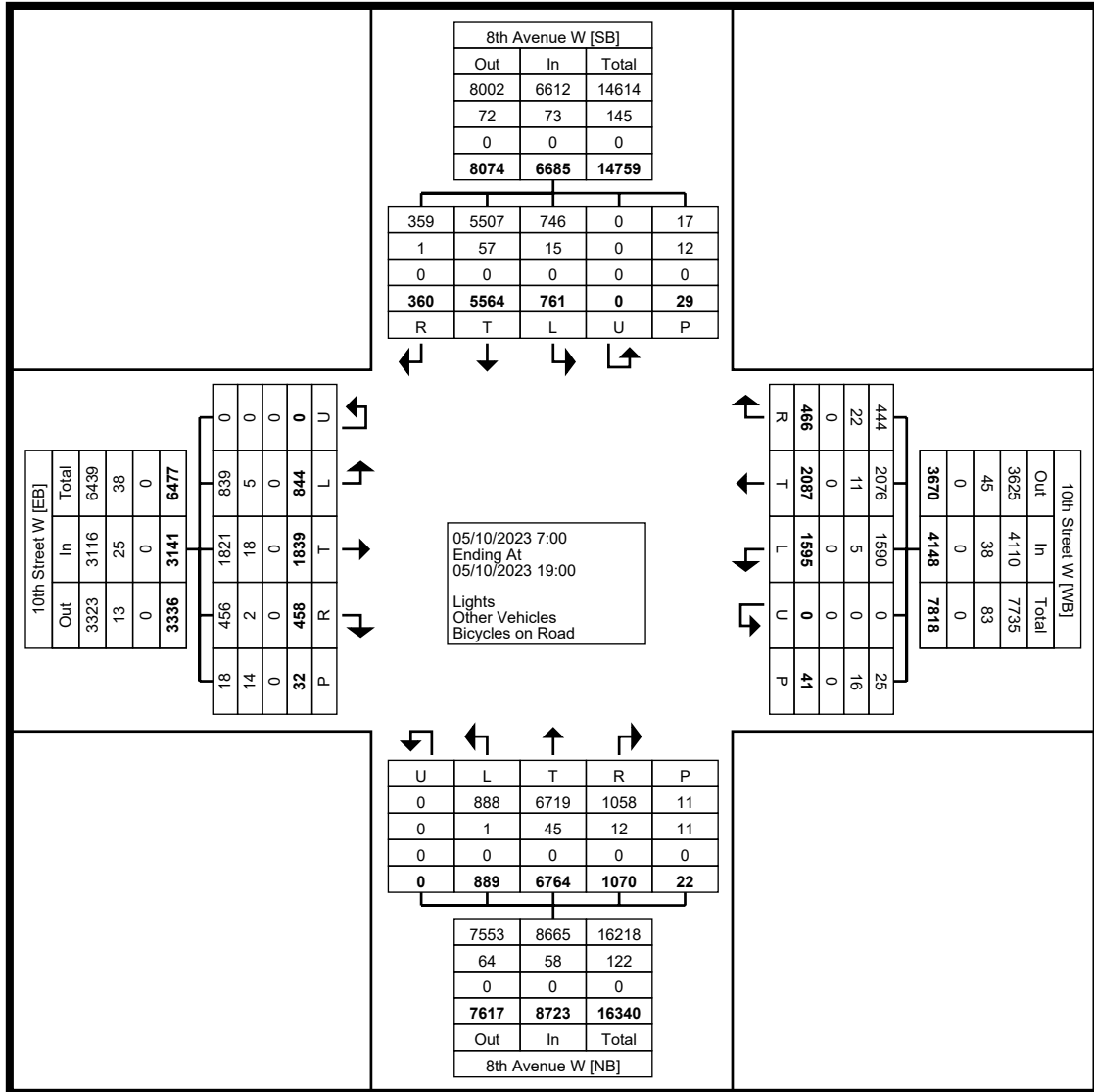
Start Time	9th St Dr W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
17:00	0	0	9	1	9	0	3	379	0	382	0	206	5	0	211	602
17:15	0	1	8	2	9	0	3	385	0	388	0	234	2	2	236	633
17:30	0	0	10	4	10	0	2	369	0	371	0	235	4	0	239	620
17:45	0	0	6	0	6	0	2	356	0	358	0	224	8	0	232	596
<b>Total</b>	<b>0</b>	<b>1</b>	<b>33</b>	<b>7</b>	<b>34</b>	<b>0</b>	<b>10</b>	<b>1489</b>	<b>0</b>	<b>1499</b>	<b>0</b>	<b>899</b>	<b>19</b>	<b>2</b>	<b>918</b>	<b>2451</b>
Approach %	0.0	2.9	97.1	-	-	0.0	0.7	99.3	-	-	0.0	97.9	2.1	-	-	-
Total %	0.0	0.0	1.3	-	1.4	0.0	0.4	60.8	-	61.2	0.0	36.7	0.8	-	37.5	-
PHF	0.000	0.250	0.825	-	0.850	0.000	0.833	0.967	-	0.966	0.000	0.956	0.594	-	0.960	0.968
Lights	0	0	32	2	32	0	10	1479	0	1489	0	898	19	2	917	2438
% Lights	-	0.0	97.0	28.6	94.1	-	100.0	99.3	-	99.3	-	99.9	100.0	100.0	99.9	99.5
Other Vehicles	0	0	0	5	0	0	0	10	0	10	0	1	0	0	1	11
% Other Vehicles	-	0.0	0.0	71.4	0.0	-	0.0	0.7	-	0.7	-	0.1	0.0	0.0	0.1	0.4
Bicycles on Road	0	1	1	0	2	0	0	0	0	0	0	0	0	0	0	2
% Bicycles on Road	-	100.0	3.0	0.0	5.9	-	0.0	0.0	-	0.0	-	0.0	0.0	0.0	0.0	0.1



Turning Movement Peak Hour Data Plot (17:00)

### Turning Movement Data

Start Time	10th Street W Eastbound						10th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	16	52	16	0	84	0	84	88	18	0	190	0	19	120	10	2	149	0	6	258	4	2	268	691
7:15	0	20	70	15	0	105	0	70	83	11	0	164	0	16	133	15	0	164	0	11	259	5	1	275	708
7:30	0	28	62	7	2	97	0	41	79	8	0	128	0	26	149	26	1	201	0	19	215	5	1	239	665
7:45	0	10	61	6	0	77	0	56	89	8	3	153	0	23	150	21	0	194	0	19	184	5	0	208	632
Hourly Total	0	74	245	44	2	363	0	251	339	45	3	635	0	84	552	72	3	708	0	55	916	19	4	990	2696
8:00	0	18	71	9	1	98	0	45	82	9	1	136	0	27	140	24	2	191	0	16	191	7	0	214	639
8:15	0	14	70	20	0	104	0	42	52	9	2	103	0	17	178	28	0	223	0	23	204	5	0	232	662
8:30	0	23	51	24	0	98	0	55	59	13	0	127	0	18	130	30	0	178	0	19	244	3	0	266	669
8:45	0	25	45	19	1	89	0	74	66	14	0	154	0	18	135	28	0	181	0	21	235	9	0	265	689
Hourly Total	0	80	237	72	2	389	0	216	259	45	3	520	0	80	583	110	2	773	0	79	874	24	0	977	2659
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	26	64	15	0	105	0	63	58	25	1	146	0	35	162	45	1	242	0	37	138	20	0	195	688
12:15	0	25	49	18	4	92	0	61	53	17	0	131	0	31	185	37	0	253	0	33	137	12	3	182	658
12:30	0	30	68	19	1	117	0	51	60	15	3	126	0	31	157	33	0	221	0	31	165	14	0	210	674
12:45	0	36	53	16	0	105	0	36	65	16	1	117	0	28	167	50	0	245	0	22	161	19	0	202	669
Hourly Total	0	117	234	68	5	419	0	211	236	73	5	520	0	125	671	165	1	961	0	123	601	65	3	789	2689
13:00	0	23	59	24	0	106	0	50	62	17	1	129	0	23	132	38	0	193	0	27	127	10	4	164	592
13:15	0	19	50	19	0	88	0	48	47	24	4	119	0	29	185	47	0	261	0	28	177	11	1	216	684
13:30	0	30	64	19	0	113	0	37	62	20	2	119	0	36	179	33	2	248	0	22	130	8	2	160	640
13:45	0	34	60	15	2	109	0	47	50	15	4	112	0	22	200	27	2	249	0	30	143	12	0	185	655
Hourly Total	0	106	233	77	2	416	0	182	221	76	11	479	0	110	696	145	4	951	0	107	577	41	7	725	2571
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	27	55	16	3	98	0	52	67	16	2	135	0	33	227	31	1	291	0	30	160	11	0	201	725
15:15	0	37	57	10	2	104	0	44	62	17	1	123	0	34	236	40	0	310	0	30	201	18	0	249	786
15:30	0	34	72	18	1	124	0	42	56	25	1	123	0	26	230	35	1	291	0	27	141	10	0	178	716
15:45	0	28	55	14	1	97	0	50	66	20	2	136	0	24	297	30	0	351	0	30	181	11	0	222	806
Hourly Total	0	126	239	58	7	423	0	188	251	78	6	517	0	117	990	136	2	1243	0	117	683	50	0	850	3033
16:00	0	29	53	12	3	94	0	45	63	20	2	128	0	33	304	49	1	386	0	15	151	15	1	181	789
16:15	0	24	43	11	1	78	0	58	65	16	1	139	0	35	285	37	0	357	0	35	162	17	0	214	788
16:30	0	27	60	11	0	98	0	45	76	12	1	133	0	16	285	40	0	341	0	26	183	11	1	220	792
16:45	0	28	54	14	2	96	0	45	67	14	2	126	0	24	287	34	3	345	0	29	165	9	0	203	770
Hourly Total	0	108	210	48	6	366	0	193	271	62	6	526	0	108	1161	160	4	1429	0	105	661	52	2	818	3139
17:00	0	34	58	11	0	103	0	48	72	8	1	128	0	24	311	34	0	369	0	20	156	7	3	183	783
17:15	0	40	59	8	3	107	0	51	67	8	1	126	0	23	318	44	0	385	0	23	180	14	3	217	835
17:30	0	30	64	9	3	103	0	46	68	6	0	120	0	19	314	36	3	369	0	23	183	14	2	220	812
17:45	0	26	65	7	1	98	0	42	75	13	1	130	0	41	290	24	0	355	0	22	182	22	4	226	809
Hourly Total	0	130	246	35	7	411	0	187	282	35	3	504	0	107	1233	138	3	1478	0	88	701	57	12	846	3239
18:00	0	30	56	9	0	95	0	38	64	11	0	113	0	51	268	46	1	365	0	21	150	14	0	185	758
18:15	0	27	55	16	0	98	0	45	58	12	0	115	0	31	245	30	0	306	0	20	138	7	0	165	684
18:30	0	28	47	15	1	90	0	46	56	14	2	116	0	31	202	33	2	266	0	26	144	17	1	187	659
18:45	0	18	37	16	0	71	0	38	50	15	2	103	0	45	163	35	0	243	0	20	119	14	0	153	570
Hourly Total	0	103	195	56	1	354	0	167	228	52	4	447	0	158	878	144	3	1180	0	87	551	52	1	690	2671
Grand Total	0	844	1839	458	32	3141	0	1595	2087	466	41	4148	0	889	6764	1070	22	8723	0	761	5564	360	29	6685	22697
Approach %	0.0	26.9	58.5	14.6	-	-	0.0	38.5	50.3	11.2	-	-	0.0	10.2	77.5	12.3	-	-	0.0	11.4	83.2	5.4	-	-	-
Total %	0.0	3.7	8.1	2.0	-	13.8	0.0	7.0	9.2	2.1	-	18.3	0.0	3.9	29.8	4.7	-	38.4	0.0	3.4	24.5	1.6	-	29.5	-
Lights	0	839	1821	456	18	3116	0	1590	2076	444	25	4110	0	888	6719	1058	11	8665	0	746	5507	359	17	6612	22503
% Lights	-	99.4	99.0	99.6	56.3	99.2	-	99.7	99.5	95.3	61.0	99.1	-	99.9	99.3	98.9	50.0	99.3	-	98.0	99.0	99.7	58.6	98.9	99.1
Other Vehicles	0	5	18	2	14	25	0	5	11	22	16	38	0	1	45	12	11	58	0	15	57	1	12	73	194
% Other Vehicles	-	0.6	1.0	0.4	43.8	0.8	-	0.3	0.5	4.7	39.0	0.9	-	0.1	0.7	1.1	50.0	0.7	-	2.0	1.0	0.3	41.4	1.1	0.9
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Data Plot

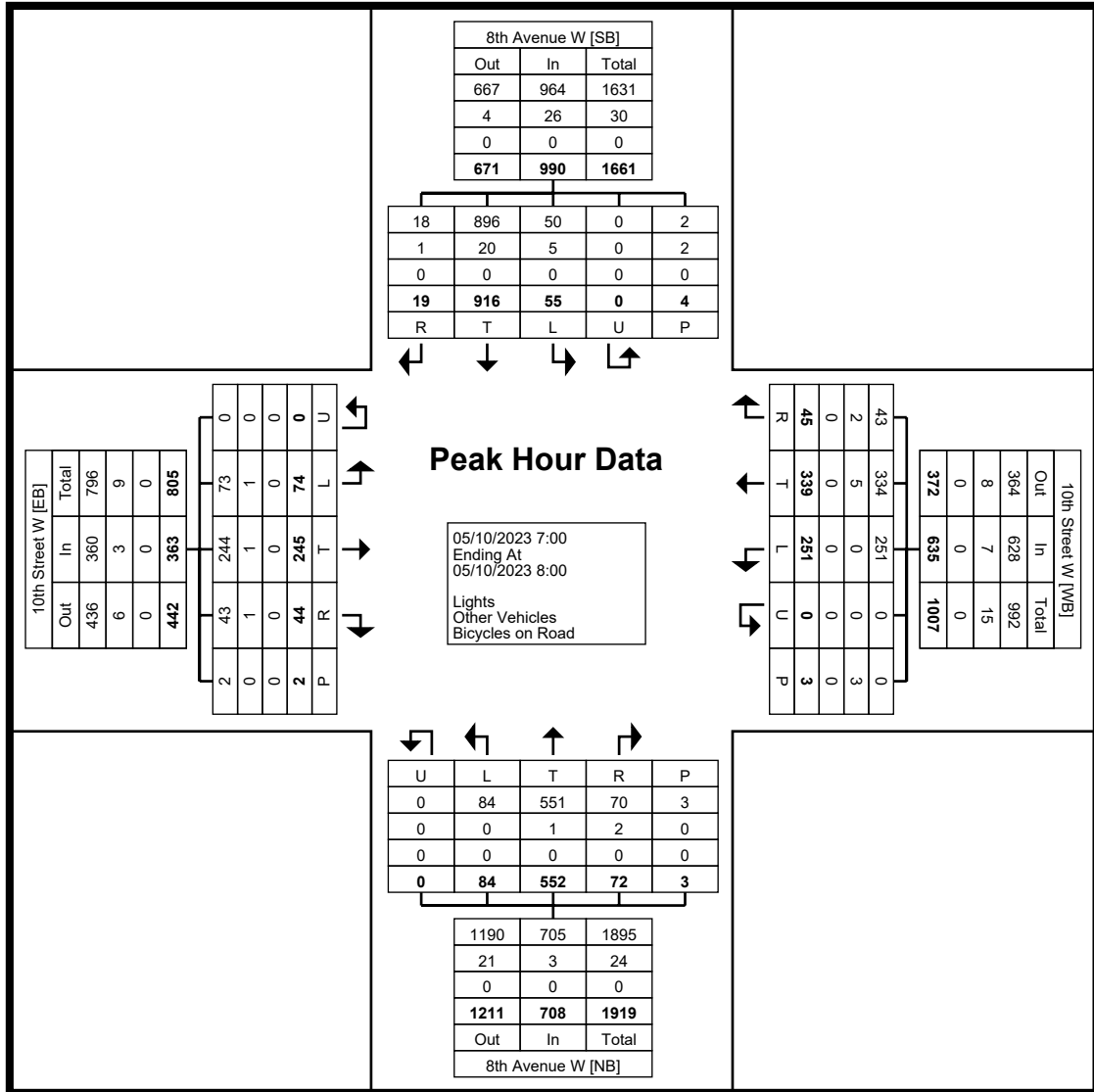
8th Avenue W and 10th Street W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 4\_8th Avenue W and 10th Street W  
Site Code: 4  
Start Date: 05/10/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:00)

Start Time	10th Street W Eastbound						10th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	16	52	16	0	84	0	84	88	18	0	190	0	19	120	10	2	149	0	6	258	4	2	268	691
7:15	0	20	70	15	0	105	0	70	83	11	0	164	0	16	133	15	0	164	0	11	259	5	1	275	708
7:30	0	28	62	7	2	97	0	41	79	8	0	128	0	26	149	26	1	201	0	19	215	5	1	239	665
7:45	0	10	61	6	0	77	0	56	89	8	3	153	0	23	150	21	0	194	0	19	184	5	0	208	632
<b>Total</b>	0	74	245	44	2	363	0	251	339	45	3	635	0	84	552	72	3	708	0	55	916	19	4	990	2696
Approach %	0.0	20.4	67.5	12.1	-	-	0.0	39.5	53.4	7.1	-	-	0.0	11.9	78.0	10.2	-	-	0.0	5.6	92.5	1.9	-	-	-
Total %	0.0	2.7	9.1	1.6	-	13.5	0.0	9.3	12.6	1.7	-	23.6	0.0	3.1	20.5	2.7	-	26.3	0.0	2.0	34.0	0.7	-	36.7	-
PHF	0.000	0.661	0.875	0.688	-	0.864	0.000	0.747	0.952	0.625	-	0.836	0.000	0.808	0.920	0.692	-	0.881	0.000	0.724	0.884	0.950	-	0.900	0.952
Lights	0	73	244	43	2	360	0	251	334	43	0	628	0	84	551	70	3	705	0	50	896	18	2	964	2657
% Lights	-	98.6	99.6	97.7	100.0	99.2	-	100.0	98.5	95.6	0.0	98.9	-	100.0	99.8	97.2	100.0	99.6	-	90.9	97.8	94.7	50.0	97.4	98.6
Other Vehicles	0	1	1	1	0	3	0	0	5	2	3	7	0	0	1	2	0	3	0	5	20	1	2	26	39
% Other Vehicles	-	1.4	0.4	2.3	0.0	0.8	-	0.0	1.5	4.4	100.0	1.1	-	0.0	0.2	2.8	0.0	0.4	-	9.1	2.2	5.3	50.0	2.6	1.4
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (7:00)

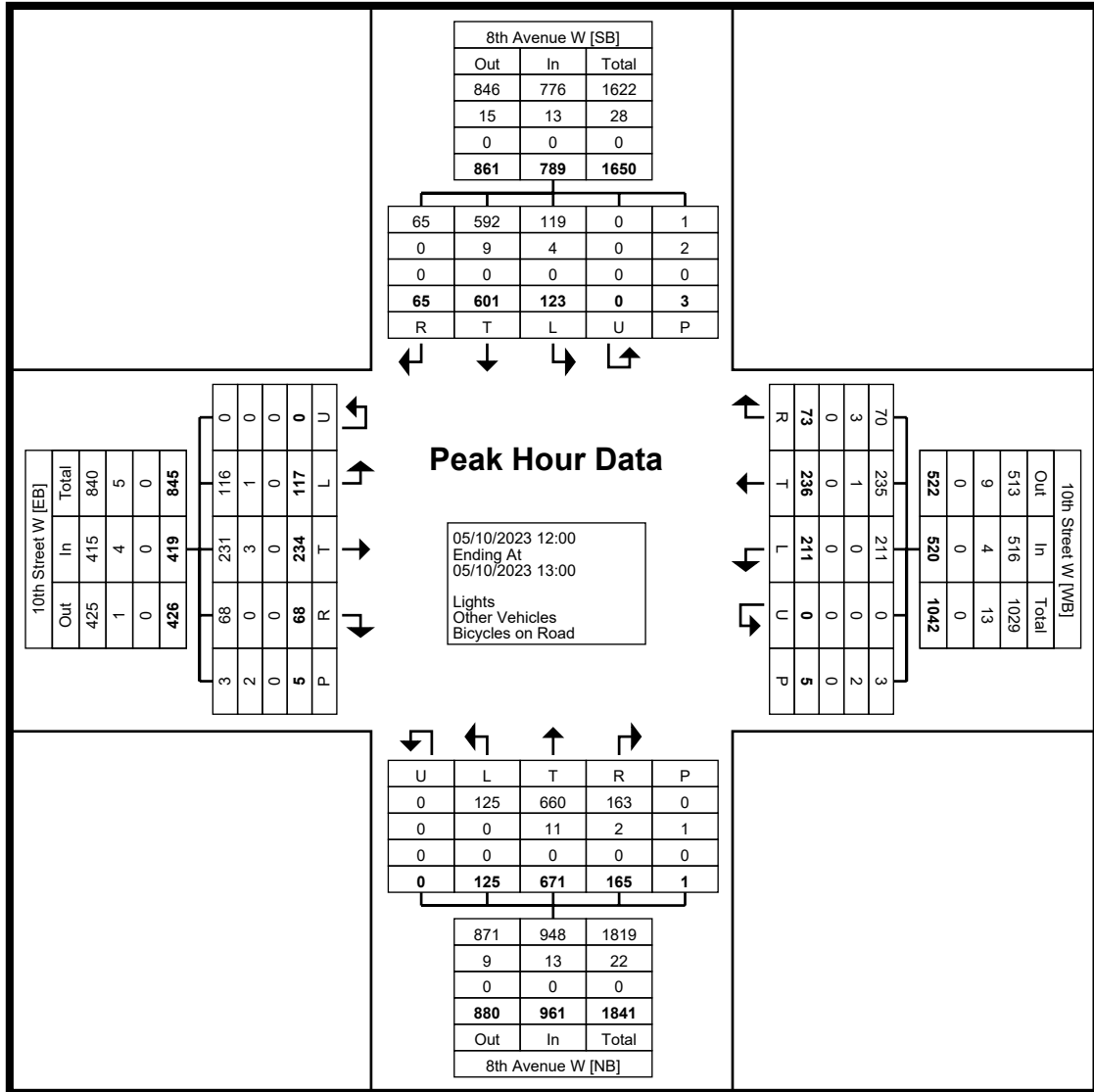
8th Avenue W and 10th Street W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 4\_8th Avenue W and 10th Street W  
Site Code: 4  
Start Date: 05/10/2023  
Page No: 5

### Turning Movement Peak Hour Data (12:00)

Start Time	10th Street W Eastbound						10th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:00	0	26	64	15	0	105	0	63	58	25	1	146	0	35	162	45	1	242	0	37	138	20	0	195	688
12:15	0	25	49	18	4	92	0	61	53	17	0	131	0	31	185	37	0	253	0	33	137	12	3	182	658
12:30	0	30	68	19	1	117	0	51	60	15	3	126	0	31	157	33	0	221	0	31	165	14	0	210	674
12:45	0	36	53	16	0	105	0	36	65	16	1	117	0	28	167	50	0	245	0	22	161	19	0	202	669
<b>Total</b>	0	117	234	68	5	419	0	211	236	73	5	520	0	125	671	165	1	961	0	123	601	65	3	789	2689
Approach %	0.0	27.9	55.8	16.2	-	-	0.0	40.6	45.4	14.0	-	-	0.0	13.0	69.8	17.2	-	-	0.0	15.6	76.2	8.2	-	-	-
Total %	0.0	4.4	8.7	2.5	-	15.6	0.0	7.8	8.8	2.7	-	19.3	0.0	4.6	25.0	6.1	-	35.7	0.0	4.6	22.4	2.4	-	29.3	-
PHF	0.000	0.813	0.860	0.895	-	0.895	0.000	0.837	0.908	0.730	-	0.890	0.000	0.893	0.907	0.825	-	0.950	0.000	0.831	0.911	0.813	-	0.939	0.977
Lights	0	116	231	68	3	415	0	211	235	70	3	516	0	125	660	163	0	948	0	119	592	65	1	776	2655
% Lights	-	99.1	98.7	100.0	60.0	99.0	-	100.0	99.6	95.9	60.0	99.2	-	100.0	98.4	98.8	0.0	98.6	-	96.7	98.5	100.0	33.3	98.4	98.7
Other Vehicles	0	1	3	0	2	4	0	0	1	3	2	4	0	0	11	2	1	13	0	4	9	0	2	13	34
% Other Vehicles	-	0.9	1.3	0.0	40.0	1.0	-	0.0	0.4	4.1	40.0	0.8	-	0.0	1.6	1.2	100.0	1.4	-	3.3	1.5	0.0	66.7	1.6	1.3
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (12:00)



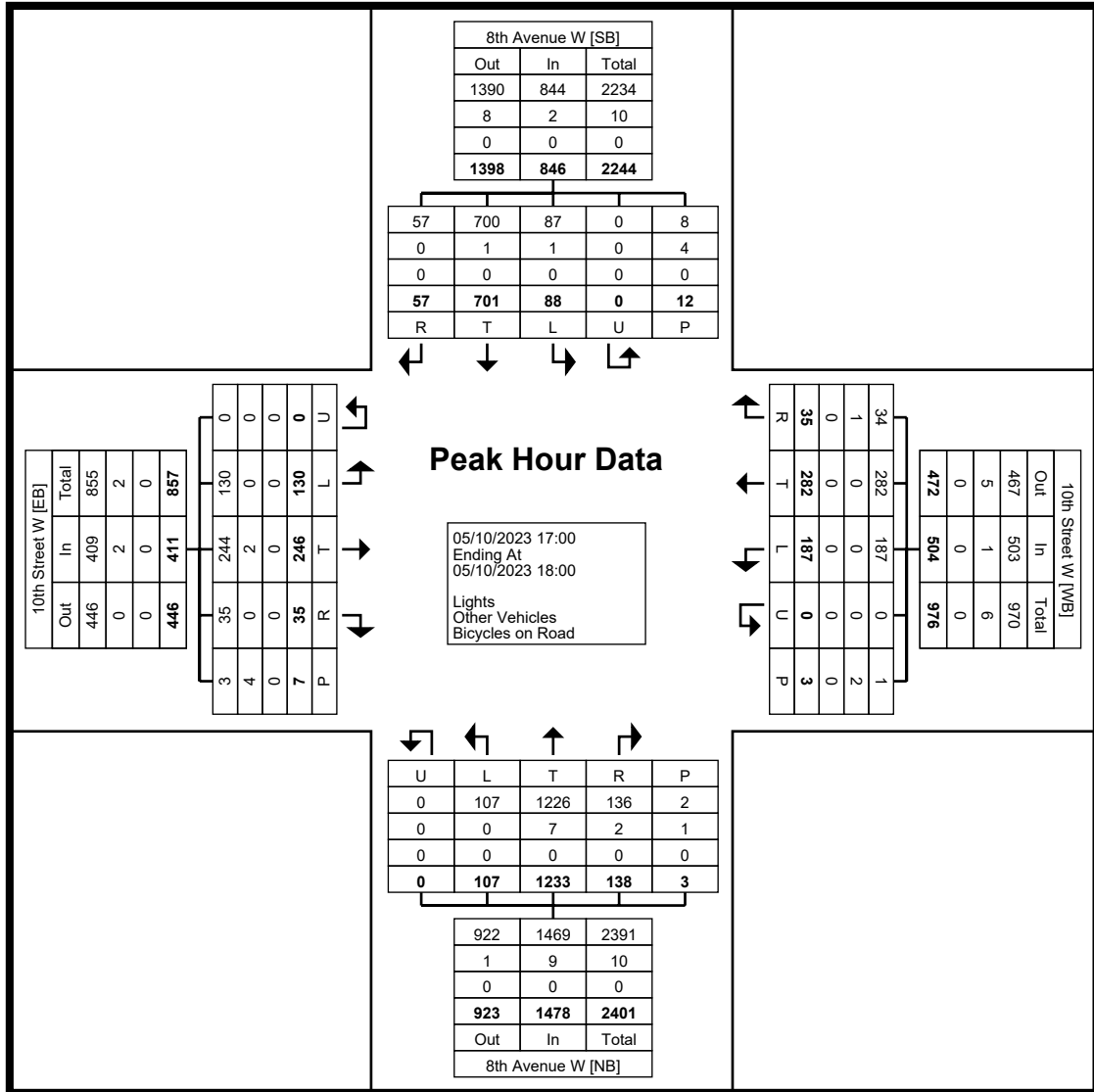
8th Avenue W and 10th Street W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 4\_8th Avenue W and 10th Street W  
Site Code: 4  
Start Date: 05/10/2023  
Page No: 7

### Turning Movement Peak Hour Data (17:00)

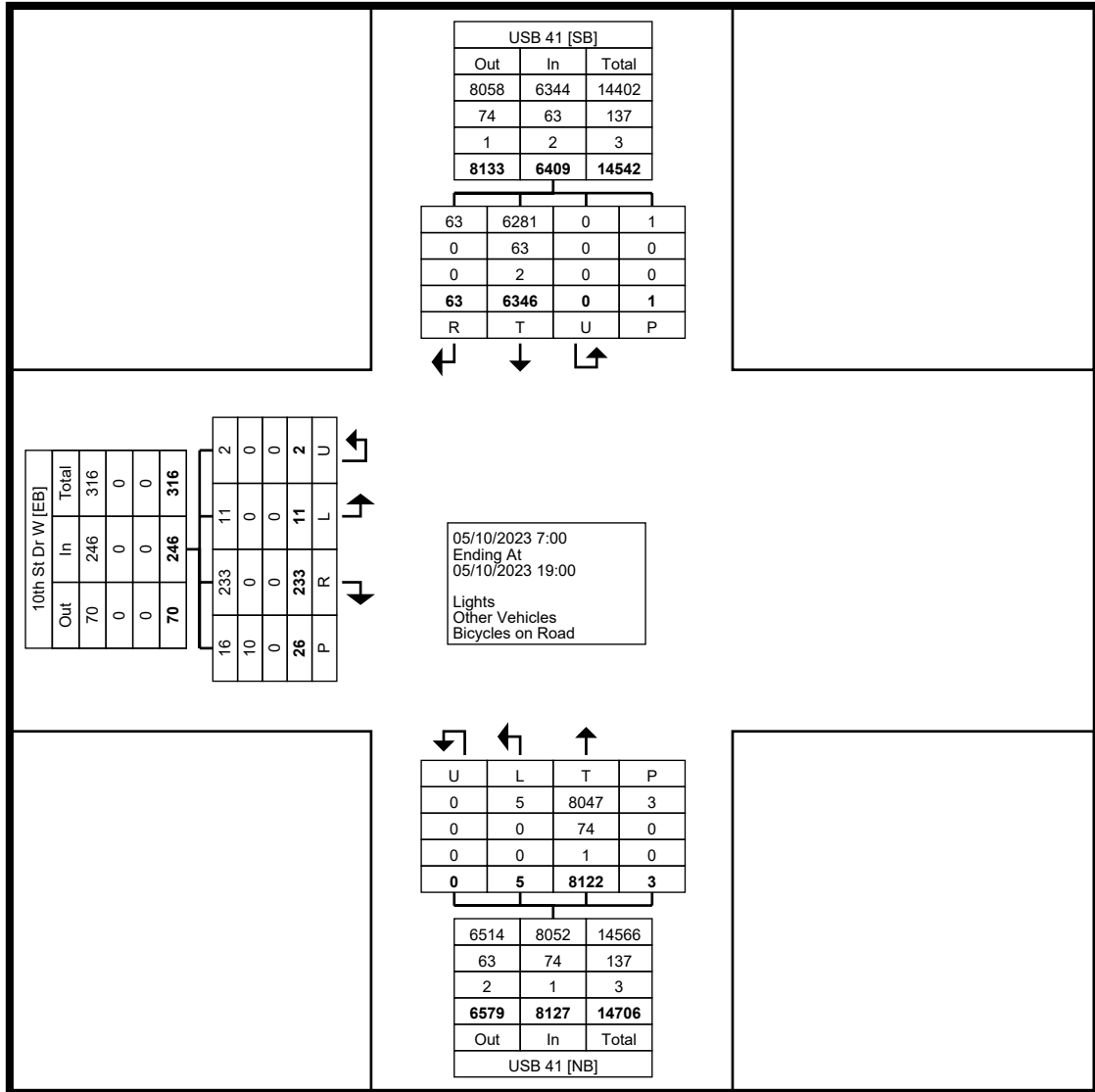
Start Time	10th Street W Eastbound						10th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
17:00	0	34	58	11	0	103	0	48	72	8	1	128	0	24	311	34	0	369	0	20	156	7	3	183	783
17:15	0	40	59	8	3	107	0	51	67	8	1	126	0	23	318	44	0	385	0	23	180	14	3	217	835
17:30	0	30	64	9	3	103	0	46	68	6	0	120	0	19	314	36	3	369	0	23	183	14	2	220	812
17:45	0	26	65	7	1	98	0	42	75	13	1	130	0	41	290	24	0	355	0	22	182	22	4	226	809
<b>Total</b>	<b>0</b>	<b>130</b>	<b>246</b>	<b>35</b>	<b>7</b>	<b>411</b>	<b>0</b>	<b>187</b>	<b>282</b>	<b>35</b>	<b>3</b>	<b>504</b>	<b>0</b>	<b>107</b>	<b>1233</b>	<b>138</b>	<b>3</b>	<b>1478</b>	<b>0</b>	<b>88</b>	<b>701</b>	<b>57</b>	<b>12</b>	<b>846</b>	<b>3239</b>
Approach %	0.0	31.6	59.9	8.5	-	-	0.0	37.1	56.0	6.9	-	-	0.0	7.2	83.4	9.3	-	-	0.0	10.4	82.9	6.7	-	-	-
Total %	0.0	4.0	7.6	1.1	-	12.7	0.0	5.8	8.7	1.1	-	15.6	0.0	3.3	38.1	4.3	-	45.6	0.0	2.7	21.6	1.8	-	26.1	-
PHF	0.000	0.813	0.946	0.795	-	0.960	0.000	0.917	0.940	0.673	-	0.969	0.000	0.652	0.969	0.784	-	0.960	0.000	0.957	0.958	0.648	-	0.936	0.970
Lights	0	130	244	35	3	409	0	187	282	34	1	503	0	107	1226	136	2	1469	0	87	700	57	8	844	3225
% Lights	-	100.0	99.2	100.0	42.9	99.5	-	100.0	100.0	97.1	33.3	99.8	-	100.0	99.4	98.6	66.7	99.4	-	98.9	99.9	100.0	66.7	99.8	99.6
Other Vehicles	0	0	2	0	4	2	0	0	0	1	2	1	0	0	7	2	1	9	0	1	1	0	4	2	14
% Other Vehicles	-	0.0	0.8	0.0	57.1	0.5	-	0.0	0.0	2.9	66.7	0.2	-	0.0	0.6	1.4	33.3	0.6	-	1.1	0.1	0.0	33.3	0.2	0.4
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (17:00)

### Turning Movement Data

Start Time	10th St Dr W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
7:00	0	0	3	0	3	0	0	162	0	162	0	255	0	0	255	420
7:15	0	0	2	0	2	0	0	165	0	165	0	278	0	0	278	445
7:30	0	0	0	0	0	0	0	192	0	192	0	238	0	0	238	430
7:45	0	0	3	0	3	0	0	167	0	167	0	206	4	0	210	380
Hourly Total	0	0	8	0	8	0	0	686	0	686	0	977	4	0	981	1675
8:00	0	0	3	0	3	0	0	160	0	160	0	218	1	0	219	382
8:15	0	0	4	0	4	0	0	204	0	204	0	223	2	0	225	433
8:30	0	2	6	1	8	0	0	165	0	165	0	267	1	0	268	441
8:45	0	0	5	1	5	0	0	174	1	174	0	255	0	0	255	434
Hourly Total	0	2	18	2	20	0	0	703	1	703	0	963	4	0	967	1690
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	8	0	8	0	0	214	1	214	0	164	2	0	166	388
12:15	1	1	13	3	15	0	0	219	0	219	0	176	4	0	180	414
12:30	0	1	4	1	5	0	0	217	0	217	0	215	5	0	220	442
12:45	0	1	8	0	9	0	0	196	0	196	0	167	3	0	170	375
Hourly Total	1	3	33	4	37	0	0	846	1	846	0	722	14	0	736	1619
13:00	0	1	4	1	5	0	0	205	0	205	0	175	0	0	175	385
13:15	0	1	7	0	8	0	0	208	0	208	0	184	4	0	188	404
13:30	0	0	11	1	11	0	0	237	0	237	0	168	0	0	168	416
13:45	0	0	8	3	8	0	1	253	0	254	0	157	3	1	160	422
Hourly Total	0	2	30	5	32	0	1	903	0	904	0	684	7	1	691	1627
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	0	6	2	6	0	0	271	0	271	0	202	2	0	204	481
15:15	0	0	10	0	10	0	0	286	0	286	0	220	1	0	221	517
15:30	0	1	12	0	13	0	0	290	0	290	0	176	2	0	178	481
15:45	0	0	9	0	9	0	0	351	1	351	0	198	3	0	201	561
Hourly Total	0	1	37	2	38	0	0	1198	1	1198	0	796	8	0	804	2040
16:00	0	0	8	2	8	0	0	349	0	349	0	174	2	0	176	533
16:15	0	0	10	1	10	0	0	327	0	327	0	201	0	0	201	538
16:30	0	0	9	1	9	0	1	333	0	334	0	204	6	0	210	553
16:45	0	2	7	2	9	0	0	332	0	332	0	200	1	0	201	542
Hourly Total	0	2	34	6	36	0	1	1341	0	1342	0	779	9	0	788	2166
17:00	0	0	5	0	5	0	0	359	0	359	0	175	3	0	178	542
17:15	0	0	13	1	13	0	0	365	0	365	0	206	4	0	210	588
17:30	0	0	8	2	8	0	3	341	0	344	0	207	1	0	208	560
17:45	1	0	16	2	17	0	0	334	0	334	0	201	4	0	205	556
Hourly Total	1	0	42	5	43	0	3	1399	0	1402	0	789	12	0	801	2246
18:00	0	0	6	0	6	0	0	316	0	316	0	169	1	0	170	492
18:15	0	0	8	0	8	0	0	278	0	278	0	152	0	0	152	438
18:30	0	1	8	2	9	0	0	240	0	240	0	174	3	0	177	426
18:45	0	0	9	0	9	0	0	212	0	212	0	141	1	0	142	363
Hourly Total	0	1	31	2	32	0	0	1046	0	1046	0	636	5	0	641	1719
Grand Total	2	11	233	26	246	0	5	8122	3	8127	0	6346	63	1	6409	14782
Approach %	0.8	4.5	94.7	-	-	0.0	0.1	99.9	-	-	0.0	99.0	1.0	-	-	-
Total %	0.0	0.1	1.6	-	1.7	0.0	0.0	54.9	-	55.0	0.0	42.9	0.4	-	43.4	-
Lights	2	11	233	16	246	0	5	8047	3	8052	0	6281	63	1	6344	14642
% Lights	100.0	100.0	100.0	61.5	100.0	-	100.0	99.1	100.0	99.1	-	99.0	100.0	100.0	99.0	99.1
Other Vehicles	0	0	0	10	0	0	0	74	0	74	0	63	0	0	63	137
% Other Vehicles	0.0	0.0	0.0	38.5	0.0	-	0.0	0.9	0.0	0.9	-	1.0	0.0	0.0	1.0	0.9
Bicycles on Road	0	0	0	0	0	0	0	1	0	1	0	2	0	0	2	3
% Bicycles on Road	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0



Turning Movement Data Plot

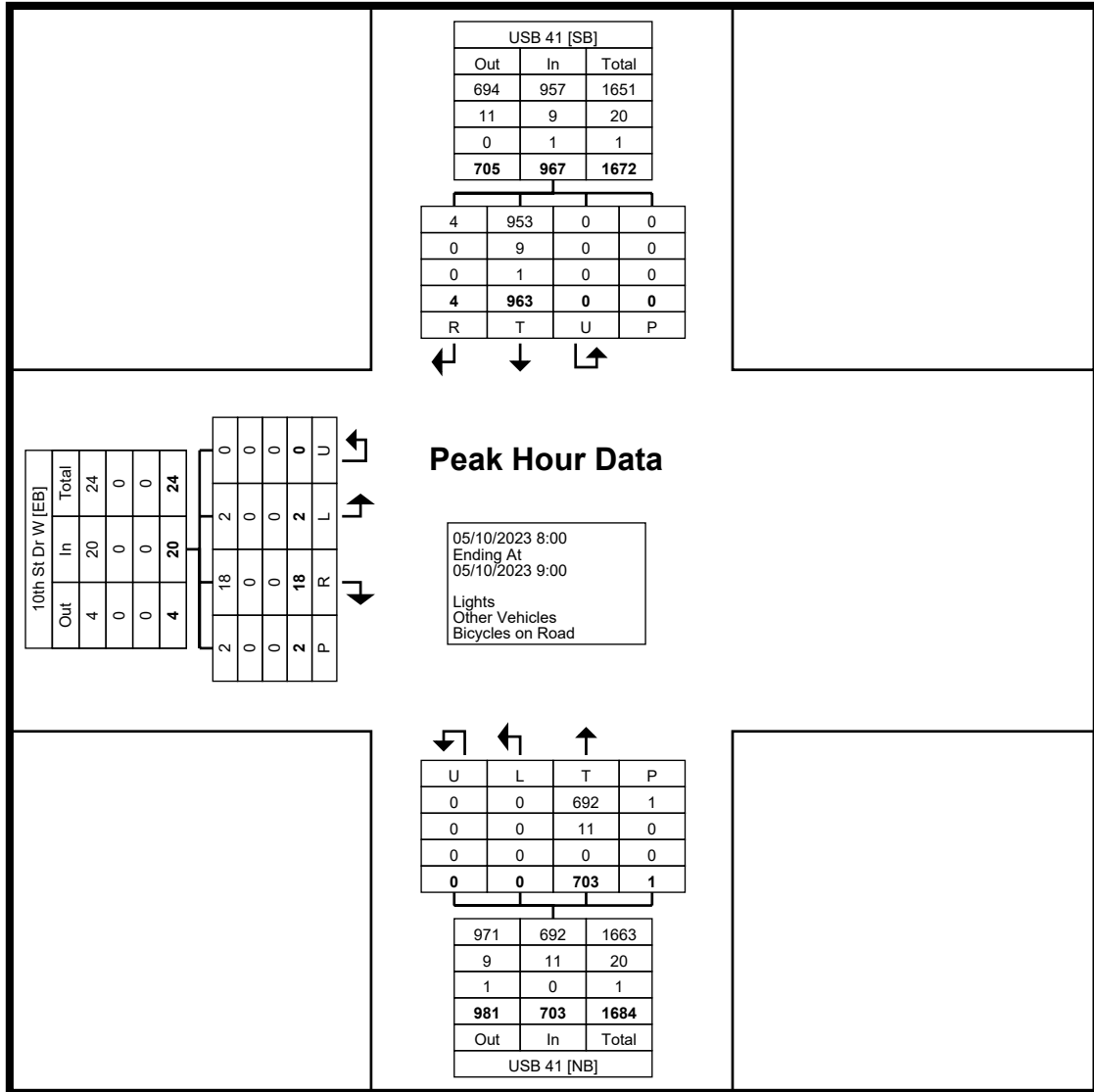
USB 41 & 10th St Dr W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 3\_USB 41 & 10th  
St Dr W  
Site Code: 3  
Start Date: 05/10/2023  
Page No: 3

### Turning Movement Peak Hour Data (8:00)

Start Time	10th St Dr W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
8:00	0	0	3	0	3	0	0	160	0	160	0	218	1	0	219	382
8:15	0	0	4	0	4	0	0	204	0	204	0	223	2	0	225	433
8:30	0	2	6	1	8	0	0	165	0	165	0	267	1	0	268	441
8:45	0	0	5	1	5	0	0	174	1	174	0	255	0	0	255	434
<b>Total</b>	<b>0</b>	<b>2</b>	<b>18</b>	<b>2</b>	<b>20</b>	<b>0</b>	<b>0</b>	<b>703</b>	<b>1</b>	<b>703</b>	<b>0</b>	<b>963</b>	<b>4</b>	<b>0</b>	<b>967</b>	<b>1690</b>
Approach %	0.0	10.0	90.0	-	-	0.0	0.0	100.0	-	-	0.0	99.6	0.4	-	-	-
Total %	0.0	0.1	1.1	-	1.2	0.0	0.0	41.6	-	41.6	0.0	57.0	0.2	-	57.2	-
PHF	0.000	0.250	0.750	-	0.625	0.000	0.000	0.862	-	0.862	0.000	0.902	0.500	-	0.902	0.958
Lights	0	2	18	2	20	0	0	692	1	692	0	953	4	0	957	1669
% Lights	-	100.0	100.0	100.0	100.0	-	-	98.4	100.0	98.4	-	99.0	100.0	-	99.0	98.8
Other Vehicles	0	0	0	0	0	0	0	11	0	11	0	9	0	0	9	20
% Other Vehicles	-	0.0	0.0	0.0	0.0	-	-	1.6	0.0	1.6	-	0.9	0.0	-	0.9	1.2
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
% Bicycles on Road	-	0.0	0.0	0.0	0.0	-	-	0.0	0.0	0.0	-	0.1	0.0	-	0.1	0.1



Turning Movement Peak Hour Data Plot (8:00)

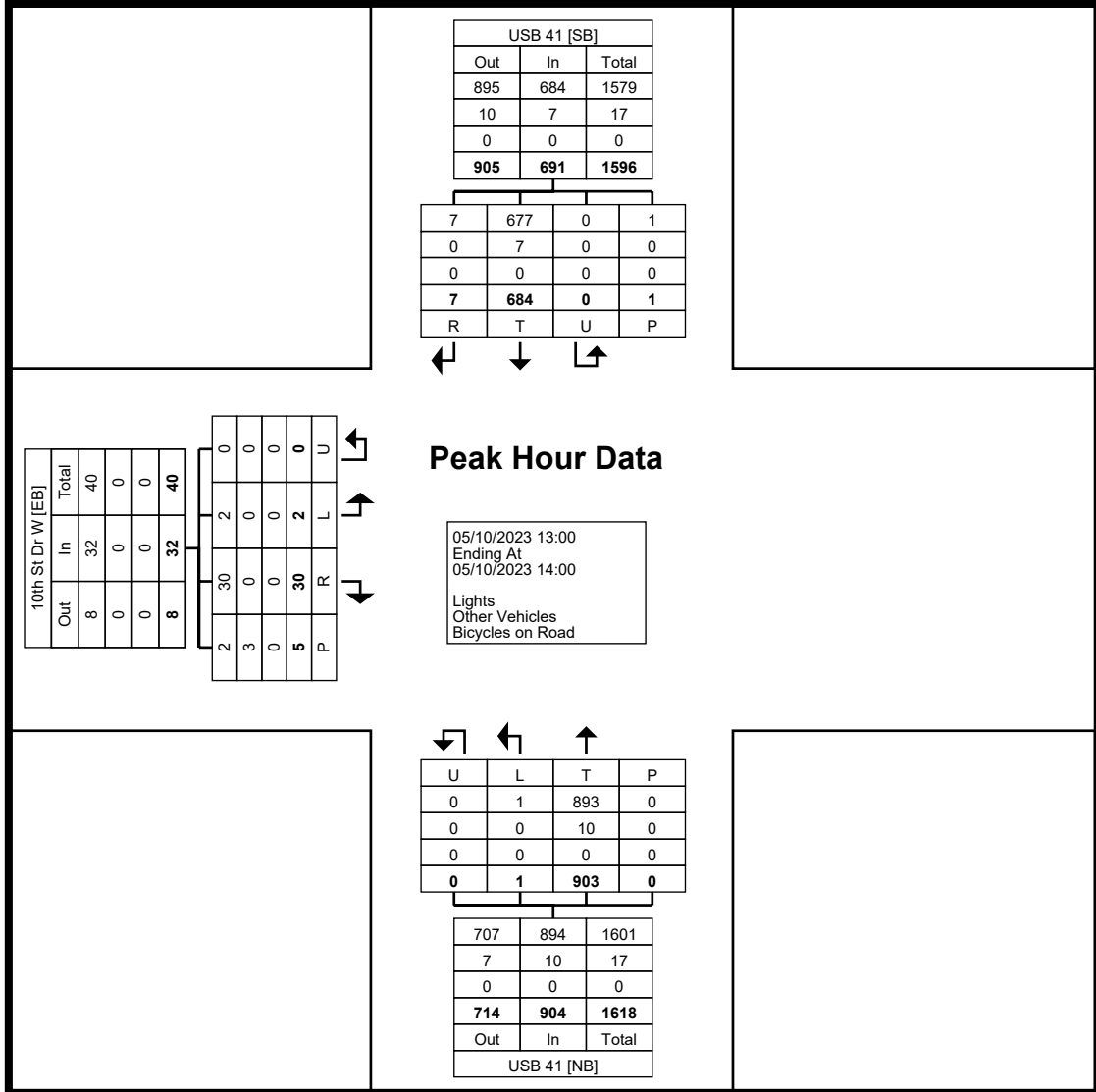
USB 41 & 10th St Dr W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 3\_USB 41 & 10th  
St Dr W  
Site Code: 3  
Start Date: 05/10/2023  
Page No: 5

### Turning Movement Peak Hour Data (13:00)

Start Time	10th St Dr W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
13:00	0	1	4	1	5	0	0	205	0	205	0	175	0	0	175	385
13:15	0	1	7	0	8	0	0	208	0	208	0	184	4	0	188	404
13:30	0	0	11	1	11	0	0	237	0	237	0	168	0	0	168	416
13:45	0	0	8	3	8	0	1	253	0	254	0	157	3	1	160	422
<b>Total</b>	<b>0</b>	<b>2</b>	<b>30</b>	<b>5</b>	<b>32</b>	<b>0</b>	<b>1</b>	<b>903</b>	<b>0</b>	<b>904</b>	<b>0</b>	<b>684</b>	<b>7</b>	<b>1</b>	<b>691</b>	<b>1627</b>
Approach %	0.0	6.3	93.8	-	-	0.0	0.1	99.9	-	-	0.0	99.0	1.0	-	-	-
Total %	0.0	0.1	1.8	-	2.0	0.0	0.1	55.5	-	55.6	0.0	42.0	0.4	-	42.5	-
PHF	0.000	0.500	0.682	-	0.727	0.000	0.250	0.892	-	0.890	0.000	0.929	0.438	-	0.919	0.964
Lights	0	2	30	2	32	0	1	893	0	894	0	677	7	1	684	1610
% Lights	-	100.0	100.0	40.0	100.0	-	100.0	98.9	-	98.9	-	99.0	100.0	100.0	99.0	99.0
Other Vehicles	0	0	0	3	0	0	0	10	0	10	0	7	0	0	7	17
% Other Vehicles	-	0.0	0.0	60.0	0.0	-	0.0	1.1	-	1.1	-	1.0	0.0	0.0	1.0	1.0
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (13:00)



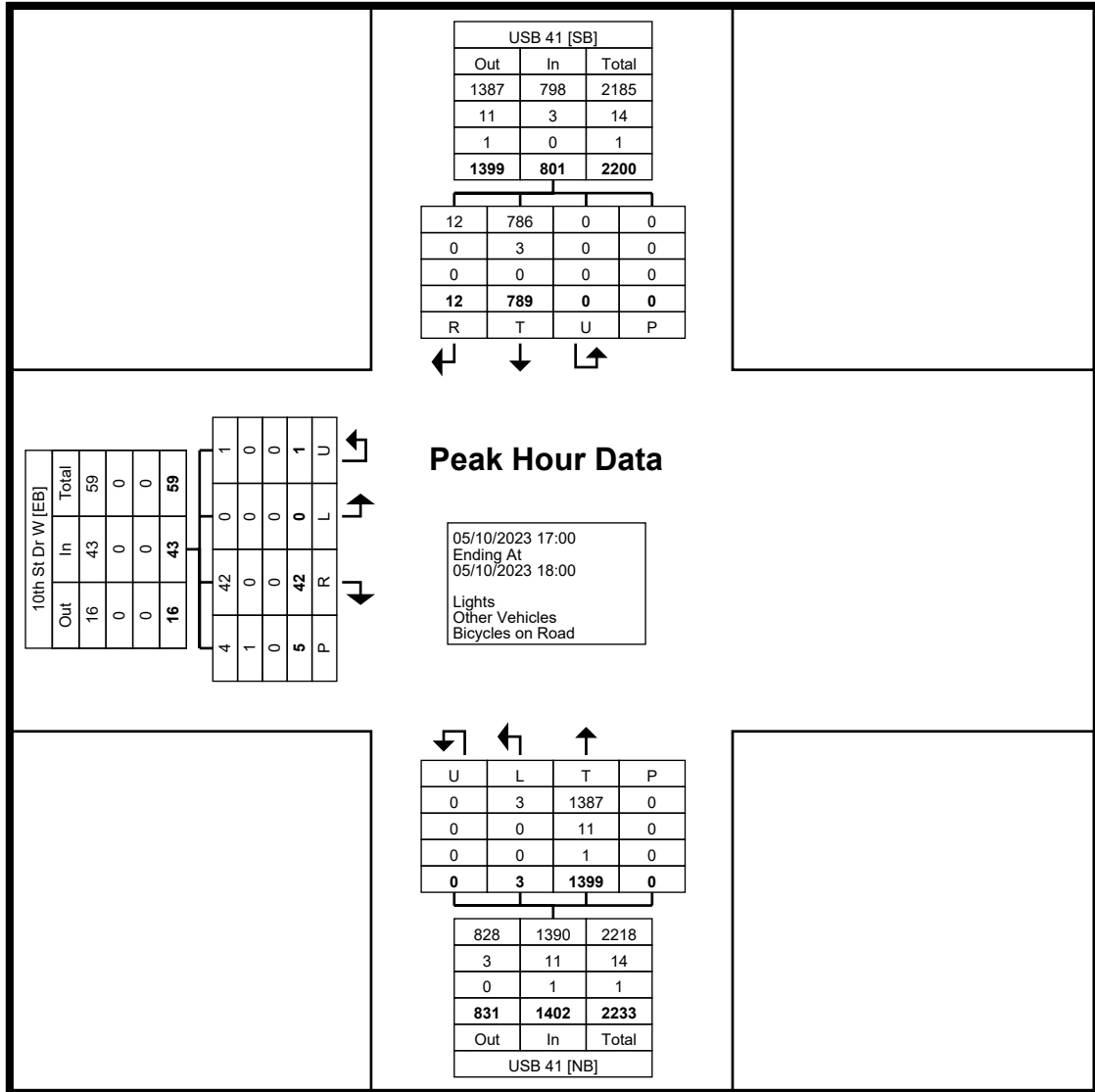
USB 41 & 10th St Dr W  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 3\_USB 41 & 10th  
St Dr W  
Site Code: 3  
Start Date: 05/10/2023  
Page No: 7

### Turning Movement Peak Hour Data (17:00)

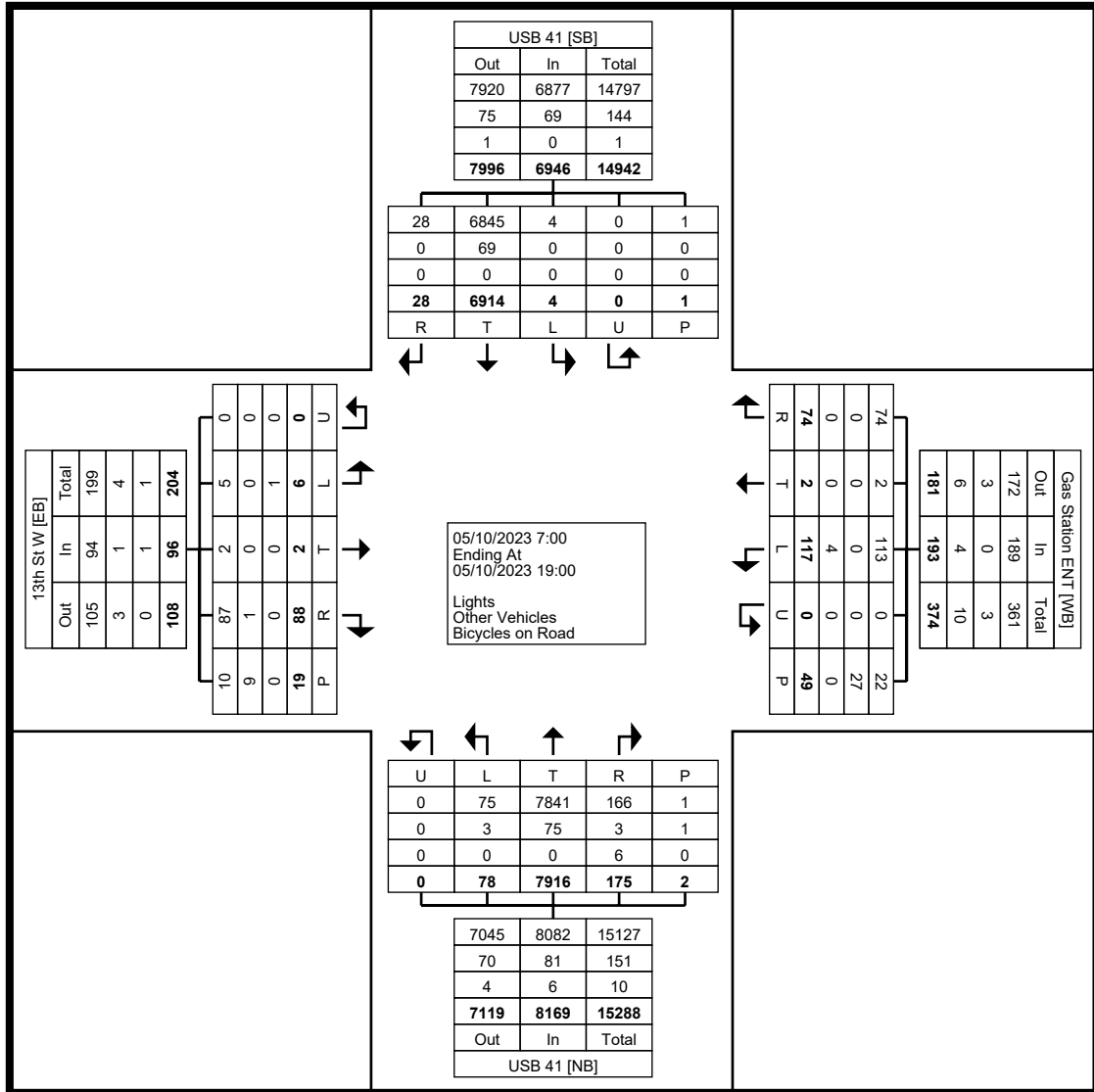
Start Time	10th St Dr W Eastbound					USB 41 Northbound					USB 41 Southbound					Int. Total
	U-Turn	Left	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	
17:00	0	0	5	0	5	0	0	359	0	359	0	175	3	0	178	542
17:15	0	0	13	1	13	0	0	365	0	365	0	206	4	0	210	588
17:30	0	0	8	2	8	0	3	341	0	344	0	207	1	0	208	560
17:45	1	0	16	2	17	0	0	334	0	334	0	201	4	0	205	556
<b>Total</b>	<b>1</b>	<b>0</b>	<b>42</b>	<b>5</b>	<b>43</b>	<b>0</b>	<b>3</b>	<b>1399</b>	<b>0</b>	<b>1402</b>	<b>0</b>	<b>789</b>	<b>12</b>	<b>0</b>	<b>801</b>	<b>2246</b>
<b>Approach %</b>	<b>2.3</b>	<b>0.0</b>	<b>97.7</b>	<b>-</b>	<b>-</b>	<b>0.0</b>	<b>0.2</b>	<b>99.8</b>	<b>-</b>	<b>-</b>	<b>0.0</b>	<b>98.5</b>	<b>1.5</b>	<b>-</b>	<b>-</b>	<b>-</b>
<b>Total %</b>	<b>0.0</b>	<b>0.0</b>	<b>1.9</b>	<b>-</b>	<b>1.9</b>	<b>0.0</b>	<b>0.1</b>	<b>62.3</b>	<b>-</b>	<b>62.4</b>	<b>0.0</b>	<b>35.1</b>	<b>0.5</b>	<b>-</b>	<b>35.7</b>	<b>-</b>
<b>PHF</b>	<b>0.250</b>	<b>0.000</b>	<b>0.656</b>	<b>-</b>	<b>0.632</b>	<b>0.000</b>	<b>0.250</b>	<b>0.958</b>	<b>-</b>	<b>0.960</b>	<b>0.000</b>	<b>0.953</b>	<b>0.750</b>	<b>-</b>	<b>0.954</b>	<b>0.955</b>
<b>Lights</b>	<b>1</b>	<b>0</b>	<b>42</b>	<b>4</b>	<b>43</b>	<b>0</b>	<b>3</b>	<b>1387</b>	<b>0</b>	<b>1390</b>	<b>0</b>	<b>786</b>	<b>12</b>	<b>0</b>	<b>798</b>	<b>2231</b>
<b>% Lights</b>	<b>100.0</b>	<b>-</b>	<b>100.0</b>	<b>80.0</b>	<b>100.0</b>	<b>-</b>	<b>100.0</b>	<b>99.1</b>	<b>-</b>	<b>99.1</b>	<b>-</b>	<b>99.6</b>	<b>100.0</b>	<b>-</b>	<b>99.6</b>	<b>99.3</b>
<b>Other Vehicles</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>11</b>	<b>0</b>	<b>11</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>14</b>
<b>% Other Vehicles</b>	<b>0.0</b>	<b>-</b>	<b>0.0</b>	<b>20.0</b>	<b>0.0</b>	<b>-</b>	<b>0.0</b>	<b>0.8</b>	<b>-</b>	<b>0.8</b>	<b>-</b>	<b>0.4</b>	<b>0.0</b>	<b>-</b>	<b>0.4</b>	<b>0.6</b>
<b>Bicycles on Road</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>
<b>% Bicycles on Road</b>	<b>0.0</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>	<b>0.0</b>	<b>-</b>	<b>0.0</b>	<b>0.1</b>	<b>-</b>	<b>0.1</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>	<b>-</b>	<b>0.0</b>	<b>0.0</b>



Turning Movement Peak Hour Data Plot (17:00)

### Turning Movement Data

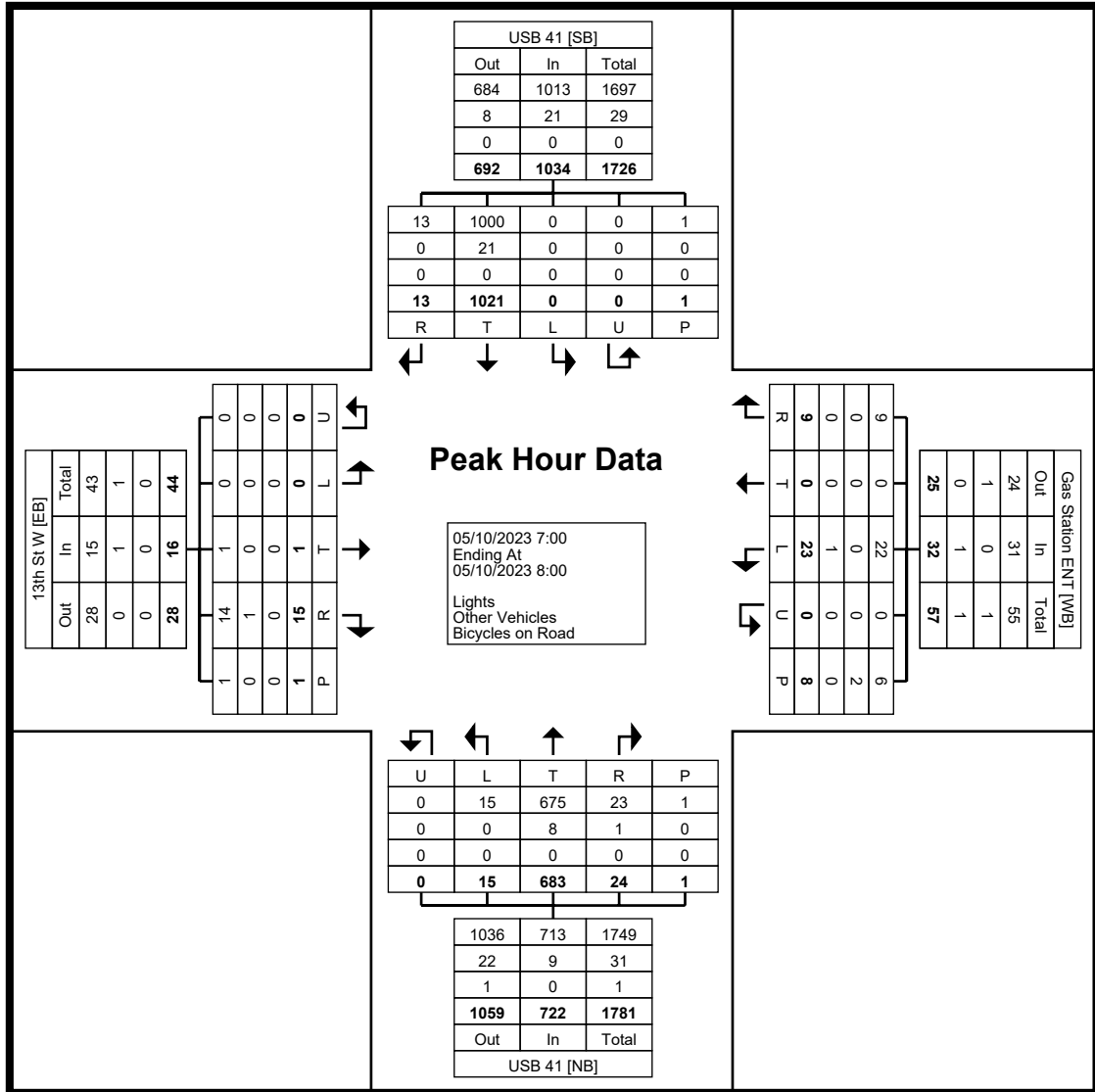
Start Time	13th St W Eastbound						Gas Station ENT Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	0	0	4	1	4	0	7	0	1	6	8	0	7	165	7	1	179	0	0	267	7	0	274	465
7:15	0	0	0	9	0	9	0	4	0	3	1	7	0	6	173	6	0	185	0	0	285	1	0	286	487
7:30	0	0	1	1	0	2	0	10	0	3	0	13	0	1	170	6	0	177	0	0	254	4	1	258	450
7:45	0	0	0	1	0	1	0	2	0	2	1	4	0	1	175	5	0	181	0	0	215	1	0	216	402
Hourly Total	0	0	1	15	1	16	0	23	0	9	8	32	0	15	683	24	1	722	0	0	1021	13	1	1034	1804
8:00	0	0	0	1	1	1	0	3	0	5	0	8	0	2	156	6	0	164	0	0	224	0	0	224	397
8:15	0	0	0	2	1	2	0	3	0	3	4	6	0	3	201	6	0	210	0	0	244	0	0	244	462
8:30	0	1	0	1	0	2	0	1	0	1	2	2	0	1	156	0	0	157	0	0	284	1	0	285	446
8:45	0	0	0	0	0	0	0	1	0	3	2	4	0	0	170	5	0	175	0	0	268	0	0	268	447
Hourly Total	0	1	0	4	2	5	0	8	0	12	8	20	0	6	683	17	0	706	0	0	1020	1	0	1021	1752
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	0	0	7	0	7	0	2	0	1	1	3	0	2	205	8	0	215	0	0	169	0	0	169	394
12:15	0	1	0	2	1	3	0	5	0	0	3	5	0	1	193	5	0	199	0	0	201	0	0	201	408
12:30	0	0	0	3	0	3	0	0	0	3	1	3	0	2	219	2	0	223	0	0	230	1	0	231	460
12:45	0	0	0	2	0	2	0	3	0	0	2	3	0	1	194	4	0	199	0	0	196	0	0	196	400
Hourly Total	0	1	0	14	1	15	0	10	0	4	7	14	0	6	811	19	0	836	0	0	796	1	0	797	1662
13:00	0	0	0	5	0	5	0	3	0	0	0	3	0	5	185	4	1	194	0	0	195	0	0	195	397
13:15	0	1	0	2	0	3	0	3	0	3	0	6	0	3	188	3	0	194	0	0	213	0	0	213	416
13:30	0	0	0	1	0	1	0	4	0	1	0	5	0	2	231	3	0	236	0	1	168	0	0	169	411
13:45	0	0	0	2	0	2	0	0	0	3	2	3	0	2	238	5	0	245	0	0	180	1	0	181	431
Hourly Total	0	1	0	10	0	11	0	10	0	7	2	17	0	12	842	15	1	869	0	1	756	1	0	758	1655
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	0	0	4	1	4	0	4	1	0	1	5	0	1	273	4	0	278	0	0	220	1	0	221	508
15:15	0	1	0	3	1	4	0	1	0	3	3	4	0	5	278	5	0	288	0	0	226	2	0	228	524
15:30	0	0	0	1	1	1	0	2	0	6	1	8	0	1	263	8	0	272	0	0	201	1	0	202	483
15:45	0	0	0	5	1	5	0	5	0	4	0	9	0	2	324	9	0	335	0	0	215	2	0	217	566
Hourly Total	0	1	0	13	4	14	0	12	1	13	5	26	0	9	1138	26	0	1173	0	0	862	6	0	868	2081
16:00	0	0	0	6	0	6	0	5	0	3	0	8	0	2	340	9	0	351	0	0	202	1	0	203	568
16:15	0	0	0	3	2	3	0	6	0	2	2	8	0	4	337	6	0	347	0	0	227	1	0	228	586
16:30	0	0	0	2	1	2	0	4	0	4	3	8	0	3	349	4	0	356	0	0	217	0	0	217	583
16:45	0	0	0	0	1	0	0	5	1	1	0	7	0	3	335	5	0	343	0	0	218	2	0	220	570
Hourly Total	0	0	0	11	4	11	0	20	1	10	5	31	0	12	1361	24	0	1397	0	0	864	4	0	868	2307
17:00	0	0	0	3	1	3	0	7	0	4	2	11	0	0	371	5	0	376	0	0	200	0	0	200	590
17:15	0	0	0	2	1	2	0	5	0	0	0	5	0	6	350	7	0	363	0	1	232	1	0	234	604
17:30	0	1	0	2	1	3	0	4	0	3	2	7	0	1	334	8	0	343	0	1	218	1	0	220	573
17:45	0	1	1	7	1	9	0	5	0	1	2	6	0	4	323	5	0	332	0	0	230	0	0	230	577
Hourly Total	0	2	1	14	4	17	0	21	0	8	6	29	0	11	1378	25	0	1414	0	2	880	2	0	884	2344
18:00	0	0	0	3	1	3	0	3	0	3	1	6	0	1	298	4	0	303	0	0	187	0	0	187	499
18:15	0	0	0	2	2	2	0	2	0	2	2	4	0	1	301	7	0	309	0	0	190	0	0	190	505
18:30	0	0	0	1	0	1	0	7	0	2	3	9	0	3	222	8	0	233	0	1	188	0	0	189	432
18:45	0	0	0	1	0	1	0	1	0	4	2	5	0	2	199	6	0	207	0	0	150	0	0	150	363
Hourly Total	0	0	0	7	3	7	0	13	0	11	8	24	0	7	1020	25	0	1052	0	1	715	0	0	716	1799
Grand Total	0	6	2	88	19	96	0	117	2	74	49	193	0	78	7916	175	2	8169	0	4	6914	28	1	6946	15404
Approach %	0.0	6.3	2.1	91.7	-	-	0.0	60.6	1.0	38.3	-	-	0.0	1.0	96.9	2.1	-	-	0.0	0.1	99.5	0.4	-	-	-
Total %	0.0	0.0	0.0	0.6	-	0.6	0.0	0.8	0.0	0.5	-	1.3	0.0	0.5	51.4	1.1	-	53.0	0.0	0.0	44.9	0.2	-	45.1	-
Lights	0	5	2	87	10	94	0	113	2	74	22	189	0	75	7841	166	1	8082	0	4	6845	28	1	6877	15242
% Lights	-	83.3	100.0	98.9	52.6	97.9	-	96.6	100.0	100.0	44.9	97.9	-	96.2	99.1	94.9	50.0	98.9	-	100.0	99.0	100.0	100.0	99.0	98.9
Other Vehicles	0	0	0	1	9	1	0	0	0	0	27	0	0	3	75	3	1	81	0	0	69	0	0	69	151
% Other Vehicles	-	0.0	0.0	1.1	47.4	1.0	-	0.0	0.0	0.0	55.1	0.0	-	3.8	0.9	1.7	50.0	1.0	-	0.0	1.0	0.0	0.0	1.0	1.0
Bicycles on Road	0	1	0	0	0	1	0	4	0	0	0	4	0	0	0	6	0	6	0	0	0	0	0	0	11
% Bicycles on Road	-	16.7	0.0	0.0	0.0	1.0	-	3.4	0.0	0.0	0.0	2.1	-	0.0	0.0	3.4	0.0	0.1	-	0.0	0.0	0.0	0.0	0.0	0.1



Turning Movement Data Plot

### Turning Movement Peak Hour Data (7:00)

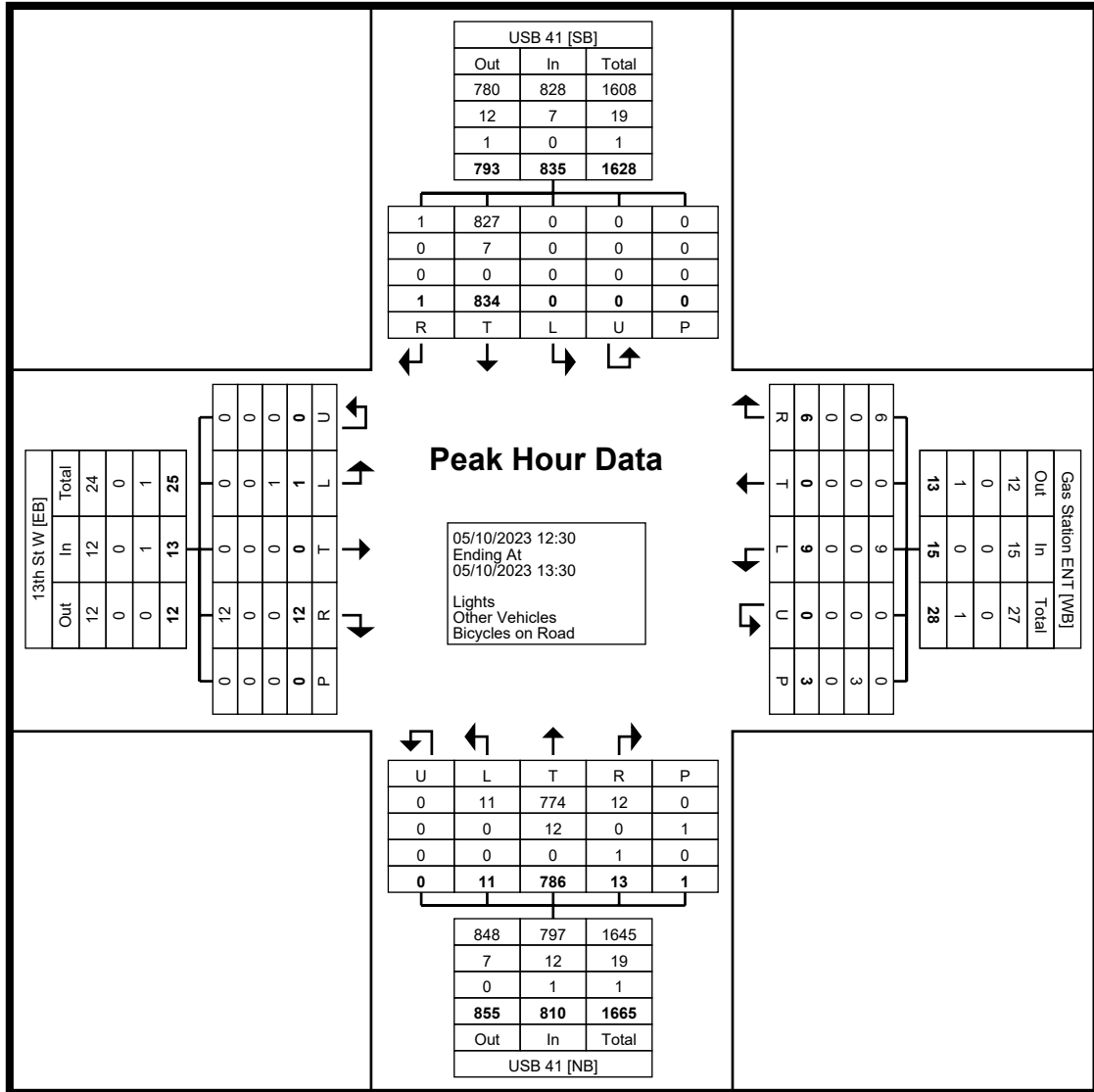
Start Time	13th St W Eastbound						Gas Station ENT Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	0	0	4	1	4	0	7	0	1	6	8	0	7	165	7	1	179	0	0	267	7	0	274	465
7:15	0	0	0	9	0	9	0	4	0	3	1	7	0	6	173	6	0	185	0	0	285	1	0	286	487
7:30	0	0	1	1	0	2	0	10	0	3	0	13	0	1	170	6	0	177	0	0	254	4	1	258	450
7:45	0	0	0	1	0	1	0	2	0	2	1	4	0	1	175	5	0	181	0	0	215	1	0	216	402
<b>Total</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>15</b>	<b>1</b>	<b>16</b>	<b>0</b>	<b>23</b>	<b>0</b>	<b>9</b>	<b>8</b>	<b>32</b>	<b>0</b>	<b>15</b>	<b>683</b>	<b>24</b>	<b>1</b>	<b>722</b>	<b>0</b>	<b>0</b>	<b>1021</b>	<b>13</b>	<b>1</b>	<b>1034</b>	<b>1804</b>
Approach %	0.0	0.0	6.3	93.8	-	-	0.0	71.9	0.0	28.1	-	-	0.0	2.1	94.6	3.3	-	-	0.0	0.0	98.7	1.3	-	-	-
Total %	0.0	0.0	0.1	0.8	-	0.9	0.0	1.3	0.0	0.5	-	1.8	0.0	0.8	37.9	1.3	-	40.0	0.0	0.0	56.6	0.7	-	57.3	-
PHF	0.000	0.000	0.250	0.417	-	0.444	0.000	0.575	0.000	0.750	-	0.615	0.000	0.536	0.976	0.857	-	0.976	0.000	0.000	0.896	0.464	-	0.904	0.926
Lights	0	0	1	14	1	15	0	22	0	9	6	31	0	15	675	23	1	713	0	0	1000	13	1	1013	1772
% Lights	-	-	100.0	93.3	100.0	93.8	-	95.7	-	100.0	75.0	96.9	-	100.0	98.8	95.8	100.0	98.8	-	-	97.9	100.0	100.0	98.0	98.2
Other Vehicles	0	0	0	1	0	1	0	0	0	0	2	0	0	0	8	1	0	9	0	0	21	0	0	21	31
% Other Vehicles	-	-	0.0	6.7	0.0	6.3	-	0.0	-	0.0	25.0	0.0	-	0.0	1.2	4.2	0.0	1.2	-	-	2.1	0.0	0.0	2.0	1.7
Bicycles on Road	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
% Bicycles on Road	-	-	0.0	0.0	0.0	0.0	-	4.3	-	0.0	0.0	3.1	-	0.0	0.0	0.0	0.0	0.0	-	-	0.0	0.0	0.0	0.0	0.1



Turning Movement Peak Hour Data Plot (7:00)

### Turning Movement Peak Hour Data (12:30)

Start Time	13th St W Eastbound						Gas Station ENT Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
12:30	0	0	0	3	0	3	0	0	0	3	1	3	0	2	219	2	0	223	0	0	230	1	0	231	460
12:45	0	0	0	2	0	2	0	3	0	0	2	3	0	1	194	4	0	199	0	0	196	0	0	196	400
13:00	0	0	0	5	0	5	0	3	0	0	0	3	0	5	185	4	1	194	0	0	195	0	0	195	397
13:15	0	1	0	2	0	3	0	3	0	3	0	6	0	3	188	3	0	194	0	0	213	0	0	213	416
<b>Total</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>12</b>	<b>0</b>	<b>13</b>	<b>0</b>	<b>9</b>	<b>0</b>	<b>6</b>	<b>3</b>	<b>15</b>	<b>0</b>	<b>11</b>	<b>786</b>	<b>13</b>	<b>1</b>	<b>810</b>	<b>0</b>	<b>0</b>	<b>834</b>	<b>1</b>	<b>0</b>	<b>835</b>	<b>1673</b>
Approach %	0.0	7.7	0.0	92.3	-	-	0.0	60.0	0.0	40.0	-	-	0.0	1.4	97.0	1.6	-	-	0.0	0.0	99.9	0.1	-	-	-
Total %	0.0	0.1	0.0	0.7	-	0.8	0.0	0.5	0.0	0.4	-	0.9	0.0	0.7	47.0	0.8	-	48.4	0.0	0.0	49.9	0.1	-	49.9	-
PHF	0.000	0.250	0.000	0.600	-	0.650	0.000	0.750	0.000	0.500	-	0.625	0.000	0.550	0.897	0.813	-	0.908	0.000	0.000	0.907	0.250	-	0.904	0.909
Lights	0	0	0	12	0	12	0	9	0	6	0	15	0	11	774	12	0	797	0	0	827	1	0	828	1652
% Lights	-	0.0	-	100.0	-	92.3	-	100.0	-	100.0	0.0	100.0	-	100.0	98.5	92.3	0.0	98.4	-	-	99.2	100.0	-	99.2	98.7
Other Vehicles	0	0	0	0	0	0	0	0	0	0	3	0	0	0	12	0	1	12	0	0	7	0	0	7	19
% Other Vehicles	-	0.0	-	0.0	-	0.0	-	0.0	-	0.0	100.0	0.0	-	0.0	1.5	0.0	100.0	1.5	-	-	0.8	0.0	-	0.8	1.1
Bicycles on Road	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	2
% Bicycles on Road	-	100.0	-	0.0	-	7.7	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	7.7	0.0	0.1	-	-	0.0	0.0	-	0.0	0.1

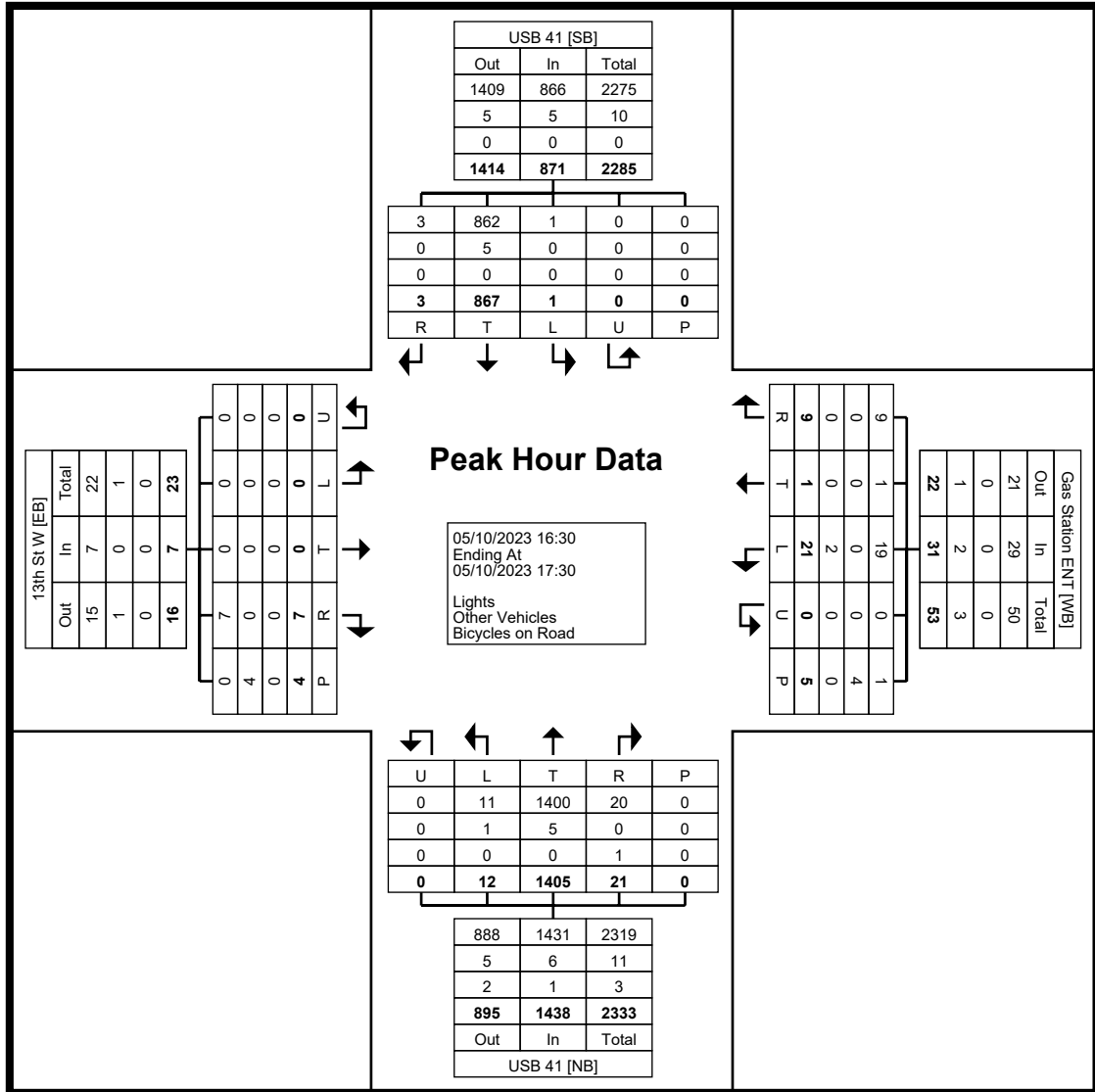


Turning Movement Peak Hour Data Plot (12:30)



### Turning Movement Peak Hour Data (16:30)

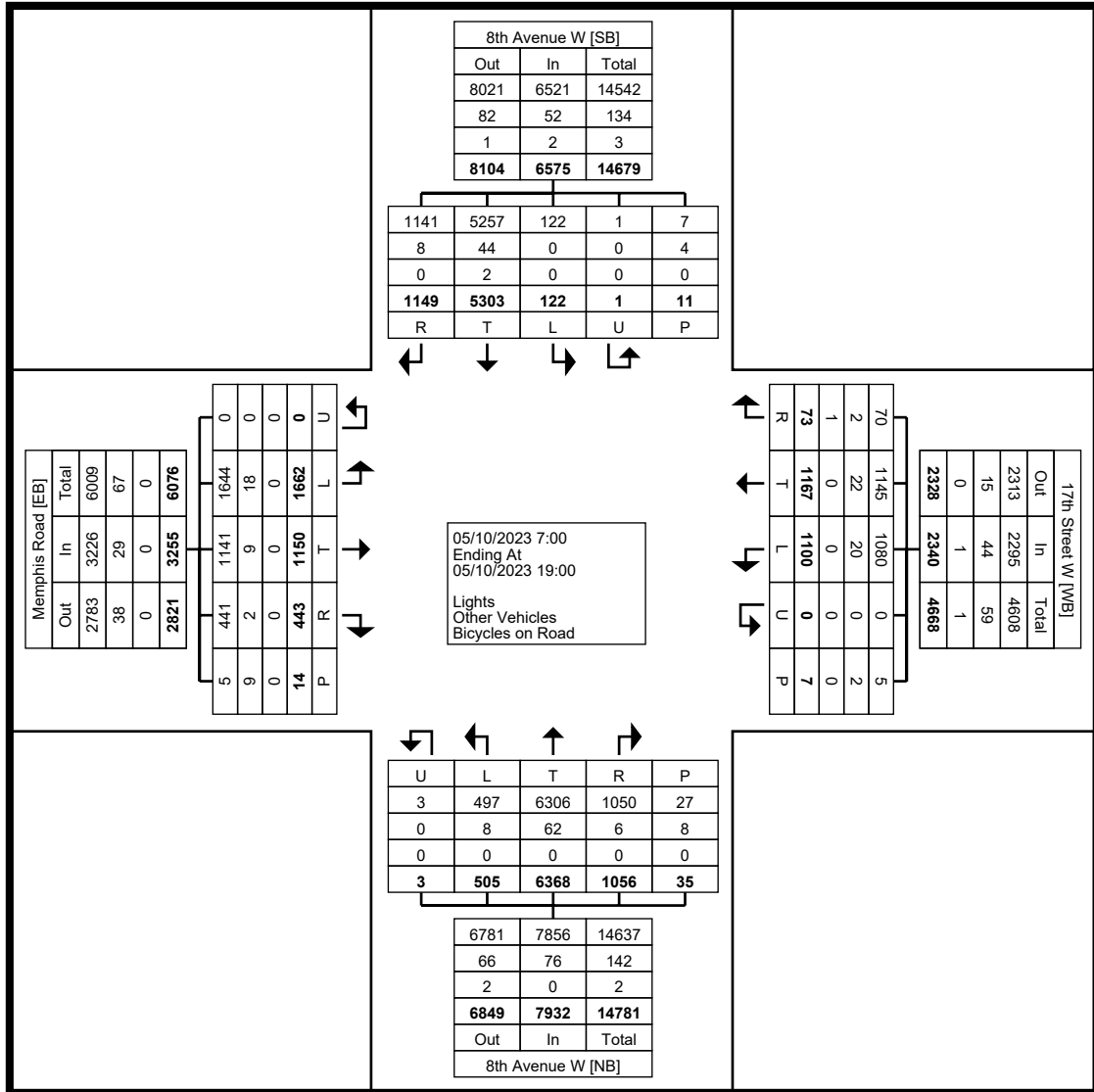
Start Time	13th St W Eastbound						Gas Station ENT Westbound						USB 41 Northbound						USB 41 Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
16:30	0	0	0	2	1	2	0	4	0	4	3	8	0	3	349	4	0	356	0	0	217	0	0	217	583
16:45	0	0	0	0	1	0	0	5	1	1	0	7	0	3	335	5	0	343	0	0	218	2	0	220	570
17:00	0	0	0	3	1	3	0	7	0	4	2	11	0	0	371	5	0	376	0	0	200	0	0	200	590
17:15	0	0	0	2	1	2	0	5	0	0	0	5	0	6	350	7	0	363	0	1	232	1	0	234	604
<b>Total</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>7</b>	<b>4</b>	<b>7</b>	<b>0</b>	<b>21</b>	<b>1</b>	<b>9</b>	<b>5</b>	<b>31</b>	<b>0</b>	<b>12</b>	<b>1405</b>	<b>21</b>	<b>0</b>	<b>1438</b>	<b>0</b>	<b>1</b>	<b>867</b>	<b>3</b>	<b>0</b>	<b>871</b>	<b>2347</b>
Approach %	0.0	0.0	0.0	100.0	-	-	0.0	67.7	3.2	29.0	-	-	0.0	0.8	97.7	1.5	-	-	0.0	0.1	99.5	0.3	-	-	-
Total %	0.0	0.0	0.0	0.3	-	0.3	0.0	0.9	0.0	0.4	-	1.3	0.0	0.5	59.9	0.9	-	61.3	0.0	0.0	36.9	0.1	-	37.1	-
PHF	0.000	0.000	0.000	0.583	-	0.583	0.000	0.750	0.250	0.563	-	0.705	0.000	0.500	0.947	0.750	-	0.956	0.000	0.250	0.934	0.375	-	0.931	0.971
Lights	0	0	0	7	0	7	0	19	1	9	1	29	0	11	1400	20	0	1431	0	1	862	3	0	866	2333
% Lights	-	-	-	100.0	0.0	100.0	-	90.5	100.0	100.0	20.0	93.5	-	91.7	99.6	95.2	-	99.5	-	100.0	99.4	100.0	-	99.4	99.4
Other Vehicles	0	0	0	0	4	0	0	0	0	0	4	0	0	1	5	0	0	6	0	0	5	0	0	5	11
% Other Vehicles	-	-	-	0.0	100.0	0.0	-	0.0	0.0	0.0	80.0	0.0	-	8.3	0.4	0.0	-	0.4	-	0.0	0.6	0.0	-	0.6	0.5
Bicycles on Road	0	0	0	0	0	0	0	2	0	0	0	2	0	0	0	1	0	1	0	0	0	0	0	0	3
% Bicycles on Road	-	-	-	0.0	0.0	0.0	-	9.5	0.0	0.0	0.0	6.5	-	0.0	0.0	4.8	-	0.1	-	0.0	0.0	0.0	-	0.0	0.1



Turning Movement Peak Hour Data Plot (16:30)

### Turning Movement Data

Start Time	Memphis Road Eastbound						17th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	52	56	11	0	119	0	40	76	0	0	116	0	48	105	13	7	166	0	0	222	72	0	294	695
7:15	0	68	64	14	0	146	0	35	67	0	0	102	0	26	146	20	7	192	0	3	235	69	3	307	747
7:30	0	51	36	12	0	99	0	25	59	1	0	85	0	10	145	26	3	181	0	2	226	69	0	297	662
7:45	0	50	36	8	0	94	0	25	44	5	0	74	0	10	149	26	0	185	0	7	186	101	1	294	647
Hourly Total	0	221	192	45	0	458	0	125	246	6	0	377	0	94	545	85	17	724	0	12	869	311	4	1192	2751
8:00	0	66	47	8	0	121	0	31	37	2	0	70	0	7	131	20	0	158	0	3	178	57	0	238	587
8:15	0	49	37	10	0	96	0	23	32	2	0	57	0	9	155	33	1	197	0	3	206	73	0	282	632
8:30	0	38	22	10	0	70	0	31	26	0	0	57	0	14	116	22	0	152	0	4	235	36	0	275	554
8:45	0	40	26	14	0	80	0	37	21	2	0	60	0	11	132	26	0	169	1	1	228	17	0	247	556
Hourly Total	0	193	132	42	0	367	0	122	116	6	0	244	0	41	534	101	1	676	1	11	847	183	0	1042	2329
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12:00	0	51	37	23	0	111	0	21	23	2	0	46	0	8	157	37	0	202	0	3	120	12	0	135	494
12:15	0	36	23	16	0	75	0	32	15	4	0	51	0	9	169	24	1	202	0	6	144	18	0	168	496
12:30	0	29	27	12	0	68	0	34	24	3	1	61	0	12	161	34	0	207	0	2	166	11	0	179	515
12:45	0	26	15	13	0	54	0	34	23	3	0	60	0	13	138	27	1	178	0	4	133	14	0	151	443
Hourly Total	0	142	102	64	0	308	0	121	85	12	1	218	0	42	625	122	2	789	0	15	563	55	0	633	1948
13:00	0	32	23	15	0	70	0	28	36	2	0	66	1	11	121	20	0	153	0	2	141	16	0	159	448
13:15	0	30	17	21	0	68	0	33	24	2	0	59	0	13	185	20	0	218	0	3	156	18	0	177	522
13:30	0	39	40	11	0	90	0	21	29	1	0	51	1	9	163	25	0	198	0	1	127	8	0	136	475
13:45	0	32	26	12	0	70	0	21	18	4	0	43	0	18	192	35	1	245	0	5	136	22	1	163	521
Hourly Total	0	133	106	59	0	298	0	103	107	9	0	219	2	51	661	100	1	814	0	11	560	64	1	635	1966
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15:00	0	71	49	25	1	145	0	36	30	2	0	68	0	20	214	31	5	265	0	5	180	36	1	221	699
15:15	0	67	53	16	0	136	0	38	54	3	0	95	0	12	226	52	1	290	0	8	168	32	0	208	729
15:30	0	62	47	21	0	130	0	38	38	6	0	82	0	17	216	33	0	266	0	4	139	39	0	182	660
15:45	0	61	35	15	0	111	0	33	40	6	1	79	0	21	276	40	0	337	0	7	158	49	0	214	741
Hourly Total	0	261	184	77	1	522	0	145	162	17	1	324	0	70	932	156	6	1158	0	24	645	156	1	825	2829
16:00	0	59	29	17	0	105	0	34	25	3	0	62	0	19	283	25	1	327	0	7	159	33	0	199	693
16:15	0	59	36	30	3	125	0	45	47	0	1	92	0	23	274	35	1	332	0	3	153	45	0	201	750
16:30	0	64	40	19	1	123	0	45	39	3	0	87	0	18	285	44	4	347	0	0	147	41	1	188	745
16:45	0	65	43	8	0	116	0	39	51	1	1	91	0	11	266	43	1	320	0	3	170	32	1	205	732
Hourly Total	0	247	148	74	4	469	0	163	162	7	2	332	0	71	1108	147	7	1326	0	13	629	151	2	793	2920
17:00	0	80	51	12	0	143	0	38	35	3	1	76	1	10	310	46	0	367	0	2	147	39	0	188	774
17:15	0	76	56	11	3	143	0	45	48	3	1	96	0	19	302	45	0	366	0	6	177	37	0	220	825
17:30	0	74	50	17	0	141	0	44	45	0	0	89	0	12	294	39	0	345	0	9	161	34	0	204	779
17:45	0	67	31	10	3	108	0	37	38	1	0	76	0	25	258	47	0	330	0	5	184	31	2	220	734
Hourly Total	0	297	188	50	6	535	0	164	166	7	2	337	1	66	1164	177	0	1408	0	22	669	141	2	832	3112
18:00	0	56	32	8	1	96	0	36	22	2	0	60	0	24	256	49	0	329	0	5	145	28	0	178	663
18:15	0	30	19	10	1	59	0	51	31	3	0	85	0	19	227	42	0	288	0	5	131	20	1	156	588
18:30	0	41	23	5	1	69	0	39	37	1	1	77	0	15	168	40	1	223	0	2	144	17	0	163	532
18:45	0	41	24	9	0	74	0	31	33	3	0	67	0	12	148	37	0	197	0	2	101	23	0	126	464
Hourly Total	0	168	98	32	3	298	0	157	123	9	1	289	0	70	799	168	1	1037	0	14	521	88	1	623	2247
Grand Total	0	1662	1150	443	14	3255	0	1100	1167	73	7	2340	3	505	6368	1056	35	7932	1	122	5303	1149	11	6575	20102
Approach %	0.0	51.1	35.3	13.6	-	-	0.0	47.0	49.9	3.1	-	-	0.0	6.4	80.3	13.3	-	-	0.0	1.9	80.7	17.5	-	-	-
Total %	0.0	8.3	5.7	2.2	-	16.2	0.0	5.5	5.8	0.4	-	11.6	0.0	2.5	31.7	5.3	-	39.5	0.0	0.6	26.4	5.7	-	32.7	-
Lights	0	1644	1141	441	5	3226	0	1080	1145	70	5	2295	3	497	6306	1050	27	7856	1	122	5257	1141	7	6521	19898
% Lights	-	98.9	99.2	99.5	35.7	99.1	-	98.2	98.1	95.9	71.4	98.1	100.0	98.4	99.0	99.4	77.1	99.0	100.0	100.0	99.1	99.3	63.6	99.2	99.0
Other Vehicles	0	18	9	2	9	29	0	20	22	2	2	44	0	8	62	6	8	76	0	0	44	8	4	52	201
% Other Vehicles	-	1.1	0.8	0.5	64.3	0.9	-	1.8	1.9	2.7	28.6	1.9	0.0	1.6	1.0	0.6	22.9	1.0	0.0	0.0	0.8	0.7	36.4	0.8	1.0
Bicycles on Road	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	2	0	0	2	3
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	1.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Data Plot

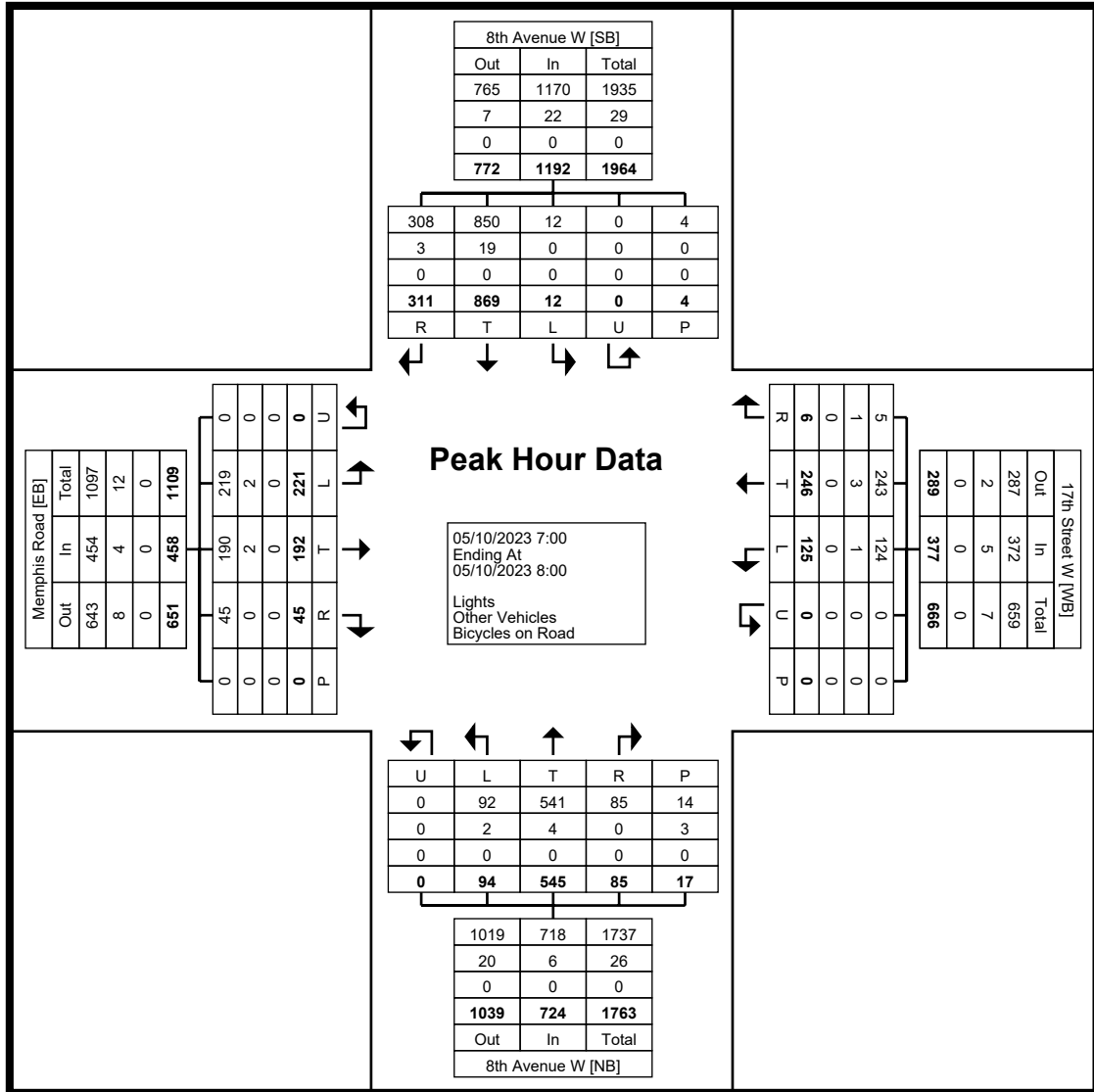
8th Avenue W and 17th Street  
W / Memphis Road  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 1 8th Avenue W  
and 17th Street W / Memphis  
Road  
Site Code: 1  
Start Date: 05/10/2023  
Page No: 3

### Turning Movement Peak Hour Data (7:00)

Start Time	Memphis Road Eastbound						17th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00	0	52	56	11	0	119	0	40	76	0	0	116	0	48	105	13	7	166	0	0	222	72	0	294	695
7:15	0	68	64	14	0	146	0	35	67	0	0	102	0	26	146	20	7	192	0	3	235	69	3	307	747
7:30	0	51	36	12	0	99	0	25	59	1	0	85	0	10	145	26	3	181	0	2	226	69	0	297	662
7:45	0	50	36	8	0	94	0	25	44	5	0	74	0	10	149	26	0	185	0	7	186	101	1	294	647
<b>Total</b>	0	221	192	45	0	458	0	125	246	6	0	377	0	94	545	85	17	724	0	12	869	311	4	1192	2751
Approach %	0.0	48.3	41.9	9.8	-	-	0.0	33.2	65.3	1.6	-	-	0.0	13.0	75.3	11.7	-	-	0.0	1.0	72.9	26.1	-	-	-
Total %	0.0	8.0	7.0	1.6	-	16.6	0.0	4.5	8.9	0.2	-	13.7	0.0	3.4	19.8	3.1	-	26.3	0.0	0.4	31.6	11.3	-	43.3	-
PHF	0.000	0.813	0.750	0.804	-	0.784	0.000	0.781	0.809	0.300	-	0.813	0.000	0.490	0.914	0.817	-	0.943	0.000	0.429	0.924	0.770	-	0.971	0.921
Lights	0	219	190	45	0	454	0	124	243	5	0	372	0	92	541	85	14	718	0	12	850	308	4	1170	2714
% Lights	-	99.1	99.0	100.0	-	99.1	-	99.2	98.8	83.3	-	98.7	-	97.9	99.3	100.0	82.4	99.2	-	100.0	97.8	99.0	100.0	98.2	98.7
Other Vehicles	0	2	2	0	0	4	0	1	3	1	0	5	0	2	4	0	3	6	0	0	19	3	0	22	37
% Other Vehicles	-	0.9	1.0	0.0	-	0.9	-	0.8	1.2	16.7	-	1.3	-	2.1	0.7	0.0	17.6	0.8	-	0.0	2.2	1.0	0.0	1.8	1.3
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (7:00)

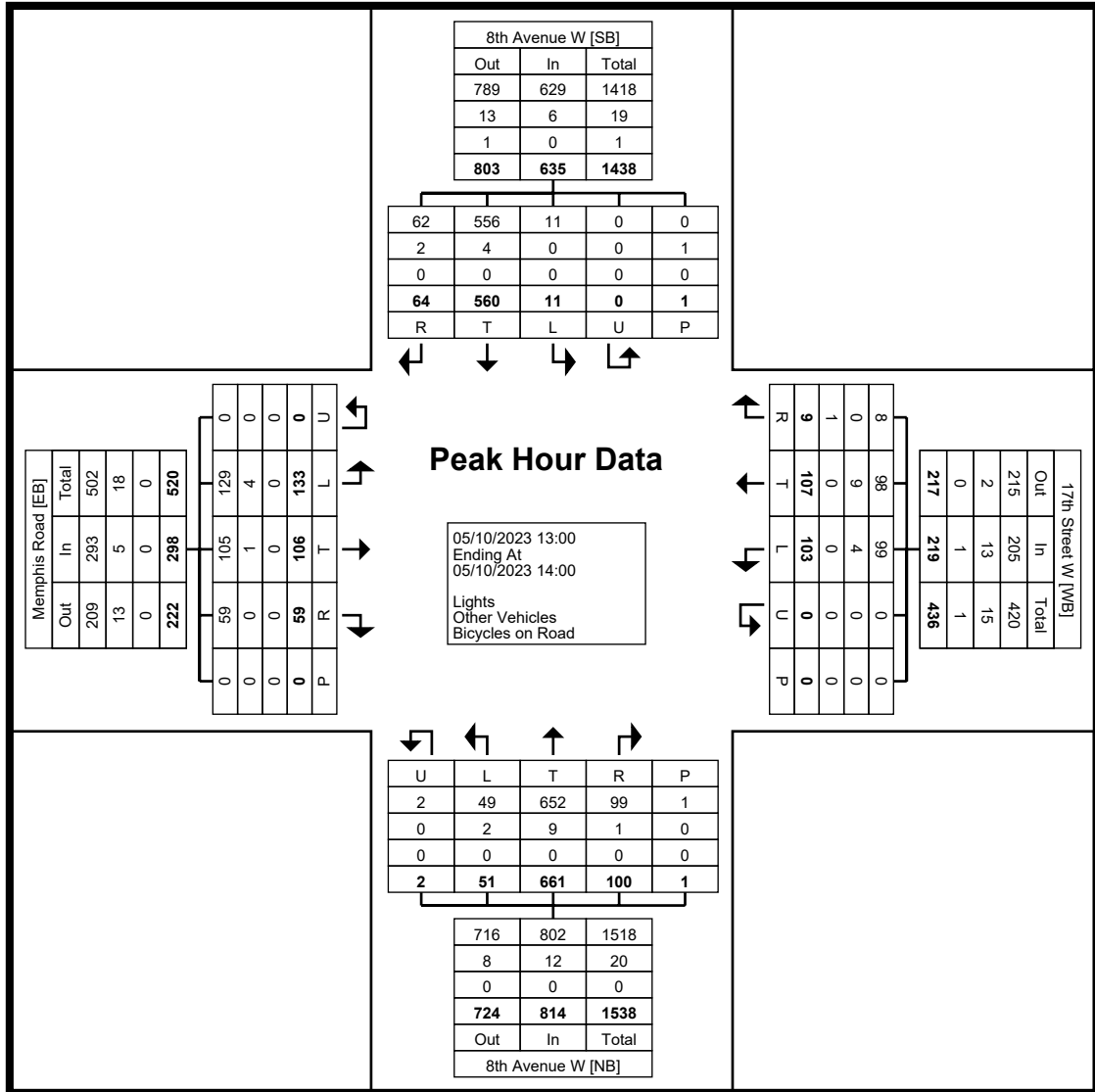
8th Avenue W and 17th Street  
W / Memphis Road  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 1 8th Avenue W  
and 17th Street W / Memphis  
Road  
Site Code: 1  
Start Date: 05/10/2023  
Page No: 5

### Turning Movement Peak Hour Data (13:00)

Start Time	Memphis Road Eastbound						17th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
13:00	0	32	23	15	0	70	0	28	36	2	0	66	1	11	121	20	0	153	0	2	141	16	0	159	448
13:15	0	30	17	21	0	68	0	33	24	2	0	59	0	13	185	20	0	218	0	3	156	18	0	177	522
13:30	0	39	40	11	0	90	0	21	29	1	0	51	1	9	163	25	0	198	0	1	127	8	0	136	475
13:45	0	32	26	12	0	70	0	21	18	4	0	43	0	18	192	35	1	245	0	5	136	22	1	163	521
<b>Total</b>	0	133	106	59	0	298	0	103	107	9	0	219	2	51	661	100	1	814	0	11	560	64	1	635	1966
Approach %	0.0	44.6	35.6	19.8	-	-	0.0	47.0	48.9	4.1	-	-	0.2	6.3	81.2	12.3	-	-	0.0	1.7	88.2	10.1	-	-	-
Total %	0.0	6.8	5.4	3.0	-	15.2	0.0	5.2	5.4	0.5	-	11.1	0.1	2.6	33.6	5.1	-	41.4	0.0	0.6	28.5	3.3	-	32.3	-
PHF	0.000	0.853	0.663	0.702	-	0.828	0.000	0.780	0.743	0.563	-	0.830	0.500	0.708	0.861	0.714	-	0.831	0.000	0.550	0.897	0.727	-	0.897	0.942
Lights	0	129	105	59	0	293	0	99	98	8	0	205	2	49	652	99	1	802	0	11	556	62	0	629	1929
% Lights	-	97.0	99.1	100.0	-	98.3	-	96.1	91.6	88.9	-	93.6	100.0	96.1	98.6	99.0	100.0	98.5	-	100.0	99.3	96.9	0.0	99.1	98.1
Other Vehicles	0	4	1	0	0	5	0	4	9	0	0	13	0	2	9	1	0	12	0	0	4	2	1	6	36
% Other Vehicles	-	3.0	0.9	0.0	-	1.7	-	3.9	8.4	0.0	-	5.9	0.0	3.9	1.4	1.0	0.0	1.5	-	0.0	0.7	3.1	100.0	0.9	1.8
Bicycles on Road	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	11.1	-	0.5	0.0	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.1



Turning Movement Peak Hour Data Plot (13:00)



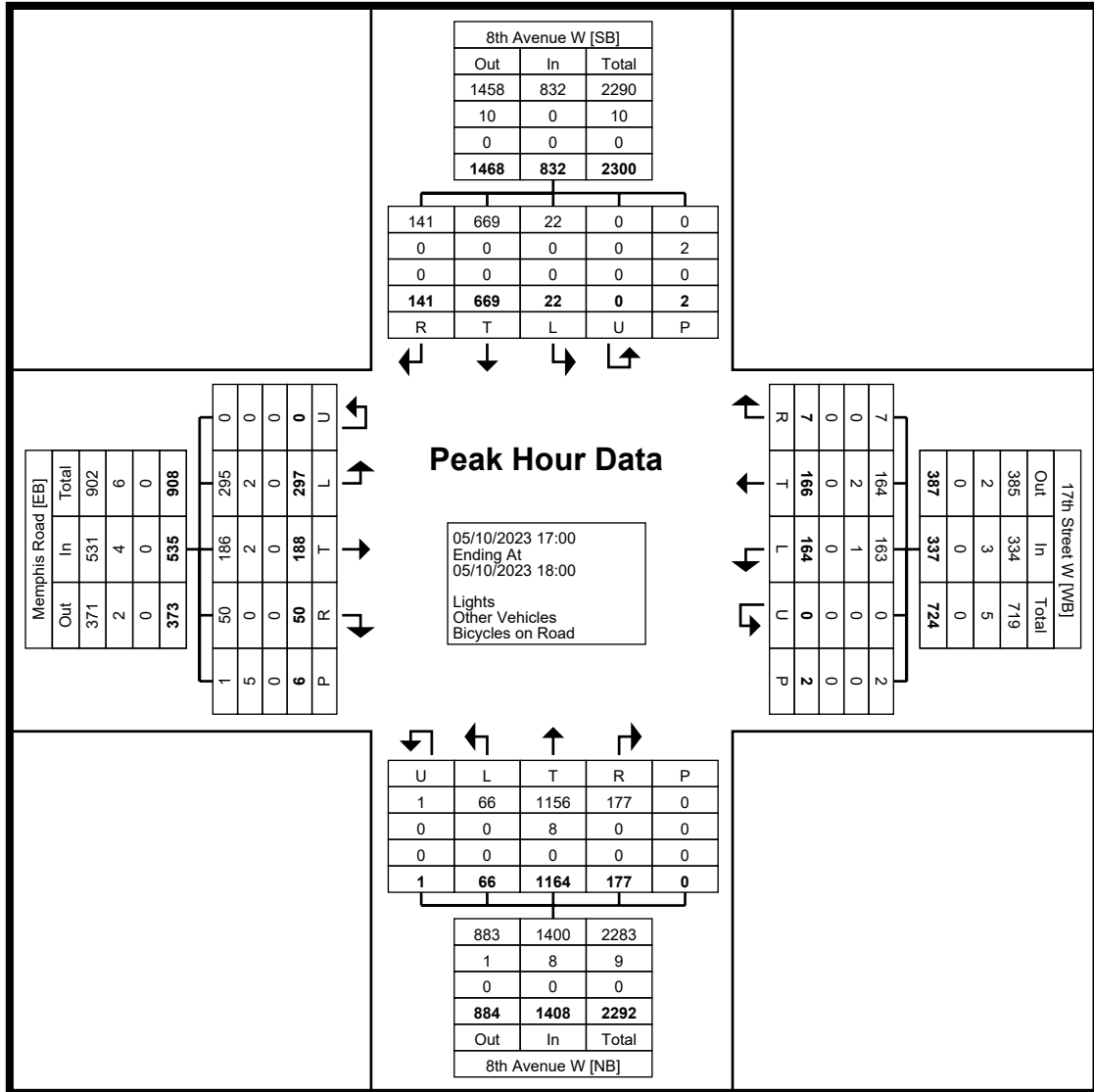
8th Avenue W and 17th Street  
W / Memphis Road  
Weekday TMC

Florida Transportation Engineering, Inc.  
(FTE)  
8250 Pascal Dr  
Punta Gorda, Florida, United States 33950  
(800) 639-4851

Count Name: 1 8th Avenue W  
and 17th Street W / Memphis  
Road  
Site Code: 1  
Start Date: 05/10/2023  
Page No: 7

### Turning Movement Peak Hour Data (17:00)

Start Time	Memphis Road Eastbound						17th Street W Westbound						8th Avenue W Northbound						8th Avenue W Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
17:00	0	80	51	12	0	143	0	38	35	3	1	76	1	10	310	46	0	367	0	2	147	39	0	188	774
17:15	0	76	56	11	3	143	0	45	48	3	1	96	0	19	302	45	0	366	0	6	177	37	0	220	825
17:30	0	74	50	17	0	141	0	44	45	0	0	89	0	12	294	39	0	345	0	9	161	34	0	204	779
17:45	0	67	31	10	3	108	0	37	38	1	0	76	0	25	258	47	0	330	0	5	184	31	2	220	734
<b>Total</b>	0	297	188	50	6	535	0	164	166	7	2	337	1	66	1164	177	0	1408	0	22	669	141	2	832	3112
Approach %	0.0	55.5	35.1	9.3	-	-	0.0	48.7	49.3	2.1	-	-	0.1	4.7	82.7	12.6	-	-	0.0	2.6	80.4	16.9	-	-	-
Total %	0.0	9.5	6.0	1.6	-	17.2	0.0	5.3	5.3	0.2	-	10.8	0.0	2.1	37.4	5.7	-	45.2	0.0	0.7	21.5	4.5	-	26.7	-
PHF	0.000	0.928	0.839	0.735	-	0.935	0.000	0.911	0.865	0.583	-	0.878	0.250	0.660	0.939	0.941	-	0.959	0.000	0.611	0.909	0.904	-	0.945	0.943
Lights	0	295	186	50	1	531	0	163	164	7	2	334	1	66	1156	177	0	1400	0	22	669	141	0	832	3097
% Lights	-	99.3	98.9	100.0	16.7	99.3	-	99.4	98.8	100.0	100.0	99.1	100.0	100.0	99.3	100.0	-	99.4	-	100.0	100.0	100.0	0.0	100.0	99.5
Other Vehicles	0	2	2	0	5	4	0	1	2	0	0	3	0	0	8	0	0	8	0	0	0	0	2	0	15
% Other Vehicles	-	0.7	1.1	0.0	83.3	0.7	-	0.6	1.2	0.0	0.0	0.9	0.0	0.0	0.7	0.0	-	0.6	-	0.0	0.0	0.0	100.0	0.0	0.5
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Bicycles on Road	-	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0



Turning Movement Peak Hour Data Plot (17:00)

**Existing Synchro Outputs**

HCM 6th Signalized Intersection Summary  
 1: US 41 & Ringling Plaza/University Pkwy

Existing AM  
 09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	14	28	3	319	51	411	11	579	117	417	1629	13
Future Volume (veh/h)	14	28	3	319	51	411	11	579	117	417	1629	13
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1796	1796	1900	1870	1811	1841	1767	1856	1796	1885	1870	1781
Adj Flow Rate, veh/h	17	33	4	424	0	489	13	689	139	496	1939	0
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Percent Heavy Veh, %	7	7	0	2	6	4	9	3	7	1	2	8
Cap, veh/h	65	68	61	715	0	554	23	1278	258	539	2039	
Arrive On Green	0.04	0.04	0.04	0.20	0.00	0.20	0.01	0.44	0.44	0.31	1.00	0.00
Sat Flow, veh/h	1711	1796	1610	3563	0	1558	1682	2923	589	3483	3647	0
Grp Volume(v), veh/h	17	33	4	424	0	489	13	415	413	496	1939	0
Grp Sat Flow(s),veh/h/ln	1711	1796	1610	1781	0	1558	1682	1763	1749	1742	1777	0
Q Serve(g_s), s	1.6	3.1	0.4	18.4	0.0	34.1	1.3	29.5	29.5	23.4	0.0	0.0
Cycle Q Clear(g_c), s	1.6	3.1	0.4	18.4	0.0	34.1	1.3	29.5	29.5	23.4	0.0	0.0
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.34	1.00		0.00
Lane Grp Cap(c), veh/h	65	68	61	715	0	554	23	771	765	539	2039	
V/C Ratio(X)	0.26	0.48	0.07	0.59	0.00	0.88	0.57	0.54	0.54	0.92	0.95	
Avail Cap(c_a), veh/h	112	117	105	715	0	554	80	771	765	633	2039	
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	0.81	0.81	0.00
Uniform Delay (d), s/veh	79.5	80.1	78.9	61.7	0.0	51.5	83.4	35.2	35.2	57.7	0.0	0.0
Incr Delay (d2), s/veh	2.1	5.2	0.4	1.3	0.0	15.5	20.7	2.7	2.7	14.7	9.6	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.8	1.5	0.2	8.3	0.0	22.2	0.7	13.1	13.0	9.9	2.7	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	81.6	85.4	79.3	63.0	0.0	67.0	104.1	37.9	37.9	72.4	9.6	0.0
LnGrp LOS	F	F	E	E	A	E	F	D	D	E	A	
Approach Vol, veh/h		54			913			841			2435	
Approach Delay, s/veh		83.7			65.1			38.9			22.4	
Approach LOS		F			E			D			C	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.2	104.5		42.0	33.4	81.3		13.4				
Change Period (Y+Rc), s	7.9	6.9		7.9	* 7.1	6.9		6.9				
Max Green Setting (Gmax), s	8.1	87.1		34.1	* 31	65.1		11.1				
Max Q Clear Time (g_c+I1), s	3.3	2.0		36.1	25.4	31.5		5.1				
Green Ext Time (p_c), s	0.0	61.2		0.0	0.9	11.0		0.1				

Intersection Summary

HCM 6th Ctrl Delay	35.7
HCM 6th LOS	D

Notes

- User approved pedestrian interval to be less than phase max green.
- User approved volume balancing among the lanes for turning movement.
- User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary  
2: US 41 & College Dr/General Spatz Blvd

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	25	5	9	17	7	4	8	953	44	53	2080	36
Future Volume (veh/h)	25	5	9	17	7	4	8	953	44	53	2080	36
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	0.97		0.99	0.99		0.97	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No		No		No		No		No		No
Adj Sat Flow, veh/h/ln	1841	1900	1900	1544	1900	1900	1530	1856	1663	1752	1870	1900
Adj Flow Rate, veh/h	28	6	10	19	8	4	9	1071	49	60	2337	40
Peak Hour Factor	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Percent Heavy Veh, %	4	0	0	24	0	0	25	3	16	10	2	0
Cap, veh/h	97	30	51	87	56	28	15	2666	122	75	4196	72
Arrive On Green	0.05	0.05	0.05	0.05	0.05	0.05	0.02	1.00	1.00	0.05	0.81	0.81
Sat Flow, veh/h	1340	638	1063	1147	1181	590	1457	3433	157	1668	5170	88
Grp Volume(v), veh/h	28	0	16	19	0	12	9	550	570	60	1537	840
Grp Sat Flow(s),veh/h/ln	1340	0	1701	1147	0	1771	1457	1763	1827	1668	1702	1854
Q Serve(g_s), s	3.5	0.0	1.5	2.8	0.0	1.1	1.0	0.0	0.0	6.1	26.4	26.5
Cycle Q Clear(g_c), s	4.6	0.0	1.5	4.3	0.0	1.1	1.0	0.0	0.0	6.1	26.4	26.5
Prop In Lane	1.00		0.63	1.00		0.33	1.00		0.09	1.00		0.05
Lane Grp Cap(c), veh/h	97	0	81	87	0	84	15	1369	1419	75	2763	1505
V/C Ratio(X)	0.29	0.00	0.20	0.22	0.00	0.14	0.61	0.40	0.40	0.79	0.56	0.56
Avail Cap(c_a), veh/h	163	0	164	143	0	171	105	1369	1419	237	2763	1505
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	0.77	0.77	0.77	1.00	1.00	1.00
Uniform Delay (d), s/veh	79.8	0.0	77.8	79.9	0.0	77.6	82.9	0.0	0.0	80.4	5.5	5.5
Incr Delay (d2), s/veh	2.3	0.0	1.7	1.8	0.0	1.1	31.9	0.7	0.7	19.8	0.8	1.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.3	0.0	0.7	0.9	0.0	0.5	0.5	0.3	0.3	3.0	7.8	8.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	82.1	0.0	79.5	81.7	0.0	78.7	114.8	0.7	0.7	100.2	6.3	7.0
LnGrp LOS	F	A	E	F	A	E	F	A	A	F	A	A
Approach Vol, veh/h		44		31		1129		2437				
Approach Delay, s/veh		81.2		80.5		1.6		8.9				
Approach LOS		F		F		A		A				
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	15.5	138.8		15.7	9.5	144.8		15.7				
Change Period (Y+Rc), s	7.8	6.8		7.6	7.8	6.8		7.6				
Max Green Setting (Gmax), s	21.2	107.2		16.4	12.2	119.2		16.4				
Max Q Clear Time (g_c+10), s	10.1	2.0		6.6	3.0	28.5		6.3				
Green Ext Time (p_c), s	0.1	32.4		0.1	0.0	82.5		0.1				

Intersection Summary

HCM 6th Ctrl Delay	8.1
HCM 6th LOS	A

Notes

User approved pedestrian interval to be less than phase max green.  
User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary  
3: US 41 & Braden Ave

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		
Traffic Volume (veh/h)	35	0	30	0	0	1	35	935	1	2	2153	52
Future Volume (veh/h)	35	0	30	0	0	1	35	935	1	2	2153	52
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	0.98		0.99	1.00		0.99	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No		No		No		No	
Adj Sat Flow, veh/h/ln	1856	1900	1856	1900	1900	1900	1900	1856	1900	1900	1870	1900
Adj Flow Rate, veh/h	39	0	33	0	0	1	39	1039	1	2	2392	58
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	3	0	3	0	0	0	0	3	0	0	2	0
Cap, veh/h	135	9	55	0	0	125	196	3086	3	377	2883	70
Arrive On Green	0.08	0.00	0.08	0.00	0.00	0.08	0.04	0.59	0.59	0.00	0.56	0.56
Sat Flow, veh/h	712	115	700	0	0	1591	1810	5227	5	1810	5128	124
Grp Volume(v), veh/h	72	0	0	0	0	1	39	671	369	2	1585	865
Grp Sat Flow(s),veh/h/ln	1526	0	0	0	0	1591	1810	1689	1855	1810	1702	1848
Q Serve(g_s), s	2.5	0.0	0.0	0.0	0.0	0.0	0.6	7.1	7.1	0.0	26.7	27.0
Cycle Q Clear(g_c), s	3.1	0.0	0.0	0.0	0.0	0.0	0.6	7.1	7.1	0.0	26.7	27.0
Prop In Lane	0.54		0.46	0.00		1.00	1.00		0.00	1.00		0.07
Lane Grp Cap(c), veh/h	199	0	0	0	0	125	196	1994	1095	377	1914	1039
V/C Ratio(X)	0.36	0.00	0.00	0.00	0.00	0.01	0.20	0.34	0.34	0.01	0.83	0.83
Avail Cap(c_a), veh/h	261	0	0	0	0	191	316	1994	1095	548	1914	1039
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	0.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	31.1	0.0	0.0	0.0	0.0	29.8	12.9	7.3	7.3	6.8	12.6	12.6
Incr Delay (d2), s/veh	1.1	0.0	0.0	0.0	0.0	0.0	0.5	0.5	0.8	0.0	4.3	7.8
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.2	0.0	0.0	0.0	0.0	0.0	0.2	1.9	2.2	0.0	8.2	10.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	32.2	0.0	0.0	0.0	0.0	29.8	13.4	7.8	8.2	6.8	16.8	20.4
LnGrp LOS	C	A	A	A	A	C	B	A	A	A	B	C
Approach Vol, veh/h		72			1			1079			2452	
Approach Delay, s/veh		32.2			29.8			8.1			18.1	
Approach LOS		C			C			A			B	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.4	47.6		12.1	8.4	49.5		12.1				
Change Period (Y+Rc), s	7.7	8.2		* 6.6	8.2	8.2		* 6.6				
Max Green Setting (Gmax), s	31.8	31.8		* 8.4	6.8	31.8		* 8.4				
Max Q Clear Time (g_c+1/2), s	29.0	29.0		2.0	2.0	9.1		5.1				
Green Ext Time (p_c), s	0.0	2.8		0.0	0.0	11.5		0.1				

Intersection Summary

HCM 6th Ctrl Delay	15.4
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- User approved ignoring U-Turning movement.
- \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

HCM 6th Signalized Intersection Summary  
4: US 41 & Gaines Ave/Tallevast Rd

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖ ↗	↖ ↗		↖ ↗	↖ ↗		↖ ↗ ↘ ↙	↖ ↗ ↘ ↙		↖ ↗ ↘ ↙	↖ ↗ ↘ ↙	
Traffic Volume (veh/h)	4	4	6	113	3	138	9	894	32	282	2112	1
Future Volume (veh/h)	4	4	6	113	3	138	9	894	32	282	2112	1
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1826	1900	1781	1900	1856	1856	1856	1870	1900
Adj Flow Rate, veh/h	4	4	7	126	3	153	10	993	36	313	2347	1
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	0	0	0	5	0	8	0	3	3	3	2	0
Cap, veh/h	96	76	133	220	4	194	147	3050	110	488	3587	2
Arrive On Green	0.12	0.12	0.12	0.12	0.12	0.12	0.01	0.61	0.61	0.09	0.68	0.68
Sat Flow, veh/h	1247	618	1081	1364	31	1576	1810	5018	182	1767	5272	2
Grp Volume(v), veh/h	4	0	11	126	0	156	10	668	361	313	1515	833
Grp Sat Flow(s),veh/h/ln1247	0	1699	1364	0	1607	1810	1689	1823	1767	1702	1870	
Q Serve(g_s), s	0.4	0.0	0.7	10.8	0.0	11.3	0.3	11.6	11.6	7.5	30.8	30.8
Cycle Q Clear(g_c), s	11.7	0.0	0.7	11.5	0.0	11.3	0.3	11.6	11.6	7.5	30.8	30.8
Prop In Lane	1.00		0.64	1.00		0.98	1.00		0.10	1.00		0.00
Lane Grp Cap(c), veh/h	96	0	209	220	0	197	147	2053	1108	488	2316	1272
V/C Ratio(X)	0.04	0.00	0.05	0.57	0.00	0.79	0.07	0.33	0.33	0.64	0.65	0.65
Avail Cap(c_a), veh/h	163	0	300	293	0	284	243	2053	1108	675	2316	1272
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(l)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	0.77	0.77	0.77
Uniform Delay (d), s/veh	56.8	0.0	46.5	51.5	0.0	51.1	10.9	11.5	11.5	8.0	11.0	11.0
Incr Delay (d2), s/veh	0.2	0.0	0.1	2.4	0.0	9.2	0.2	0.4	0.8	1.1	1.1	2.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.1	0.0	0.3	3.9	0.0	5.1	0.1	4.0	4.4	2.4	9.7	11.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	57.0	0.0	46.6	53.9	0.0	60.3	11.1	11.9	12.3	9.1	12.2	13.1
LnGrp LOS	E	A	D	D	A	E	B	B	B	A	B	B
Approach Vol, veh/h		15		282		1039		2661				
Approach Delay, s/veh		49.4		57.5		12.0		12.1				
Approach LOS		D		E		B		B				
Timer - Assigned Phs	1	2	4	5	6	8						
Phs Duration (G+Y+Rc), s	8.6	88.9	22.5	17.3	80.1	22.5						
Change Period (Y+Rc), s	7.2	7.2	7.8	7.1	7.2	7.8						
Max Green Setting (Gmax), s	7.8	68.8	21.2	22.9	53.8	21.2						
Max Q Clear Time (g_c+1/3), s	12.3	32.8	13.5	9.5	13.6	13.7						
Green Ext Time (p_c), s	0.0	32.6	0.8	0.7	14.9	0.0						

Intersection Summary

HCM 6th Ctrl Delay	15.4
HCM 6th LOS	B

Notes

User approved pedestrian interval to be less than phase max green.

HCM 6th Signalized Intersection Summary  
5: US 41 & Whitfield Ave

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖ ↑↑↑	↗ ↑↑↑		↖ ↑↑↑	↗ ↑↑↑	
Traffic Volume (veh/h)	2	2	4	60	3	19	4	975	34	30	2298	4
Future Volume (veh/h)	2	2	4	60	3	19	4	975	34	30	2298	4
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		0.98
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No		No		No		No		No		No
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1856	1856	1900	1870	1900
Adj Flow Rate, veh/h	2	2	4	65	3	20	4	1048	37	32	2471	4
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Percent Heavy Veh, %	0	0	0	0	0	0	0	3	3	0	2	0
Cap, veh/h	130	36	71	146	13	90	169	3644	129	427	3909	6
Arrive On Green	0.06	0.06	0.06	0.06	0.06	0.06	0.00	0.49	0.49	0.04	0.99	0.99
Sat Flow, veh/h	1405	565	1131	1432	213	1423	1810	5024	177	1810	5264	9
Grp Volume(v), veh/h	2	0	6	65	0	23	4	704	381	32	1598	877
Grp Sat Flow(s),veh/h/ln	1405	0	1696	1432	0	1636	1810	1689	1824	1810	1702	1869
Q Serve(g_s), s	0.2	0.0	0.4	5.4	0.0	1.6	0.1	15.0	15.0	0.5	1.8	1.9
Cycle Q Clear(g_c), s	1.8	0.0	0.4	5.8	0.0	1.6	0.1	15.0	15.0	0.5	1.8	1.9
Prop In Lane	1.00		0.67	1.00		0.87	1.00		0.10	1.00		0.00
Lane Grp Cap(c), veh/h	130	0	107	146	0	103	169	2450	1323	427	2528	1388
V/C Ratio(X)	0.02	0.00	0.06	0.45	0.00	0.22	0.02	0.29	0.29	0.07	0.63	0.63
Avail Cap(c_a), veh/h	171	0	157	188	0	151	266	2450	1323	493	2528	1388
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	0.67	0.67	0.67	1.33	1.33	1.33
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	0.94	0.94	0.94	0.73	0.73	0.73
Uniform Delay (d), s/veh	54.3	0.0	52.9	55.6	0.0	53.4	4.4	12.3	12.3	4.7	0.2	0.2
Incr Delay (d2), s/veh	0.0	0.0	0.2	2.1	0.0	1.1	0.1	0.3	0.5	0.1	0.9	1.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.1	0.0	0.2	2.0	0.0	0.7	0.0	5.9	6.5	0.1	0.5	0.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	54.3	0.0	53.1	57.7	0.0	54.5	4.4	12.6	12.8	4.7	1.1	1.8
LnGrp LOS	D	A	D	E	A	D	A	B	B	A	A	A
Approach Vol, veh/h		8			88			1089			2507	
Approach Delay, s/veh		53.4			56.9			12.6			1.4	
Approach LOS		D			E			B			A	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	8.5	97.0		14.5	10.6	95.0		14.5				
Change Period (Y+Rc), s	7.9	* 7.9		6.9	* 7.3	* 7.9		6.9				
Max Green Setting (Gmax), s	79	* 79		11.1	* 7.7	* 79		11.1				
Max Q Clear Time (g_c+1), s	12	3.9		7.8	2.5	17.0		3.8				
Green Ext Time (p_c), s	0.0	64.8		0.1	0.0	18.3		0.0				

Intersection Summary

HCM 6th Ctrl Delay	6.1
HCM 6th LOS	A

Notes

User approved pedestrian interval to be less than phase max green.  
\* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.



# HCM 6th Signalized Intersection Summary

## 6: US 41 & Pearl Ave

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕		↙	↘		↙	↑↑↑		↙	↑↑↑	
Traffic Volume (veh/h)	26	10	8	82	6	116	6	931	43	72	2250	47
Future Volume (veh/h)	26	10	8	82	6	116	6	931	43	72	2250	47
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1752	1900	1811	1900	1856	1648	1856	1900	1885	1885	1856
Adj Flow Rate, veh/h	28	11	9	89	7	126	7	1012	47	78	2446	51
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	0	10	0	6	0	3	17	3	0	1	1	3
Cap, veh/h	77	28	13	194	10	179	124	3307	153	487	3966	82
Arrive On Green	0.12	0.12	0.12	0.12	0.12	0.12	1.00	1.00	1.00	0.04	0.76	0.76
Sat Flow, veh/h	250	239	113	1348	85	1538	116	4961	230	1795	5189	108
Grp Volume(v), veh/h	48	0	0	89	0	133	7	689	370	78	1615	882
Grp Sat Flow(s),veh/h/ln	602	0	0	1348	0	1623	116	1689	1814	1795	1716	1865
Q Serve(g_s), s	2.4	0.0	0.0	0.0	0.0	9.5	1.4	0.0	0.0	1.5	25.1	25.4
Cycle Q Clear(g_c), s	11.9	0.0	0.0	9.2	0.0	9.5	15.0	0.0	0.0	1.5	25.1	25.4
Prop In Lane	0.58		0.19	1.00		0.95	1.00		0.13	1.00		0.06
Lane Grp Cap(c), veh/h	118	0	0	194	0	189	124	2251	1209	487	2623	1426
V/C Ratio(X)	0.41	0.00	0.00	0.46	0.00	0.70	0.06	0.31	0.31	0.16	0.62	0.62
Avail Cap(c_a), veh/h	185	0	0	263	0	272	124	2251	1209	626	2623	1426
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	1.00	0.97	0.97	0.97	0.71	0.71	0.71
Uniform Delay (d), s/veh	52.6	0.0	0.0	50.9	0.0	51.0	1.3	0.0	0.0	4.9	6.3	6.3
Incr Delay (d2), s/veh	2.3	0.0	0.0	1.7	0.0	4.7	0.8	0.3	0.6	0.1	0.8	1.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.5	0.0	0.0	2.7	0.0	4.1	0.0	0.1	0.2	0.5	6.5	7.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	54.8	0.0	0.0	52.6	0.0	55.7	2.1	0.3	0.6	5.0	7.1	7.8
LnGrp LOS	D	A	A	D	A	E	A	A	A	A	A	A
Approach Vol, veh/h		48			222			1066			2575	
Approach Delay, s/veh		54.8			54.5			0.5			7.2	
Approach LOS		D			D			A			A	
Timer - Assigned Phs		2		4	5	6		8				
Phs Duration (G+Y+Rc), s		99.1		20.9	11.7	87.4		20.9				
Change Period (Y+Rc), s		7.4		6.9	7.1	7.4		6.9				
Max Green Setting (Gmax), s		85.6		20.1	13.9	64.6		20.1				
Max Q Clear Time (g_c+I1), s		27.4		11.5	3.5	17.0		13.9				
Green Ext Time (p_c), s		52.0		0.6	0.1	17.4		0.1				

### Intersection Summary

HCM 6th Ctrl Delay	8.7
HCM 6th LOS	A

### Notes

- User approved pedestrian interval to be less than phase max green.
- User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary  
 7: US 41 & Bay Dr/69th Ave W

Existing AM  
 09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	42	1	98	32	2	12	30	1057	7	2	2271	19
Future Volume (veh/h)	42	1	98	32	2	12	30	1057	7	2	2271	19
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1870	1900	1900	1885	1900
Adj Flow Rate, veh/h	45	1	105	34	2	13	32	1137	8	2	2442	20
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Percent Heavy Veh, %	0	0	0	0	0	0	0	2	0	0	1	0
Cap, veh/h	207	2	171	93	12	20	166	4035	28	383	3606	29
Arrive On Green	0.11	0.11	0.11	0.11	0.11	0.11	0.02	0.52	0.52	0.68	0.68	0.68
Sat Flow, veh/h	1418	15	1593	397	115	185	1810	5231	37	499	5265	43
Grp Volume(v), veh/h	45	0	106	49	0	0	32	740	405	2	1590	872
Grp Sat Flow(s),veh/h/ln	1418	0	1608	697	0	0	1810	1702	1864	499	1716	1877
Q Serve(g_s), s	0.0	0.0	7.6	3.0	0.0	0.0	0.6	14.8	14.8	0.2	32.7	32.8
Cycle Q Clear(g_c), s	3.7	0.0	7.6	10.6	0.0	0.0	0.6	14.8	14.8	4.5	32.7	32.8
Prop In Lane	1.00		0.99	0.69		0.27	1.00		0.02	1.00		0.02
Lane Grp Cap(c), veh/h	207	0	172	125	0	0	166	2625	1437	383	2350	1286
V/C Ratio(X)	0.22	0.00	0.62	0.39	0.00	0.00	0.19	0.28	0.28	0.01	0.68	0.68
Avail Cap(c_a), veh/h	285	0	261	203	0	0	236	2625	1437	383	2350	1286
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	0.67	0.67	0.67	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	0.00	0.96	0.96	0.96	0.48	0.48	0.48
Uniform Delay (d), s/veh	49.5	0.0	51.2	53.9	0.0	0.0	11.2	10.2	10.2	7.5	11.1	11.1
Incr Delay (d2), s/veh	0.5	0.0	3.5	2.0	0.0	0.0	0.5	0.3	0.5	0.0	0.8	1.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3	0.0	3.2	1.5	0.0	0.0	0.3	6.0	6.6	0.0	10.5	11.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	50.0	0.0	54.8	55.9	0.0	0.0	11.7	10.5	10.7	7.5	11.9	12.5
LnGrp LOS	D	A	D	E	A	A	B	B	B	A	B	B
Approach Vol, veh/h	151			49			1177			2464		
Approach Delay, s/veh	53.3			55.9			10.6			12.1		
Approach LOS	D			E			B			B		
Timer - Assigned Phs	1	2	4		6		8					
Phs Duration (G+Y+Rc), s	10.4	89.3	20.3		99.7		20.3					
Change Period (Y+Rc), s	7.1	* 7.1	7.5		* 7.1		7.5					
Max Green Setting (Gmax), s	71	* 71	19.5		* 86		19.5					
Max Q Clear Time (g_c+1/2), s	34.8		12.6		16.8		9.6					
Green Ext Time (p_c), s	0.0	33.6	0.1		20.9		0.5					

Intersection Summary

HCM 6th Ctrl Delay	13.8
HCM 6th LOS	B

Notes

User approved pedestrian interval to be less than phase max green.  
 \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings  
8: US 41 & Florida Blvd

Existing AM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	44	2	456	1	1	1	178	926	4	0	1856	42
Future Volume (vph)	44	2	456	1	1	1	178	926	4	0	1856	42
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	220		0	0		0	175		0	160		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	0			0			0			0		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.91	0.91	1.00	0.91	0.91
Frt		0.851			0.925			0.999			0.997	
Flt Protected	0.950			0.950			0.950					
Satd. Flow (prot)	1687	1601	0	1805	1758	0	1752	5081	0	1900	5059	0
Flt Permitted	0.757			0.138			0.061					
Satd. Flow (perm)	1344	1601	0	262	1758	0	113	5081	0	1900	5059	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		187			1			1				4
Link Speed (mph)		25			25			45				45
Link Distance (ft)		745			154			1576				2174
Travel Time (s)		20.3			4.2			23.9				32.9
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Heavy Vehicles (%)	7%	0%	1%	0%	0%	0%	3%	2%	0%	0%	2%	12%
Adj. Flow (vph)	47	2	485	1	1	1	189	985	4	0	1974	45
Shared Lane Traffic (%)												
Lane Group Flow (vph)	47	487	0	1	2	0	189	989	0	0	2019	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		12			12			12				12
Link Offset(ft)		0			0			0				0
Crosswalk Width(ft)		16			16			16				16
Two way Left Turn Lane		Yes						Yes				Yes
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Number of Detectors	1	2		1	2		1	2		1	2	
Detector Template	Left	Thru		Left	Thru		Left	Thru		Left	Thru	
Leading Detector (ft)	20	100		20	100		20	100		20	100	
Trailing Detector (ft)	0	0		0	0		0	0		0	0	
Detector 1 Position(ft)	0	0		0	0		0	0		0	0	
Detector 1 Size(ft)	20	6		20	6		20	6		20	6	
Detector 1 Type	Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex	
Detector 1 Channel												
Detector 1 Extend (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Queue (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Delay (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 2 Position(ft)		94			94			94			94	
Detector 2 Size(ft)		6			6			6			6	
Detector 2 Type		Cl+Ex			Cl+Ex			Cl+Ex			Cl+Ex	
Detector 2 Channel												
Detector 2 Extend (s)		0.0			0.0			0.0			0.0	
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		Perm	NA	
Protected Phases		4			4		1	6			2	

Lanes, Volumes, Timings  
8: US 41 & Florida Blvd

Existing AM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Permitted Phases	4			4			6			2		
Detector Phase	4	4		4	4		1	6		2	2	
Switch Phase												
Minimum Initial (s)	7.0	7.0		7.0	7.0		5.0	15.0		15.0	15.0	
Minimum Split (s)	38.2	38.2		38.2	38.2		11.8	32.1		28.1	28.1	
Total Split (s)	40.0	40.0		40.0	40.0		18.0	80.0		62.0	62.0	
Total Split (%)	33.3%	33.3%		33.3%	33.3%		15.0%	66.7%		51.7%	51.7%	
Maximum Green (s)	32.8	32.8		32.8	32.8		11.2	72.9		54.9	54.9	
Yellow Time (s)	3.4	3.4		3.4	3.4		4.8	4.8		4.8	4.8	
All-Red Time (s)	3.8	3.8		3.8	3.8		2.0	2.3		2.3	2.3	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)	7.2	7.2		7.2	7.2		6.8	7.1		7.1	7.1	
Lead/Lag							Lead			Lag	Lag	
Lead-Lag Optimize?							Yes			Yes	Yes	
Vehicle Extension (s)	3.0	3.0		3.0	3.0		3.0	5.0		5.0	5.0	
Recall Mode	None	None		None	None		None	C-Max		C-Max	C-Max	
Walk Time (s)	7.0	7.0		7.0	7.0			7.0		7.0	7.0	
Flash Dont Walk (s)	24.0	24.0		24.0	24.0			18.0		14.0	14.0	
Pedestrian Calls (#/hr)	0	0		0	0			0		0	0	
Act Effct Green (s)	29.0	29.0		29.0	29.0		77.0	76.7			58.9	
Actuated g/C Ratio	0.24	0.24		0.24	0.24		0.64	0.64			0.49	
v/c Ratio	0.15	0.92		0.02	0.00		0.85	0.30			0.81	
Control Delay	34.8	51.0		33.0	27.0		46.4	11.5			30.1	
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0			0.0	
Total Delay	34.8	51.0		33.0	27.0		46.4	11.5			30.1	
LOS	C	D		C	C		D	B			C	
Approach Delay		49.6			29.0			17.1			30.1	
Approach LOS		D			C			B			C	

Intersection Summary

Area Type: Other  
 Cycle Length: 120  
 Actuated Cycle Length: 120  
 Offset: 109 (91%), Referenced to phase 2:SBTL and 6:NBTL, Start of Green  
 Natural Cycle: 100  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.92  
 Intersection Signal Delay: 28.8  
 Intersection LOS: C  
 Intersection Capacity Utilization 92.6%  
 ICU Level of Service F  
 Analysis Period (min) 15

Splits and Phases: 8: US 41 & Florida Blvd



Intersection												
Int Delay, s/veh	0.6											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		
Traffic Vol, veh/h	7	0	2	0	0	5	0	971	7	23	1908	3
Future Vol, veh/h	7	0	2	0	0	5	0	971	7	23	1908	3
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	100	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	93	93	93	93	93	93	93	93	93	93	93	93
Heavy Vehicles, %	0	0	0	0	0	0	0	3	0	0	2	0
Mvmt Flow	8	0	2	0	0	5	0	1044	8	25	2052	3

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2522	3156	1028	1919	3153	526	2055	0	0	1052	0	0
Stage 1	2104	2104	-	1048	1048	-	-	-	-	-	-	-
Stage 2	418	1052	-	871	2105	-	-	-	-	-	-	-
Critical Hdwy	6.4	6.5	7.1	6.4	6.5	7.1	5.3	-	-	5.3	-	-
Critical Hdwy Stg 1	7.3	5.5	-	7.3	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.7	5.5	-	6.7	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.8	4	3.9	3.8	4	3.9	3.1	-	-	3.1	-	-
Pot Cap-1 Maneuver	31	11	202	73	11	429	120	-	-	375	-	-
Stage 1	33	94	-	187	307	-	-	-	-	-	-	-
Stage 2	538	306	-	286	93	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	29	10	202	69	10	429	120	-	-	375	-	-
Mov Cap-2 Maneuver	29	10	-	69	10	-	-	-	-	-	-	-
Stage 1	33	88	-	187	307	-	-	-	-	-	-	-
Stage 2	531	306	-	264	87	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	138.4		13.5		0		0.2	
HCM LOS	F		B					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	120	-	-	36	429	375	-
HCM Lane V/C Ratio	-	-	-	0.269	0.013	0.066	-
HCM Control Delay (s)	0	-	-	138.4	13.5	15.3	-
HCM Lane LOS	A	-	-	F	B	C	-
HCM 95th %tile Q(veh)	0	-	-	0.9	0	0.2	-

HCM 6th Signalized Intersection Summary  
 10: US 41 & Bayshore Gardens Pkwy

Existing AM  
 09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↗		↖	↗	
Traffic Volume (veh/h)	111	317	271	60	232	131	176	839	3	152	1559	38
Future Volume (veh/h)	111	317	271	60	232	131	176	839	3	152	1559	38
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.99	1.00		0.99	1.00		0.98	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1885	1841	1885	1796	1856	1826	1870	1856	907	1856	1870	1781
Adj Flow Rate, veh/h	118	337	288	64	247	139	187	893	3	162	1659	40
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	1	4	1	7	3	5	2	3	67	3	2	8
Cap, veh/h	139	366	307	80	393	213	210	2454	8	182	2333	56
Arrive On Green	0.08	0.20	0.20	0.05	0.18	0.18	0.12	0.47	0.47	0.10	0.45	0.45
Sat Flow, veh/h	1795	1793	1502	1711	2201	1195	1781	5212	18	1767	5128	124
Grp Volume(v), veh/h	118	328	297	64	196	190	187	579	317	162	1101	598
Grp Sat Flow(s),veh/h/ln	1795	1749	1547	1711	1763	1633	1781	1689	1852	1767	1702	1848
Q Serve(g_s), s	10.4	29.4	30.2	5.9	16.5	17.3	16.6	17.5	17.5	14.5	41.7	41.7
Cycle Q Clear(g_c), s	10.4	29.4	30.2	5.9	16.5	17.3	16.6	17.5	17.5	14.5	41.7	41.7
Prop In Lane	1.00		0.97	1.00		0.73	1.00		0.01	1.00		0.07
Lane Grp Cap(c), veh/h	139	357	316	80	315	291	210	1590	872	182	1548	841
V/C Ratio(X)	0.85	0.92	0.94	0.80	0.62	0.65	0.89	0.36	0.36	0.89	0.71	0.71
Avail Cap(c_a), veh/h	194	364	322	112	315	291	299	1590	872	189	1548	841
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	72.8	62.4	62.7	75.5	60.8	61.1	69.6	27.0	27.0	70.8	35.1	35.1
Incr Delay (d2), s/veh	21.0	27.6	34.1	23.2	3.8	5.1	20.4	0.6	1.2	35.8	2.8	5.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.6	15.9	14.9	3.1	7.7	7.6	8.6	7.1	7.9	8.3	17.4	19.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	93.9	90.0	96.8	98.7	64.5	66.2	90.0	27.7	28.2	106.7	37.9	40.2
LnGrp LOS	F	F	F	F	E	E	F	C	C	F	D	D
Approach Vol, veh/h		743			450			1083			1861	
Approach Delay, s/veh		93.3			70.1			38.6			44.7	
Approach LOS		F			E			D			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	25.9	79.6	19.1	35.4	23.4	82.1	15.0	39.5				
Change Period (Y+Rc), s	* 7.1	6.8	* 6.7	* 6.8	6.9	6.8	7.5	* 6.8				
Max Green Setting (Gmax), s	* 27	61.2	* 17	* 27	17.1	71.2	10.5	* 33				
Max Q Clear Time (g_c+I1), s	18.6	43.7	12.4	19.3	16.5	19.5	7.9	32.2				
Green Ext Time (p_c), s	0.3	14.5	0.1	1.4	0.0	13.6	0.0	0.5				

Intersection Summary

HCM 6th Ctrl Delay	54.6
HCM 6th LOS	D

Notes

- User approved pedestrian interval to be less than phase max green.
- User approved ignoring U-Turning movement.
- \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

HCM 6th Signalized Intersection Summary  
11: US 41 & 60th Ave W

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖↗↘↙			↖↗↘↙		
Traffic Volume (veh/h)	85	74	161	41	109	72	78	984	9	75	1544	36
Future Volume (veh/h)	85	74	161	41	109	72	78	984	9	75	1544	36
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	0.99		0.99	1.00		0.99	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1841	1885	1856	1826	1885	1856	1826	1870	1900	1900	1870	1900
Adj Flow Rate, veh/h	92	80	175	45	118	78	85	1070	10	82	1678	39
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	4	1	3	5	1	3	5	2	0	0	2	0
Cap, veh/h	181	88	194	118	156	103	232	3126	29	363	3073	71
Arrive On Green	0.05	0.17	0.17	0.03	0.15	0.15	0.03	0.60	0.60	0.04	0.80	0.80
Sat Flow, veh/h	1753	522	1143	1739	1052	696	1739	5217	49	1810	5134	119
Grp Volume(v), veh/h	92	0	255	45	0	196	85	698	382	82	1113	604
Grp Sat Flow(s),veh/h/ln	1753	0	1665	1739	0	1748	1739	1702	1861	1810	1702	1849
Q Serve(g_s), s	7.1	0.0	24.0	3.5	0.0	17.2	3.0	16.5	16.6	2.8	18.9	18.9
Cycle Q Clear(g_c), s	7.1	0.0	24.0	3.5	0.0	17.2	3.0	16.5	16.6	2.8	18.9	18.9
Prop In Lane	1.00		0.69	1.00		0.40	1.00		0.03	1.00		0.06
Lane Grp Cap(c), veh/h	181	0	282	118	0	260	232	2040	1115	363	2037	1107
V/C Ratio(X)	0.51	0.00	0.90	0.38	0.00	0.75	0.37	0.34	0.34	0.23	0.55	0.55
Avail Cap(c_a), veh/h	181	0	303	154	0	318	267	2040	1115	401	2037	1107
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.33	1.33	1.33
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	0.77	0.77	0.77
Uniform Delay (d), s/veh	55.2	0.0	65.2	56.8	0.0	65.3	13.1	16.2	16.2	12.4	8.5	8.5
Incr Delay (d2), s/veh	2.3	0.0	27.6	2.0	0.0	7.9	1.0	0.5	0.8	0.2	0.8	1.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.3	0.0	12.4	1.6	0.0	8.3	1.2	6.4	7.1	1.1	5.1	5.7
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	57.5	0.0	92.8	58.9	0.0	73.3	14.1	16.6	17.0	12.6	9.3	10.0
LnGrp LOS	E	A	F	E	A	E	B	B	B	B	A	A
Approach Vol, veh/h		347			241			1165			1799	
Approach Delay, s/veh		83.4			70.6			16.6			9.7	
Approach LOS		F			E			B			A	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	118	102.6	15.0	30.7	11.7	102.7	11.7	34.0				
Change Period (Y+Rc), s	6.8	6.8	6.9	6.9	6.8	6.8	6.9	6.9				
Max Green Setting (Gmax), s	87.2	87.2	8.1	29.1	8.2	87.2	8.1	29.1				
Max Q Clear Time (g_c+1/3), s	20.9	20.9	9.1	19.2	4.8	18.6	5.5	26.0				
Green Ext Time (p_c), s	0.0	39.4	0.0	0.8	0.0	19.0	0.0	0.4				

Intersection Summary

HCM 6th Ctrl Delay	23.3
HCM 6th LOS	C

Notes

- User approved pedestrian interval to be less than phase max green.
- User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary  
12: US 41 & 57th Ave W

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	117	194	68	107	218	167	107	1028	49	134	1441	87
Future Volume (veh/h)	117	194	68	107	218	167	107	1028	49	134	1441	87
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1856	1870	1900	1885	1841	1841	1826	1870	1870	1841	1856	1856
Adj Flow Rate, veh/h	121	200	70	110	225	172	110	1060	51	138	1486	90
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Percent Heavy Veh, %	3	2	0	1	4	4	5	2	2	4	3	3
Cap, veh/h	207	217	76	175	290	245	229	2723	131	324	2703	164
Arrive On Green	0.07	0.16	0.16	0.06	0.16	0.16	0.03	0.37	0.37	0.05	0.55	0.55
Sat Flow, veh/h	1767	1324	463	1795	1841	1554	1739	4991	240	1753	4883	296
Grp Volume(v), veh/h	121	0	270	110	225	172	110	723	388	138	1028	548
Grp Sat Flow(s),veh/h/ln	1767	0	1787	1795	1841	1554	1739	1702	1827	1753	1689	1802
Q Serve(g_s), s	9.1	0.0	23.8	8.1	18.8	16.8	4.4	25.1	25.2	5.5	31.2	31.2
Cycle Q Clear(g_c), s	9.1	0.0	23.8	8.1	18.8	16.8	4.4	25.1	25.2	5.5	31.2	31.2
Prop In Lane	1.00		0.26	1.00		1.00	1.00		0.13	1.00		0.16
Lane Grp Cap(c), veh/h	207	0	293	175	290	245	229	1857	997	324	1870	997
V/C Ratio(X)	0.58	0.00	0.92	0.63	0.77	0.70	0.48	0.39	0.39	0.43	0.55	0.55
Avail Cap(c_a), veh/h	228	0	313	208	322	272	297	1857	997	466	1870	997
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	0.67	0.67	0.67	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	1.00	0.94	0.94	0.94	0.31	0.31	0.31
Uniform Delay (d), s/veh	52.6	0.0	65.9	53.5	64.7	63.8	19.2	31.0	31.0	17.0	22.9	22.9
Incr Delay (d2), s/veh	3.2	0.0	30.6	4.4	10.2	7.0	1.5	0.6	1.1	0.3	0.4	0.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	4.2	0.0	13.3	3.9	9.6	7.1	1.9	11.0	12.0	2.2	12.1	13.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	55.7	0.0	96.5	58.0	74.9	70.8	20.7	31.6	32.1	17.3	23.3	23.6
LnGrp LOS	E	A	F	E	E	E	C	C	C	B	C	C
Approach Vol, veh/h		391			507			1221			1714	
Approach Delay, s/veh		83.9			69.8			30.8			22.9	
Approach LOS		F			E			C			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	33.8	95.9	18.1	32.2	15.1	94.6	17.1	33.2				
Change Period (Y+Rc), s	7.3	7.3	7.0	7.0	7.3	7.3	7.0	7.0				
Max Green Setting (Gmax), s	12.8	77.7	13.0	28.0	20.7	69.7	13.0	28.0				
Max Q Clear Time (g_c+10), s	10.4	33.2	11.1	20.8	7.5	27.2	10.1	25.8				
Green Ext Time (p_c), s	0.1	27.8	0.0	1.0	0.3	17.3	0.1	0.3				

Intersection Summary

HCM 6th Ctrl Delay	37.8
HCM 6th LOS	D

Notes

- User approved pedestrian interval to be less than phase max green.
- User approved ignoring U-Turning movement.



HCM 6th Signalized Intersection Summary  
13: US 41 & 53rd Ave W

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	119	716	170	174	669	127	179	915	127	164	1156	78
Future Volume (veh/h)	119	716	170	174	669	127	179	915	127	164	1156	78
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.99	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1856	1856	1870	1870	1870	1811	1885	1885	1870	1870	1856	1826
Adj Flow Rate, veh/h	127	762	181	185	712	135	190	973	135	174	1230	83
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	3	3	2	2	2	6	1	1	2	2	3	5
Cap, veh/h	217	806	191	213	923	175	199	1540	213	196	1630	110
Arrive On Green	0.06	0.29	0.29	0.09	0.31	0.31	0.04	0.11	0.11	0.04	0.11	0.11
Sat Flow, veh/h	1767	2821	670	1781	2979	565	1795	4567	632	1781	4846	327
Grp Volume(v), veh/h	127	476	467	185	425	422	190	730	378	174	857	456
Grp Sat Flow(s),veh/h/ln	1767	1763	1728	1781	1777	1767	1795	1716	1768	1781	1689	1796
Q Serve(g_s), s	8.1	42.3	42.3	11.6	34.7	34.7	16.9	32.6	32.7	15.6	39.4	39.4
Cycle Q Clear(g_c), s	8.1	42.3	42.3	11.6	34.7	34.7	16.9	32.6	32.7	15.6	39.4	39.4
Prop In Lane	1.00		0.39	1.00		0.32	1.00		0.36	1.00		0.18
Lane Grp Cap(c), veh/h	217	504	494	213	550	547	199	1157	596	196	1136	604
V/C Ratio(X)	0.59	0.95	0.95	0.87	0.77	0.77	0.96	0.63	0.63	0.89	0.75	0.75
Avail Cap(c_a), veh/h	247	517	507	324	643	639	199	1157	596	197	1136	604
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	0.33	0.33	0.33	0.33	0.33	0.33
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	0.90	0.90	0.90	0.96	0.96	0.96
Uniform Delay (d), s/veh	40.2	55.9	55.9	41.5	50.1	50.1	76.7	61.6	61.6	76.1	64.7	64.7
Incr Delay (d2), s/veh	2.8	26.2	26.6	14.6	4.9	5.0	48.3	2.4	4.6	34.4	4.5	8.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	8.7	22.2	21.8	5.9	16.0	15.9	10.9	15.4	16.3	9.4	18.6	20.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	43.0	82.2	82.5	56.2	55.0	55.1	125.0	63.9	66.2	110.6	69.2	72.9
LnGrp LOS	D	F	F	E	D	E	F	E	E	F	E	E
Approach Vol, veh/h	1070			1032			1298			1487		
Approach Delay, s/veh	77.7			55.2			73.5			75.2		
Approach LOS	E			E			E			E		
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	35.0	61.1	17.2	56.7	24.9	61.3	21.1	52.8				
Change Period (Y+Rc), s	7.3	7.3	* 7.1	* 7.1	7.3	7.3	* 7.1	* 7.1				
Max Green Setting (Gmax), s	42.7	* 13	* 58	17.7	42.7	* 24	* 47					
Max Q Clear Time (g_c+1/3), s	41.4	10.1	36.7	17.6	34.7	13.6	44.3					
Green Ext Time (p_c), s	0.0	1.1	0.1	5.1	0.0	5.7	0.3	1.4				

Intersection Summary

HCM 6th Ctrl Delay	71.1
HCM 6th LOS	E

Notes

\* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings  
 14: US 41 & 49th Ave W/49th Dr W

Existing AM  
 09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	15	0	23	10	0	15	31	1122	6	8	1331	21
Future Volume (vph)	15	0	23	10	0	15	31	1122	6	8	1331	21
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	0		0	125		0	110		0
Storage Lanes	1		0	0		0	1		0	1		0
Taper Length (ft)	0			0			0			0		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.91	0.91	1.00	0.91	0.91
Frt		0.850			0.920			0.999			0.998	
Flt Protected	0.950				0.980		0.950			0.950		
Satd. Flow (prot)	1805	1380	0	0	1713	0	1752	5032	0	1805	5021	0
Flt Permitted	0.740				0.857		0.171			0.223		
Satd. Flow (perm)	1406	1380	0	0	1498	0	315	5032	0	424	5021	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		115			26			2			6	
Link Speed (mph)		25			25			45			45	
Link Distance (ft)		171			252			2357			1525	
Travel Time (s)		4.7			6.9			35.7			23.1	
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Heavy Vehicles (%)	0%	0%	17%	0%	0%	0%	3%	3%	0%	0%	3%	10%
Adj. Flow (vph)	16	0	24	11	0	16	33	1194	6	9	1416	22
Shared Lane Traffic (%)												
Lane Group Flow (vph)	16	24	0	0	27	0	33	1200	0	9	1438	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		12			12			12			12	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane								Yes			Yes	
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Number of Detectors	1	2		1	2		1	2		1	2	
Detector Template	Left	Thru		Left	Thru		Left	Thru		Left	Thru	
Leading Detector (ft)	20	100		20	100		20	100		20	100	
Trailing Detector (ft)	0	0		0	0		0	0		0	0	
Detector 1 Position(ft)	0	0		0	0		0	0		0	0	
Detector 1 Size(ft)	20	6		20	6		20	6		20	6	
Detector 1 Type	Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex	
Detector 1 Channel												
Detector 1 Extend (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Queue (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Delay (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 2 Position(ft)		94			94			94			94	
Detector 2 Size(ft)		6			6			6			6	
Detector 2 Type		Cl+Ex			Cl+Ex			Cl+Ex			Cl+Ex	
Detector 2 Channel												
Detector 2 Extend (s)		0.0			0.0			0.0			0.0	
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			4			2			2	

Lanes, Volumes, Timings  
14: US 41 & 49th Ave W/49th Dr W

Existing AM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Permitted Phases	4			4			2			2		
Detector Phase	4	4		4	4		2	2		2	2	
Switch Phase												
Minimum Initial (s)	7.0	7.0		7.0	7.0		15.0	15.0		15.0	15.0	
Minimum Split (s)	39.7	39.7		39.7	39.7		32.8	32.8		32.8	32.8	
Total Split (s)	20.0	20.0		20.0	20.0		140.0	140.0		140.0	140.0	
Total Split (%)	12.5%	12.5%		12.5%	12.5%		87.5%	87.5%		87.5%	87.5%	
Maximum Green (s)	13.3	13.3		13.3	13.3		133.2	133.2		133.2	133.2	
Yellow Time (s)	3.7	3.7		3.7	3.7		4.8	4.8		4.8	4.8	
All-Red Time (s)	3.0	3.0		3.0	3.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	0.0	0.0			0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)	6.7	6.7			6.7		6.8	6.8		6.8	6.8	
Lead/Lag												
Lead-Lag Optimize?												
Vehicle Extension (s)	3.0	3.0		3.0	3.0		5.0	5.0		5.0	5.0	
Recall Mode	None	None		None	None		C-Min	C-Min		C-Min	C-Min	
Walk Time (s)	7.0	7.0		7.0	7.0		7.0	7.0		7.0	7.0	
Flash Dont Walk (s)	26.0	26.0		26.0	26.0		19.0	19.0		19.0	19.0	
Pedestrian Calls (#/hr)	0	0		0	0		0	0		0	0	
Act Effct Green (s)	7.9	7.9			7.9		142.7	142.7		142.7	142.7	
Actuated g/C Ratio	0.05	0.05			0.05		0.89	0.89		0.89	0.89	
v/c Ratio	0.23	0.14			0.28		0.12	0.27		0.02	0.32	
Control Delay	80.7	1.6			31.5		9.2	11.1		2.6	5.1	
Queue Delay	0.0	0.0			0.0		0.0	0.0		0.0	0.0	
Total Delay	80.7	1.6			31.5		9.2	11.1		2.6	5.1	
LOS	F	A			C		A	B		A	A	
Approach Delay		33.3			31.5			11.0			5.1	
Approach LOS		C			C			B			A	

Intersection Summary

Area Type:	Other
Cycle Length:	160
Actuated Cycle Length:	160
Offset:	118 (74%), Referenced to phase 2:NBSB, Start of Green
Natural Cycle:	75
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.32
Intersection Signal Delay:	8.4
Intersection LOS:	A
Intersection Capacity Utilization:	45.6%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 14: US 41 & 49th Ave W/49th Dr W



HCM 6th Signalized Intersection Summary  
 15: US 41 & 46th Ave Dr W/Orlando Ave

Existing AM  
 09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	0	0	3	87	0	114	3	1124	69	68	1260	1
Future Volume (veh/h)	0	0	3	87	0	114	3	1124	69	68	1260	1
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.99	1.00		1.00	1.00		0.98
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1900	1900	418	1856	1900	1856	418	1856	1900	1885	1856	1900
Adj Flow Rate, veh/h	0	0	3	92	0	120	3	1183	73	72	1326	1
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Percent Heavy Veh, %	0	0	100	3	0	3	100	3	0	1	3	0
Cap, veh/h	45	0	145	169	0	144	115	3675	227	346	4318	3
Arrive On Green	0.00	0.00	0.09	0.09	0.00	0.09	0.25	0.25	0.25	0.06	1.00	1.00
Sat Flow, veh/h	1292	0	1610	1403	0	1599	92	4877	301	1795	5228	4
Grp Volume(v), veh/h	0	0	3	92	0	120	3	819	437	72	857	470
Grp Sat Flow(s),veh/h/ln	1292	0	1610	1403	0	1599	92	1689	1801	1795	1689	1855
Q Serve(g_s), s	0.0	0.0	0.3	10.2	0.0	11.8	3.9	31.7	31.7	1.4	0.0	0.0
Cycle Q Clear(g_c), s	0.0	0.0	0.3	10.5	0.0	11.8	3.9	31.7	31.7	1.4	0.0	0.0
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.17	1.00		0.00
Lane Grp Cap(c), veh/h	45	0	145	169	0	144	115	2544	1357	346	2789	1532
V/C Ratio(X)	0.00	0.00	0.02	0.55	0.00	0.84	0.03	0.32	0.32	0.21	0.31	0.31
Avail Cap(c_a), veh/h	77	0	184	203	0	183	115	2544	1357	440	2789	1532
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	0.33	0.33	0.33	2.00	2.00	2.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	0.00	1.00	0.97	0.97	0.97	1.00	1.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	66.4	71.2	0.0	71.7	16.3	26.7	26.7	7.4	0.0	0.0
Incr Delay (d2), s/veh	0.0	0.0	0.1	2.7	0.0	22.5	0.4	0.3	0.6	0.3	0.3	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	0.1	3.9	0.0	5.8	0.1	14.5	15.6	0.5	0.1	0.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	0.0	0.0	66.5	73.9	0.0	94.2	16.7	27.1	27.3	7.7	0.3	0.5
LnGrp LOS	A	A	E	E	A	F	B	C	C	A	A	A
Approach Vol, veh/h	3			212			1259			1399		
Approach Delay, s/veh	66.5			85.4			27.1			0.7		
Approach LOS	E			F			C			A		
Timer - Assigned Phs	2		4		5		6		8			
Phs Duration (G+Y+Rc), s	138.9		21.1		11.6		127.3		21.1			
Change Period (Y+Rc), s	6.8		* 6.7		6.8		6.8		* 6.7			
Max Green Setting (Gmax), s	128.2		* 18		13.2		108.2		* 18			
Max Q Clear Time (g_c+I1), s	2.0		13.8		3.4		33.7		2.3			
Green Ext Time (p_c), s	30.1		0.4		0.1		25.6		0.0			

Intersection Summary

HCM 6th Ctrl Delay	18.6
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Intersection												
Int Delay, s/veh	0.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕		↕	↕	↕
Traffic Vol, veh/h	4	0	3	0	0	26	2	1195	9	40	1348	1
Future Vol, veh/h	4	0	3	0	0	26	2	1195	9	40	1348	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	100	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	95	95	95	95	95	95	95	95	95	95	95	95
Heavy Vehicles, %	25	0	0	0	0	0	50	2	0	3	3	0
Mvmt Flow	4	0	3	0	0	27	2	1258	9	42	1419	1

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2137	2775	710	1919	2771	634	1420	0	0	1267	0	0
Stage 1	1504	1504	-	1267	1267	-	-	-	-	-	-	-
Stage 2	633	1271	-	652	1504	-	-	-	-	-	-	-
Critical Hdwy	7.45	6.5	7.1	6.95	6.5	6.9	6.3	-	-	4.16	-	-
Critical Hdwy Stg 1	7.8	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	7	5.5	-	6.7	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.9	4	3.9	3.65	4	3.3	3.6	-	-	2.23	-	-
Pot Cap-1 Maneuver	28	19	326	55	20	427	156	-	-	539	-	-
Stage 1	72	186	-	178	242	-	-	-	-	-	-	-
Stage 2	372	241	-	401	186	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	24	17	326	51	18	427	156	-	-	539	-	-
Mov Cap-2 Maneuver	24	17	-	51	18	-	-	-	-	-	-	-
Stage 1	71	171	-	176	239	-	-	-	-	-	-	-
Stage 2	344	238	-	366	171	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	114.3		14		0		0.4	
HCM LOS	F		B					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	156	-	-	40	427	539	-
HCM Lane V/C Ratio	0.013	-	-	0.184	0.064	0.078	-
HCM Control Delay (s)	28.4	-	-	114.3	14	12.2	-
HCM Lane LOS	D	-	-	F	B	B	-
HCM 95th %tile Q(veh)	0	-	-	0.6	0.2	0.3	-

HCM 6th Signalized Intersection Summary  
17: US 41 & SR 684 Cortez Rd

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↔↔	↑↑↑	↗	↔↔	↑↑↑	↗	↔↔	↑↑	↗	↔↔	↑↑	↗
Traffic Volume (veh/h)	171	1077	428	433	1013	45	488	392	366	106	508	153
Future Volume (veh/h)	171	1077	428	433	1013	45	488	392	366	106	508	153
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.99	1.00		1.00	1.00		1.00	1.00		0.99
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1885	1856	1870	1841	1856	1900	1870	1870	1841	1870	1870	1856
Adj Flow Rate, veh/h	178	1122	446	451	1055	47	508	408	381	110	529	159
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	1	3	2	4	3	0	2	2	4	2	2	3
Cap, veh/h	226	1617	503	498	2039	646	525	1130	495	156	746	328
Arrive On Green	0.06	0.32	0.32	0.15	0.40	0.40	0.25	0.53	0.53	0.01	0.07	0.07
Sat Flow, veh/h	3483	5066	1576	3401	5066	1604	3456	3554	1556	3456	3554	1561
Grp Volume(v), veh/h	178	1122	446	451	1055	47	508	408	381	110	529	159
Grp Sat Flow(s),veh/h/ln	1742	1689	1576	1700	1689	1604	1728	1777	1556	1728	1777	1561
Q Serve(g_s), s	8.1	31.0	43.0	20.9	25.1	2.9	23.3	10.7	31.1	5.1	23.3	15.7
Cycle Q Clear(g_c), s	8.1	31.0	43.0	20.9	25.1	2.9	23.3	10.7	31.1	5.1	23.3	15.7
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	226	1617	503	498	2039	646	525	1130	495	156	746	328
V/C Ratio(X)	0.79	0.69	0.89	0.91	0.52	0.07	0.97	0.36	0.77	0.71	0.71	0.48
Avail Cap(c_a), veh/h	401	1617	503	555	2039	646	525	1130	495	378	746	328
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.67	1.67	1.67	0.33	0.33	0.33
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.96	0.96	0.96
Uniform Delay (d), s/veh	73.7	47.6	51.7	67.2	36.1	29.4	59.3	28.1	32.9	77.8	69.7	66.1
Incr Delay (d2), s/veh	6.0	2.5	20.1	17.3	0.9	0.2	31.1	0.9	11.0	5.5	5.4	4.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.8	13.4	19.5	10.3	10.6	1.2	11.5	4.2	11.3	2.4	11.8	7.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	79.7	50.1	71.8	84.5	37.0	29.6	90.5	29.0	43.9	83.3	75.1	71.0
LnGrp LOS	E	D	E	F	D	C	F	C	D	F	E	E
Approach Vol, veh/h		1746			1553			1297			798	
Approach Delay, s/veh		58.7			50.6			57.4			75.4	
Approach LOS		E			D			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.0	71.0	14.7	57.3	30.3	57.7	32.0	40.0				
Change Period (Y+Rc), s	6.6	6.6	7.5	6.4	6.9	* 6.6	* 7.7	6.4				
Max Green Setting (Gmax), s	18.4	56.4	17.5	40.6	26.1	* 49	* 24	33.6				
Max Q Clear Time (g_c+I1), s	10.1	27.1	7.1	33.1	22.9	45.0	25.3	25.3				
Green Ext Time (p_c), s	0.3	15.3	0.2	2.4	0.6	3.2	0.0	2.5				

Intersection Summary

HCM 6th Ctrl Delay	58.5
HCM 6th LOS	E

Notes

- User approved pedestrian interval to be less than phase max green.
- User approved ignoring U-Turning movement.
- \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

HCM 6th Signalized Intersection Summary  
18: US 41 & 39th Ave W

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	61	0	37	21	1	27	21	595	17	15	747	55
Future Volume (veh/h)	61	0	37	21	1	27	21	595	17	15	747	55
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	0.99		0.99	0.99		0.99	1.00		0.98	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1826	1900	1826	1900	1900	1841	1826	1885	1900	1900	1870	1841
Adj Flow Rate, veh/h	65	0	39	22	1	29	22	633	18	16	795	59
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	5	0	5	0	0	4	5	1	0	0	2	4
Cap, veh/h	144	0	121	65	14	60	528	2784	79	584	2615	194
Arrive On Green	0.08	0.00	0.08	0.08	0.08	0.08	0.01	0.26	0.26	0.02	0.78	0.78
Sat Flow, veh/h	1335	0	1597	441	184	788	1739	3554	101	1810	3353	249
Grp Volume(v), veh/h	65	0	39	52	0	0	22	319	332	16	421	433
Grp Sat Flow(s),veh/h/ln	1335	0	1597	1414	0	0	1739	1791	1864	1810	1777	1825
Q Serve(g_s), s	1.8	0.0	3.7	2.3	0.0	0.0	0.4	22.4	22.5	0.3	11.0	11.0
Cycle Q Clear(g_c), s	7.9	0.0	3.7	6.0	0.0	0.0	0.4	22.4	22.5	0.3	11.0	11.0
Prop In Lane	1.00		1.00	0.42		0.56	1.00		0.05	1.00		0.14
Lane Grp Cap(c), veh/h	144	0	121	139	0	0	528	1403	1460	584	1385	1423
V/C Ratio(X)	0.45	0.00	0.32	0.37	0.00	0.00	0.04	0.23	0.23	0.03	0.30	0.30
Avail Cap(c_a), veh/h	280	0	284	292	0	0	638	1403	1460	705	1385	1423
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	0.33	0.33	0.33	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	0.00	0.91	0.91	0.91	1.00	1.00	1.00
Uniform Delay (d), s/veh	72.0	0.0	70.0	71.0	0.0	0.0	3.7	21.2	21.2	4.9	5.1	5.1
Incr Delay (d2), s/veh	2.2	0.0	1.5	1.7	0.0	0.0	0.0	0.3	0.3	0.0	0.6	0.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.7	0.0	1.6	2.1	0.0	0.0	0.1	10.9	11.4	0.1	3.8	3.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	74.2	0.0	71.6	72.6	0.0	0.0	3.7	21.5	21.5	4.9	5.7	5.6
LnGrp LOS	E	A	E	E	A	A	A	C	C	A	A	A
Approach Vol, veh/h	104			52			673			870		
Approach Delay, s/veh	73.2			72.6			20.9			5.6		
Approach LOS	E			E			C			A		
Timer - Assigned Phs	1	2	4		5	6	8					
Phs Duration (G+Y+Rc), s	9.2	132.0		18.7	9.8	131.5		18.7				
Change Period (Y+Rc), s	6.7	* 6.7	* 6.6		* 6.7	* 6.7	* 6.6					
Max Green Setting (Gmax), s	3	* 98	* 28		* 13	* 98	* 28					
Max Q Clear Time (g_c+1), s	3	24.5	9.9		2.4	13.0	8.0					
Green Ext Time (p_c), s	0.0	9.3	0.3		0.0	14.1	0.2					

Intersection Summary

HCM 6th Ctrl Delay	17.9
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- User approved ignoring U-Turning movement.
- \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Intersection												
Int Delay, s/veh	1.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕		↕	↕	
Traffic Vol, veh/h	20	0	25	14	0	14	12	690	8	7	739	19
Future Vol, veh/h	20	0	25	14	0	14	12	690	8	7	739	19
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	70	-	-	55	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	93	93	93	93	93	93	93	93	93	93	93	93
Heavy Vehicles, %	0	0	0	7	0	14	0	3	0	0	4	0
Mvmt Flow	22	0	27	15	0	15	13	742	9	8	795	20

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	1218	1598	408	1187	1604	376	815	0	0	751	0	0
Stage 1	821	821	-	773	773	-	-	-	-	-	-	-
Stage 2	397	777	-	414	831	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.64	6.5	7.18	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.64	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.64	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.57	4	3.44	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	139	107	598	138	107	589	821	-	-	868	-	-
Stage 1	339	391	-	347	412	-	-	-	-	-	-	-
Stage 2	605	410	-	573	387	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	133	104	598	129	104	589	821	-	-	868	-	-
Mov Cap-2 Maneuver	133	104	-	129	104	-	-	-	-	-	-	-
Stage 1	334	387	-	341	405	-	-	-	-	-	-	-
Stage 2	580	403	-	542	384	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	24.4		24.8		0.2		0.1	
HCM LOS	C		C					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	821	-	-	234	212	868	-
HCM Lane V/C Ratio	0.016	-	-	0.207	0.142	0.009	-
HCM Control Delay (s)	9.5	-	-	24.4	24.8	9.2	-
HCM Lane LOS	A	-	-	C	C	A	-
HCM 95th %tile Q(veh)	0	-	-	0.8	0.5	0	-



Intersection												
Int Delay, s/veh	0.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕		↕	↕	
Traffic Vol, veh/h	1	1	1	6	0	16	8	712	2	12	761	2
Future Vol, veh/h	1	1	1	6	0	16	8	712	2	12	761	2
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	55	-	-	50	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	88	88	88	88	88	88	88	88	88	88	88	88
Heavy Vehicles, %	0	0	0	0	0	0	13	3	0	0	3	0
Mvmt Flow	1	1	1	7	0	18	9	809	2	14	865	2

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1317	1723	434	1289	1723	406	867	0	0	811	0	0
Stage 1	894	894	-	828	828	-	-	-	-	-	-	-
Stage 2	423	829	-	461	895	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.36	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.33	-	-	2.2	-	-
Pot Cap-1 Maneuver	117	90	576	123	90	600	706	-	-	824	-	-
Stage 1	306	362	-	336	389	-	-	-	-	-	-	-
Stage 2	585	388	-	555	362	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	111	87	576	119	87	600	706	-	-	824	-	-
Mov Cap-2 Maneuver	111	87	-	119	87	-	-	-	-	-	-	-
Stage 1	302	356	-	332	384	-	-	-	-	-	-	-
Stage 2	560	383	-	543	356	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	32.4		18.8		0.1		0.1	
HCM LOS	D		C					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	706	-	-	135	285	824	-
HCM Lane V/C Ratio	0.013	-	-	0.025	0.088	0.017	-
HCM Control Delay (s)	10.2	-	-	32.4	18.8	9.4	-
HCM Lane LOS	B	-	-	D	C	A	-
HCM 95th %tile Q(veh)	0	-	-	0.1	0.3	0.1	-

Intersection						
Int Delay, s/veh	0.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↘↗		↑↓		↘	↑↑
Traffic Vol, veh/h	3	10	717	7	5	779
Future Vol, veh/h	3	10	717	7	5	779
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	55	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	0	10	1	14	0	1
Mvmt Flow	3	11	797	8	6	866

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1246	403	0	0	805
Stage 1	801	-	-	-	-
Stage 2	445	-	-	-	-
Critical Hdwy	6.8	7.1	-	-	4.1
Critical Hdwy Stg 1	5.8	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-
Follow-up Hdwy	3.5	3.4	-	-	2.2
Pot Cap-1 Maneuver	169	575	-	-	828
Stage 1	407	-	-	-	-
Stage 2	619	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	168	575	-	-	828
Mov Cap-2 Maneuver	168	-	-	-	-
Stage 1	407	-	-	-	-
Stage 2	615	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	15.2	0	0.1
HCM LOS	C		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	369	828
HCM Lane V/C Ratio	-	-	0.039	0.007
HCM Control Delay (s)	-	-	15.2	9.4
HCM Lane LOS	-	-	C	A
HCM 95th %tile Q(veh)	-	-	0.1	0

HCM 6th Signalized Intersection Summary  
22: US 41 & 30th Ave W

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	86	276	104	158	257	60	73	508	150	35	529	94
Future Volume (veh/h)	86	276	104	158	257	60	73	508	150	35	529	94
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.99	1.00		0.99	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1885	1885	1841	1885	1826	1856	1885	1856	1693	1900	1885
Adj Flow Rate, veh/h	96	307	116	176	286	67	81	564	167	39	588	104
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	0	1	1	4	1	5	3	1	3	14	0	1
Cap, veh/h	233	308	257	238	383	312	381	1303	385	336	1413	249
Arrive On Green	0.06	0.16	0.16	0.10	0.20	0.20	0.04	0.48	0.48	0.03	0.46	0.46
Sat Flow, veh/h	1810	1885	1574	1753	1885	1534	1767	2726	805	1612	3065	541
Grp Volume(v), veh/h	96	307	116	176	286	67	81	370	361	39	346	346
Grp Sat Flow(s),veh/h/ln	1810	1885	1574	1753	1885	1534	1767	1791	1739	1612	1805	1801
Q Serve(g_s), s	5.0	18.7	7.7	9.4	16.4	4.2	2.8	15.6	15.7	1.4	14.7	14.8
Cycle Q Clear(g_c), s	5.0	18.7	7.7	9.4	16.4	4.2	2.8	15.6	15.7	1.4	14.7	14.8
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.46	1.00		0.30
Lane Grp Cap(c), veh/h	233	308	257	238	383	312	381	856	831	336	832	830
V/C Ratio(X)	0.41	1.00	0.45	0.74	0.75	0.21	0.21	0.43	0.43	0.12	0.42	0.42
Avail Cap(c_a), veh/h	343	308	257	275	383	312	504	856	831	476	832	830
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	37.4	48.1	43.4	35.9	43.0	38.2	16.1	19.7	19.8	16.1	20.7	20.7
Incr Delay (d2), s/veh	1.2	50.2	1.2	8.7	7.8	0.3	0.3	1.6	1.7	0.2	1.5	1.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.3	13.0	3.1	4.6	8.4	1.6	1.1	6.6	6.4	0.5	6.3	6.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	38.5	98.2	44.7	44.6	50.8	38.5	16.3	21.3	21.4	16.2	22.2	22.2
LnGrp LOS	D	F	D	D	D	D	B	C	C	B	C	C
Approach Vol, veh/h		519			529			812			731	
Approach Delay, s/veh		75.2			47.2			20.9			21.9	
Approach LOS		E			D			C			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	12.0	60.4	13.0	29.6	10.1	62.4	17.6	25.0				
Change Period (Y+Rc), s	7.4	7.4	* 6.2	* 6.2	6.5	7.4	* 6.1	* 6.2				
Max Green Setting (Gmax), s	12.6	42.6	* 14	* 19	13.5	42.6	* 14	* 19				
Max Q Clear Time (g_c+I1), s	4.8	16.8	7.0	18.4	3.4	17.7	11.4	20.7				
Green Ext Time (p_c), s	0.1	4.2	0.1	0.1	0.0	4.5	0.1	0.0				

Intersection Summary

HCM 6th Ctrl Delay	37.4
HCM 6th LOS	D

Notes

User approved pedestrian interval to be less than phase max green.  
 User approved ignoring U-Turning movement.  
 \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Intersection												
Int Delay, s/veh	2.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕		↕	↕	
Traffic Vol, veh/h	4	1	1	73	1	9	5	564	0	5	583	2
Future Vol, veh/h	4	1	1	73	1	9	5	564	0	5	583	2
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	1	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	89	89	89	89	89	89	89	89	89	89	89	89
Heavy Vehicles, %	0	0	0	0	0	11	0	3	0	20	3	0
Mvmt Flow	4	1	1	82	1	10	6	634	0	6	655	2

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	998	1314	329	986	1315	317	657	0	0	634	0	0
Stage 1	668	668	-	646	646	-	-	-	-	-	-	-
Stage 2	330	646	-	340	669	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	7.12	4.1	-	-	4.5	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.41	2.2	-	-	2.4	-	-
Pot Cap-1 Maneuver	201	160	673	205	159	653	940	-	-	833	-	-
Stage 1	419	459	-	431	470	-	-	-	-	-	-	-
Stage 2	663	470	-	654	459	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	194	157	673	201	156	653	940	-	-	833	-	-
Mov Cap-2 Maneuver	194	157	-	201	156	-	-	-	-	-	-	-
Stage 1	415	456	-	427	465	-	-	-	-	-	-	-
Stage 2	645	465	-	647	456	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	22.6		33.8		0.1		0.1	
HCM LOS	C		D					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	940	-	-	211	216	833	-
HCM Lane V/C Ratio	0.006	-	-	0.032	0.432	0.007	-
HCM Control Delay (s)	8.9	0	-	22.6	33.8	9.4	-
HCM Lane LOS	A	A	-	C	D	A	-
HCM 95th %tile Q(veh)	0	-	-	0.1	2	0	-

Intersection												
Int Delay, s/veh	1.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	5	6	11	25	1	23	5	586	24	19	579	2
Future Vol, veh/h	5	6	11	25	1	23	5	586	24	19	579	2
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	0	0	0	0	0	0	0	2	0	0	1	0
Mvmt Flow	5	6	12	27	1	24	5	623	26	20	616	2

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	979	1316	309	997	1304	325	618	0	0	649	0	0
Stage 1	657	657	-	646	646	-	-	-	-	-	-	-
Stage 2	322	659	-	351	658	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	207	159	693	201	162	677	972	-	-	947	-	-
Stage 1	425	465	-	431	470	-	-	-	-	-	-	-
Stage 2	670	464	-	644	464	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	193	153	693	186	156	677	972	-	-	947	-	-
Mov Cap-2 Maneuver	193	153	-	186	156	-	-	-	-	-	-	-
Stage 1	422	450	-	428	466	-	-	-	-	-	-	-
Stage 2	639	460	-	604	449	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	19.5		20.8		0.1		0.4	
HCM LOS	C		C					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	972	-	-	272	280	947	-
HCM Lane V/C Ratio	0.005	-	-	0.086	0.186	0.021	-
HCM Control Delay (s)	8.7	0	-	19.5	20.8	8.9	0.1
HCM Lane LOS	A	A	-	C	C	A	A
HCM 95th %tile Q(veh)	0	-	-	0.3	0.7	0.1	-

Lanes, Volumes, Timings  
26: US 41 & 21st Ave W

Existing AM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Volume (vph)	41	48	38	12	55	11	29	562	17	2	578	25
Future Volume (vph)	41	48	38	12	55	11	29	562	17	2	578	25
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	0.95	0.95	0.95	0.95
Fr <sub>t</sub>		0.960			0.981			0.996			0.994	
Fl <sub>t</sub> Protected		0.984			0.992			0.998				
Satd. Flow (prot)	0	1784	0	0	1755	0	0	3550	0	0	3554	0
Fl <sub>t</sub> Permitted		0.984			0.992			0.898			0.954	
Satd. Flow (perm)	0	1784	0	0	1755	0	0	3195	0	0	3391	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		21			8			4			5	
Link Speed (mph)		25			25			35			35	
Link Distance (ft)		252			255			1316			950	
Travel Time (s)		6.9			7.0			25.6			18.5	
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Heavy Vehicles (%)	0%	0%	2%	8%	4%	9%	3%	1%	0%	0%	1%	0%
Adj. Flow (vph)	44	51	40	13	59	12	31	598	18	2	615	27
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	135	0	0	84	0	0	647	0	0	644	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		0			0			0			0	
Link Offset(ft)		-15			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Number of Detectors	1	2		1	2		1	2		1	2	
Detector Template	Left	Thru		Left	Thru		Left	Thru		Left	Thru	
Leading Detector (ft)	20	100		20	100		20	100		20	100	
Trailing Detector (ft)	0	0		0	0		0	0		0	0	
Detector 1 Position(ft)	0	0		0	0		0	0		0	0	
Detector 1 Size(ft)	20	6		20	6		20	6		20	6	
Detector 1 Type	Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex	
Detector 1 Channel												
Detector 1 Extend (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Queue (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Delay (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 2 Position(ft)		94			94			94			94	
Detector 2 Size(ft)		6			6			6			6	
Detector 2 Type		Cl+Ex			Cl+Ex			Cl+Ex			Cl+Ex	
Detector 2 Channel												
Detector 2 Extend (s)		0.0			0.0			0.0			0.0	
Turn Type	Split	NA		Split	NA		pm+pt	NA		Perm	NA	
Protected Phases	4	4		8	8		1	6			2	
Permitted Phases							6			2		
Detector Phase	4	4		8	8		1	6		2	2	
Switch Phase												

Lanes, Volumes, Timings  
26: US 41 & 21st Ave W

Existing AM  
09/06/2023

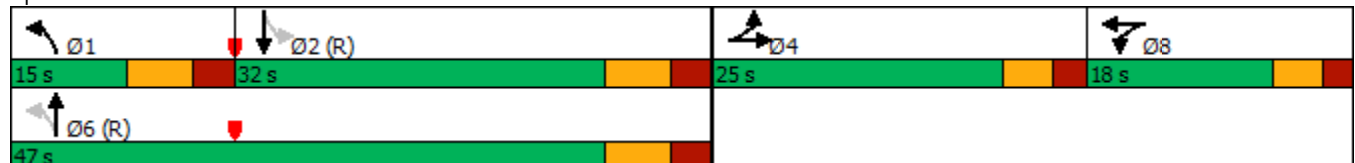


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Minimum Initial (s)	7.0	7.0		7.0	7.0		5.0	12.0		12.0	12.0	
Minimum Split (s)	24.6	24.6		25.5	25.5		12.2	26.2		25.2	25.2	
Total Split (s)	25.0	25.0		18.0	18.0		15.0	47.0		32.0	32.0	
Total Split (%)	27.8%	27.8%		20.0%	20.0%		16.7%	52.2%		35.6%	35.6%	
Maximum Green (s)	19.4	19.4		12.5	12.5		7.8	39.8		24.8	24.8	
Yellow Time (s)	3.4	3.4		3.4	3.4		4.4	4.4		4.4	4.4	
All-Red Time (s)	2.2	2.2		2.1	2.1		2.8	2.8		2.8	2.8	
Lost Time Adjust (s)		0.0			0.0			0.0			0.0	
Total Lost Time (s)		5.6			5.5			7.2			7.2	
Lead/Lag	Lead	Lead		Lag	Lag		Lead			Lag	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes			Yes	Yes	
Vehicle Extension (s)	3.0	3.0		3.0	3.0		3.0	5.0		5.0	5.0	
Recall Mode	None	None		None	None		None	C-Max		C-Max	C-Max	
Walk Time (s)	7.0	7.0		7.0	7.0			7.0		7.0	7.0	
Flash Dont Walk (s)	12.0	12.0		13.0	13.0			12.0		10.0	10.0	
Pedestrian Calls (#/hr)	0	0		0	0			0		0	0	
Act Effect Green (s)		11.2			9.5			53.5			53.5	
Actuated g/C Ratio		0.12			0.11			0.59			0.59	
v/c Ratio		0.56			0.44			0.34			0.32	
Control Delay		39.5			40.7			11.6			18.5	
Queue Delay		0.0			0.0			0.0			0.0	
Total Delay		39.5			40.7			11.6			18.5	
LOS		D			D			B			B	
Approach Delay		39.5			40.7			11.6			18.5	
Approach LOS		D			D			B			B	

Intersection Summary

Area Type: Other  
 Cycle Length: 90  
 Actuated Cycle Length: 90  
 Offset: 59 (66%), Referenced to phase 2:SBTL and 6:NBTL, Start of Green  
 Natural Cycle: 90  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.56  
 Intersection Signal Delay: 18.6  
 Intersection LOS: B  
 Intersection Capacity Utilization 61.6%  
 ICU Level of Service B  
 Analysis Period (min) 15

Splits and Phases: 26: US 41 & 21st Ave W



Intersection						
Int Delay, s/veh	0					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	Y		↑↑			↑↑
Traffic Vol, veh/h	2	1	642	4	2	615
Future Vol, veh/h	2	1	642	4	2	615
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	0	0	1	0	0	1
Mvmt Flow	2	1	713	4	2	683

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1061	359	0	0	717
Stage 1	715	-	-	-	-
Stage 2	346	-	-	-	-
Critical Hdwy	6.8	6.9	-	-	4.1
Critical Hdwy Stg 1	5.8	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-
Follow-up Hdwy	3.5	3.3	-	-	2.2
Pot Cap-1 Maneuver	222	643	-	-	893
Stage 1	451	-	-	-	-
Stage 2	694	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	221	643	-	-	893
Mov Cap-2 Maneuver	221	-	-	-	-
Stage 1	451	-	-	-	-
Stage 2	691	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	17.9	0	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	283	893
HCM Lane V/C Ratio	-	-	0.012	0.002
HCM Control Delay (s)	-	-	17.9	9
HCM Lane LOS	-	-	C	A
HCM 95th %tile Q(veh)	-	-	0	0



Intersection						
Int Delay, s/veh	0.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		
Traffic Vol, veh/h	1	33	12	631	582	4
Future Vol, veh/h	1	33	12	631	582	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	89	89	89	89	89	89
Heavy Vehicles, %	0	0	0	1	1	0
Mvmt Flow	1	37	13	709	654	4

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1037	329	658	0	-	0
Stage 1	656	-	-	-	-	-
Stage 2	381	-	-	-	-	-
Critical Hdwy	6.8	6.9	4.1	-	-	-
Critical Hdwy Stg 1	5.8	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.5	3.3	2.2	-	-	-
Pot Cap-1 Maneuver	230	673	939	-	-	-
Stage 1	483	-	-	-	-	-
Stage 2	666	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	225	673	939	-	-	-
Mov Cap-2 Maneuver	225	-	-	-	-	-
Stage 1	472	-	-	-	-	-
Stage 2	666	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	11	0.3	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	939	-	636	-	-
HCM Lane V/C Ratio	0.014	-	0.06	-	-
HCM Control Delay (s)	8.9	0.1	11	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0	-	0.2	-	-

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		
Traffic Vol, veh/h	0	3	1	631	579	4
Future Vol, veh/h	0	3	1	631	579	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	0	0	0	1	1	0
Mvmt Flow	0	3	1	686	629	4

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	976	317	633	0	-	0
Stage 1	631	-	-	-	-	-
Stage 2	345	-	-	-	-	-
Critical Hdwy	6.8	6.9	4.1	-	-	-
Critical Hdwy Stg 1	5.8	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.5	3.3	2.2	-	-	-
Pot Cap-1 Maneuver	252	685	960	-	-	-
Stage 1	498	-	-	-	-	-
Stage 2	694	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	251	685	960	-	-	-
Mov Cap-2 Maneuver	251	-	-	-	-	-
Stage 1	497	-	-	-	-	-
Stage 2	694	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	10.3	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	960	-	685	-	-
HCM Lane V/C Ratio	0.001	-	0.005	-	-
HCM Control Delay (s)	8.8	0	10.3	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

Lanes, Volumes, Timings  
30: US 41 & 17th Ave W

Existing AM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	58	235	37	15	213	15	49	561	26	15	535	67
Future Volume (vph)	58	235	37	15	213	15	49	561	26	15	535	67
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	45		0	55		0	0		0	0		0
Storage Lanes	1		0	1		0	0		0	0		0
Taper Length (ft)	40			60			0			0		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	0.95	0.95	0.95	0.95
Frt		0.980			0.990			0.994			0.984	
Flt Protected	0.950			0.950				0.996			0.999	
Satd. Flow (prot)	1805	1862	0	1805	1881	0	0	3537	0	0	3488	0
Flt Permitted	0.434			0.339				0.844			0.930	
Satd. Flow (perm)	825	1862	0	644	1881	0	0	2998	0	0	3247	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		9			4			8			25	
Link Speed (mph)		30			30			35			35	
Link Distance (ft)		252			242			152			1665	
Travel Time (s)		5.7			5.5			3.0			32.4	
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	0%	0%	0%	0%	0%	0%	2%	1%	0%	0%	2%	0%
Adj. Flow (vph)	64	261	41	17	237	17	54	623	29	17	594	74
Shared Lane Traffic (%)												
Lane Group Flow (vph)	64	302	0	17	254	0	0	706	0	0	685	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		12			12			0			0	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Number of Detectors	1	2		1	2		1	2		1	2	
Detector Template	Left	Thru		Left	Thru		Left	Thru		Left	Thru	
Leading Detector (ft)	20	100		20	100		20	100		20	100	
Trailing Detector (ft)	0	0		0	0		0	0		0	0	
Detector 1 Position(ft)	0	0		0	0		0	0		0	0	
Detector 1 Size(ft)	20	6		20	6		20	6		20	6	
Detector 1 Type	Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex	
Detector 1 Channel												
Detector 1 Extend (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Queue (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Delay (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 2 Position(ft)		94			94			94			94	
Detector 2 Size(ft)		6			6			6			6	
Detector 2 Type		Cl+Ex			Cl+Ex			Cl+Ex			Cl+Ex	
Detector 2 Channel												
Detector 2 Extend (s)		0.0			0.0			0.0			0.0	
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			4			2			2	

Lanes, Volumes, Timings  
30: US 41 & 17th Ave W

Existing AM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Permitted Phases	4			4			2			2		
Detector Phase	4	4		4	4		2	2		2	2	
Switch Phase												
Minimum Initial (s)	7.0	7.0		7.0	7.0		12.0	12.0		12.0	12.0	
Minimum Split (s)	26.0	26.0		26.0	26.0		25.0	25.0		25.0	25.0	
Total Split (s)	30.0	30.0		30.0	30.0		60.0	60.0		60.0	60.0	
Total Split (%)	33.3%	33.3%		33.3%	33.3%		66.7%	66.7%		66.7%	66.7%	
Maximum Green (s)	24.0	24.0		24.0	24.0		53.0	53.0		53.0	53.0	
Yellow Time (s)	3.7	3.7		3.7	3.7		4.0	4.0		4.0	4.0	
All-Red Time (s)	2.3	2.3		2.3	2.3		3.0	3.0		3.0	3.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0			0.0			0.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0			7.0			7.0	
Lead/Lag												
Lead-Lag Optimize?												
Vehicle Extension (s)	3.0	3.0		3.0	3.0		5.0	5.0		5.0	5.0	
Recall Mode	None	None		None	None		C-Max	C-Max		C-Max	C-Max	
Walk Time (s)	7.0	7.0		7.0	7.0		7.0	7.0		7.0	7.0	
Flash Dont Walk (s)	13.0	13.0		13.0	13.0		10.0	10.0		10.0	10.0	
Pedestrian Calls (#/hr)	0	0		0	0		0	0		0	0	
Act Effct Green (s)	19.8	19.8		19.8	19.8			57.2			57.2	
Actuated g/C Ratio	0.22	0.22		0.22	0.22			0.64			0.64	
v/c Ratio	0.35	0.72		0.12	0.61			0.37			0.33	
Control Delay	33.8	41.4		27.9	36.9			7.7			9.9	
Queue Delay	0.0	0.0		0.0	0.0			0.0			0.0	
Total Delay	33.8	41.4		27.9	36.9			7.7			9.9	
LOS	C	D		C	D			A			A	
Approach Delay		40.0			36.3			7.7			9.9	
Approach LOS		D			D			A			A	

Intersection Summary

Area Type:	Other
Cycle Length:	90
Actuated Cycle Length:	90
Offset:	73 (81%), Referenced to phase 2:NBSB, Start of Green
Natural Cycle:	55
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.72
Intersection Signal Delay:	18.1
Intersection LOS:	B
Intersection Capacity Utilization:	77.2%
ICU Level of Service:	D
Analysis Period (min):	15

Splits and Phases: 30: US 41 & 17th Ave W



Lanes, Volumes, Timings  
31: US 41 & 12th Ave W

Existing AM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Volume (vph)	17	15	32	5	10	6	32	601	16	8	536	10
Future Volume (vph)	17	15	32	5	10	6	32	601	16	8	536	10
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	0.95	0.95	0.95	0.95
Fr <sub>t</sub>		0.932			0.963			0.996			0.997	
Fl <sub>t</sub> Protected		0.987			0.989			0.998			0.999	
Satd. Flow (prot)	0	1748	0	0	1810	0	0	3555	0	0	3561	0
Fl <sub>t</sub> Permitted		0.903			0.929			0.904			0.946	
Satd. Flow (perm)	0	1599	0	0	1700	0	0	3221	0	0	3372	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		34			6			5			4	
Link Speed (mph)		25			25			35			35	
Link Distance (ft)		216			234			1665			1011	
Travel Time (s)		5.9			6.4			32.4			19.7	
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Heavy Vehicles (%)	0%	0%	0%	0%	0%	0%	0%	1%	0%	0%	1%	0%
Adj. Flow (vph)	18	16	34	5	11	6	34	633	17	8	564	11
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	68	0	0	22	0	0	684	0	0	583	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		0			0			0			0	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Number of Detectors	1	2		1	2		1	2		1	2	
Detector Template	Left	Thru		Left	Thru		Left	Thru		Left	Thru	
Leading Detector (ft)	20	100		20	100		20	100		20	100	
Trailing Detector (ft)	0	0		0	0		0	0		0	0	
Detector 1 Position(ft)	0	0		0	0		0	0		0	0	
Detector 1 Size(ft)	20	6		20	6		20	6		20	6	
Detector 1 Type	Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex	
Detector 1 Channel												
Detector 1 Extend (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Queue (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Delay (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 2 Position(ft)		94			94			94			94	
Detector 2 Size(ft)		6			6			6			6	
Detector 2 Type		Cl+Ex			Cl+Ex			Cl+Ex			Cl+Ex	
Detector 2 Channel												
Detector 2 Extend (s)		0.0			0.0			0.0			0.0	
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			4			2			2	
Permitted Phases	4			4			2			2		
Detector Phase	4	4		4	4		2	2		2	2	
Switch Phase												

Lanes, Volumes, Timings  
31: US 41 & 12th Ave W

Existing AM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Minimum Initial (s)	7.0	7.0		7.0	7.0		12.0	12.0		12.0	12.0	
Minimum Split (s)	26.1	26.1		26.1	26.1		24.0	24.0		24.0	24.0	
Total Split (s)	27.0	27.0		27.0	27.0		63.0	63.0		63.0	63.0	
Total Split (%)	30.0%	30.0%		30.0%	30.0%		70.0%	70.0%		70.0%	70.0%	
Maximum Green (s)	20.9	20.9		20.9	20.9		57.0	57.0		57.0	57.0	
Yellow Time (s)	3.7	3.7		3.7	3.7		4.0	4.0		4.0	4.0	
All-Red Time (s)	2.4	2.4		2.4	2.4		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)		0.0			0.0			0.0			0.0	
Total Lost Time (s)		6.1			6.1			6.0			6.0	
Lead/Lag												
Lead-Lag Optimize?												
Vehicle Extension (s)	3.0	3.0		3.0	3.0		5.0	5.0		5.0	5.0	
Recall Mode	None	None		None	None		C-Max	C-Max		C-Max	C-Max	
Walk Time (s)	7.0	7.0		7.0	7.0		7.0	7.0		7.0	7.0	
Flash Dont Walk (s)	13.0	13.0		13.0	13.0		10.0	10.0		10.0	10.0	
Pedestrian Calls (#/hr)	0	0		0	0		0	0		0	0	
Act Effect Green (s)		8.2			8.2			73.5			73.5	
Actuated g/C Ratio		0.09			0.09			0.82			0.82	
v/c Ratio		0.39			0.14			0.26			0.21	
Control Delay		28.8			31.5			8.1			1.7	
Queue Delay		0.0			0.0			0.0			0.0	
Total Delay		28.8			31.5			8.1			1.7	
LOS		C			C			A			A	
Approach Delay		28.8			31.5			8.1			1.7	
Approach LOS		C			C			A			A	

Intersection Summary

Area Type: Other  
 Cycle Length: 90  
 Actuated Cycle Length: 90  
 Offset: 41 (46%), Referenced to phase 2:NBSB, Start of Green  
 Natural Cycle: 55  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.39  
 Intersection Signal Delay: 6.8  
 Intersection Capacity Utilization 54.3%  
 Analysis Period (min) 15  
 Intersection LOS: A  
 ICU Level of Service A

Splits and Phases: 31: US 41 & 12th Ave W



Lanes, Volumes, Timings  
32: US 41 & 9th Ave W

Existing AM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	27	186	45	45	182	22	44	466	71	6	438	32
Future Volume (vph)	27	186	45	45	182	22	44	466	71	6	438	32
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	95		0	0		0	0		0
Storage Lanes	0		0	1		0	0		0	0		0
Taper Length (ft)	0			45			0			0		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	0.95	0.95	0.95	0.95
Frt		0.976			0.984			0.982			0.990	
Flt Protected		0.995		0.950				0.996			0.999	
Satd. Flow (prot)	0	1819	0	1770	1853	0	0	3508	0	0	3524	0
Flt Permitted		0.943		0.400				0.878			0.949	
Satd. Flow (perm)	0	1724	0	745	1853	0	0	3093	0	0	3347	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		12			7			27			13	
Link Speed (mph)		30			30			35			35	
Link Distance (ft)		180			228			1011			162	
Travel Time (s)		4.1			5.2			19.7			3.2	
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Heavy Vehicles (%)	0%	2%	0%	2%	1%	0%	2%	0%	4%	0%	1%	6%
Adj. Flow (vph)	29	198	48	48	194	23	47	496	76	6	466	34
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	275	0	48	217	0	0	619	0	0	506	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		12			12			0			0	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Number of Detectors	1	2		1	2		1	2		1	2	
Detector Template	Left	Thru		Left	Thru		Left	Thru		Left	Thru	
Leading Detector (ft)	20	100		20	100		20	100		20	100	
Trailing Detector (ft)	0	0		0	0		0	0		0	0	
Detector 1 Position(ft)	0	0		0	0		0	0		0	0	
Detector 1 Size(ft)	20	6		20	6		20	6		20	6	
Detector 1 Type	Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex	
Detector 1 Channel												
Detector 1 Extend (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Queue (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Delay (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 2 Position(ft)		94			94			94			94	
Detector 2 Size(ft)		6			6			6			6	
Detector 2 Type		Cl+Ex			Cl+Ex			Cl+Ex			Cl+Ex	
Detector 2 Channel												
Detector 2 Extend (s)		0.0			0.0			0.0			0.0	
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			4			2			2	

Lanes, Volumes, Timings  
32: US 41 & 9th Ave W

Existing AM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Permitted Phases	4			4			2			2		
Detector Phase	4	4		4	4		2	2		2	2	
Switch Phase												
Minimum Initial (s)	7.0	7.0		7.0	7.0		12.0	12.0		12.0	12.0	
Minimum Split (s)	33.8	33.8		33.8	33.8		33.2	33.2		33.2	33.2	
Total Split (s)	34.0	34.0		34.0	34.0		56.0	56.0		56.0	56.0	
Total Split (%)	37.8%	37.8%		37.8%	37.8%		62.2%	62.2%		62.2%	62.2%	
Maximum Green (s)	28.2	28.2		28.2	28.2		49.8	49.8		49.8	49.8	
Yellow Time (s)	3.7	3.7		3.7	3.7		4.0	4.0		4.0	4.0	
All-Red Time (s)	2.1	2.1		2.1	2.1		2.2	2.2		2.2	2.2	
Lost Time Adjust (s)		0.0		0.0	0.0			0.0			0.0	
Total Lost Time (s)		5.8		5.8	5.8			6.2			6.2	
Lead/Lag												
Lead-Lag Optimize?												
Vehicle Extension (s)	3.0	3.0		3.0	3.0		5.0	5.0		5.0	5.0	
Recall Mode	None	None		None	None		C-Max	C-Max		C-Max	C-Max	
Walk Time (s)	10.0	10.0		10.0	10.0		10.0	10.0		10.0	10.0	
Flash Dont Walk (s)	18.0	18.0		18.0	18.0		17.0	17.0		17.0	17.0	
Pedestrian Calls (#/hr)	0	0		0	0		0	0		0	0	
Act Effct Green (s)		19.9		19.9	19.9			58.1			58.1	
Actuated g/C Ratio		0.22		0.22	0.22			0.65			0.65	
v/c Ratio		0.71		0.29	0.52			0.31			0.23	
Control Delay		40.1		31.5	33.2			4.4			19.7	
Queue Delay		0.0		0.0	0.0			0.0			0.0	
Total Delay		40.1		31.5	33.2			4.4			19.7	
LOS		D		C	C			A			B	
Approach Delay		40.1			32.9			4.4			19.7	
Approach LOS		D			C			A			B	

Intersection Summary

Area Type: Other  
 Cycle Length: 90  
 Actuated Cycle Length: 90  
 Offset: 18 (20%), Referenced to phase 2:NBSB, Start of Green  
 Natural Cycle: 70  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.71  
 Intersection Signal Delay: 19.5  
 Intersection LOS: B  
 Intersection Capacity Utilization 74.7%  
 ICU Level of Service D  
 Analysis Period (min) 15

Splits and Phases: 32: US 41 & 9th Ave W





Intersection						
Int Delay, s/veh	0.5					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		
Traffic Vol, veh/h	6	21	14	497	451	4
Future Vol, veh/h	6	21	14	497	451	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	97	97	97	97	97	97
Heavy Vehicles, %	0	0	0	0	2	0
Mvmt Flow	6	22	14	512	465	4

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	751	235	469	0	-	0
Stage 1	467	-	-	-	-	-
Stage 2	284	-	-	-	-	-
Critical Hdwy	6.8	6.9	4.1	-	-	-
Critical Hdwy Stg 1	5.8	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.5	3.3	2.2	-	-	-
Pot Cap-1 Maneuver	351	773	1103	-	-	-
Stage 1	603	-	-	-	-	-
Stage 2	745	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	345	773	1103	-	-	-
Mov Cap-2 Maneuver	345	-	-	-	-	-
Stage 1	592	-	-	-	-	-
Stage 2	745	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	11.2	0.3	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1103	-	606	-	-
HCM Lane V/C Ratio	0.013	-	0.046	-	-
HCM Control Delay (s)	8.3	0.1	11.2	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0	-	0.1	-	-

HCM 6th Signalized Intersection Summary  
34: US 41 & 8th Ave W

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	7	41	15	355	64	9	5	185	299	2	83	6
Future Volume (veh/h)	7	41	15	355	64	9	5	185	299	2	83	6
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.99	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1870	1796	1841	1870	1737	1604	1870	1870	1900	1870	1648
Adj Flow Rate, veh/h	8	45	16	386	70	10	5	201	325	2	90	7
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	0	2	7	4	2	11	20	2	2	0	2	17
Cap, veh/h	184	102	36	480	459	66	48	1038	888	549	959	75
Arrive On Green	0.08	0.08	0.08	0.14	0.29	0.29	0.74	0.74	0.74	0.56	0.56	0.56
Sat Flow, veh/h	1335	1315	468	3401	1600	229	12	1853	1585	891	1713	133
Grp Volume(v), veh/h	8	0	61	386	0	80	206	0	325	2	0	97
Grp Sat Flow(s),veh/h/ln	1335	0	1783	1700	0	1829	1865	0	1585	891	0	1846
Q Serve(g_s), s	0.5	0.0	2.9	9.9	0.0	2.9	0.0	0.0	6.5	0.1	0.0	2.2
Cycle Q Clear(g_c), s	0.5	0.0	2.9	9.9	0.0	2.9	3.0	0.0	6.5	3.1	0.0	2.2
Prop In Lane	1.00		0.26	1.00		0.13	0.02		1.00	1.00		0.07
Lane Grp Cap(c), veh/h	184	0	138	480	0	524	1085	0	888	549	0	1034
V/C Ratio(X)	0.04	0.00	0.44	0.80	0.00	0.15	0.19	0.00	0.37	0.00	0.00	0.09
Avail Cap(c_a), veh/h	276	0	261	714	0	776	1085	0	888	549	0	1034
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.33	1.33	1.33	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	0.93	0.00	0.93	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	38.5	0.0	39.6	37.4	0.0	23.9	5.4	0.0	5.9	10.1	0.0	9.2
Incr Delay (d2), s/veh	0.1	0.0	2.2	3.8	0.0	0.1	0.4	0.0	1.2	0.0	0.0	0.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.2	0.0	1.4	4.2	0.0	1.2	1.1	0.0	2.0	0.0	0.0	0.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	38.6	0.0	41.8	41.3	0.0	24.1	5.8	0.0	7.0	10.1	0.0	9.3
LnGrp LOS	D	A	D	D	A	C	A	A	A	B	A	A
Approach Vol, veh/h		69			466			531				99
Approach Delay, s/veh		41.5			38.3			6.6				9.3
Approach LOS		D			D			A				A
Timer - Assigned Phs		2		4		6	7	8				
Phs Duration (G+Y+Rc), s		57.4		32.6		57.4	18.8	13.8				
Change Period (Y+Rc), s		7.0		* 6.8		7.0	6.1	* 6.8				
Max Green Setting (Gmax), s		38.0		* 38		38.0	18.9	* 13				
Max Q Clear Time (g_c+I1), s		8.5		4.9		5.1	11.9	4.9				
Green Ext Time (p_c), s		5.3		0.4		1.0	0.8	0.1				

Intersection Summary

HCM 6th Ctrl Delay	21.6
HCM 6th LOS	C

Notes

- User approved pedestrian interval to be less than phase max green.
- \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

HCM 6th Signalized Intersection Summary  
35: 9th St W & 8th Ave W

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖↗	↘			↔			↔			↔	↗
Traffic Volume (veh/h)	244	27	23	2	10	8	117	412	7	3	708	497
Future Volume (veh/h)	244	27	23	2	10	8	117	412	7	3	708	497
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	0.99		0.98	1.00		0.98	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1885	1900	1900	1900	1900	1900	1781	1885	1900	1900	1885	1870
Adj Flow Rate, veh/h	257	28	24	2	11	8	123	434	7	3	745	523
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Percent Heavy Veh, %	1	0	0	0	0	0	8	1	0	0	1	2
Cap, veh/h	298	168	144	28	55	36	319	1313	21	25	2592	1171
Arrive On Green	0.09	0.18	0.18	0.05	0.05	0.05	0.74	0.74	0.74	0.74	0.74	0.74
Sat Flow, veh/h	3483	944	809	65	1013	664	377	1778	29	3	3509	1585
Grp Volume(v), veh/h	257	0	52	21	0	0	152	0	412	401	347	523
Grp Sat Flow(s),veh/h/ln	1742	0	1753	1742	0	0	475	0	1710	1882	1630	1585
Q Serve(g_s), s	11.7	0.0	4.0	0.0	0.0	0.0	19.8	0.0	13.3	0.0	11.3	20.6
Cycle Q Clear(g_c), s	11.7	0.0	4.0	1.8	0.0	0.0	31.1	0.0	13.3	11.3	11.3	20.6
Prop In Lane	1.00		0.46	0.10		0.38	0.81		0.02	0.01		1.00
Lane Grp Cap(c), veh/h	298	0	313	119	0	0	391	0	1263	1413	1204	1171
V/C Ratio(X)	0.86	0.00	0.17	0.18	0.00	0.00	0.39	0.00	0.33	0.28	0.29	0.45
Avail Cap(c_a), veh/h	300	0	546	344	0	0	391	0	1263	1413	1204	1171
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.96	0.00	0.96	1.00	0.00	0.00	1.00	0.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	72.2	0.0	55.7	72.4	0.0	0.0	11.3	0.0	7.2	6.9	6.9	8.2
Incr Delay (d2), s/veh	21.1	0.0	0.2	0.7	0.0	0.0	2.9	0.0	0.7	0.5	0.6	1.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	6.1	0.0	1.8	0.9	0.0	0.0	2.7	0.0	4.9	4.5	4.0	7.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	93.4	0.0	55.9	73.1	0.0	0.0	14.2	0.0	7.9	7.4	7.6	9.4
LnGrp LOS	F	A	E	E	A	A	B	A	A	A	A	A
Approach Vol, veh/h		309			21			564			1271	
Approach Delay, s/veh		87.1			73.1			9.6			8.3	
Approach LOS		F			E			A			A	
Timer - Assigned Phs		2	3	4		6		8				
Phs Duration (G+Y+Rc), s		125.3	19.9	14.9		125.3		34.7				
Change Period (Y+Rc), s		7.1	6.2	6.2		7.1		6.2				
Max Green Setting (Gmax), s		96.9	13.8	29.8		96.9		49.8				
Max Q Clear Time (g_c+I1), s		22.6	13.7	3.8		33.1		6.0				
Green Ext Time (p_c), s		21.7	0.0	0.1		11.5		0.3				

Intersection Summary

HCM 6th Ctrl Delay	20.5
HCM 6th LOS	C

Notes

User approved pedestrian interval to be less than phase max green.

Intersection												
Int Delay, s/veh	0.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕↕	
Traffic Vol, veh/h	0	0	0	0	0	4	0	659	3	5	1211	4
Future Vol, veh/h	0	0	0	0	0	4	0	659	3	5	1211	4
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	96	96	96	96	96	96	96	96	96	96	96	96
Heavy Vehicles, %	0	0	0	0	0	25	0	3	0	0	3	0
Mvmt Flow	0	0	0	0	0	4	0	686	3	5	1261	4

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1616	1962	633	1202	1963	345	1265	0	0	689	0	0
Stage 1	1273	1273	-	688	688	-	-	-	-	-	-	-
Stage 2	343	689	-	514	1275	-	-	-	-	-	-	-
Critical Hdwy	6.95	6.5	7.1	6.95	6.5	7.4	5.3	-	-	4.1	-	-
Critical Hdwy Stg 1	7.3	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.7	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.65	4	3.9	3.65	4	3.55	3.1	-	-	2.2	-	-
Pot Cap-1 Maneuver	89	64	366	168	64	589	296	-	-	915	-	-
Stage 1	133	241	-	396	450	-	-	-	-	-	-	-
Stage 2	629	450	-	486	240	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	87	63	366	166	63	589	296	-	-	915	-	-
Mov Cap-2 Maneuver	87	63	-	166	63	-	-	-	-	-	-	-
Stage 1	133	237	-	396	450	-	-	-	-	-	-	-
Stage 2	625	450	-	477	236	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB			
HCM Control Delay, s	0		11.2		0		0.1			
HCM LOS	A		B							

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	296	-	-	-	589	915	-
HCM Lane V/C Ratio	-	-	-	-	0.007	0.006	-
HCM Control Delay (s)	0	-	-	0	11.2	9	0.1
HCM Lane LOS	A	-	-	A	B	A	A
HCM 95th %tile Q(veh)	0	-	-	-	0	0	-

HCM 6th Signalized Intersection Summary  
 37: 9th St W & 6th Ave W

Existing AM  
 09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↶	↶↷						↶↷		↶	↶↷	
Traffic Volume (veh/h)	527	977	52	0	0	0	0	579	85	175	1174	0
Future Volume (veh/h)	527	977	52	0	0	0	0	579	85	175	1174	0
Initial Q (Qb), veh	0	0	0				0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00				1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00				1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No						No			No	
Adj Sat Flow, veh/h/ln	1885	1885	1604				0	1885	1870	1856	1885	0
Adj Flow Rate, veh/h	538	997	53				0	591	87	179	1198	0
Peak Hour Factor	0.98	0.98	0.98				0.98	0.98	0.98	0.98	0.98	0.98
Percent Heavy Veh, %	1	1	20				0	1	2	3	1	0
Cap, veh/h	800	1541	82				0	1126	165	320	1713	0
Arrive On Green	0.45	0.45	0.45				0.00	0.12	0.12	0.16	0.96	0.00
Sat Flow, veh/h	1795	3459	184				0	3226	460	1767	3676	0
Grp Volume(v), veh/h	538	516	534				0	337	341	179	1198	0
Grp Sat Flow(s),veh/h/ln	1795	1791	1852				0	1791	1801	1767	1791	0
Q Serve(g_s), s	38.0	35.9	35.9				0.0	28.3	28.4	10.2	7.1	0.0
Cycle Q Clear(g_c), s	38.0	35.9	35.9				0.0	28.3	28.4	10.2	7.1	0.0
Prop In Lane	1.00		0.10				0.00		0.26	1.00		0.00
Lane Grp Cap(c), veh/h	800	798	825				0	644	647	320	1713	0
V/C Ratio(X)	0.67	0.65	0.65				0.00	0.52	0.53	0.56	0.70	0.00
Avail Cap(c_a), veh/h	800	798	825				0	644	647	398	1713	0
HCM Platoon Ratio	1.00	1.00	1.00				1.00	0.33	0.33	2.00	2.00	1.00
Upstream Filter(I)	1.00	1.00	1.00				0.00	1.00	1.00	0.43	0.43	0.00
Uniform Delay (d), s/veh	35.1	34.5	34.5				0.0	57.6	57.7	28.1	2.0	0.0
Incr Delay (d2), s/veh	4.5	4.0	3.9				0.0	3.0	3.0	0.7	1.0	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0				0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	17.5	16.5	17.0				0.0	14.3	14.4	3.9	1.3	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	39.6	38.6	38.4				0.0	60.7	60.7	28.8	3.0	0.0
LnGrp LOS	D	D	D				A	E	E	C	A	A
Approach Vol, veh/h		1588						678			1377	
Approach Delay, s/veh		38.9						60.7			6.4	
Approach LOS		D						E			A	
Timer - Assigned Phs			3	4		6		8				
Phs Duration (G+Y+Rc), s			19.0	64.0		77.0		83.0				
Change Period (Y+Rc), s			6.5	6.5		5.7		6.5				
Max Green Setting (Gmax), s			19.5	50.5		71.3		76.5				
Max Q Clear Time (g_c+I1), s			12.2	30.4		40.0		9.1				
Green Ext Time (p_c), s			0.3	4.0		20.5		12.1				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay			30.7									
HCM 6th LOS			C									

HCM 6th Signalized Intersection Summary  
38: 9th St W & Manatee Ave W

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations				↖	↖↖	↖	↖	↖↖			↖↖	↖
Traffic Volume (veh/h)	0	0	0	46	1145	72	52	1040	0	0	1309	547
Future Volume (veh/h)	0	0	0	46	1145	72	52	1040	0	0	1309	547
Initial Q (Qb), veh				0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)				1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj				1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach				No			No			No		
Adj Sat Flow, veh/h/ln				1841	1870	1900	1900	1885	0	0	1885	1870
Adj Flow Rate, veh/h				47	1168	73	53	1061	0	0	1336	558
Peak Hour Factor				0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Percent Heavy Veh, %				4	2	0	0	1	0	0	1	2
Cap, veh/h				774	1568	698	113	1730	0	0	1493	661
Arrive On Green				0.44	0.44	0.44	0.06	0.97	0.00	0.00	0.42	0.42
Sat Flow, veh/h				1753	3554	1581	1810	3676	0	0	3676	1585
Grp Volume(v), veh/h				47	1168	73	53	1061	0	0	1336	558
Grp Sat Flow(s),veh/h/ln				1753	1777	1581	1810	1791	0	0	1791	1585
Q Serve(g_s), s				2.5	43.8	4.3	2.6	3.9	0.0	0.0	55.5	50.7
Cycle Q Clear(g_c), s				2.5	43.8	4.3	2.6	3.9	0.0	0.0	55.5	50.7
Prop In Lane				1.00		1.00	1.00		0.00	0.00		1.00
Lane Grp Cap(c), veh/h				774	1568	698	113	1730	0	0	1493	661
V/C Ratio(X)				0.06	0.74	0.10	0.47	0.61	0.00	0.00	0.90	0.84
Avail Cap(c_a), veh/h				774	1568	698	163	1923	0	0	1587	702
HCM Platoon Ratio				1.00	1.00	1.00	2.00	2.00	1.00	1.00	1.00	1.00
Upstream Filter(I)				1.00	1.00	1.00	0.76	0.76	0.00	0.00	0.75	0.75
Uniform Delay (d), s/veh				25.7	37.2	26.2	36.1	1.5	0.0	0.0	43.4	42.0
Incr Delay (d2), s/veh				0.2	3.3	0.3	2.3	0.4	0.0	0.0	5.2	6.9
Initial Q Delay(d3),s/veh				0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln				1.1	19.6	1.7	1.2	0.8	0.0	0.0	25.4	20.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh				25.8	40.5	26.5	38.4	1.8	0.0	0.0	48.6	48.9
LnGrp LOS				C	D	C	D	A	A	A	D	D
Approach Vol, veh/h				1288			1114			1894		
Approach Delay, s/veh				39.1			3.6			48.7		
Approach LOS				D			A			D		
Timer - Assigned Phs		2		4			7	8				
Phs Duration (G+Y+Rc), s		76.6		83.4			10.6	72.8				
Change Period (Y+Rc), s		6.0		6.1			6.1	6.1				
Max Green Setting (Gmax), s		62.0		85.9			8.9	70.9				
Max Q Clear Time (g_c+I1), s		45.8		5.9			4.6	57.5				
Green Ext Time (p_c), s		11.7		10.0			0.0	9.2				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay				34.1								
HCM 6th LOS				C								

HCM 6th Signalized Intersection Summary  
39: 9th St W & 3rd Ave W

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	43	26	9	51	121	41	29	953	88	110	1736	411
Future Volume (veh/h)	43	26	9	51	121	41	29	953	88	110	1736	411
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.99	1.00		1.00	1.00		0.98	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1841	1870	1900	1856	1900	1870	1900	1885	1900
Adj Flow Rate, veh/h	40	33	9	52	123	42	30	972	90	112	1771	419
Peak Hour Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Percent Heavy Veh, %	0	0	0	4	2	0	3	0	2	0	1	0
Cap, veh/h	74	59	16	140	149	128	167	2371	220	412	3000	696
Arrive On Green	0.04	0.04	0.04	0.08	0.08	0.08	0.02	0.71	0.71	0.03	0.72	0.72
Sat Flow, veh/h	1810	1435	391	1753	1870	1610	1767	3333	309	1810	4173	968
Grp Volume(v), veh/h	40	0	42	52	123	42	30	526	536	112	1450	740
Grp Sat Flow(s),veh/h/ln	1810	0	1826	1753	1870	1610	1767	1805	1836	1810	1716	1710
Q Serve(g_s), s	3.9	0.0	4.1	5.1	11.7	4.4	0.8	21.4	21.4	3.1	37.0	38.6
Cycle Q Clear(g_c), s	3.9	0.0	4.1	5.1	11.7	4.4	0.8	21.4	21.4	3.1	37.0	38.6
Prop In Lane	1.00		0.21	1.00		1.00	1.00		0.17	1.00		0.57
Lane Grp Cap(c), veh/h	74	0	75	140	149	128	167	1284	1306	412	2466	1229
V/C Ratio(X)	0.54	0.00	0.56	0.37	0.82	0.33	0.18	0.41	0.41	0.27	0.59	0.60
Avail Cap(c_a), veh/h	261	0	264	263	281	242	213	1284	1306	496	2466	1229
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	1.00	0.81	0.81	0.81	1.00	1.00	1.00
Uniform Delay (d), s/veh	84.7	0.0	84.7	78.5	81.6	78.3	11.2	10.6	10.6	8.1	12.3	12.5
Incr Delay (d2), s/veh	6.0	0.0	6.5	1.6	10.8	1.5	0.4	0.8	0.8	0.4	1.0	2.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.0	0.0	2.1	2.4	6.1	1.9	0.3	8.7	8.9	1.2	14.1	15.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	90.7	0.0	91.2	80.2	92.4	79.7	11.6	11.4	11.4	8.4	13.4	14.7
LnGrp LOS	F	A	F	F	F	E	B	B	B	A	B	B
Approach Vol, veh/h		82			217			1092			2302	
Approach Delay, s/veh		90.9			87.0			11.4			13.6	
Approach LOS		F			F			B			B	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.4	135.9		20.5	11.7	134.6		13.3				
Change Period (Y+Rc), s	6.5	6.5		6.1	6.5	6.5		5.9				
Max Green Setting (Gmax), s	5	93.5		27.0	13.5	88.5		26.0				
Max Q Clear Time (g_c+1), s	12.8	40.6		13.7	5.1	23.4		6.1				
Green Ext Time (p_c), s	0.0	45.4		0.7	0.1	20.3		0.2				

Intersection Summary

HCM 6th Ctrl Delay	18.9
HCM 6th LOS	B

Notes

User approved volume balancing among the lanes for turning movement.  
User approved changes to right turn type.

Lanes, Volumes, Timings  
40: 8th Ave W & 5th St W

Existing AM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	14	7	240	132	2	1	42	828	14	2	1635	2
Future Volume (vph)	14	7	240	132	2	1	42	828	14	2	1635	2
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	80		0	0		0	0		0	0		0
Storage Lanes	1		0	0		0	0		0	0		0
Taper Length (ft)	20			0			0			0		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	0.95	0.95	0.95	0.95
Frt		0.854			0.999			0.998				
Flt Protected	0.950				0.953			0.998				
Satd. Flow (prot)	1805	1607	0	0	1809	0	0	3562	0	0	3574	0
Flt Permitted	0.751				0.328			0.689			0.954	
Satd. Flow (perm)	1427	1607	0	0	623	0	0	2459	0	0	3410	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		169						3				
Link Speed (mph)		25			25			30				30
Link Distance (ft)		405			371			3271				637
Travel Time (s)		11.0			10.1			74.3				14.5
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	0%	1%	0%	0%	0%	0%	1%	0%	0%	1%	0%
Adj. Flow (vph)	15	7	250	138	2	1	44	863	15	2	1703	2
Shared Lane Traffic (%)												
Lane Group Flow (vph)	15	257	0	0	141	0	0	922	0	0	1707	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		12			12			0			0	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Number of Detectors	1	2		1	2		1	2		1	2	
Detector Template	Left	Thru		Left	Thru		Left	Thru		Left	Thru	
Leading Detector (ft)	20	100		20	100		20	100		20	100	
Trailing Detector (ft)	0	0		0	0		0	0		0	0	
Detector 1 Position(ft)	0	0		0	0		0	0		0	0	
Detector 1 Size(ft)	20	6		20	6		20	6		20	6	
Detector 1 Type	Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex	
Detector 1 Channel												
Detector 1 Extend (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Queue (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Delay (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 2 Position(ft)		94			94			94			94	
Detector 2 Size(ft)		6			6			6			6	
Detector 2 Type		Cl+Ex			Cl+Ex			Cl+Ex			Cl+Ex	
Detector 2 Channel												
Detector 2 Extend (s)		0.0			0.0			0.0			0.0	
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		Perm	NA	
Protected Phases		8			4		1	6			2	



Lanes, Volumes, Timings  
40: 8th Ave W & 5th St W

Existing AM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Permitted Phases	8			4			6			2		
Detector Phase	8	8		4	4		1	6		2	2	
Switch Phase												
Minimum Initial (s)	7.0	7.0		7.0	7.0		5.0	8.0		8.0	8.0	
Minimum Split (s)	30.7	30.7		31.7	31.7		10.8	24.9		29.9	29.9	
Total Split (s)	35.0	35.0		35.0	35.0		20.0	115.0		95.0	95.0	
Total Split (%)	23.3%	23.3%		23.3%	23.3%		13.3%	76.7%		63.3%	63.3%	
Maximum Green (s)	29.3	29.3		29.3	29.3		14.2	109.1		89.1	89.1	
Yellow Time (s)	3.7	3.7		3.7	3.7		3.7	3.7		3.7	3.7	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.1	2.2		2.2	2.2	
Lost Time Adjust (s)	0.0	0.0			0.0			0.0			0.0	
Total Lost Time (s)	5.7	5.7			5.7			5.9			5.9	
Lead/Lag							Lead			Lag	Lag	
Lead-Lag Optimize?							Yes			Yes	Yes	
Vehicle Extension (s)	3.0	3.0		3.0	3.0		3.0	5.0		5.0	5.0	
Recall Mode	None	None		None	None		None	C-Min		C-Min	C-Min	
Walk Time (s)	7.0	7.0		7.0	7.0			7.0		7.0	7.0	
Flash Dont Walk (s)	18.0	18.0		19.0	19.0			12.0		17.0	17.0	
Pedestrian Calls (#/hr)	0	0		0	0			0		0	0	
Act Effct Green (s)	33.2	33.2			33.2			105.2			105.2	
Actuated g/C Ratio	0.22	0.22			0.22			0.70			0.70	
v/c Ratio	0.05	0.53			1.03			0.53			0.71	
Control Delay	49.6	22.9			140.5			11.7			21.3	
Queue Delay	0.0	0.0			0.0			0.0			2.5	
Total Delay	49.6	22.9			140.5			11.7			23.9	
LOS	D	C			F			B			C	
Approach Delay		24.4			140.5			11.7			23.9	
Approach LOS		C			F			B			C	

Intersection Summary

Area Type: Other  
 Cycle Length: 150  
 Actuated Cycle Length: 150  
 Offset: 48 (32%), Referenced to phase 2:SBTL and 6:NBTL, Start of Green  
 Natural Cycle: 90  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 1.03  
 Intersection Signal Delay: 25.6      Intersection LOS: C  
 Intersection Capacity Utilization 91.6%      ICU Level of Service F  
 Analysis Period (min) 15

Splits and Phases: 40: 8th Ave W & 5th St W



Intersection												
Int Delay, s/veh	0.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	0	0	55	0	0	0	10	801	1	1	1560	13
Future Vol, veh/h	0	0	55	0	0	0	10	801	1	1	1560	13
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	0	0	0	0	0	0	0	1	0	0	1	15
Mvmt Flow	0	0	60	0	0	0	11	871	1	1	1696	14

Major/Minor	Minor2		Minor1			Major1		Major2				
Conflicting Flow All	2163	2599	855	1744	2606	436	1710	0	0	872	0	0
Stage 1	1705	1705	-	894	894	-	-	-	-	-	-	-
Stage 2	458	894	-	850	1712	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	27	25	306	56	25	574	376	-	-	782	-	-
Stage 1	97	148	-	306	362	-	-	-	-	-	-	-
Stage 2	557	362	-	326	147	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	25	23	306	42	23	574	376	-	-	782	-	-
Mov Cap-2 Maneuver	25	23	-	42	23	-	-	-	-	-	-	-
Stage 1	91	144	-	289	341	-	-	-	-	-	-	-
Stage 2	525	341	-	255	143	-	-	-	-	-	-	-

Approach	EB		WB			NB		SB		
HCM Control Delay, s	19.6		0			0.6		0.1		
HCM LOS	C		A							

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	376	-	-	306	-	782	-
HCM Lane V/C Ratio	0.029	-	-	0.195	-	0.001	-
HCM Control Delay (s)	14.9	0.4	-	19.6	0	9.6	0.1
HCM Lane LOS	B	A	-	C	A	A	A
HCM 95th %tile Q(veh)	0.1	-	-	0.7	-	0	-

HCM 6th Signalized Intersection Summary  
42: 8th Ave W & 7th St W

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕		↕	↕		↕	↕↕		↕	↕↕	
Traffic Volume (veh/h)	22	84	134	268	83	11	19	714	64	9	1177	11
Future Volume (veh/h)	22	84	134	268	83	11	19	714	64	9	1177	11
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1826	1900	1885	1870	1885	1900	1900	1900	1870	1900	1885	1900
Adj Flow Rate, veh/h	23	89	143	285	88	12	20	760	68	10	1252	12
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	5	0	1	2	1	0	0	0	2	0	1	0
Cap, veh/h	58	214	314	349	538	73	287	1799	161	430	2136	20
Arrive On Green	0.33	0.33	0.33	0.33	0.33	0.33	1.00	1.00	1.00	0.02	1.00	1.00
Sat Flow, veh/h	95	645	945	1148	1623	221	446	3351	300	1810	3635	35
Grp Volume(v), veh/h	255	0	0	285	0	100	20	409	419	10	617	647
Grp Sat Flow(s),veh/h/ln	1685	0	0	1148	0	1845	446	1805	1846	1810	1791	1879
Q Serve(g_s), s	0.0	0.0	0.0	29.6	0.0	5.7	0.0	0.0	0.0	0.4	0.0	0.0
Cycle Q Clear(g_c), s	17.4	0.0	0.0	47.0	0.0	5.7	0.0	0.0	0.0	0.4	0.0	0.0
Prop In Lane	0.09		0.56	1.00		0.12	1.00		0.16	1.00		0.02
Lane Grp Cap(c), veh/h	585	0	0	349	0	612	287	969	991	430	1052	1104
V/C Ratio(X)	0.44	0.00	0.00	0.82	0.00	0.16	0.07	0.42	0.42	0.02	0.59	0.59
Avail Cap(c_a), veh/h	631	0	0	381	0	663	287	969	991	579	1052	1104
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	2.00	2.00	2.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	39.3	0.0	0.0	53.3	0.0	35.4	0.0	0.0	0.0	14.3	0.0	0.0
Incr Delay (d2), s/veh	0.5	0.0	0.0	12.2	0.0	0.1	0.5	1.4	1.3	0.0	2.4	2.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	7.5	0.0	0.0	11.7	0.0	2.6	0.0	0.4	0.4	0.2	0.7	0.7
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	39.8	0.0	0.0	65.5	0.0	35.5	0.5	1.4	1.3	14.4	2.4	2.3
LnGrp LOS	D	A	A	E	A	D	A	A	A	B	A	A
Approach Vol, veh/h		255			385			848				1274
Approach Delay, s/veh		39.8			57.7			1.3				2.4
Approach LOS		D			E			A				A
Timer - Assigned Phs		2		4	5	6		8				
Phs Duration (G+Y+Rc), s		94.1		55.9	7.6	86.5		55.9				
Change Period (Y+Rc), s		6.0		* 6.1	5.9	6.0		* 6.1				
Max Green Setting (Gmax), s		84.0		* 54	14.1	64.0		* 54				
Max Q Clear Time (g_c+I1), s		2.0		49.0	2.4	2.0		19.4				
Green Ext Time (p_c), s		30.7		0.8	0.0	14.9		1.6				

Intersection Summary

HCM 6th Ctrl Delay	13.2
HCM 6th LOS	B

Notes

\* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	1	0	2	1	0	11	0	764	4	4	1173	0
Future Vol, veh/h	1	0	2	1	0	11	0	764	4	4	1173	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	95	95	95	95	95	95	95	95	95	95	95	95
Heavy Vehicles, %	0	0	0	0	0	0	0	1	0	0	1	0
Mvmt Flow	1	0	2	1	0	12	0	804	4	4	1235	0

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1645	2051	618	1432	2049	404	1235	0	0	808	0	0
Stage 1	1243	1243	-	806	806	-	-	-	-	-	-	-
Stage 2	402	808	-	626	1243	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	67	56	437	96	56	602	571	-	-	826	-	-
Stage 1	188	249	-	346	398	-	-	-	-	-	-	-
Stage 2	601	397	-	443	249	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	65	55	437	94	55	602	571	-	-	826	-	-
Mov Cap-2 Maneuver	65	55	-	94	55	-	-	-	-	-	-	-
Stage 1	188	245	-	346	398	-	-	-	-	-	-	-
Stage 2	589	397	-	434	245	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB		
HCM Control Delay, s	29.5		13.9		0		0.1		
HCM LOS	D		B						

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	571	-	-	150	415	826	-
HCM Lane V/C Ratio	-	-	-	0.021	0.03	0.005	-
HCM Control Delay (s)	0	-	-	29.5	13.9	9.4	0.1
HCM Lane LOS	A	-	-	D	B	A	A
HCM 95th %tile Q(veh)	0	-	-	0.1	0.1	0	-

Intersection						
Int Delay, s/veh	0.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations		↗		↖↗	↖↗	
Traffic Vol, veh/h	1	32	6	778	1168	17
Future Vol, veh/h	1	32	6	778	1168	17
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	0	3	0	1	1	0
Mvmt Flow	1	34	6	819	1229	18

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1660	624	1247	0	-	0
Stage 1	1238	-	-	-	-	-
Stage 2	422	-	-	-	-	-
Critical Hdwy	6.8	6.96	4.1	-	-	-
Critical Hdwy Stg 1	5.8	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.5	3.33	2.2	-	-	-
Pot Cap-1 Maneuver	90	426	565	-	-	-
Stage 1	241	-	-	-	-	-
Stage 2	635	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	88	426	565	-	-	-
Mov Cap-2 Maneuver	88	-	-	-	-	-
Stage 1	236	-	-	-	-	-
Stage 2	635	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	14.2	0.2	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	565	-	426	-	-
HCM Lane V/C Ratio	0.011	-	0.079	-	-
HCM Control Delay (s)	11.4	0.1	14.2	-	-
HCM Lane LOS	B	A	B	-	-
HCM 95th %tile Q(veh)	0	-	0.3	-	-

HCM 6th Signalized Intersection Summary  
45: 8th Ave W & 10th St W

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↕		↖	↗	
Traffic Volume (veh/h)	75	247	44	254	342	45	105	593	73	56	900	19
Future Volume (veh/h)	75	247	44	254	342	45	105	593	73	56	900	19
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1885	1900	1870	1900	1885	1841	1900	1900	1856	1767	1870	1826
Adj Flow Rate, veh/h	79	260	46	267	360	47	111	624	77	59	947	20
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Percent Heavy Veh, %	1	0	2	0	1	4	0	0	3	9	2	5
Cap, veh/h	199	290	51	313	445	58	386	1568	193	335	1672	35
Arrive On Green	0.05	0.18	0.18	0.13	0.27	0.27	0.03	0.32	0.32	0.06	0.94	0.94
Sat Flow, veh/h	1795	1571	278	1810	1633	213	1810	3234	398	1682	3558	75
Grp Volume(v), veh/h	79	0	306	267	0	407	111	348	353	59	473	494
Grp Sat Flow(s),veh/h/ln	1795	0	1849	1810	0	1846	1810	1805	1827	1682	1777	1856
Q Serve(g_s), s	5.3	0.0	24.3	17.4	0.0	30.9	4.7	22.4	22.5	2.7	5.2	5.2
Cycle Q Clear(g_c), s	5.3	0.0	24.3	17.4	0.0	30.9	4.7	22.4	22.5	2.7	5.2	5.2
Prop In Lane	1.00		0.15	1.00		0.12	1.00		0.22	1.00		0.04
Lane Grp Cap(c), veh/h	199	0	341	313	0	503	386	876	886	335	835	872
V/C Ratio(X)	0.40	0.00	0.90	0.85	0.00	0.81	0.29	0.40	0.40	0.18	0.57	0.57
Avail Cap(c_a), veh/h	255	0	399	415	0	608	450	876	886	420	835	872
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	0.67	0.67	0.67	2.00	2.00	2.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	47.4	0.0	59.8	42.2	0.0	50.9	19.2	33.6	33.7	20.2	2.6	2.6
Incr Delay (d2), s/veh	1.3	0.0	21.3	12.4	0.0	7.6	0.4	1.3	1.3	0.2	2.8	2.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.5	0.0	13.3	8.8	0.0	15.3	2.1	10.8	11.0	1.1	1.7	1.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	48.7	0.0	81.1	54.6	0.0	58.5	19.6	35.0	35.0	20.5	5.3	5.2
LnGrp LOS	D	A	F	D	A	E	B	C	C	C	A	A
Approach Vol, veh/h		385			674			812			1026	
Approach Delay, s/veh		74.5			56.9			32.9			6.2	
Approach LOS		E			E			C			A	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	12.8	76.5	26.5	34.3	10.5	78.8	13.3	47.5				
Change Period (Y+Rc), s	6.0	6.0	6.6	6.6	5.9	6.0	* 6.2	6.6				
Max Green Setting (Gmax), s	12.0	52.0	28.4	32.4	12.1	52.0	* 12	49.4				
Max Q Clear Time (g_c+I1), s	6.7	7.2	19.4	26.3	4.7	24.5	7.3	32.9				
Green Ext Time (p_c), s	0.1	16.3	0.5	1.1	0.1	9.0	0.1	3.1				

Intersection Summary

HCM 6th Ctrl Delay	34.5
HCM 6th LOS	C

Notes

\* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Intersection						
Int Delay, s/veh	0.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		
Traffic Vol, veh/h	2	18	0	710	972	4
Future Vol, veh/h	2	18	0	710	972	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	96	96	96	96	96	96
Heavy Vehicles, %	0	0	0	2	1	0
Mvmt Flow	2	19	0	740	1013	4

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1385	509	1017	0	-	0
Stage 1	1015	-	-	-	-	-
Stage 2	370	-	-	-	-	-
Critical Hdwy	6.8	6.9	4.1	-	-	-
Critical Hdwy Stg 1	5.8	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.5	3.3	2.2	-	-	-
Pot Cap-1 Maneuver	137	515	690	-	-	-
Stage 1	315	-	-	-	-	-
Stage 2	675	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	137	515	690	-	-	-
Mov Cap-2 Maneuver	137	-	-	-	-	-
Stage 1	315	-	-	-	-	-
Stage 2	675	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	14.4	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	690	-	404	-	-
HCM Lane V/C Ratio	-	-	0.052	-	-
HCM Control Delay (s)	0	-	14.4	-	-
HCM Lane LOS	A	-	B	-	-
HCM 95th %tile Q(veh)	0	-	0.2	-	-

Intersection												
Int Delay, s/veh	1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	0	1	15	22	0	9	15	690	24	0	951	13
Future Vol, veh/h	0	1	15	22	0	9	15	690	24	0	951	13
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	0	0	7	0	0	0	0	1	4	0	2	0
Mvmt Flow	0	1	16	24	0	10	16	750	26	0	1034	14

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1448	1849	524	1313	1843	388	1048	0	0	776	0	0
Stage 1	1041	1041	-	795	795	-	-	-	-	-	-	-
Stage 2	407	808	-	518	1048	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	7.04	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.37	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	94	75	485	118	76	616	672	-	-	849	-	-
Stage 1	250	310	-	351	402	-	-	-	-	-	-	-
Stage 2	597	397	-	514	307	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	90	72	485	109	73	616	672	-	-	849	-	-
Mov Cap-2 Maneuver	90	72	-	109	73	-	-	-	-	-	-	-
Stage 1	240	310	-	336	385	-	-	-	-	-	-	-
Stage 2	563	380	-	495	307	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	15.6	37.8	0.4	0
HCM LOS	C	E		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	672	-	-	357	143	849	-
HCM Lane V/C Ratio	0.024	-	-	0.049	0.236	-	-
HCM Control Delay (s)	10.5	0.2	-	15.6	37.8	0	-
HCM Lane LOS	B	A	-	C	E	A	-
HCM 95th %tile Q(veh)	0.1	-	-	0.2	0.9	0	-



HCM 6th Signalized Intersection Summary  
48: 8th Ave W & 17th St W

Existing AM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↕		↖	↗	
Traffic Volume (veh/h)	223	194	45	126	248	6	95	550	86	12	878	314
Future Volume (veh/h)	223	194	45	126	248	6	95	550	86	12	878	314
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		0.98	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1885	1885	1900	1885	1885	1648	1870	1885	1900	1900	1870	1885
Adj Flow Rate, veh/h	242	211	49	137	270	7	103	598	93	13	954	341
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	1	1	0	1	1	17	2	1	0	0	2	1
Cap, veh/h	280	301	70	266	302	8	209	1638	254	347	1290	458
Arrive On Green	0.12	0.20	0.20	0.08	0.16	0.16	0.01	0.17	0.17	0.01	0.50	0.50
Sat Flow, veh/h	1795	1480	344	1795	1829	47	1781	3099	481	1810	2567	911
Grp Volume(v), veh/h	242	0	260	137	0	277	103	345	346	13	658	637
Grp Sat Flow(s),veh/h/ln	1795	0	1823	1795	0	1877	1781	1791	1789	1810	1777	1702
Q Serve(g_s), s	16.6	0.0	19.9	9.4	0.0	21.7	4.2	25.5	25.6	0.5	43.9	44.6
Cycle Q Clear(g_c), s	16.6	0.0	19.9	9.4	0.0	21.7	4.2	25.5	25.6	0.5	43.9	44.6
Prop In Lane	1.00		0.19	1.00		0.03	1.00		0.27	1.00		0.54
Lane Grp Cap(c), veh/h	280	0	371	266	0	310	209	946	946	347	893	855
V/C Ratio(X)	0.86	0.00	0.70	0.51	0.00	0.89	0.49	0.36	0.37	0.04	0.74	0.74
Avail Cap(c_a), veh/h	280	0	405	335	0	417	320	946	946	507	893	855
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	0.33	0.33	0.33	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	45.8	0.0	55.5	47.4	0.0	61.3	26.1	39.7	39.8	19.3	29.5	29.7
Incr Delay (d2), s/veh	23.3	0.0	4.8	1.5	0.0	17.2	1.8	1.1	1.1	0.0	5.4	5.8
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	9.3	0.0	9.7	4.4	0.0	11.8	1.9	12.6	12.6	0.2	19.8	19.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	69.0	0.0	60.3	49.0	0.0	78.5	27.9	40.8	40.9	19.3	34.9	35.5
LnGrp LOS	E	A	E	D	A	E	C	D	D	B	C	D
Approach Vol, veh/h		502			414			794			1308	
Approach Delay, s/veh		64.5			68.8			39.2			35.1	
Approach LOS		E			E			D			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	8.7	85.9	18.2	37.2	12.6	81.9	24.0	31.4				
Change Period (Y+Rc), s	6.6	6.6	* 6.7	* 6.7	6.4	6.6	6.5	* 6.7				
Max Green Setting (Gmax), s	15.4	57.4	* 17	* 33	15.6	57.4	17.5	* 33				
Max Q Clear Time (g_c+I1), s	2.5	27.6	11.4	21.9	6.2	46.6	18.6	23.7				
Green Ext Time (p_c), s	0.0	8.9	0.2	1.1	0.1	8.5	0.0	1.1				

Intersection Summary

HCM 6th Ctrl Delay	45.7
HCM 6th LOS	D

Notes

\* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

HCM 6th Signalized Intersection Summary  
1: US 41 & Ringling Plaza/University Pkwy

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	39	79	38	301	45	539	12	1326	249	338	826	7
Future Volume (veh/h)	39	79	38	301	45	539	12	1326	249	338	826	7
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1870	1900	1885	1870	1885	1885	1900
Adj Flow Rate, veh/h	40	81	39	343	0	556	12	1367	257	348	852	0
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Percent Heavy Veh, %	0	0	0	0	0	2	0	1	2	1	1	0
Cap, veh/h	100	105	87	364	0	334	23	1697	314	390	2354	
Arrive On Green	0.06	0.06	0.06	0.10	0.00	0.10	0.01	0.56	0.56	0.11	0.66	0.00
Sat Flow, veh/h	1810	1900	1571	3619	0	1559	1810	3016	559	3483	3676	0
Grp Volume(v), veh/h	40	81	39	343	0	556	12	804	820	348	852	0
Grp Sat Flow(s),veh/h/ln	1810	1900	1571	1810	0	1559	1810	1791	1784	1742	1791	0
Q Serve(g_s), s	3.6	7.2	4.1	16.0	0.0	17.1	1.1	60.5	63.3	16.8	18.2	0.0
Cycle Q Clear(g_c), s	3.6	7.2	4.1	16.0	0.0	17.1	1.1	60.5	63.3	16.8	18.2	0.0
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.31	1.00		0.00
Lane Grp Cap(c), veh/h	100	105	87	364	0	334	23	1008	1004	390	2354	
V/C Ratio(X)	0.40	0.77	0.45	0.94	0.00	1.66	0.52	0.80	0.82	0.89	0.36	
Avail Cap(c_a), veh/h	161	169	140	364	0	334	395	1008	1004	428	2354	
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	0.96	0.96	0.00
Uniform Delay (d), s/veh	77.5	79.2	77.8	76.0	0.0	67.0	83.4	29.5	30.1	74.5	13.1	0.0
Incr Delay (d2), s/veh	2.5	11.1	3.6	32.6	0.0	311.4	17.0	5.2	6.0	18.7	0.4	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.8	3.9	1.8	9.0	0.0	43.6	0.6	26.4	27.6	8.4	7.2	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	80.1	90.3	81.3	108.5	0.0	378.4	100.4	34.7	36.1	93.2	13.5	0.0
LnGrp LOS	F	F	F	F	A	F	F	C	D	F	B	
Approach Vol, veh/h		160			899			1636			1200	
Approach Delay, s/veh		85.5			275.4			35.9			36.6	
Approach LOS		F			F			D			D	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.1	118.6		25.0	26.1	102.5		16.3				
Change Period (Y+Rc), s	7.9	6.9		7.9	* 7.1	6.9		6.9				
Max Green Setting (Gmax), s	37.1	71.1		17.1	* 21	88.1		15.1				
Max Q Clear Time (g_c+I1), s	3.1	20.2		19.1	18.8	65.3		9.2				
Green Ext Time (p_c), s	0.0	13.7		0.0	0.3	18.5		0.3				

Intersection Summary

HCM 6th Ctrl Delay	93.5
HCM 6th LOS	F

Notes

- User approved pedestrian interval to be less than phase max green.
- User approved volume balancing among the lanes for turning movement.
- User approved ignoring U-Turning movement.

# HCM 6th Signalized Intersection Summary

## 2: US 41 & College Dr/General Spatz Blvd

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	52	23	24	30	4	14	12	1858	68	80	1108	15
Future Volume (veh/h)	52	23	24	30	4	14	12	1858	68	80	1108	15
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No		No		No		No		No		No
Adj Sat Flow, veh/h/ln	1900	1900	1841	1707	1900	1900	1781	1885	1767	1781	1885	1900
Adj Flow Rate, veh/h	57	25	26	33	4	15	13	2042	75	88	1218	16
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Percent Heavy Veh, %	0	0	4	13	0	0	8	1	9	8	1	0
Cap, veh/h	121	56	59	90	23	86	23	2609	95	106	4131	54
Arrive On Green	0.07	0.07	0.07	0.07	0.07	0.07	0.01	0.74	0.74	0.06	0.79	0.79
Sat Flow, veh/h	1410	843	876	1236	344	1289	1697	3524	129	1697	5235	69
Grp Volume(v), veh/h	57	0	51	33	0	19	13	1031	1086	88	798	436
Grp Sat Flow(s),veh/h/ln	1410	0	1719	1236	0	1632	1697	1791	1862	1697	1716	1873
Q Serve(g_s), s	6.8	0.0	4.9	4.5	0.0	1.9	1.3	60.0	61.7	8.7	10.9	10.9
Cycle Q Clear(g_c), s	8.6	0.0	4.9	9.3	0.0	1.9	1.3	60.0	61.7	8.7	10.9	10.9
Prop In Lane	1.00		0.51	1.00		0.79	1.00		0.07	1.00		0.04
Lane Grp Cap(c), veh/h	121	0	115	90	0	109	23	1326	1378	106	2707	1478
V/C Ratio(X)	0.47	0.00	0.44	0.37	0.00	0.17	0.57	0.78	0.79	0.83	0.29	0.29
Avail Cap(c_a), veh/h	146	0	146	112	0	138	82	1326	1378	122	2707	1478
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	0.20	0.20	0.20	1.00	1.00	1.00
Uniform Delay (d), s/veh	79.0	0.0	76.3	80.8	0.0	74.9	83.4	13.5	13.8	78.8	4.9	4.9
Incr Delay (d2), s/veh	4.0	0.0	3.8	3.5	0.0	1.1	5.2	0.9	1.0	34.5	0.3	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.6	0.0	2.3	1.5	0.0	0.8	0.6	21.5	23.0	4.8	3.4	3.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	82.9	0.0	80.1	84.3	0.0	75.9	88.6	14.5	14.7	113.3	5.2	5.4
LnGrp LOS	F	A	F	F	A	E	F	B	B	F	A	A
Approach Vol, veh/h		108			52			2130			1322	
Approach Delay, s/veh		81.6			81.3			15.0			12.5	
Approach LOS		F			F			B			B	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	18.4	132.6		19.0	10.1	140.9		19.0				
Change Period (Y+Rc), s	7.8	6.8		7.6	7.8	6.8		7.6				
Max Green Setting (Gmax), s	12.2	121.2		14.4	8.2	125.2		14.4				
Max Q Clear Time (g_c+10), s	11.0	63.7		10.6	3.3	12.9		11.3				
Green Ext Time (p_c), s	0.0	54.1		0.2	0.0	37.2		0.0				

### Intersection Summary

HCM 6th Ctrl Delay	17.0
HCM 6th LOS	B

### Notes

- User approved pedestrian interval to be less than phase max green.
- User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary  
3: US 41 & Braden Ave

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		
Traffic Volume (veh/h)	42	0	43	0	0	2	33	1990	0	0	1118	24
Future Volume (veh/h)	42	0	43	0	0	2	33	1990	0	0	1118	24
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	0.99		0.99	1.00		0.97	1.00		1.00	1.00		0.99
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No		No		No		No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1885	1900	1900	1885	1900
Adj Flow Rate, veh/h	46	0	47	0	0	2	36	2163	0	0	1215	26
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	0	0	0	0	0	0	0	1	0	0	1	0
Cap, veh/h	134	9	67	0	0	136	359	3611	0	196	2881	62
Arrive On Green	0.09	0.00	0.09	0.00	0.00	0.09	0.04	0.70	0.00	0.00	0.56	0.56
Sat Flow, veh/h	653	103	772	0	0	1570	1810	5316	0	1810	5185	111
Grp Volume(v), veh/h	93	0	0	0	0	2	36	2163	0	0	804	437
Grp Sat Flow(s),veh/h/ln	1528	0	0	0	0	1570	1810	1716	0	1810	1716	1865
Q Serve(g_s), s	3.4	0.0	0.0	0.0	0.0	0.1	0.5	15.1	0.0	0.0	9.5	9.5
Cycle Q Clear(g_c), s	4.1	0.0	0.0	0.0	0.0	0.1	0.5	15.1	0.0	0.0	9.5	9.5
Prop In Lane	0.49		0.51	0.00		1.00	1.00		0.00	1.00		0.06
Lane Grp Cap(c), veh/h	210	0	0	0	0	136	359	3611	0	196	1906	1036
V/C Ratio(X)	0.44	0.00	0.00	0.00	0.00	0.01	0.10	0.60	0.00	0.00	0.42	0.42
Avail Cap(c_a), veh/h	259	0	0	0	0	188	483	3611	0	369	1906	1036
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	0.00	0.00	1.00	1.00	1.00	0.00	0.00	1.00	1.00
Uniform Delay (d), s/veh	31.0	0.0	0.0	0.0	0.0	29.2	6.2	5.4	0.0	0.0	9.0	9.0
Incr Delay (d2), s/veh	1.5	0.0	0.0	0.0	0.0	0.0	0.1	0.7	0.0	0.0	0.7	1.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	6	0.0	0.0	0.0	0.0	0.0	0.1	2.8	0.0	0.0	2.7	3.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	32.5	0.0	0.0	0.0	0.0	29.3	6.3	6.1	0.0	0.0	9.7	10.3
LnGrp LOS	C	A	A	A	A	C	A	A	A	A	A	B
Approach Vol, veh/h		93			2			2199			1241	
Approach Delay, s/veh		32.5			29.3			6.1			9.9	
Approach LOS		C			C			A			A	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.2	47.1		12.7	0.0	57.3		12.7				
Change Period (Y+Rc), s	7.7	8.2		* 6.6	8.2	8.2		* 6.6				
Max Green Setting (Gmax), s	31.8	31.8		* 8.4	6.8	31.8		* 8.4				
Max Q Clear Time (g_c+1/2), s	11.5	11.5		2.1	0.0	17.1		6.1				
Green Ext Time (p_c), s	0.0	12.7		0.0	0.0	13.7		0.1				

Intersection Summary

HCM 6th Ctrl Delay	8.2
HCM 6th LOS	A

Notes

User approved pedestrian interval to be less than phase max green.  
 User approved ignoring U-Turning movement.  
 \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

# HCM 6th Signalized Intersection Summary

## 4: US 41 & Gaines Ave/Tallevast Rd

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↶	↷		↶	↷		↶	↷	↷	↶	↷	↷
Traffic Volume (veh/h)	3	5	3	92	5	373	4	1961	37	108	1086	3
Future Volume (veh/h)	3	5	3	92	5	373	4	1961	37	108	1086	3
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	0.99		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1856	1900	1885	1900	1885	1856	1870	1885	1411
Adj Flow Rate, veh/h	3	5	3	95	5	385	4	2022	38	111	1120	3
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Percent Heavy Veh, %	0	0	0	3	0	1	0	1	3	2	1	33
Cap, veh/h	45	187	112	275	3	267	389	3419	64	196	3636	10
Arrive On Green	0.17	0.17	0.17	0.17	0.17	0.17	0.01	0.66	0.66	0.07	1.00	1.00
Sat Flow, veh/h	1010	1102	661	1387	20	1569	1810	5201	98	1781	5299	14
Grp Volume(v), veh/h	3	0	8	95	0	390	4	1333	727	111	725	398
Grp Sat Flow(s),veh/h/ln1010	0	1764	1387	0	1589	1810	1716	1868	1781	1716	1883	
Q Serve(g_s), s	0.0	0.0	0.6	9.8	0.0	27.2	0.1	34.8	34.9	3.4	0.0	0.0
Cycle Q Clear(g_c), s	27.2	0.0	0.6	10.4	0.0	27.2	0.1	34.8	34.9	3.4	0.0	0.0
Prop In Lane	1.00		0.38	1.00		0.99	1.00		0.05	1.00		0.01
Lane Grp Cap(c), veh/h	45	0	300	275	0	270	389	2255	1228	196	2354	1292
V/C Ratio(X)	0.07	0.00	0.03	0.34	0.00	1.44	0.01	0.59	0.59	0.57	0.31	0.31
Avail Cap(c_a), veh/h	45	0	300	275	0	270	468	2255	1228	333	2354	1292
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	0.96	0.96	0.96
Uniform Delay (d), s/veh	80.0	0.0	55.4	59.7	0.0	66.4	9.1	15.4	15.4	15.6	0.0	0.0
Incr Delay (d2), s/veh	0.6	0.0	0.0	0.7	0.0	219.3	0.0	1.1	2.1	2.5	0.3	0.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.1	0.0	0.3	3.6	0.0	27.6	0.0	12.9	14.3	1.5	0.1	0.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	80.6	0.0	55.4	60.4	0.0	285.7	9.1	16.5	17.5	18.1	0.3	0.6
LnGrp LOS	F	A	E	E	A	F	A	B	B	B	A	A
Approach Vol, veh/h		11			485			2064			1234	
Approach Delay, s/veh		62.3			241.6			16.8			2.0	
Approach LOS		E			F			B			A	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	8.0	117.0		35.0	12.6	112.4		35.0				
Change Period (Y+Rc), s	7.2	7.2		7.8	7.1	7.2		7.8				
Max Green Setting (Gmax), s	7.8	102.8		27.2	17.9	92.8		27.2				
Max Q Clear Time (g_c+1/2), s	7.8	2.0		29.2	5.4	36.9		29.2				
Green Ext Time (p_c), s	0.0	20.6		0.0	0.2	43.0		0.0				

### Intersection Summary

HCM 6th Ctrl Delay	40.9
HCM 6th LOS	D

### Notes

- User approved pedestrian interval to be less than phase max green.
- User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary  
5: US 41 & Whitfield Ave

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖ ↑↑↑	↗ ↑↑↑		↖ ↑↑↑	↗ ↑↑↑	
Traffic Volume (veh/h)	5	5	5	38	3	31	4	2236	87	37	1135	0
Future Volume (veh/h)	5	5	5	38	3	31	4	2236	87	37	1135	0
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.97	0.99		1.00	1.00		0.98	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No		No		No		No		No		No
Adj Sat Flow, veh/h/ln	1604	1900	1900	1900	1900	1900	1900	1885	1856	1856	1885	1900
Adj Flow Rate, veh/h	6	6	6	45	4	36	5	2631	102	44	1335	0
Peak Hour Factor	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85
Percent Heavy Veh, %	20	0	0	0	0	0	0	1	3	3	1	0
Cap, veh/h	77	44	44	108	8	75	383	3985	153	173	4123	0
Arrive On Green	0.05	0.05	0.05	0.05	0.05	0.05	0.01	1.00	1.00	0.05	1.00	0.00
Sat Flow, veh/h	1172	857	857	1417	164	1472	1810	5081	195	1767	5316	0
Grp Volume(v), veh/h	6	0	12	45	0	40	5	1767	966	44	1335	0
Grp Sat Flow(s),veh/h/ln	1172	0	1714	1417	0	1635	1810	1716	1845	1767	1716	0
Q Serve(g_s), s	0.8	0.0	1.1	5.0	0.0	3.8	0.1	0.0	0.0	0.8	0.0	0.0
Cycle Q Clear(g_c), s	4.6	0.0	1.1	6.1	0.0	3.8	0.1	0.0	0.0	0.8	0.0	0.0
Prop In Lane	1.00		0.50	1.00		0.90	1.00		0.11	1.00		0.00
Lane Grp Cap(c), veh/h	77	0	87	108	0	83	383	2691	1447	173	4123	0
V/C Ratio(X)	0.08	0.00	0.14	0.42	0.00	0.48	0.01	0.66	0.67	0.25	0.32	0.00
Avail Cap(c_a), veh/h	98	0	119	134	0	113	452	2691	1447	211	4123	0
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.33	1.33	1.33	2.00	2.00	2.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	0.70	0.70	0.70	0.96	0.96	0.00
Uniform Delay (d), s/veh	76.1	0.0	72.6	75.5	0.0	73.9	3.5	0.0	0.0	2.8	0.0	0.0
Incr Delay (d2), s/veh	0.4	0.0	0.7	2.6	0.0	4.3	0.0	0.9	1.7	0.7	0.2	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.3	0.0	0.5	1.9	0.0	1.7	0.0	0.3	0.7	0.2	0.1	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	76.6	0.0	73.3	78.1	0.0	78.1	3.5	0.9	1.7	3.6	0.2	0.0
LnGrp LOS	E	A	E	E	A	E	A	A	A	A	A	A
Approach Vol, veh/h		18		85		2738		1379				
Approach Delay, s/veh		74.4		78.1		1.2		0.3				
Approach LOS		E		E		A		A				
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	8.9	136.1		15.0	11.6	133.4		15.0				
Change Period (Y+Rc), s	7.9	* 7.9		6.9	* 7.3	* 7.9		6.9				
Max Green Setting (Gmax), s	1.2E2			11.1	* 7.7	1.2E2		11.1				
Max Q Clear Time (g_c+1), s		2.0		8.1	2.8	2.0		6.6				
Green Ext Time (p_c), s	0.0	30.8		0.1	0.0	103.2		0.0				

Intersection Summary

HCM 6th Ctrl Delay	2.8
HCM 6th LOS	A

Notes

- User approved pedestrian interval to be less than phase max green.
- User approved ignoring U-Turning movement.
- \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

# HCM 6th Signalized Intersection Summary

## 6: US 41 & Pearl Ave

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕		↙	↘		↙	↑↑↑		↙	↑↑↑	
Traffic Volume (veh/h)	21	8	6	34	6	171	8	2190	64	57	1137	15
Future Volume (veh/h)	21	8	6	34	6	171	8	2190	64	57	1137	15
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		1.00	1.00		0.98	1.00		0.98
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1885	1900	1885	1856	1870	1870	1900
Adj Flow Rate, veh/h	24	9	7	40	7	199	9	2547	74	66	1322	17
Peak Hour Factor	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86
Percent Heavy Veh, %	0	0	0	0	0	1	0	1	3	2	2	0
Cap, veh/h	61	22	10	224	8	240	328	3508	101	177	3931	51
Arrive On Green	0.15	0.15	0.15	0.15	0.15	0.15	1.00	1.00	1.00	0.06	1.00	1.00
Sat Flow, veh/h	162	143	65	1416	55	1564	415	5137	148	1781	5194	67
Grp Volume(v), veh/h	40	0	0	40	0	206	9	1695	926	66	866	473
Grp Sat Flow(s),veh/h/ln	369	0	0	1416	0	1619	415	1716	1854	1781	1702	1857
Q Serve(g_s), s	2.8	0.0	0.0	0.0	0.0	19.7	0.0	0.0	0.0	1.7	0.0	0.0
Cycle Q Clear(g_c), s	22.6	0.0	0.0	4.9	0.0	19.7	0.0	0.0	0.0	1.7	0.0	0.0
Prop In Lane	0.60		0.17	1.00		0.97	1.00		0.08	1.00		0.04
Lane Grp Cap(c), veh/h	93	0	0	224	0	249	328	2343	1266	177	2576	1405
V/C Ratio(X)	0.43	0.00	0.00	0.18	0.00	0.83	0.03	0.72	0.73	0.37	0.34	0.34
Avail Cap(c_a), veh/h	97	0	0	228	0	254	328	2343	1266	257	2576	1405
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	2.00	2.00	2.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	1.00	0.70	0.70	0.70	0.93	0.93	0.93
Uniform Delay (d), s/veh	68.8	0.0	0.0	59.4	0.0	65.6	0.0	0.0	0.0	6.1	0.0	0.0
Incr Delay (d2), s/veh	3.1	0.0	0.0	0.4	0.0	19.5	0.1	1.4	2.6	1.2	0.3	0.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	7	0.0	0.0	1.5	0.0	9.5	0.0	0.5	0.9	0.6	0.1	0.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	71.9	0.0	0.0	59.7	0.0	85.2	0.1	1.4	2.6	7.3	0.3	0.6
LnGrp LOS	E	A	A	E	A	F	A	A	A	A	A	A
Approach Vol, veh/h		40			246			2630			1405	
Approach Delay, s/veh		71.9			81.0			1.8			0.7	
Approach LOS		E			F			A			A	
Timer - Assigned Phs		2		4	5	6		8				
Phs Duration (G+Y+Rc), s		128.5		31.5	11.8	116.7		31.5				
Change Period (Y+Rc), s		7.4		6.9	7.1	7.4		6.9				
Max Green Setting (Gmax), s		120.6		25.1	11.9	101.6		25.1				
Max Q Clear Time (g_c+I1), s		2.0		21.7	3.7	2.0		24.6				
Green Ext Time (p_c), s		29.3		0.4	0.1	86.9		0.0				

### Intersection Summary

HCM 6th Ctrl Delay	6.6
HCM 6th LOS	A

### Notes

- User approved pedestrian interval to be less than phase max green.
- User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary  
 7: US 41 & Bay Dr/69th Ave W

Existing PM  
 09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	78	2	52	13	6	12	145	2222	23	6	1169	35
Future Volume (veh/h)	78	2	52	13	6	12	145	2222	23	6	1169	35
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.99	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No		No		No		No	
Adj Sat Flow, veh/h/ln	1885	1900	1870	1900	1648	1900	1900	1885	1900	1648	1885	1856
Adj Flow Rate, veh/h	93	2	62	15	7	14	173	2645	27	7	1392	42
Peak Hour Factor	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Percent Heavy Veh, %	1	0	2	0	17	0	0	1	0	17	1	3
Cap, veh/h	156	4	133	58	28	35	310	4322	44	117	3797	115
Arrive On Green	0.09	0.09	0.09	0.09	0.09	0.09	0.08	1.00	1.00	0.24	0.24	0.24
Sat Flow, veh/h	1398	50	1545	306	328	403	1810	5253	53	97	5133	155
Grp Volume(v), veh/h	93	0	64	36	0	0	173	1725	947	7	930	504
Grp Sat Flow(s),veh/h/ln	1398	0	1595	1037	0	0	1810	1716	1876	97	1716	1857
Q Serve(g_s), s	4.4	0.0	6.1	1.0	0.0	0.0	3.8	0.0	0.0	8.9	36.0	36.0
Cycle Q Clear(g_c), s	11.5	0.0	6.1	7.1	0.0	0.0	3.8	0.0	0.0	8.9	36.0	36.0
Prop In Lane	1.00		0.97	0.42		0.39	1.00		0.03	1.00		0.08
Lane Grp Cap(c), veh/h	156	0	137	121	0	0	310	2823	1543	117	2538	1374
V/C Ratio(X)	0.60	0.00	0.47	0.30	0.00	0.00	0.56	0.61	0.61	0.06	0.37	0.37
Avail Cap(c_a), veh/h	180	0	164	144	0	0	533	2823	1543	117	2538	1374
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	0.33	0.33	0.33
Upstream Filter(l)	1.00	0.00	1.00	1.00	0.00	0.00	0.64	0.64	0.64	0.86	0.86	0.86
Uniform Delay (d), s/veh	72.3	0.0	69.6	69.2	0.0	0.0	11.5	0.0	0.0	19.1	29.4	29.4
Incr Delay (d2), s/veh	4.0	0.0	2.5	1.4	0.0	0.0	1.0	0.6	1.2	0.8	0.4	0.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	2.6	1.5	0.0	0.0	2.2	0.3	0.5	0.2	16.7	18.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	76.4	0.0	72.1	70.6	0.0	0.0	12.5	0.6	1.2	20.0	29.7	30.0
LnGrp LOS	E	A	E	E	A	A	B	A	A	B	C	C
Approach Vol, veh/h		157			36			2845			1441	
Approach Delay, s/veh		74.6			70.6			1.5			29.8	
Approach LOS		E			E			A			C	
Timer - Assigned Phs	1	2		4		6		8				
Phs Duration (G+Y+Rc), s	125.4			21.3		138.7		21.3				
Change Period (Y+Rc), s	* 7.1	* 7.1		7.5		* 7.1		7.5				
Max Green Setting (Gmax), s	* 96			16.5		* 1.3E2		16.5				
Max Q Clear Time (g_c+1), s	38.0			9.1		2.0		13.5				
Green Ext Time (p_c), s	0.4	29.4		0.0		109.7		0.2				

Intersection Summary

HCM 6th Ctrl Delay	13.7
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.



Lanes, Volumes, Timings  
8: US 41 & Florida Blvd

Existing PM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	79	1	185	6	10	19	416	1850	8	15	1042	85
Future Volume (vph)	79	1	185	6	10	19	416	1850	8	15	1042	85
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	220		0	0		0	175		0	160		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	0			0			0			0		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.91	0.91	1.00	0.91	0.91
Frt		0.851			0.903			0.999			0.989	
Flt Protected	0.950			0.950			0.950			0.950		
Satd. Flow (prot)	1787	1585	0	1805	1716	0	1787	5081	0	1805	5037	0
Flt Permitted	0.735			0.248			0.138			0.080		
Satd. Flow (perm)	1383	1585	0	471	1716	0	260	5081	0	152	5037	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		215			21			1				12
Link Speed (mph)		25			25			45				45
Link Distance (ft)		745			154			1576				2173
Travel Time (s)		20.3			4.2			23.9				32.9
Peak Hour Factor	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86
Heavy Vehicles (%)	1%	0%	2%	0%	0%	0%	1%	2%	0%	0%	2%	0%
Adj. Flow (vph)	92	1	215	7	12	22	484	2151	9	17	1212	99
Shared Lane Traffic (%)												
Lane Group Flow (vph)	92	216	0	7	34	0	484	2160	0	17	1311	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		12			12			12				12
Link Offset(ft)		0			0			0				0
Crosswalk Width(ft)		16			16			16				16
Two way Left Turn Lane		Yes						Yes				Yes
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Number of Detectors	1	2		1	2		1	2		1	2	
Detector Template	Left	Thru		Left	Thru		Left	Thru		Left	Thru	
Leading Detector (ft)	20	100		20	100		20	100		20	100	
Trailing Detector (ft)	0	0		0	0		0	0		0	0	
Detector 1 Position(ft)	0	0		0	0		0	0		0	0	
Detector 1 Size(ft)	20	6		20	6		20	6		20	6	
Detector 1 Type	Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex	
Detector 1 Channel												
Detector 1 Extend (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Queue (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Delay (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 2 Position(ft)		94			94			94			94	
Detector 2 Size(ft)		6			6			6			6	
Detector 2 Type		Cl+Ex			Cl+Ex			Cl+Ex			Cl+Ex	
Detector 2 Channel												
Detector 2 Extend (s)		0.0			0.0			0.0			0.0	
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		Perm	NA	
Protected Phases		4			4		1	6			2	

Lanes, Volumes, Timings  
8: US 41 & Florida Blvd

Existing PM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Permitted Phases	4			4			6			2		
Detector Phase	4	4		4	4		1	6		2	2	
Switch Phase												
Minimum Initial (s)	7.0	7.0		7.0	7.0		5.0	15.0		15.0	15.0	
Minimum Split (s)	38.2	38.2		38.2	38.2		11.8	32.1		28.1	28.1	
Total Split (s)	35.0	35.0		35.0	35.0		35.0	125.0		90.0	90.0	
Total Split (%)	21.9%	21.9%		21.9%	21.9%		21.9%	78.1%		56.3%	56.3%	
Maximum Green (s)	27.8	27.8		27.8	27.8		28.2	117.9		82.9	82.9	
Yellow Time (s)	3.4	3.4		3.4	3.4		4.8	4.8		4.8	4.8	
All-Red Time (s)	3.8	3.8		3.8	3.8		2.0	2.3		2.3	2.3	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)	7.2	7.2		7.2	7.2		6.8	7.1		7.1	7.1	
Lead/Lag							Lead			Lag	Lag	
Lead-Lag Optimize?							Yes			Yes	Yes	
Vehicle Extension (s)	3.0	3.0		3.0	3.0		3.0	5.0		5.0	5.0	
Recall Mode	None	None		None	None		None	C-Max		C-Max	C-Max	
Walk Time (s)	7.0	7.0		7.0	7.0			7.0		7.0	7.0	
Flash Dont Walk (s)	24.0	24.0		24.0	24.0			18.0		14.0	14.0	
Pedestrian Calls (#/hr)	0	0		0	0			0		0	0	
Act Effct Green (s)	16.1	16.1		16.1	16.1		129.9	129.6		84.0	84.0	
Actuated g/C Ratio	0.10	0.10		0.10	0.10		0.81	0.81		0.52	0.52	
v/c Ratio	0.66	0.61		0.15	0.18		0.83	0.52		0.22	0.49	
Control Delay	90.3	15.2		69.5	34.2		58.6	5.2		60.6	57.7	
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Delay	90.3	15.2		69.5	34.2		58.6	5.2		60.6	57.7	
LOS	F	B		E	C		E	A		E	E	
Approach Delay		37.7			40.2			15.0			57.7	
Approach LOS		D			D			B			E	

Intersection Summary

Area Type:	Other
Cycle Length:	160
Actuated Cycle Length:	160
Offset:	16 (10%), Referenced to phase 2:SBTL and 6:NBTL, Start of Green
Natural Cycle:	100
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.83
Intersection Signal Delay:	30.0
Intersection LOS:	C
Intersection Capacity Utilization	77.8%
ICU Level of Service	D
Analysis Period (min)	15

Splits and Phases: 8: US 41 & Florida Blvd



Intersection												
Int Delay, s/veh	1.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕ ↑↑↑			↕ ↑↑↑		
Traffic Vol, veh/h	3	0	6	5	1	25	2	1982	0	18	1157	9
Future Vol, veh/h	3	0	6	5	1	25	2	1982	0	18	1157	9
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	100	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	95	95	95	95	95	95	95	95	95	95	95	95
Heavy Vehicles, %	0	0	0	0	0	0	0	2	0	6	2	0
Mvmt Flow	3	0	6	5	1	26	2	2086	0	19	1218	9

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2100	3351	614	2615	3355	1043	1227	0	0	2086	0	0
Stage 1	1261	1261	-	2090	2090	-	-	-	-	-	-	-
Stage 2	839	2090	-	525	1265	-	-	-	-	-	-	-
Critical Hdwy	6.4	6.5	7.1	6.4	6.5	7.1	5.3	-	-	5.42	-	-
Critical Hdwy Stg 1	7.3	5.5	-	7.3	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.7	5.5	-	6.7	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.8	4	3.9	3.8	4	3.9	3.1	-	-	3.16	-	-
Pot Cap-1 Maneuver	56	8	377	27	8	197	309	-	-	107	-	-
Stage 1	133	244	-	34	95	-	-	-	-	-	-	-
Stage 2	300	95	-	464	243	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	37	7	377	23	7	197	309	-	-	107	-	-
Mov Cap-2 Maneuver	37	7	-	23	7	-	-	-	-	-	-	-
Stage 1	132	201	-	34	94	-	-	-	-	-	-	-
Stage 2	255	94	-	375	200	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	48.1		109.3		0		0.7	
HCM LOS	E		F					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	309	-	-	93	64	107	-
HCM Lane V/C Ratio	0.007	-	-	0.102	0.51	0.177	-
HCM Control Delay (s)	16.7	-	-	48.1	109.3	45.7	-
HCM Lane LOS	C	-	-	E	F	E	-
HCM 95th %tile Q(veh)	0	-	-	0.3	2	0.6	-

HCM 6th Signalized Intersection Summary  
10: US 41 & Bayshore Gardens Pkwy

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕		↵	↕		↵	↕		↵	↕	
Traffic Volume (veh/h)	99	303	251	85	507	146	413	1557	24	164	845	104
Future Volume (veh/h)	99	303	251	85	507	146	413	1557	24	164	845	104
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.98	1.00		0.99	1.00		0.99
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1870	1870	1885	1885	1885	1885	1885	1841	1885	1870	1885
Adj Flow Rate, veh/h	102	312	259	88	523	151	426	1605	25	169	871	107
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Percent Heavy Veh, %	0	2	2	1	1	1	1	1	4	1	2	1
Cap, veh/h	123	403	327	108	584	168	403	2310	36	190	1486	182
Arrive On Green	0.07	0.22	0.22	0.06	0.21	0.21	0.22	0.44	0.44	0.11	0.32	0.32
Sat Flow, veh/h	1810	1864	1511	1795	2732	785	1795	5219	81	1795	4601	563
Grp Volume(v), veh/h	102	297	274	88	342	332	426	1055	575	169	643	335
Grp Sat Flow(s),veh/h/ln	1810	1777	1598	1795	1791	1726	1795	1716	1869	1795	1702	1760
Q Serve(g_s), s	8.9	25.2	25.9	7.8	29.7	30.0	35.9	39.6	39.6	14.9	25.2	25.4
Cycle Q Clear(g_c), s	8.9	25.2	25.9	7.8	29.7	30.0	35.9	39.6	39.6	14.9	25.2	25.4
Prop In Lane	1.00		0.95	1.00		0.45	1.00		0.04	1.00		0.32
Lane Grp Cap(c), veh/h	123	385	346	108	383	369	403	1519	827	190	1099	568
V/C Ratio(X)	0.83	0.77	0.79	0.82	0.89	0.90	1.06	0.69	0.69	0.89	0.59	0.59
Avail Cap(c_a), veh/h	150	425	383	140	428	412	403	1519	827	214	1099	568
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	73.7	59.0	59.3	74.3	61.1	61.3	62.1	35.9	35.9	70.6	45.2	45.3
Incr Delay (d2), s/veh	26.7	7.8	9.9	24.0	19.3	21.0	60.8	2.6	4.8	31.2	2.3	4.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.1	12.2	11.5	4.3	15.6	15.3	22.9	16.7	18.7	8.4	10.9	11.7
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	100.3	66.8	69.2	98.4	80.4	82.2	122.9	38.5	40.7	101.8	47.5	49.7
LnGrp LOS	F	E	E	F	F	F	F	D	D	F	D	D
Approach Vol, veh/h		673			762			2056			1147	
Approach Delay, s/veh		72.8			83.3			56.6			56.1	
Approach LOS		E			F			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	43.0	58.5	17.5	41.0	23.8	77.6	17.1	41.4				
Change Period (Y+Rc), s	* 7.1	6.8	* 6.7	* 6.8	6.9	6.8	7.5	* 6.8				
Max Green Setting (Gmax), s	* 36	45.2	* 13	* 38	19.1	62.2	12.5	* 38				
Max Q Clear Time (g_c+I1), s	37.9	27.4	10.9	32.0	16.9	41.6	9.8	27.9				
Green Ext Time (p_c), s	0.0	9.5	0.0	2.2	0.1	16.2	0.0	2.6				

Intersection Summary

HCM 6th Ctrl Delay	63.2
HCM 6th LOS	E

Notes

User approved ignoring U-Turning movement.

\* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

HCM 6th Signalized Intersection Summary  
11: US 41 & 60th Ave W

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖ ↗	↖ ↗		↖ ↗	↖ ↗		↖ ↗ ↘ ↙	↖ ↗ ↘ ↙		↖ ↗ ↘ ↙	↖ ↗ ↘ ↙	
Traffic Volume (veh/h)	138	106	76	41	81	69	182	1578	40	129	1017	103
Future Volume (veh/h)	138	106	76	41	81	69	182	1578	40	129	1017	103
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	0.99		0.97	0.99		0.97	1.00		0.98	1.00		0.98
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1885	1870	1856	1870	1900	1885	1870	1870	1900	1900	1870	1870
Adj Flow Rate, veh/h	139	107	77	41	82	70	184	1594	40	130	1027	104
Peak Hour Factor	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99
Percent Heavy Veh, %	1	2	3	2	0	1	2	2	0	0	2	2
Cap, veh/h	223	167	120	174	111	95	434	3017	76	251	2711	274
Arrive On Green	0.08	0.17	0.17	0.03	0.12	0.12	0.06	0.59	0.59	0.09	1.00	1.00
Sat Flow, veh/h	1795	999	719	1781	931	795	1781	5119	128	1810	4701	475
Grp Volume(v), veh/h	139	0	184	41	0	152	184	1060	574	130	743	388
Grp Sat Flow(s),veh/h/ln	1795	0	1718	1781	0	1726	1781	1702	1844	1810	1702	1773
Q Serve(g_s), s	10.6	0.0	16.0	3.2	0.0	13.6	6.8	29.7	29.7	4.9	0.0	0.0
Cycle Q Clear(g_c), s	10.6	0.0	16.0	3.2	0.0	13.6	6.8	29.7	29.7	4.9	0.0	0.0
Prop In Lane	1.00		0.42	1.00		0.46	1.00		0.07	1.00		0.27
Lane Grp Cap(c), veh/h	223	0	288	174	0	206	434	2006	1086	251	1963	1022
V/C Ratio(X)	0.62	0.00	0.64	0.24	0.00	0.74	0.42	0.53	0.53	0.52	0.38	0.38
Avail Cap(c_a), veh/h	223	0	355	216	0	314	590	2006	1086	456	1963	1022
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	0.84	0.84	0.84
Uniform Delay (d), s/veh	55.3	0.0	62.1	59.8	0.0	68.0	12.0	19.6	19.6	15.6	0.0	0.0
Incr Delay (d2), s/veh	5.3	0.0	2.7	0.7	0.0	5.1	0.7	1.0	1.8	1.4	0.5	0.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.2	0.0	7.2	1.5	0.0	6.4	2.7	11.6	12.8	1.9	0.1	0.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	60.6	0.0	64.7	60.5	0.0	73.1	12.6	20.6	21.4	17.0	0.5	0.9
LnGrp LOS	E	A	E	E	A	E	B	C	C	B	A	A
Approach Vol, veh/h		323			193			1818			1261	
Approach Delay, s/veh		62.9			70.4			20.1			2.3	
Approach LOS		E			E			C			A	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	15.9	99.1	19.0	26.0	13.9	101.1	11.3	33.7				
Change Period (Y+Rc), s	6.8	6.8	6.9	6.9	6.8	6.8	6.9	6.9				
Max Green Setting (Gmax), s	23.2	68.2	12.1	29.1	25.2	66.2	8.1	33.1				
Max Q Clear Time (g_c+1), s	19.8	2.0	12.6	15.6	6.9	31.7	5.2	18.0				
Green Ext Time (p_c), s	0.4	20.5	0.0	0.7	0.3	24.3	0.0	0.9				

Intersection Summary

HCM 6th Ctrl Delay	20.4
HCM 6th LOS	C

Notes

User approved pedestrian interval to be less than phase max green.  
User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary  
12: US 41 & 57th Ave W

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗	↖	↖↗↘	↖↗↘		↖↗↘	↖↗↘	
Traffic Volume (veh/h)	139	227	71	132	270	154	210	1437	144	189	1030	100
Future Volume (veh/h)	139	227	71	132	270	154	210	1437	144	189	1030	100
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		0.98	1.00		0.98	1.00		0.98
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1885	1856	1900	1885	1885	1885	1900	1870	1856	1885	1870	1900
Adj Flow Rate, veh/h	145	236	74	138	281	160	219	1497	150	197	1073	104
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	1	3	0	1	1	1	0	2	3	1	2	0
Cap, veh/h	221	256	80	188	342	283	322	2324	233	315	2282	221
Arrive On Green	0.08	0.19	0.19	0.07	0.18	0.18	0.16	0.99	0.99	0.02	0.16	0.16
Sat Flow, veh/h	1795	1347	422	1795	1885	1564	1810	4705	471	1795	4722	457
Grp Volume(v), veh/h	145	0	310	138	281	160	219	1083	564	197	773	404
Grp Sat Flow(s),veh/h/ln	1795	0	1769	1795	1885	1564	1810	1702	1772	1795	1702	1776
Q Serve(g_s), s	10.4	0.0	27.5	10.0	22.9	14.9	10.2	1.7	1.7	8.6	33.0	33.1
Cycle Q Clear(g_c), s	10.4	0.0	27.5	10.0	22.9	14.9	10.2	1.7	1.7	8.6	33.0	33.1
Prop In Lane	1.00		0.24	1.00		1.00	1.00		0.27	1.00		0.26
Lane Grp Cap(c), veh/h	221	0	337	188	342	283	322	1681	875	315	1645	858
V/C Ratio(X)	0.66	0.00	0.92	0.73	0.82	0.56	0.68	0.64	0.64	0.62	0.47	0.47
Avail Cap(c_a), veh/h	238	0	398	188	389	322	424	1681	875	425	1645	858
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	0.33	0.33	0.33
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	1.00	0.80	0.80	0.80	0.55	0.55	0.55
Uniform Delay (d), s/veh	49.7	0.0	63.6	51.2	63.0	59.7	21.4	0.5	0.5	18.9	48.6	48.6
Incr Delay (d2), s/veh	5.8	0.0	24.2	13.8	12.0	1.8	2.3	1.5	2.9	1.1	0.5	1.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.1	0.0	14.6	5.2	12.0	6.1	3.8	0.6	1.0	3.9	15.2	16.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	55.5	0.0	87.8	64.9	75.0	61.5	23.7	2.0	3.4	20.0	49.2	49.7
LnGrp LOS	E	A	F	E	E	E	C	A	A	C	D	D
Approach Vol, veh/h		455			579			1866			1374	
Approach Delay, s/veh		77.5			68.9			5.0			45.1	
Approach LOS		E			E			A			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	19.9	84.6	19.4	36.0	18.2	86.3	18.0	37.5				
Change Period (Y+Rc), s	7.3	7.3	7.0	7.0	7.3	7.3	7.0	7.0				
Max Green Setting (Gmax), s	21.7	62.7	14.0	33.0	20.7	63.7	11.0	36.0				
Max Q Clear Time (g_c+1/2), s	11.2	35.1	12.4	24.9	10.6	3.7	12.0	29.5				
Green Ext Time (p_c), s	0.4	15.1	0.1	1.3	0.4	35.3	0.0	0.9				

Intersection Summary

HCM 6th Ctrl Delay	34.3
HCM 6th LOS	C

Notes

User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary  
13: US 41 & 53rd Ave W

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	135	722	127	184	675	140	244	1296	212	158	988	144
Future Volume (veh/h)	135	722	127	184	675	140	244	1296	212	158	988	144
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.97	1.00		0.97	1.00		0.98	1.00		0.98
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No		No		No		No		No		No
Adj Sat Flow, veh/h/ln	1856	1885	1885	1870	1870	1841	1885	1870	1885	1841	1885	1856
Adj Flow Rate, veh/h	136	729	128	186	682	141	246	1309	214	160	998	145
Peak Hour Factor	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99
Percent Heavy Veh, %	3	1	1	2	2	4	1	2	1	4	1	3
Cap, veh/h	207	787	138	221	818	169	255	1628	266	178	1488	216
Arrive On Green	0.07	0.26	0.26	0.09	0.28	0.28	0.10	0.25	0.25	0.20	0.66	0.66
Sat Flow, veh/h	1767	3032	532	1781	2917	602	1795	4409	721	1753	4526	656
Grp Volume(v), veh/h	136	430	427	186	415	408	246	1011	512	160	756	387
Grp Sat Flow(s),veh/h/ln	1767	1791	1773	1781	1777	1742	1795	1702	1726	1753	1716	1751
Q Serve(g_s), s	9.0	37.5	37.5	12.1	35.1	35.2	21.8	44.6	44.6	14.2	21.6	21.7
Cycle Q Clear(g_c), s	9.0	37.5	37.5	12.1	35.1	35.2	21.8	44.6	44.6	14.2	21.6	21.7
Prop In Lane	1.00		0.30	1.00		0.35	1.00		0.42	1.00		0.37
Lane Grp Cap(c), veh/h	207	465	460	221	498	489	255	1257	637	178	1128	576
V/C Ratio(X)	0.66	0.93	0.93	0.84	0.83	0.83	0.97	0.80	0.80	0.90	0.67	0.67
Avail Cap(c_a), veh/h	249	491	487	283	543	533	255	1257	637	183	1128	576
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	0.67	0.67	0.67	2.00	2.00	2.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	0.66	0.66	0.66	0.96	0.96	0.96
Uniform Delay (d), s/veh	43.1	57.7	57.7	43.0	54.1	54.1	72.0	54.8	54.8	63.0	22.1	22.1
Incr Delay (d2), s/veh	4.7	23.0	23.3	16.3	10.1	10.3	36.8	3.7	7.1	37.9	3.0	5.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	4.2	19.7	19.6	6.3	16.9	16.7	12.8	20.2	21.1	7.5	6.2	6.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	47.7	80.8	81.1	59.3	64.1	64.4	108.8	58.5	61.9	100.8	25.1	28.0
LnGrp LOS	D	F	F	E	E	E	F	E	E	F	C	C
Approach Vol, veh/h		993			1009			1769			1303	
Approach Delay, s/veh		76.4			63.4			66.5			35.3	
Approach LOS		E			E			E			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	30.0	59.9	18.1	52.0	23.5	66.4	21.5	48.6				
Change Period (Y+Rc), s	7.3	7.3	* 7.1	* 7.1	7.3	7.3	* 7.1	* 7.1				
Max Green Setting (Gmax), s	22.7	44.7	* 15	* 49	16.7	50.7	* 20	* 44				
Max Q Clear Time (g_c+Q), s	23.8	23.7	11.0	37.2	16.2	46.6	14.1	39.5				
Green Ext Time (p_c), s	0.0	12.3	0.1	3.8	0.0	3.6	0.2	2.0				

Intersection Summary

HCM 6th Ctrl Delay	59.8
HCM 6th LOS	E

Notes

User approved ignoring U-Turning movement.  
\* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings  
14: US 41 & 49th Ave W/49th Dr W

Existing PM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	51	4	71	21	0	28	65	1473	29	26	1217	38
Future Volume (vph)	51	4	71	21	0	28	65	1473	29	26	1217	38
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	0		0	125		0	110		0
Storage Lanes	1		0	0		0	1		0	1		0
Taper Length (ft)	0			0			0			0		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.91	0.91	1.00	0.91	0.91
Frt		0.858			0.923			0.997			0.995	
Flt Protected	0.950				0.979		0.950			0.950		
Satd. Flow (prot)	1805	1630	0	0	1651	0	1805	5065	0	1805	5112	0
Flt Permitted	0.780				0.822		0.195			0.146		
Satd. Flow (perm)	1482	1630	0	0	1386	0	370	5065	0	277	5112	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		74			29			6			11	
Link Speed (mph)		25			25			45			45	
Link Distance (ft)		171			252			2357			1525	
Travel Time (s)		4.7			6.9			35.7			23.1	
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	0%	0%	0%	0%	7%	0%	2%	7%	0%	1%	0%
Adj. Flow (vph)	53	4	74	22	0	29	68	1534	30	27	1268	40
Shared Lane Traffic (%)												
Lane Group Flow (vph)	53	78	0	0	51	0	68	1564	0	27	1308	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		12			12			12			12	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane								Yes			Yes	
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Number of Detectors	1	2		1	2		1	2		1	2	
Detector Template	Left	Thru		Left	Thru		Left	Thru		Left	Thru	
Leading Detector (ft)	20	100		20	100		20	100		20	100	
Trailing Detector (ft)	0	0		0	0		0	0		0	0	
Detector 1 Position(ft)	0	0		0	0		0	0		0	0	
Detector 1 Size(ft)	20	6		20	6		20	6		20	6	
Detector 1 Type	Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex	
Detector 1 Channel												
Detector 1 Extend (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Queue (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Delay (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 2 Position(ft)		94			94			94			94	
Detector 2 Size(ft)		6			6			6			6	
Detector 2 Type		Cl+Ex			Cl+Ex			Cl+Ex			Cl+Ex	
Detector 2 Channel												
Detector 2 Extend (s)		0.0			0.0			0.0			0.0	
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			4			2			2	



Lanes, Volumes, Timings  
14: US 41 & 49th Ave W/49th Dr W

Existing PM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Permitted Phases	4			4			2			2		
Detector Phase	4	4		4	4		2	2		2	2	
Switch Phase												
Minimum Initial (s)	7.0	7.0		7.0	7.0		15.0	15.0		15.0	15.0	
Minimum Split (s)	39.7	39.7		39.7	39.7		32.8	32.8		32.8	32.8	
Total Split (s)	24.0	24.0		24.0	24.0		136.0	136.0		136.0	136.0	
Total Split (%)	15.0%	15.0%		15.0%	15.0%		85.0%	85.0%		85.0%	85.0%	
Maximum Green (s)	17.3	17.3		17.3	17.3		129.2	129.2		129.2	129.2	
Yellow Time (s)	3.7	3.7		3.7	3.7		4.8	4.8		4.8	4.8	
All-Red Time (s)	3.0	3.0		3.0	3.0		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	0.0	0.0			0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)	6.7	6.7			6.7		6.8	6.8		6.8	6.8	
Lead/Lag												
Lead-Lag Optimize?												
Vehicle Extension (s)	3.0	3.0		3.0	3.0		5.0	5.0		5.0	5.0	
Recall Mode	None	None		None	None		C-Min	C-Min		C-Min	C-Min	
Walk Time (s)	7.0	7.0		7.0	7.0		7.0	7.0		7.0	7.0	
Flash Dont Walk (s)	26.0	26.0		26.0	26.0		19.0	19.0		19.0	19.0	
Pedestrian Calls (#/hr)	0	0		0	0		0	0		0	0	
Act Effct Green (s)	11.3	11.3			11.3		135.2	135.2		135.2	135.2	
Actuated g/C Ratio	0.07	0.07			0.07		0.84	0.84		0.84	0.84	
v/c Ratio	0.51	0.43			0.41		0.22	0.37		0.12	0.30	
Control Delay	87.6	22.4			45.2		2.1	1.3		3.6	2.6	
Queue Delay	0.0	0.0			0.0		0.0	0.0		0.0	0.0	
Total Delay	87.6	22.4			45.2		2.1	1.3		3.6	2.6	
LOS	F	C			D		A	A		A	A	
Approach Delay		48.8			45.2			1.4			2.6	
Approach LOS		D			D			A			A	

Intersection Summary

Area Type: Other  
 Cycle Length: 160  
 Actuated Cycle Length: 160  
 Offset: 98 (61%), Referenced to phase 2:NBSB, Start of Green  
 Natural Cycle: 75  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.51  
 Intersection Signal Delay: 4.6  
 Intersection Capacity Utilization 68.1%  
 Analysis Period (min) 15  
 Intersection LOS: A  
 ICU Level of Service C

Splits and Phases: 14: US 41 & 49th Ave W/49th Dr W



HCM 6th Signalized Intersection Summary  
 15: US 41 & 46th Ave Dr W/Orlando Ave

Existing PM  
 09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↑↑↑	↗	↖	↑↑↑	↗
Traffic Volume (veh/h)	1	0	1	140	0	140	2	1359	155	135	1173	0
Future Volume (veh/h)	1	0	1	140	0	140	2	1359	155	135	1173	0
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	0.99		0.96	0.98		0.97	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1900	1900	1900	1885	1900	1885	1900	1870	1885	1870	1885	1900
Adj Flow Rate, veh/h	1	0	1	144	0	144	2	1401	160	139	1209	0
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Percent Heavy Veh, %	0	0	0	1	0	1	0	2	1	2	1	0
Cap, veh/h	106	0	211	234	0	213	374	3262	373	340	4014	0
Arrive On Green	0.14	0.00	0.14	0.14	0.00	0.14	1.00	1.00	1.00	0.04	0.78	0.00
Sat Flow, veh/h	1256	0	1553	1402	0	1568	469	4648	531	1781	5316	0
Grp Volume(v), veh/h	1	0	1	144	0	144	2	1026	535	139	1209	0
Grp Sat Flow(s),veh/h/ln1256	0	1553	1402	0	1568	469	1702	1774	1781	1716	0	0
Q Serve(g_s), s	0.1	0.0	0.1	15.8	0.0	14.0	0.0	0.0	0.0	3.4	10.8	0.0
Cycle Q Clear(g_c), s	14.1	0.0	0.1	15.9	0.0	14.0	0.0	0.0	0.0	3.4	10.8	0.0
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.30	1.00		0.00
Lane Grp Cap(c), veh/h	106	0	211	234	0	213	374	2389	1246	340	4014	0
V/C Ratio(X)	0.01	0.00	0.00	0.61	0.00	0.68	0.01	0.43	0.43	0.41	0.30	0.00
Avail Cap(c_a), veh/h	236	0	372	380	0	375	374	2389	1246	546	4014	0
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	0.94	0.94	0.94	1.00	1.00	0.00
Uniform Delay (d), s/veh	72.5	0.0	59.8	66.7	0.0	65.8	0.0	0.0	0.0	5.4	5.1	0.0
Incr Delay (d2), s/veh	0.0	0.0	0.0	2.6	0.0	3.7	0.0	0.5	1.0	0.8	0.2	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln0.0	0.0	0.0	0.0	5.9	0.0	5.9	0.0	0.2	0.4	1.2	3.4	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	72.6	0.0	59.8	69.3	0.0	69.5	0.0	0.5	1.0	6.2	5.3	0.0
LnGrp LOS	E	A	E	E	A	E	A	A	A	A	A	A
Approach Vol, veh/h	2			288			1563			1348		
Approach Delay, s/veh	66.2			69.4			0.7			5.4		
Approach LOS	E			E			A			A		
Timer - Assigned Phs	2		4		5		6		8			
Phs Duration (G+Y+Rc), s	131.6		28.4		12.5		119.1		28.4			
Change Period (Y+Rc), s	6.8		* 6.7		6.8		6.8		* 6.7			
Max Green Setting (Gmax), s	108.2		* 38		24.2		77.2		* 38			
Max Q Clear Time (g_c+I1), s	12.8		17.9		5.4		2.0		16.1			
Green Ext Time (p_c), s	25.6		1.3		0.3		36.6		0.0			

Intersection Summary

HCM 6th Ctrl Delay	8.9
HCM 6th LOS	A

Notes

User approved ignoring U-Turning movement.  
 \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Intersection												
Int Delay, s/veh	0.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕		↕	↕	↕
Traffic Vol, veh/h	2	0	5	2	0	52	20	1404	63	48	1305	4
Future Vol, veh/h	2	0	5	2	0	52	20	1404	63	48	1305	4
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	100	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	99	99	99	99	99	99	99	99	99	99	99	99
Heavy Vehicles, %	0	0	0	0	0	0	0	2	3	0	1	0
Mvmt Flow	2	0	5	2	0	53	20	1418	64	48	1318	4




































Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2165	2938	661	2113	2908	741	1322	0	0	1482	0	0
Stage 1	1416	1416	-	1490	1490	-	-	-	-	-	-	-
Stage 2	749	1522	-	623	1418	-	-	-	-	-	-	-
Critical Hdwy	6.95	6.5	7.1	6.95	6.5	6.9	5.3	-	-	4.1	-	-
Critical Hdwy Stg 1	7.3	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.7	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.65	4	3.9	3.65	4	3.3	3.1	-	-	2.2	-	-
Pot Cap-1 Maneuver	37	15	351	41	16	363	278	-	-	460	-	-
Stage 1	105	205	-	130	189	-	-	-	-	-	-	-
Stage 2	364	182	-	417	205	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	28	12	351	35	13	363	278	-	-	460	-	-
Mov Cap-2 Maneuver	28	12	-	35	13	-	-	-	-	-	-	-
Stage 1	97	184	-	121	175	-	-	-	-	-	-	-
Stage 2	289	169	-	368	184	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	53	21.8	0.3	0.5
HCM LOS	F	C		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	278	-	-	82	269	460	-
HCM Lane V/C Ratio	0.073	-	-	0.086	0.203	0.105	-
HCM Control Delay (s)	19	-	-	53	21.8	13.7	-
HCM Lane LOS	C	-	-	F	C	B	-
HCM 95th %tile Q(veh)	0.2	-	-	0.3	0.7	0.4	-

HCM 6th Signalized Intersection Summary  
17: US 41 & SR 684 Cortez Rd

Existing PM  
09/06/2023

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	  		  	  		 	 		 	 	
Traffic Volume (veh/h)	262	1053	410	449	1099	91	603	470	360	130	521	147
Future Volume (veh/h)	262	1053	410	449	1099	91	603	470	360	130	521	147
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.99	1.00		0.98	1.00		0.99	1.00		0.98
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1885	1870	1870	1885	1885	1900	1885	1885	1885	1900	1885	1885
Adj Flow Rate, veh/h	270	1086	423	463	1133	94	622	485	371	134	537	152
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Percent Heavy Veh, %	1	2	2	1	1	0	1	1	1	0	1	1
Cap, veh/h	315	1646	504	505	1948	598	650	1241	546	172	745	325
Arrive On Green	0.09	0.32	0.32	0.15	0.38	0.38	0.19	0.35	0.35	0.07	0.28	0.28
Sat Flow, veh/h	3483	5106	1564	3483	5147	1579	3483	3582	1575	3510	3582	1561
Grp Volume(v), veh/h	270	1086	423	463	1133	94	622	485	371	134	537	152
Grp Sat Flow(s),veh/h/ln	1742	1702	1564	1742	1716	1579	1742	1791	1575	1755	1791	1561
Q Serve(g_s), s	15.3	36.6	50.2	26.2	35.1	7.9	35.4	20.5	40.3	7.5	27.1	16.2
Cycle Q Clear(g_c), s	15.3	36.6	50.2	26.2	35.1	7.9	35.4	20.5	40.3	7.5	27.1	16.2
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	315	1646	504	505	1948	598	650	1241	546	172	745	325
V/C Ratio(X)	0.86	0.66	0.84	0.92	0.58	0.16	0.96	0.39	0.68	0.78	0.72	0.47
Avail Cap(c_a), veh/h	582	1646	504	576	1948	598	650	1241	546	377	745	325
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.33	1.33	1.33
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.96	0.96	0.96
Uniform Delay (d), s/veh	89.7	58.3	62.9	84.3	49.5	41.1	80.6	49.4	55.9	92.4	67.1	63.1
Incr Delay (d2), s/veh	6.7	2.1	15.3	18.2	1.3	0.6	25.2	0.9	6.7	7.2	5.7	4.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	7.2	16.2	22.0	13.1	15.4	3.2	18.1	9.4	17.0	3.6	12.6	6.7
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	96.4	60.4	78.3	102.5	50.8	41.6	105.7	50.3	62.5	99.6	72.8	67.7
LnGrp LOS	F	E	E	F	D	D	F	D	E	F	E	E
Approach Vol, veh/h		1779			1690			1478			823	
Approach Delay, s/veh		70.1			64.4			76.7			76.2	
Approach LOS		E			E			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	24.7	82.3	17.3	75.7	35.9	71.1	45.0	48.0				
Change Period (Y+Rc), s	6.6	6.6	7.5	6.4	6.9	* 6.6	* 7.7	6.4				
Max Green Setting (Gmax), s	33.4	60.4	21.5	57.6	33.1	* 61	* 37	41.6				
Max Q Clear Time (g_c+I1), s	17.3	37.1	9.5	42.3	28.2	52.2	37.4	29.1				
Green Ext Time (p_c), s	0.8	14.5	0.3	3.9	0.8	7.0	0.0	3.1				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay			71.0									
HCM 6th LOS			E									
<b>Notes</b>												
User approved ignoring U-Turning movement.												
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.												

HCM 6th Signalized Intersection Summary  
18: US 41 & 39th Ave W

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	140	2	32	23	3	23	44	761	22	21	748	68
Future Volume (veh/h)	140	2	32	23	3	23	44	761	22	21	748	68
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	0.99		0.97	1.00		0.98	1.00		0.98	1.00		0.98
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1900	1900	1900	1900	1900	1870	1885	1900	1900	1885	1856
Adj Flow Rate, veh/h	146	2	33	24	3	24	46	793	23	22	779	71
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	2	0	0	0	0	0	2	1	0	0	1	3
Cap, veh/h	202	11	187	101	20	84	514	2686	78	579	2487	227
Arrive On Green	0.13	0.13	0.13	0.13	0.13	0.13	0.05	1.00	1.00	0.02	0.75	0.75
Sat Flow, veh/h	1373	90	1484	592	156	665	1781	3552	103	1810	3312	302
Grp Volume(v), veh/h	146	0	35	51	0	0	46	400	416	22	421	429
Grp Sat Flow(s),veh/h/ln	1373	0	1574	1413	0	0	1781	1791	1864	1810	1791	1823
Q Serve(g_s), s	14.4	0.0	4.0	3.6	0.0	0.0	1.2	0.0	0.0	0.6	15.3	15.3
Cycle Q Clear(g_c), s	22.0	0.0	4.0	7.6	0.0	0.0	1.2	0.0	0.0	0.6	15.3	15.3
Prop In Lane	1.00		0.94	0.47		0.47	1.00		0.06	1.00		0.17
Lane Grp Cap(c), veh/h	202	0	199	205	0	0	514	1354	1409	579	1345	1369
V/C Ratio(X)	0.72	0.00	0.18	0.25	0.00	0.00	0.09	0.30	0.30	0.04	0.31	0.31
Avail Cap(c_a), veh/h	258	0	263	266	0	0	592	1354	1409	667	1345	1369
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	0.00	0.87	0.87	0.87	1.00	1.00	1.00
Uniform Delay (d), s/veh	86.3	0.0	78.1	79.7	0.0	0.0	5.7	0.0	0.0	5.4	8.1	8.1
Incr Delay (d2), s/veh	7.0	0.0	0.4	0.6	0.0	0.0	0.1	0.5	0.5	0.0	0.6	0.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	7.9	0.0	1.7	2.5	0.0	0.0	0.4	0.2	0.2	0.2	6.0	6.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	93.3	0.0	78.5	80.3	0.0	0.0	5.8	0.5	0.5	5.5	8.7	8.7
LnGrp LOS	F	A	E	F	A	A	A	A	A	A	A	A
Approach Vol, veh/h		181			51			862			872	
Approach Delay, s/veh		90.5			80.3			0.8			8.6	
Approach LOS		F			F			A			A	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.2	157.9		31.8	11.3	156.8		31.8				
Change Period (Y+Rc), s	6.7	* 6.7		* 6.6	* 6.7	* 6.7		* 6.6				
Max Green Setting (Gmax), s	1.3E2			* 33	* 13	* 1.3E2		* 33				
Max Q Clear Time (g_c+1), s	2.0			24.0	3.2	17.3		9.6				
Green Ext Time (p_c), s	0.0	13.1		0.4	0.0	14.1		0.2				

Intersection Summary

HCM 6th Ctrl Delay	14.6
HCM 6th LOS	B

Notes

- User approved ignoring U-Turning movement.
- \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Intersection												
Int Delay, s/veh	1.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕		↕	↕	
Traffic Vol, veh/h	9	2	29	7	1	8	43	850	16	10	819	36
Future Vol, veh/h	9	2	29	7	1	8	43	850	16	10	819	36
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	70	-	-	55	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	99	99	99	99	99	99	99	99	99	99	99	99
Heavy Vehicles, %	0	0	3	0	0	0	2	1	0	0	2	0
Mvmt Flow	9	2	29	7	1	8	43	859	16	10	827	36

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	1381	1826	432	1388	1836	438	863	0	0	875	0	0
Stage 1	865	865	-	953	953	-	-	-	-	-	-	-
Stage 2	516	961	-	435	883	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.96	7.5	6.5	6.9	4.14	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.33	3.5	4	3.3	2.22	-	-	2.2	-	-
Pot Cap-1 Maneuver	105	78	569	104	77	572	775	-	-	780	-	-
Stage 1	319	374	-	282	340	-	-	-	-	-	-	-
Stage 2	515	337	-	575	367	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	97	73	569	92	72	572	775	-	-	780	-	-
Mov Cap-2 Maneuver	97	73	-	92	72	-	-	-	-	-	-	-
Stage 1	301	369	-	266	321	-	-	-	-	-	-	-
Stage 2	478	318	-	535	362	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	23.6		31.1		0.5		0.1	
HCM LOS	C		D					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	775	-	-	234	154	780	-
HCM Lane V/C Ratio	0.056	-	-	0.173	0.105	0.013	-
HCM Control Delay (s)	9.9	-	-	23.6	31.1	9.7	-
HCM Lane LOS	A	-	-	C	D	A	-
HCM 95th %tile Q(veh)	0.2	-	-	0.6	0.3	0	-

Intersection												
Int Delay, s/veh	0.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕		↕	↕	
Traffic Vol, veh/h	2	0	2	5	1	17	2	856	12	21	860	1
Future Vol, veh/h	2	0	2	5	1	17	2	856	12	21	860	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	55	-	-	50	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	0	0	0	0	0	0	0	1	0	0	1	0
Mvmt Flow	2	0	2	5	1	18	2	911	13	22	915	1

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	1420	1888	458	1424	1882	462	916	0	0	924	0	0
Stage 1	960	960	-	922	922	-	-	-	-	-	-	-
Stage 2	460	928	-	502	960	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	98	71	555	98	72	552	753	-	-	748	-	-
Stage 1	280	338	-	295	352	-	-	-	-	-	-	-
Stage 2	556	349	-	525	338	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	91	69	555	95	70	552	753	-	-	748	-	-
Mov Cap-2 Maneuver	91	69	-	95	70	-	-	-	-	-	-	-
Stage 1	279	328	-	294	351	-	-	-	-	-	-	-
Stage 2	535	348	-	508	328	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	28.7		22.1		0		0.2	
HCM LOS	D		C					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	753	-	-	156	235	748	-
HCM Lane V/C Ratio	0.003	-	-	0.027	0.104	0.03	-
HCM Control Delay (s)	9.8	-	-	28.7	22.1	10	-
HCM Lane LOS	A	-	-	D	C	A	-
HCM 95th %tile Q(veh)	0	-	-	0.1	0.3	0.1	-

Intersection						
Int Delay, s/veh	0.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↔		↑↓		↔	↑↑
Traffic Vol, veh/h	7	11	888	5	16	865
Future Vol, veh/h	7	11	888	5	16	865
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	55	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	98	98	98	98	98	98
Heavy Vehicles, %	0	0	1	0	0	1
Mvmt Flow	7	11	906	5	16	883

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1383	456	0	0	911
Stage 1	909	-	-	-	-
Stage 2	474	-	-	-	-
Critical Hdwy	6.8	6.9	-	-	4.1
Critical Hdwy Stg 1	5.8	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-
Follow-up Hdwy	3.5	3.3	-	-	2.2
Pot Cap-1 Maneuver	137	557	-	-	756
Stage 1	358	-	-	-	-
Stage 2	598	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	134	557	-	-	756
Mov Cap-2 Maneuver	134	-	-	-	-
Stage 1	358	-	-	-	-
Stage 2	585	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	20.5	0	0.2
HCM LOS	C		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	250	756
HCM Lane V/C Ratio	-	-	0.073	0.022
HCM Control Delay (s)	-	-	20.5	9.9
HCM Lane LOS	-	-	C	A
HCM 95th %tile Q(veh)	-	-	0.2	0.1



HCM 6th Signalized Intersection Summary  
22: US 41 & 30th Ave W

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	107	228	113	210	324	76	69	657	140	50	581	130
Future Volume (veh/h)	107	228	113	210	324	76	69	657	140	50	581	130
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		0.98
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1900	1900	1885	1900	1826	1900	1900	1811	1678	1885	1900
Adj Flow Rate, veh/h	111	238	118	219	338	79	72	684	146	52	605	135
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	2	0	0	1	0	5	0	0	6	15	1	0
Cap, veh/h	202	274	232	297	366	298	368	1405	300	307	1342	299
Arrive On Green	0.07	0.14	0.14	0.12	0.19	0.19	0.04	0.47	0.47	0.04	0.46	0.46
Sat Flow, veh/h	1781	1900	1610	1795	1900	1547	1810	2960	631	1598	2899	645
Grp Volume(v), veh/h	111	238	118	219	338	79	72	417	413	52	373	367
Grp Sat Flow(s),veh/h/ln	1781	1900	1610	1795	1900	1547	1810	1805	1786	1598	1791	1753
Q Serve(g_s), s	6.0	14.1	7.8	11.6	20.1	5.0	2.4	18.1	18.2	1.9	16.3	16.3
Cycle Q Clear(g_c), s	6.0	14.1	7.8	11.6	20.1	5.0	2.4	18.1	18.2	1.9	16.3	16.3
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.35	1.00		0.37
Lane Grp Cap(c), veh/h	202	274	232	297	366	298	368	857	848	307	829	812
V/C Ratio(X)	0.55	0.87	0.51	0.74	0.92	0.26	0.20	0.49	0.49	0.17	0.45	0.45
Avail Cap(c_a), veh/h	294	311	263	302	366	298	495	857	848	438	829	812
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	39.0	48.2	45.5	35.7	45.6	39.5	16.1	20.6	20.6	16.3	20.9	21.0
Incr Delay (d2), s/veh	2.3	20.6	1.7	9.0	28.5	0.5	0.3	2.0	2.0	0.3	1.8	1.8
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.8	8.2	3.2	5.8	12.3	1.9	1.0	7.8	7.7	0.7	6.9	6.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	41.4	68.7	47.2	44.7	74.1	40.0	16.4	22.6	22.6	16.6	22.7	22.8
LnGrp LOS	D	E	D	D	E	D	B	C	C	B	C	C
Approach Vol, veh/h		467			636			902			792	
Approach Delay, s/veh		56.8			59.7			22.1			22.3	
Approach LOS		E			E			C			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	11.9	60.6	14.1	28.4	10.6	62.0	19.7	22.8				
Change Period (Y+Rc), s	7.4	7.4	* 6.2	* 6.2	6.5	7.4	* 6.1	* 6.2				
Max Green Setting (Gmax), s	12.6	42.6	* 14	* 19	13.5	42.6	* 14	* 19				
Max Q Clear Time (g_c+I1), s	4.4	18.3	8.0	22.1	3.9	20.2	13.6	16.1				
Green Ext Time (p_c), s	0.1	4.5	0.1	0.0	0.1	5.1	0.0	0.5				

Intersection Summary

HCM 6th Ctrl Delay	36.5
HCM 6th LOS	D

Notes

- User approved pedestrian interval to be less than phase max green.
- User approved ignoring U-Turning movement.
- \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Intersection												
Int Delay, s/veh	3.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕		↕	↕	
Traffic Vol, veh/h	0	0	4	84	0	25	5	711	0	4	669	1
Future Vol, veh/h	0	0	4	84	0	25	5	711	0	4	669	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	1	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	96	96	96	96	96	96	96	96	96	96	96	96
Heavy Vehicles, %	0	0	0	0	0	0	0	2	0	0	1	0
Mvmt Flow	0	0	4	88	0	26	5	741	0	4	697	1

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1087	1457	349	1108	1457	371	698	0	0	741	0	0
Stage 1	706	706	-	751	751	-	-	-	-	-	-	-
Stage 2	381	751	-	357	706	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	173	131	653	167	131	632	908	-	-	875	-	-
Stage 1	397	442	-	373	421	-	-	-	-	-	-	-
Stage 2	619	421	-	639	442	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	164	129	653	164	129	632	908	-	-	875	-	-
Mov Cap-2 Maneuver	164	129	-	164	129	-	-	-	-	-	-	-
Stage 1	393	440	-	370	417	-	-	-	-	-	-	-
Stage 2	588	417	-	632	440	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	10.5		45.1		0.1		0.1	
HCM LOS	B		E					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	908	-	-	653	198	875	-
HCM Lane V/C Ratio	0.006	-	-	0.006	0.573	0.005	-
HCM Control Delay (s)	9	0	-	10.5	45.1	9.1	-
HCM Lane LOS	A	A	-	B	E	A	-
HCM 95th %tile Q(veh)	0	-	-	0	3.1	0	-

Intersection												
Int Delay, s/veh	2.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	3	3	11	29	8	31	13	714	28	24	646	3
Future Vol, veh/h	3	3	11	29	8	31	13	714	28	24	646	3
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	90	90	90	90	90	90	90	90	90	90	90	90
Heavy Vehicles, %	0	0	0	3	0	0	0	1	0	0	1	0
Mvmt Flow	3	3	12	32	9	34	14	793	31	27	718	3

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1203	1626	361	1252	1612	412	721	0	0	824	0	0
Stage 1	774	774	-	837	837	-	-	-	-	-	-	-
Stage 2	429	852	-	415	775	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.56	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.56	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.56	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.53	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	142	103	641	128	105	595	890	-	-	815	-	-
Stage 1	362	411	-	325	385	-	-	-	-	-	-	-
Stage 2	580	379	-	583	411	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	117	95	641	115	96	595	890	-	-	815	-	-
Mov Cap-2 Maneuver	117	95	-	115	96	-	-	-	-	-	-	-
Stage 1	352	388	-	316	374	-	-	-	-	-	-	-
Stage 2	518	368	-	536	388	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	22.1		40.3		0.3		0.5	
HCM LOS	C		E					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	890	-	-	229	175	815	-
HCM Lane V/C Ratio	0.016	-	-	0.082	0.432	0.033	-
HCM Control Delay (s)	9.1	0.1	-	22.1	40.3	9.6	0.2
HCM Lane LOS	A	A	-	C	E	A	A
HCM 95th %tile Q(veh)	0	-	-	0.3	2	0.1	-

Lanes, Volumes, Timings  
26: US 41 & 21st Ave W

Existing PM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Volume (vph)	35	63	46	26	70	8	53	679	31	3	581	43
Future Volume (vph)	35	63	46	26	70	8	53	679	31	3	581	43
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	0.95	0.95	0.95	0.95
Flt		0.957			0.990			0.994			0.990	
Flt Protected		0.988			0.988			0.997				
Satd. Flow (prot)	0	1796	0	0	1840	0	0	3546	0	0	3541	0
Flt Permitted		0.988			0.988			0.831			0.952	
Satd. Flow (perm)	0	1796	0	0	1840	0	0	2956	0	0	3371	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		17			3			5			7	
Link Speed (mph)		25			25			35			35	
Link Distance (ft)		252			255			1307			960	
Travel Time (s)		6.9			7.0			25.5			18.7	
Peak Hour Factor	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Heavy Vehicles (%)	0%	0%	0%	4%	0%	0%	0%	1%	0%	0%	1%	0%
Adj. Flow (vph)	39	71	52	29	79	9	60	763	35	3	653	48
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	162	0	0	117	0	0	858	0	0	704	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		0			0			0			0	
Link Offset(ft)		-15			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Number of Detectors	1	2		1	2		1	2		1	2	
Detector Template	Left	Thru		Left	Thru		Left	Thru		Left	Thru	
Leading Detector (ft)	20	100		20	100		20	100		20	100	
Trailing Detector (ft)	0	0		0	0		0	0		0	0	
Detector 1 Position(ft)	0	0		0	0		0	0		0	0	
Detector 1 Size(ft)	20	6		20	6		20	6		20	6	
Detector 1 Type	Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex	
Detector 1 Channel												
Detector 1 Extend (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Queue (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Delay (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 2 Position(ft)		94			94			94			94	
Detector 2 Size(ft)		6			6			6			6	
Detector 2 Type		Cl+Ex			Cl+Ex			Cl+Ex			Cl+Ex	
Detector 2 Channel												
Detector 2 Extend (s)		0.0			0.0			0.0			0.0	
Turn Type	Split	NA		Split	NA		pm+pt	NA		Perm	NA	
Protected Phases	4	4		8	8		1	6			2	
Permitted Phases							6			2		
Detector Phase	4	4		8	8		1	6		2	2	
Switch Phase												

Lanes, Volumes, Timings  
26: US 41 & 21st Ave W

Existing PM  
09/06/2023

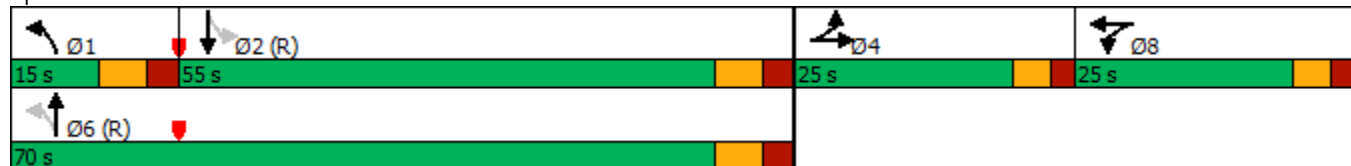


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Minimum Initial (s)	7.0	7.0		7.0	7.0		5.0	12.0		12.0	12.0	
Minimum Split (s)	24.6	24.6		25.5	25.5		12.2	26.2		25.2	25.2	
Total Split (s)	25.0	25.0		25.0	25.0		15.0	70.0		55.0	55.0	
Total Split (%)	20.8%	20.8%		20.8%	20.8%		12.5%	58.3%		45.8%	45.8%	
Maximum Green (s)	19.4	19.4		19.5	19.5		7.8	62.8		47.8	47.8	
Yellow Time (s)	3.4	3.4		3.4	3.4		4.4	4.4		4.4	4.4	
All-Red Time (s)	2.2	2.2		2.1	2.1		2.8	2.8		2.8	2.8	
Lost Time Adjust (s)		0.0			0.0			0.0			0.0	
Total Lost Time (s)		5.6			5.5			7.2			7.2	
Lead/Lag	Lead	Lead		Lag	Lag		Lead			Lag	Lag	
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes			Yes	Yes	
Vehicle Extension (s)	3.0	3.0		3.0	3.0		3.0	5.0		5.0	5.0	
Recall Mode	None	None		None	None		None	C-Max		C-Max	C-Max	
Walk Time (s)	7.0	7.0		7.0	7.0			7.0		7.0	7.0	
Flash Dont Walk (s)	12.0	12.0		13.0	13.0			12.0		10.0	10.0	
Pedestrian Calls (#/hr)	0	0		0	0			0		0	0	
Act Effect Green (s)		14.7			12.7			74.2			74.2	
Actuated g/C Ratio		0.12			0.11			0.62			0.62	
v/c Ratio		0.69			0.59			0.47			0.34	
Control Delay		59.8			61.5			14.4			5.3	
Queue Delay		0.0			0.0			0.0			0.0	
Total Delay		59.8			61.5			14.4			5.3	
LOS		E			E			B			A	
Approach Delay		59.8			61.5			14.4			5.3	
Approach LOS		E			E			B			A	

Intersection Summary

Area Type: Other  
 Cycle Length: 120  
 Actuated Cycle Length: 120  
 Offset: 47 (39%), Referenced to phase 2:SBTL and 6:NBTL, Start of Green  
 Natural Cycle: 90  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.69  
 Intersection Signal Delay: 17.9  
 Intersection LOS: B  
 Intersection Capacity Utilization 66.1%  
 ICU Level of Service C  
 Analysis Period (min) 15

Splits and Phases: 26: US 41 & 21st Ave W



Intersection						
Int Delay, s/veh	0.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	4	2	724	13	3	612
Future Vol, veh/h	4	2	724	13	3	612
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	87	87	87	87	87	87
Heavy Vehicles, %	0	0	1	0	0	1
Mvmt Flow	5	2	832	15	3	703

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1198	424	0	0	847
Stage 1	840	-	-	-	-
Stage 2	358	-	-	-	-
Critical Hdwy	6.8	6.9	-	-	4.1
Critical Hdwy Stg 1	5.8	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-
Follow-up Hdwy	3.5	3.3	-	-	2.2
Pot Cap-1 Maneuver	181	584	-	-	799
Stage 1	389	-	-	-	-
Stage 2	684	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	180	584	-	-	799
Mov Cap-2 Maneuver	180	-	-	-	-
Stage 1	389	-	-	-	-
Stage 2	680	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	20.9	0	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	234	799
HCM Lane V/C Ratio	-	-	0.029	0.004
HCM Control Delay (s)	-	-	20.9	9.5
HCM Lane LOS	-	-	C	A
HCM 95th %tile Q(veh)	-	-	0.1	0

Intersection						
Int Delay, s/veh	0.5					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		
Traffic Vol, veh/h	3	21	25	704	594	3
Future Vol, veh/h	3	21	25	704	594	3
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	87	87	87	87	87	87
Heavy Vehicles, %	0	0	0	1	1	0
Mvmt Flow	3	24	29	809	683	3

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1148	343	686	0	-	0
Stage 1	685	-	-	-	-	-
Stage 2	463	-	-	-	-	-
Critical Hdwy	6.8	6.9	4.1	-	-	-
Critical Hdwy Stg 1	5.8	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.5	3.3	2.2	-	-	-
Pot Cap-1 Maneuver	195	659	917	-	-	-
Stage 1	467	-	-	-	-	-
Stage 2	606	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	184	659	917	-	-	-
Mov Cap-2 Maneuver	184	-	-	-	-	-
Stage 1	440	-	-	-	-	-
Stage 2	606	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	12.7	0.5	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	917	-	498	-	-
HCM Lane V/C Ratio	0.031	-	0.055	-	-
HCM Control Delay (s)	9.1	0.2	12.7	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0.1	-	0.2	-	-

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		
Traffic Vol, veh/h	2	1	2	725	618	0
Future Vol, veh/h	2	1	2	725	618	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	87	87	87	87	87	87
Heavy Vehicles, %	0	0	0	1	1	0
Mvmt Flow	2	1	2	833	710	0

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1131	355	710	0	-	0
Stage 1	710	-	-	-	-	-
Stage 2	421	-	-	-	-	-
Critical Hdwy	6.8	6.9	4.1	-	-	-
Critical Hdwy Stg 1	5.8	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.5	3.3	2.2	-	-	-
Pot Cap-1 Maneuver	200	647	899	-	-	-
Stage 1	454	-	-	-	-	-
Stage 2	636	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	199	647	899	-	-	-
Mov Cap-2 Maneuver	199	-	-	-	-	-
Stage 1	452	-	-	-	-	-
Stage 2	636	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	19.1	0	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	899	-	259	-	-
HCM Lane V/C Ratio	0.003	-	0.013	-	-
HCM Control Delay (s)	9	0	19.1	-	-
HCM Lane LOS	A	A	C	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-



Lanes, Volumes, Timings  
30: US 41 & 17th Ave W

Existing PM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	90	185	41	22	170	16	46	669	27	17	554	26
Future Volume (vph)	90	185	41	22	170	16	46	669	27	17	554	26
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	45		0	55		0	0		0	0		0
Storage Lanes	1		0	1		0	0		0	0		0
Taper Length (ft)	40			60			0			0		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	0.95	0.95	0.95	0.95
Frt		0.973			0.987			0.995			0.993	
Flt Protected	0.950			0.950				0.997			0.999	
Satd. Flow (prot)	1805	1819	0	1719	1875	0	0	3549	0	0	3548	0
Flt Permitted	0.435			0.328				0.857			0.916	
Satd. Flow (perm)	826	1819	0	594	1875	0	0	3051	0	0	3253	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		9			4			6			8	
Link Speed (mph)		30			30			35			35	
Link Distance (ft)		252			242			152			1665	
Travel Time (s)		5.7			5.5			3.0			32.4	
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	0%	2%	0%	5%	0%	0%	0%	1%	0%	0%	1%	0%
Adj. Flow (vph)	100	206	46	24	189	18	51	743	30	19	616	29
Shared Lane Traffic (%)												
Lane Group Flow (vph)	100	252	0	24	207	0	0	824	0	0	664	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		12			12			0			0	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Number of Detectors	1	2		1	2		1	2		1	2	
Detector Template	Left	Thru		Left	Thru		Left	Thru		Left	Thru	
Leading Detector (ft)	20	100		20	100		20	100		20	100	
Trailing Detector (ft)	0	0		0	0		0	0		0	0	
Detector 1 Position(ft)	0	0		0	0		0	0		0	0	
Detector 1 Size(ft)	20	6		20	6		20	6		20	6	
Detector 1 Type	Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex	
Detector 1 Channel												
Detector 1 Extend (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Queue (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Delay (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 2 Position(ft)		94			94			94			94	
Detector 2 Size(ft)		6			6			6			6	
Detector 2 Type		Cl+Ex			Cl+Ex			Cl+Ex			Cl+Ex	
Detector 2 Channel												
Detector 2 Extend (s)		0.0			0.0			0.0			0.0	
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			4			2			2	

Lanes, Volumes, Timings  
30: US 41 & 17th Ave W

Existing PM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Permitted Phases	4			4			2			2		
Detector Phase	4	4		4	4		2	2		2	2	
Switch Phase												
Minimum Initial (s)	7.0	7.0		7.0	7.0		12.0	12.0		12.0	12.0	
Minimum Split (s)	26.0	26.0		26.0	26.0		25.0	25.0		25.0	25.0	
Total Split (s)	35.0	35.0		35.0	35.0		85.0	85.0		85.0	85.0	
Total Split (%)	29.2%	29.2%		29.2%	29.2%		70.8%	70.8%		70.8%	70.8%	
Maximum Green (s)	29.0	29.0		29.0	29.0		78.0	78.0		78.0	78.0	
Yellow Time (s)	3.7	3.7		3.7	3.7		4.0	4.0		4.0	4.0	
All-Red Time (s)	2.3	2.3		2.3	2.3		3.0	3.0		3.0	3.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0			0.0			0.0	
Total Lost Time (s)	6.0	6.0		6.0	6.0			7.0			7.0	
Lead/Lag												
Lead-Lag Optimize?												
Vehicle Extension (s)	3.0	3.0		3.0	3.0		5.0	5.0		5.0	5.0	
Recall Mode	None	None		None	None		C-Max	C-Max		C-Max	C-Max	
Walk Time (s)	7.0	7.0		7.0	7.0		7.0	7.0		7.0	7.0	
Flash Dont Walk (s)	13.0	13.0		13.0	13.0		10.0	10.0		10.0	10.0	
Pedestrian Calls (#/hr)	0	0		0	0		0	0		0	0	
Act Effct Green (s)	21.9	21.9		21.9	21.9			85.1			85.1	
Actuated g/C Ratio	0.18	0.18		0.18	0.18			0.71			0.71	
v/c Ratio	0.67	0.74		0.22	0.60			0.38			0.29	
Control Delay	65.7	57.6		44.3	50.6			3.5			9.1	
Queue Delay	0.0	0.0		0.0	0.0			0.0			0.0	
Total Delay	65.7	57.6		44.3	50.6			3.5			9.1	
LOS	E	E		D	D			A			A	
Approach Delay		59.9			50.0			3.5			9.1	
Approach LOS		E			D			A			A	

Intersection Summary

Area Type:	Other
Cycle Length:	120
Actuated Cycle Length:	120
Offset:	36 (30%), Referenced to phase 2:NBSB, Start of Green
Natural Cycle:	55
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.74
Intersection Signal Delay:	20.1
Intersection LOS:	C
Intersection Capacity Utilization:	77.0%
ICU Level of Service:	D
Analysis Period (min):	15

Splits and Phases: 30: US 41 & 17th Ave W



Lanes, Volumes, Timings  
31: US 41 & 12th Ave W

Existing PM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Volume (vph)	20	30	38	11	22	0	39	738	31	12	529	21
Future Volume (vph)	20	30	38	11	22	0	39	738	31	12	529	21
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	0.95	0.95	0.95	0.95
Flt		0.942						0.994			0.994	
Flt Protected		0.989			0.983			0.998			0.999	
Satd. Flow (prot)	0	1770	0	0	1868	0	0	3549	0	0	3551	0
Flt Permitted		0.910			0.816			0.886			0.927	
Satd. Flow (perm)	0	1629	0	0	1550	0	0	3150	0	0	3295	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		27						9			8	
Link Speed (mph)		25			25			35			35	
Link Distance (ft)		216			234			1665			1011	
Travel Time (s)		5.9			6.4			32.4			19.7	
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Heavy Vehicles (%)	0%	0%	0%	0%	0%	0%	0%	1%	0%	0%	1%	0%
Adj. Flow (vph)	23	34	43	13	25	0	44	839	35	14	601	24
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	100	0	0	38	0	0	918	0	0	639	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		0			0			0			0	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Number of Detectors	1	2		1	2		1	2		1	2	
Detector Template	Left	Thru		Left	Thru		Left	Thru		Left	Thru	
Leading Detector (ft)	20	100		20	100		20	100		20	100	
Trailing Detector (ft)	0	0		0	0		0	0		0	0	
Detector 1 Position(ft)	0	0		0	0		0	0		0	0	
Detector 1 Size(ft)	20	6		20	6		20	6		20	6	
Detector 1 Type	Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex	
Detector 1 Channel												
Detector 1 Extend (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Queue (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Delay (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 2 Position(ft)		94			94			94			94	
Detector 2 Size(ft)		6			6			6			6	
Detector 2 Type		Cl+Ex			Cl+Ex			Cl+Ex			Cl+Ex	
Detector 2 Channel												
Detector 2 Extend (s)		0.0			0.0			0.0			0.0	
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			4			2			2	
Permitted Phases	4			4			2			2		
Detector Phase	4	4		4	4		2	2		2	2	
Switch Phase												

Lanes, Volumes, Timings  
31: US 41 & 12th Ave W

Existing PM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Minimum Initial (s)	7.0	7.0		7.0	7.0		12.0	12.0		12.0	12.0	
Minimum Split (s)	26.1	26.1		26.1	26.1		24.0	24.0		24.0	24.0	
Total Split (s)	27.0	27.0		27.0	27.0		93.0	93.0		93.0	93.0	
Total Split (%)	22.5%	22.5%		22.5%	22.5%		77.5%	77.5%		77.5%	77.5%	
Maximum Green (s)	20.9	20.9		20.9	20.9		87.0	87.0		87.0	87.0	
Yellow Time (s)	3.7	3.7		3.7	3.7		4.0	4.0		4.0	4.0	
All-Red Time (s)	2.4	2.4		2.4	2.4		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)		0.0			0.0			0.0			0.0	
Total Lost Time (s)		6.1			6.1			6.0			6.0	
Lead/Lag												
Lead-Lag Optimize?												
Vehicle Extension (s)	3.0	3.0		3.0	3.0		5.0	5.0		5.0	5.0	
Recall Mode	None	None		None	None		C-Max	C-Max		C-Max	C-Max	
Walk Time (s)	7.0	7.0		7.0	7.0		7.0	7.0		7.0	7.0	
Flash Dont Walk (s)	13.0	13.0		13.0	13.0		10.0	10.0		10.0	10.0	
Pedestrian Calls (#/hr)	0	0		0	0		0	0		0	0	
Act Effect Green (s)		11.0			11.0			96.9			96.9	
Actuated g/C Ratio		0.09			0.09			0.81			0.81	
v/c Ratio		0.58			0.27			0.36			0.24	
Control Delay		50.6			54.0			8.3			5.4	
Queue Delay		0.0			0.0			0.0			0.0	
Total Delay		50.6			54.0			8.3			5.4	
LOS		D			D			A			A	
Approach Delay		50.6			54.0			8.3			5.4	
Approach LOS		D			D			A			A	

Intersection Summary

Area Type: Other  
 Cycle Length: 120  
 Actuated Cycle Length: 120  
 Offset: 118 (98%), Referenced to phase 2:NBSB, Start of Green  
 Natural Cycle: 55  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.58  
 Intersection Signal Delay: 10.8  
 Intersection Capacity Utilization 59.1%  
 Analysis Period (min) 15  
 Intersection LOS: B  
 ICU Level of Service B

Splits and Phases: 31: US 41 & 12th Ave W



Lanes, Volumes, Timings  
32: US 41 & 9th Ave W

Existing PM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕		↖	↗			↕			↕	
Traffic Volume (vph)	65	294	34	40	184	14	42	592	103	13	464	38
Future Volume (vph)	65	294	34	40	184	14	42	592	103	13	464	38
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	95		0	0		0	0		0
Storage Lanes	0		0	1		0	0		0	0		0
Taper Length (ft)	0			45			0			0		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	0.95	0.95	0.95	0.95
Frt		0.988			0.989			0.979			0.989	
Flt Protected		0.992		0.950				0.997			0.999	
Satd. Flow (prot)	0	1837	0	1752	1879	0	0	3481	0	0	3522	0
Flt Permitted		0.836		0.348				0.885			0.927	
Satd. Flow (perm)	0	1548	0	642	1879	0	0	3090	0	0	3268	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		4			4			23			11	
Link Speed (mph)		30			30			35			35	
Link Distance (ft)		180			228			1011			142	
Travel Time (s)		4.1			5.2			19.7			2.8	
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Heavy Vehicles (%)	0%	1%	7%	3%	0%	0%	0%	1%	3%	0%	1%	5%
Adj. Flow (vph)	68	306	35	42	192	15	44	617	107	14	483	40
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	409	0	42	207	0	0	768	0	0	537	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		12			12			0			0	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Number of Detectors	1	2		1	2		1	2		1	2	
Detector Template	Left	Thru		Left	Thru		Left	Thru		Left	Thru	
Leading Detector (ft)	20	100		20	100		20	100		20	100	
Trailing Detector (ft)	0	0		0	0		0	0		0	0	
Detector 1 Position(ft)	0	0		0	0		0	0		0	0	
Detector 1 Size(ft)	20	6		20	6		20	6		20	6	
Detector 1 Type	Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex	
Detector 1 Channel												
Detector 1 Extend (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Queue (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Delay (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 2 Position(ft)		94			94			94			94	
Detector 2 Size(ft)		6			6			6			6	
Detector 2 Type		Cl+Ex			Cl+Ex			Cl+Ex			Cl+Ex	
Detector 2 Channel												
Detector 2 Extend (s)		0.0			0.0			0.0			0.0	
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			4			2			2	

Lanes, Volumes, Timings  
32: US 41 & 9th Ave W

Existing PM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Permitted Phases	4			4			2			2		
Detector Phase	4	4		4	4		2	2		2	2	
Switch Phase												
Minimum Initial (s)	7.0	7.0		7.0	7.0		12.0	12.0		12.0	12.0	
Minimum Split (s)	33.8	33.8		33.8	33.8		33.2	33.2		33.2	33.2	
Total Split (s)	50.0	50.0		50.0	50.0		70.0	70.0		70.0	70.0	
Total Split (%)	41.7%	41.7%		41.7%	41.7%		58.3%	58.3%		58.3%	58.3%	
Maximum Green (s)	44.2	44.2		44.2	44.2		63.8	63.8		63.8	63.8	
Yellow Time (s)	3.7	3.7		3.7	3.7		4.0	4.0		4.0	4.0	
All-Red Time (s)	2.1	2.1		2.1	2.1		2.2	2.2		2.2	2.2	
Lost Time Adjust (s)		0.0		0.0	0.0			0.0			0.0	
Total Lost Time (s)		5.8		5.8	5.8			6.2			6.2	
Lead/Lag												
Lead-Lag Optimize?												
Vehicle Extension (s)	3.0	3.0		3.0	3.0		5.0	5.0		5.0	5.0	
Recall Mode	None	None		None	None		C-Max	C-Max		C-Max	C-Max	
Walk Time (s)	10.0	10.0		10.0	10.0		10.0	10.0		10.0	10.0	
Flash Dont Walk (s)	18.0	18.0		18.0	18.0		17.0	17.0		17.0	17.0	
Pedestrian Calls (#/hr)	0	0		0	0		0	0		0	0	
Act Effct Green (s)		35.4		35.4	35.4			72.6			72.6	
Actuated g/C Ratio		0.30		0.30	0.30			0.60			0.60	
v/c Ratio		0.89		0.22	0.37			0.41			0.27	
Control Delay		61.4		32.1	33.4			11.3			26.3	
Queue Delay		0.0		0.0	0.0			0.0			0.0	
Total Delay		61.4		32.1	33.4			11.3			26.3	
LOS		E		C	C			B			C	
Approach Delay		61.4			33.2			11.3			26.3	
Approach LOS		E			C			B			C	

Intersection Summary

Area Type:	Other
Cycle Length:	120
Actuated Cycle Length:	120
Offset:	89 (74%), Referenced to phase 2:NBSB, Start of Green
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.89
Intersection Signal Delay:	28.6
Intersection LOS:	C
Intersection Capacity Utilization:	86.9%
ICU Level of Service:	E
Analysis Period (min):	15

Splits and Phases: 32: US 41 & 9th Ave W



Intersection						
Int Delay, s/veh	0.5					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		
Traffic Vol, veh/h	6	32	17	654	483	4
Future Vol, veh/h	6	32	17	654	483	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	89	89	89	89	89	89
Heavy Vehicles, %	0	6	0	0	1	0
Mvmt Flow	7	36	19	735	543	4

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	951	274	547	0	-	0
Stage 1	545	-	-	-	-	-
Stage 2	406	-	-	-	-	-
Critical Hdwy	6.8	7.02	4.1	-	-	-
Critical Hdwy Stg 1	5.8	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.5	3.36	2.2	-	-	-
Pot Cap-1 Maneuver	262	712	1033	-	-	-
Stage 1	551	-	-	-	-	-
Stage 2	647	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	254	712	1033	-	-	-
Mov Cap-2 Maneuver	254	-	-	-	-	-
Stage 1	534	-	-	-	-	-
Stage 2	647	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	12	0.3	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1033	-	554	-	-
HCM Lane V/C Ratio	0.018	-	0.077	-	-
HCM Control Delay (s)	8.6	0.1	12	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0.1	-	0.2	-	-

HCM 6th Signalized Intersection Summary  
34: US 41 & 8th Ave W

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	26	89	28	289	52	8	15	202	428	5	161	4
Future Volume (veh/h)	26	89	28	289	52	8	15	202	428	5	161	4
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.92	1.00		1.00	1.00		1.00	1.00		0.98
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1841	1870	1841	1870	1900	1900	1900	1870	1870	1900	1885	1900
Adj Flow Rate, veh/h	28	97	30	314	57	9	16	220	465	5	175	4
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	4	2	4	2	0	0	0	2	2	0	1	0
Cap, veh/h	180	122	38	391	409	65	83	1099	998	464	1155	26
Arrive On Green	0.09	0.09	0.09	0.11	0.26	0.26	0.21	0.21	0.21	0.63	0.63	0.63
Sat Flow, veh/h	1310	1341	415	3456	1601	253	81	1745	1585	769	1835	42
Grp Volume(v), veh/h	28	0	127	314	0	66	236	0	465	5	0	179
Grp Sat Flow(s),veh/h/ln	1310	0	1756	1728	0	1854	1826	0	1585	769	0	1877
Q Serve(g_s), s	2.4	0.0	8.5	10.6	0.0	3.3	0.0	0.0	30.9	0.4	0.0	4.7
Cycle Q Clear(g_c), s	2.4	0.0	8.5	10.6	0.0	3.3	12.5	0.0	30.9	12.9	0.0	4.7
Prop In Lane	1.00		0.24	1.00		0.14	0.07		1.00	1.00		0.02
Lane Grp Cap(c), veh/h	180	0	160	391	0	473	1182	0	998	464	0	1182
V/C Ratio(X)	0.16	0.00	0.79	0.80	0.00	0.14	0.20	0.00	0.47	0.01	0.00	0.15
Avail Cap(c_a), veh/h	204	0	193	832	0	745	1182	0	998	464	0	1182
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	0.33	0.33	0.33	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	0.99	0.00	0.99	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	50.6	0.0	53.4	51.9	0.0	34.5	22.6	0.0	29.8	13.6	0.0	9.1
Incr Delay (d2), s/veh	0.4	0.0	16.9	3.8	0.0	0.1	0.4	0.0	1.6	0.0	0.0	0.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.8	0.0	4.5	4.8	0.0	1.5	6.4	0.0	13.5	0.1	0.0	1.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	51.0	0.0	70.3	55.7	0.0	34.6	22.9	0.0	31.4	13.6	0.0	9.2
LnGrp LOS	D	A	E	E	A	C	C	A	C	B	A	A
Approach Vol, veh/h		155			380			701				184
Approach Delay, s/veh		66.8			52.1			28.6				9.3
Approach LOS		E			D			C				A
Timer - Assigned Phs		2		4		6	7	8				
Phs Duration (G+Y+Rc), s		82.6		37.4		82.6	19.7	17.7				
Change Period (Y+Rc), s		7.0		* 6.8		7.0	6.1	* 6.8				
Max Green Setting (Gmax), s		58.0		* 48		58.0	28.9	* 13				
Max Q Clear Time (g_c+I1), s		32.9		5.3		14.9	12.6	10.5				
Green Ext Time (p_c), s		6.9		0.3		2.2	1.0	0.2				

Intersection Summary

HCM 6th Ctrl Delay	36.5
HCM 6th LOS	D

Notes

- User approved pedestrian interval to be less than phase max green.
- \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.



# HCM 6th Signalized Intersection Summary

## 35: 9th St W & 8th Ave W

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↔↔	↔			↔↔			↔↔			↔↔	↔
Traffic Volume (veh/h)	415	31	34	1	5	0	20	511	5	10	492	237
Future Volume (veh/h)	415	31	34	1	5	0	20	511	5	10	492	237
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1885	1900	1900	1900	1900	1900	1307	1900	1900	1900	1885	1870
Adj Flow Rate, veh/h	466	35	38	1	6	0	22	574	6	11	553	266
Peak Hour Factor	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Percent Heavy Veh, %	1	0	0	0	0	0	40	0	0	0	1	2
Cap, veh/h	502	176	191	28	70	0	91	2359	25	51	2435	1127
Arrive On Green	0.14	0.21	0.21	0.04	0.04	0.00	0.71	0.71	0.71	0.71	0.71	0.71
Sat Flow, veh/h	3483	833	904	105	1752	0	96	3318	35	41	3424	1585
Grp Volume(v), veh/h	466	0	73	7	0	0	303	0	299	299	265	266
Grp Sat Flow(s),veh/h/ln	1742	0	1737	1857	0	0	1726	0	1723	1835	1630	1585
Q Serve(g_s), s	22.5	0.0	5.9	0.0	0.0	0.0	0.0	0.0	10.3	0.0	9.5	9.9
Cycle Q Clear(g_c), s	22.5	0.0	5.9	0.6	0.0	0.0	9.3	0.0	10.3	9.3	9.5	9.9
Prop In Lane	1.00		0.52	0.14		0.00	0.07		0.02	0.04		1.00
Lane Grp Cap(c), veh/h	502	0	366	99	0	0	1250	0	1225	1327	1159	1127
V/C Ratio(X)	0.93	0.00	0.20	0.07	0.00	0.00	0.24	0.00	0.24	0.23	0.23	0.24
Avail Cap(c_a), veh/h	502	0	601	341	0	0	1250	0	1225	1327	1159	1127
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.89	0.00	0.89	1.00	0.00	0.00	1.00	0.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	71.9	0.0	55.3	78.6	0.0	0.0	8.4	0.0	8.6	8.4	8.5	8.5
Incr Delay (d2), s/veh	22.0	0.0	0.2	0.3	0.0	0.0	0.5	0.0	0.5	0.4	0.5	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	11.6	0.0	2.6	0.3	0.0	0.0	4.0	0.0	4.0	3.9	3.5	3.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	93.9	0.0	55.5	78.9	0.0	0.0	8.9	0.0	9.1	8.8	8.9	9.0
LnGrp LOS	F	A	E	E	A	A	A	A	A	A	A	A
Approach Vol, veh/h		539			7			602			830	
Approach Delay, s/veh		88.7			78.9			9.0			8.9	
Approach LOS		F			E			A			A	
Timer - Assigned Phs		2	3	4		6		8				
Phs Duration (G+Y+Rc), s		128.0	29.0	13.0		128.0		42.0				
Change Period (Y+Rc), s		7.1	4.5	6.2		7.1		6.2				
Max Green Setting (Gmax), s		97.9	24.5	29.8		97.9		58.8				
Max Q Clear Time (g_c+I1), s		11.9	24.5	2.6		12.3		7.9				
Green Ext Time (p_c), s		11.5	0.0	0.0		9.0		0.4				

### Intersection Summary

HCM 6th Ctrl Delay	30.9
HCM 6th LOS	C

### Notes

User approved pedestrian interval to be less than phase max green.

Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔			↔			↔			↔↔	
Traffic Vol, veh/h	0	0	1	0	0	20	0	924	2	1	738	2
Future Vol, veh/h	0	0	1	0	0	20	0	924	2	1	738	2
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	0	0	0	0	0	0	0	1	0	0	2	0
Mvmt Flow	0	0	1	0	0	21	0	983	2	1	785	2

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1280	1773	394	1300	1773	493	787	0	0	985	0	0
Stage 1	788	788	-	984	984	-	-	-	-	-	-	-
Stage 2	492	985	-	316	789	-	-	-	-	-	-	-
Critical Hdwy	6.95	6.5	7.1	6.95	6.5	6.9	5.3	-	-	4.1	-	-
Critical Hdwy Stg 1	7.3	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.7	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.65	4	3.9	3.65	4	3.3	3.1	-	-	2.2	-	-
Pot Cap-1 Maneuver	149	84	521	144	84	527	502	-	-	709	-	-
Stage 1	290	405	-	264	329	-	-	-	-	-	-	-
Stage 2	515	329	-	640	405	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	143	84	521	143	84	527	502	-	-	709	-	-
Mov Cap-2 Maneuver	143	84	-	143	84	-	-	-	-	-	-	-
Stage 1	290	404	-	264	329	-	-	-	-	-	-	-
Stage 2	494	329	-	637	404	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB			
HCM Control Delay, s	11.9		12.1		0		0			
HCM LOS	B		B							

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	502	-	-	521	527	709	-
HCM Lane V/C Ratio	-	-	-	0.002	0.04	0.002	-
HCM Control Delay (s)	0	-	-	11.9	12.1	10.1	0
HCM Lane LOS	A	-	-	B	B	B	A
HCM 95th %tile Q(veh)	0	-	-	0	0.1	0	-

HCM 6th Signalized Intersection Summary  
37: 9th St W & 6th Ave W

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗						↖	↗	↖	↗	
Traffic Volume (veh/h)	551	1173	46	0	0	0	0	927	37	132	666	0
Future Volume (veh/h)	551	1173	46	0	0	0	0	927	37	132	666	0
Initial Q (Qb), veh	0	0	0				0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00				1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00				1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No						No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1648				0	1900	1856	1900	1900	0
Adj Flow Rate, veh/h	580	1235	48				0	976	39	139	701	0
Peak Hour Factor	0.95	0.95	0.95				0.95	0.95	0.95	0.95	0.95	0.95
Percent Heavy Veh, %	0	0	17				0	0	3	0	0	0
Cap, veh/h	1004	1965	76				0	953	38	162	1348	0
Arrive On Green	0.55	0.55	0.55				0.00	0.27	0.27	0.02	0.12	0.00
Sat Flow, veh/h	1810	3543	138				0	3633	141	1810	3705	0
Grp Volume(v), veh/h	580	629	654				0	498	517	139	701	0
Grp Sat Flow(s),veh/h/ln	1810	1805	1875				0	1805	1875	1810	1805	0
Q Serve(g_s), s	35.7	40.5	40.6				0.0	45.8	45.8	9.2	30.9	0.0
Cycle Q Clear(g_c), s	35.7	40.5	40.6				0.0	45.8	45.8	9.2	30.9	0.0
Prop In Lane	1.00		0.07				0.00		0.08	1.00		0.00
Lane Grp Cap(c), veh/h	1004	1001	1040				0	486	505	162	1348	0
V/C Ratio(X)	0.58	0.63	0.63				0.00	1.02	1.02	0.86	0.52	0.00
Avail Cap(c_a), veh/h	1004	1001	1040				0	486	505	186	1348	0
HCM Platoon Ratio	1.00	1.00	1.00				1.00	1.00	1.00	0.33	0.33	1.00
Upstream Filter(I)	1.00	1.00	1.00				0.00	1.00	1.00	0.88	0.88	0.00
Uniform Delay (d), s/veh	24.8	25.9	25.9				0.0	62.1	62.1	48.1	60.2	0.0
Incr Delay (d2), s/veh	2.4	3.0	2.9				0.0	47.1	46.4	25.7	1.3	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0				0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	16.0	18.1	18.8				0.0	27.3	28.2	5.5	15.4	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	27.2	28.8	28.8				0.0	109.2	108.5	73.8	61.5	0.0
LnGrp LOS	C	C	C				A	F	F	E	E	A
Approach Vol, veh/h		1863						1015			840	
Approach Delay, s/veh		28.3						108.9			63.5	
Approach LOS		C						F			E	
Timer - Assigned Phs			3	4		6		8				
Phs Duration (G+Y+Rc), s			17.7	52.3		100.0		70.0				
Change Period (Y+Rc), s			6.5	6.5		5.7		6.5				
Max Green Setting (Gmax), s			13.5	43.5		94.3		63.5				
Max Q Clear Time (g_c+I1), s			11.2	47.8		42.6		32.9				
Green Ext Time (p_c), s			0.1	0.0		34.8		5.2				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay			58.3									
HCM 6th LOS			E									

HCM 6th Signalized Intersection Summary  
38: 9th St W & Manatee Ave W

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations				↵	↕	↗	↵	↕			↕	↗
Traffic Volume (veh/h)	0	0	0	80	1176	343	27	1443	0	0	745	460
Future Volume (veh/h)	0	0	0	80	1176	343	27	1443	0	0	745	460
Initial Q (Qb), veh				0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)				1.00		1.00	1.00		1.00	1.00		0.99
Parking Bus, Adj				1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach				No			No			No		
Adj Sat Flow, veh/h/ln				1885	1885	1900	1900	1900	0	0	1900	1900
Adj Flow Rate, veh/h				85	1251	365	29	1535	0	0	793	489
Peak Hour Factor				0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %				1	1	0	0	0	0	0	0	0
Cap, veh/h				794	1585	712	211	1756	0	0	1547	682
Arrive On Green				0.44	0.44	0.44	0.01	0.16	0.00	0.00	0.57	0.57
Sat Flow, veh/h				1795	3582	1610	1810	3705	0	0	3705	1590
Grp Volume(v), veh/h				85	1251	365	29	1535	0	0	793	489
Grp Sat Flow(s),veh/h/ln				1795	1791	1610	1810	1805	0	0	1805	1590
Q Serve(g_s), s				4.7	50.9	27.8	1.5	70.6	0.0	0.0	22.7	38.0
Cycle Q Clear(g_c), s				4.7	50.9	27.8	1.5	70.6	0.0	0.0	22.7	38.0
Prop In Lane				1.00		1.00	1.00		0.00	0.00		1.00
Lane Grp Cap(c), veh/h				794	1585	712	211	1756	0	0	1547	682
V/C Ratio(X)				0.11	0.79	0.51	0.14	0.87	0.00	0.00	0.51	0.72
Avail Cap(c_a), veh/h				794	1585	712	266	1888	0	0	1569	691
HCM Platoon Ratio				1.00	1.00	1.00	0.33	0.33	1.00	1.00	1.33	1.33
Upstream Filter(I)				1.00	1.00	1.00	0.31	0.31	0.00	0.00	0.92	0.92
Uniform Delay (d), s/veh				27.7	40.6	34.2	27.3	66.3	0.0	0.0	25.8	29.1
Incr Delay (d2), s/veh				0.3	4.1	2.6	0.1	1.5	0.0	0.0	0.3	3.3
Initial Q Delay(d3),s/veh				0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln				2.1	23.2	11.5	0.7	34.9	0.0	0.0	9.1	14.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh				28.0	44.7	36.8	27.4	67.8	0.0	0.0	26.0	32.3
LnGrp LOS				C	D	D	C	E	A	A	C	C
Approach Vol, veh/h					1701			1564			1282	
Approach Delay, s/veh					42.2			67.0			28.4	
Approach LOS					D			E			C	
Timer - Assigned Phs		2		4			7	8				
Phs Duration (G+Y+Rc), s		81.2		88.8			9.8	79.0				
Change Period (Y+Rc), s		6.0		6.1			6.1	6.1				
Max Green Setting (Gmax), s		69.0		88.9			8.9	73.9				
Max Q Clear Time (g_c+I1), s		52.9		72.6			3.5	40.0				
Green Ext Time (p_c), s		13.4		10.1			0.0	8.7				

Intersection Summary

HCM 6th Ctrl Delay	46.8
HCM 6th LOS	D

Notes

User approved ignoring U-Turning movement.

HCM 6th Signalized Intersection Summary  
39: 9th St W & 3rd Ave W

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	337	46	9	64	65	169	20	1758	36	29	1099	124
Future Volume (veh/h)	337	46	9	64	65	169	20	1758	36	29	1099	124
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1870	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	398	0	0	67	68	178	21	1851	38	31	1157	131
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Percent Heavy Veh, %	0	0	0	0	2	0	0	0	0	0	0	0
Cap, veh/h	448	235	0	221	228	196	274	2115	43	138	2783	315
Arrive On Green	0.12	0.00	0.00	0.12	0.12	0.12	0.02	0.78	0.78	0.02	0.59	0.59
Sat Flow, veh/h	3619	1900	0	1810	1870	1610	1810	3618	74	1810	4727	535
Grp Volume(v), veh/h	398	0	0	67	68	178	21	921	968	31	846	442
Grp Sat Flow(s),veh/h/ln	1810	1900	0	1810	1870	1610	1810	1805	1887	1810	1729	1804
Q Serve(g_s), s	18.4	0.0	0.0	5.7	5.6	18.6	0.8	60.0	61.1	1.2	22.7	22.7
Cycle Q Clear(g_c), s	18.4	0.0	0.0	5.7	5.6	18.6	0.8	60.0	61.1	1.2	22.7	22.7
Prop In Lane	1.00		0.00	1.00		1.00	1.00		0.04	1.00		0.30
Lane Grp Cap(c), veh/h	448	235	0	221	228	196	274	1055	1103	138	2036	1062
V/C Ratio(X)	0.89	0.00	0.00	0.30	0.30	0.91	0.08	0.87	0.88	0.22	0.42	0.42
Avail Cap(c_a), veh/h	534	281	0	233	241	207	331	1055	1103	188	2036	1062
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.33	1.33	1.33	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	1.00	1.00	0.52	0.52	0.52	1.00	1.00	1.00
Uniform Delay (d), s/veh	73.3	0.0	0.0	68.1	68.0	73.7	14.9	14.5	14.7	25.9	19.0	19.0
Incr Delay (d2), s/veh	14.8	0.0	0.0	0.8	0.7	36.9	0.1	5.5	5.5	0.8	0.6	1.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	9.5	0.0	0.0	2.7	2.8	9.7	0.3	19.9	21.1	0.5	9.3	9.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	88.1	0.0	0.0	68.8	68.7	110.6	15.0	20.1	20.2	26.7	19.7	20.2
LnGrp LOS	F	A	A	E	E	F	B	C	C	C	B	C
Approach Vol, veh/h		398			313			1910			1319	
Approach Delay, s/veh		88.1			92.6			20.1			20.0	
Approach LOS		F			F			C			C	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	9.6	106.6		26.8	10.3	105.9		26.9				
Change Period (Y+Rc), s	6.5	6.5		6.1	6.5	6.5		5.9				
Max Green Setting (Gmax), s	5	89.5		21.9	8.5	89.5		25.1				
Max Q Clear Time (g_c+1), s	12.8	24.7		20.6	3.2	63.1		20.4				
Green Ext Time (p_c), s	0.0	26.8		0.2	0.0	23.6		0.6				

Intersection Summary

HCM 6th Ctrl Delay	32.7
HCM 6th LOS	C

Notes

- User approved pedestrian interval to be less than phase max green.
- User approved volume balancing among the lanes for turning movement.
- User approved ignoring U-Turning movement.

Lanes, Volumes, Timings  
40: 8th Ave W & 5th St W

Existing PM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	12	18	28	49	14	14	24	1528	61	2	1025	29
Future Volume (vph)	12	18	28	49	14	14	24	1528	61	2	1025	29
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	80		0	0		0	0		0	0		0
Storage Lanes	1		0	0		0	0		0	0		0
Taper Length (ft)	20			0			0			0		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	0.95	0.95	0.95	0.95
Frt		0.907			0.976			0.994			0.996	
Flt Protected	0.950				0.969			0.999				
Satd. Flow (prot)	1805	1723	0	0	1797	0	0	3551	0	0	3596	0
Flt Permitted	0.697				0.778			0.918			0.953	
Satd. Flow (perm)	1324	1723	0	0	1443	0	0	3263	0	0	3427	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		29			7			5			3	
Link Speed (mph)		25			25			30			30	
Link Distance (ft)		405			371			3271			629	
Travel Time (s)		11.0			10.1			74.3			14.3	
Peak Hour Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Heavy Vehicles (%)	0%	0%	0%	0%	0%	0%	0%	1%	0%	0%	0%	0%
Adj. Flow (vph)	12	18	29	50	14	14	24	1559	62	2	1046	30
Shared Lane Traffic (%)												
Lane Group Flow (vph)	12	47	0	0	78	0	0	1645	0	0	1078	0
Enter Blocked Intersection	No	No	No	No	No	No	No	No	No	No	No	No
Lane Alignment	Left	Left	Right	Left	Left	Right	Left	Left	Right	Left	Left	Right
Median Width(ft)		12			12			0			0	
Link Offset(ft)		0			0			0			0	
Crosswalk Width(ft)		16			16			16			16	
Two way Left Turn Lane												
Headway Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Turning Speed (mph)	15		9	15		9	15		9	15		9
Number of Detectors	1	2		1	2		1	2		1	2	
Detector Template	Left	Thru		Left	Thru		Left	Thru		Left	Thru	
Leading Detector (ft)	20	100		20	100		20	100		20	100	
Trailing Detector (ft)	0	0		0	0		0	0		0	0	
Detector 1 Position(ft)	0	0		0	0		0	0		0	0	
Detector 1 Size(ft)	20	6		20	6		20	6		20	6	
Detector 1 Type	Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex		Cl+Ex	Cl+Ex	
Detector 1 Channel												
Detector 1 Extend (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Queue (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 1 Delay (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Detector 2 Position(ft)		94			94			94			94	
Detector 2 Size(ft)		6			6			6			6	
Detector 2 Type		Cl+Ex			Cl+Ex			Cl+Ex			Cl+Ex	
Detector 2 Channel												
Detector 2 Extend (s)		0.0			0.0			0.0			0.0	
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		Perm	NA	
Protected Phases		8			4		1	6			2	

Lanes, Volumes, Timings  
40: 8th Ave W & 5th St W

Existing PM  
09/06/2023



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Permitted Phases	8			4			6			2			
Detector Phase	8	8		4	4		1	6		2	2		
Switch Phase													
Minimum Initial (s)	7.0	7.0		7.0	7.0		5.0	8.0		8.0	8.0		
Minimum Split (s)	30.7	30.7		31.7	31.7		10.8	24.9		29.9	29.9		
Total Split (s)	45.0	45.0		45.0	45.0		20.0	105.0		85.0	85.0		
Total Split (%)	30.0%	30.0%		30.0%	30.0%		13.3%	70.0%		56.7%	56.7%		
Maximum Green (s)	39.3	39.3		39.3	39.3		14.2	99.1		79.1	79.1		
Yellow Time (s)	3.7	3.7		3.7	3.7		3.7	3.7		3.7	3.7		
All-Red Time (s)	2.0	2.0		2.0	2.0		2.1	2.2		2.2	2.2		
Lost Time Adjust (s)	0.0	0.0			0.0			0.0			0.0		
Total Lost Time (s)	5.7	5.7			5.7			5.9			5.9		
Lead/Lag							Lead					Lag	Lag
Lead-Lag Optimize?							Yes					Yes	Yes
Vehicle Extension (s)	3.0	3.0		3.0	3.0		3.0	5.0		5.0	5.0		
Recall Mode	None	None		None	None		None	C-Min		C-Min	C-Min		
Walk Time (s)	7.0	7.0		7.0	7.0			7.0		7.0	7.0		
Flash Dont Walk (s)	18.0	18.0		19.0	19.0			12.0		17.0	17.0		
Pedestrian Calls (#/hr)	0	0		0	0			0		0	0		
Act Effct Green (s)	12.6	12.6			12.6			125.8			125.8		
Actuated g/C Ratio	0.08	0.08			0.08			0.84			0.84		
v/c Ratio	0.11	0.27			0.61			0.60			0.38		
Control Delay	63.1	33.9			79.4			5.4			2.0		
Queue Delay	0.0	0.0			0.0			0.0			0.0		
Total Delay	63.1	33.9			79.4			5.4			2.0		
LOS	E	C			E			A			A		
Approach Delay		39.8			79.4			5.4			2.0		
Approach LOS		D			E			A			A		

Intersection Summary

Area Type:	Other
Cycle Length:	150
Actuated Cycle Length:	150
Offset:	27 (18%), Referenced to phase 2:SBTL and 6:NBTL, Start of Green
Natural Cycle:	80
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.61
Intersection Signal Delay:	6.8
Intersection LOS:	A
Intersection Capacity Utilization	81.9%
ICU Level of Service	D
Analysis Period (min)	15

Splits and Phases: 40: 8th Ave W & 5th St W



Intersection												
Int Delay, s/veh	0.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	0	0	40	0	0	1	20	1538	1	0	985	13
Future Vol, veh/h	0	0	40	0	0	1	20	1538	1	0	985	13
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	99	99	99	99	99	99	99	99	99	99	99	99
Heavy Vehicles, %	0	0	0	0	0	0	0	1	0	0	0	0
Mvmt Flow	0	0	40	0	0	1	20	1554	1	0	995	13

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1819	2597	504	2093	2603	778	1008	0	0	1555	0	0
Stage 1	1002	1002	-	1595	1595	-	-	-	-	-	-	-
Stage 2	817	1595	-	498	1008	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	50	25	518	31	25	343	695	-	-	431	-	-
Stage 1	264	323	-	114	168	-	-	-	-	-	-	-
Stage 2	341	168	-	528	321	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	42	20	518	24	20	343	695	-	-	431	-	-
Mov Cap-2 Maneuver	42	20	-	24	20	-	-	-	-	-	-	-
Stage 1	208	323	-	90	133	-	-	-	-	-	-	-
Stage 2	268	133	-	487	321	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	12.5		15.5		1.2		0	
HCM LOS	B		C					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	695	-	-	518	343	431	-
HCM Lane V/C Ratio	0.029	-	-	0.078	0.003	-	-
HCM Control Delay (s)	10.3	1.1	-	12.5	15.5	0	-
HCM Lane LOS	B	A	-	B	C	A	-
HCM 95th %tile Q(veh)	0.1	-	-	0.3	0	0	-



HCM 6th Signalized Intersection Summary  
42: 8th Ave W & 7th St W

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕		↕	↕		↕	↕↔		↕	↕↔	
Traffic Volume (veh/h)	20	120	24	133	132	95	46	1372	120	46	841	26
Future Volume (veh/h)	20	120	24	133	132	95	46	1372	120	46	841	26
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1885	1885	1900	1900	1870	1900	1900	1900
Adj Flow Rate, veh/h	20	122	24	136	135	97	47	1400	122	47	858	27
Peak Hour Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Percent Heavy Veh, %	0	0	0	0	1	1	0	0	2	0	0	0
Cap, veh/h	44	236	43	208	209	150	460	2172	188	325	2551	80
Arrive On Green	0.21	0.21	0.21	0.21	0.21	0.21	1.00	1.00	1.00	0.06	1.00	1.00
Sat Flow, veh/h	85	1151	209	1262	1020	733	638	3361	292	1810	3572	112
Grp Volume(v), veh/h	166	0	0	136	0	232	47	749	773	47	434	451
Grp Sat Flow(s),veh/h/ln	1445	0	0	1262	0	1753	638	1805	1848	1810	1805	1880
Q Serve(g_s), s	0.6	0.0	0.0	8.9	0.0	18.2	0.0	0.0	0.0	1.2	0.0	0.0
Cycle Q Clear(g_c), s	18.8	0.0	0.0	27.6	0.0	18.2	0.0	0.0	0.0	1.2	0.0	0.0
Prop In Lane	0.12		0.14	1.00		0.42	1.00		0.16	1.00		0.06
Lane Grp Cap(c), veh/h	323	0	0	208	0	360	460	1166	1194	325	1289	1343
V/C Ratio(X)	0.51	0.00	0.00	0.65	0.00	0.65	0.10	0.64	0.65	0.14	0.34	0.34
Avail Cap(c_a), veh/h	417	0	0	276	0	455	460	1166	1194	443	1289	1343
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	2.00	2.00	2.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	52.2	0.0	0.0	61.6	0.0	54.6	0.0	0.0	0.0	7.3	0.0	0.0
Incr Delay (d2), s/veh	1.3	0.0	0.0	3.5	0.0	2.1	0.4	2.7	2.7	0.2	0.7	0.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.6	0.0	0.0	5.3	0.0	8.2	0.1	0.9	0.9	0.5	0.3	0.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	53.4	0.0	0.0	65.1	0.0	56.7	0.4	2.7	2.7	7.5	0.7	0.7
LnGrp LOS	D	A	A	E	A	E	A	A	A	A	A	A
Approach Vol, veh/h		166			368			1569			932	
Approach Delay, s/veh		53.4			59.8			2.7			1.0	
Approach LOS		D			E			A			A	
Timer - Assigned Phs		2		4	5	6		8				
Phs Duration (G+Y+Rc), s		113.1		36.9	10.2	102.9		36.9				
Change Period (Y+Rc), s		6.0		* 6.1	5.9	6.0		* 6.1				
Max Green Setting (Gmax), s		99.0		* 39	14.1	79.0		* 39				
Max Q Clear Time (g_c+I1), s		2.0		29.6	3.2	2.0		20.8				
Green Ext Time (p_c), s		16.3		1.2	0.0	44.5		0.8				

Intersection Summary

HCM 6th Ctrl Delay	11.9
HCM 6th LOS	B

Notes

\* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Intersection												
Int Delay, s/veh	0.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	3	1	8	0	0	37	3	1465	8	3	905	0
Future Vol, veh/h	3	1	8	0	0	37	3	1465	8	3	905	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	98	98	98	98	98	98	98	98	98	98	98	98
Heavy Vehicles, %	0	0	0	0	0	5	0	0	0	0	0	0
Mvmt Flow	3	1	8	0	0	38	3	1495	8	3	923	0

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1683	2438	462	1973	2434	752	923	0	0	1503	0	0
Stage 1	929	929	-	1505	1505	-	-	-	-	-	-	-
Stage 2	754	1509	-	468	929	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	7	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.35	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	63	32	552	38	32	346	748	-	-	452	-	-
Stage 1	292	349	-	129	186	-	-	-	-	-	-	-
Stage 2	372	185	-	550	349	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	54	31	552	35	31	346	748	-	-	452	-	-
Mov Cap-2 Maneuver	54	31	-	35	31	-	-	-	-	-	-	-
Stage 1	285	344	-	126	182	-	-	-	-	-	-	-
Stage 2	323	181	-	533	344	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	39.3		16.7		0.1		0.1	
HCM LOS	E		C					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	748	-	-	117	346	452	-
HCM Lane V/C Ratio	0.004	-	-	0.105	0.109	0.007	-
HCM Control Delay (s)	9.8	0.1	-	39.3	16.7	13	0.1
HCM Lane LOS	A	A	-	E	C	B	A
HCM 95th %tile Q(veh)	0	-	-	0.3	0.4	0	-

Intersection						
Int Delay, s/veh	0.5					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations		↗		↖↗	↖↗	
Traffic Vol, veh/h	0	32	10	1504	908	19
Future Vol, veh/h	0	32	10	1504	908	19
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	96	96	96	96	96	96
Heavy Vehicles, %	0	0	0	1	0	0
Mvmt Flow	0	33	10	1567	946	20

Major/Minor	Minor2	Major1	Major2		
Conflicting Flow All	-	483	966	0	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	6.9	4.1	-	-
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	3.3	2.2	-	-
Pot Cap-1 Maneuver	0	535	721	-	-
Stage 1	0	-	-	-	-
Stage 2	0	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	-	535	721	-	-
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	12.2	0.6	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	721	-	535	-	-
HCM Lane V/C Ratio	0.014	-	0.062	-	-
HCM Control Delay (s)	10.1	0.5	12.2	-	-
HCM Lane LOS	B	A	B	-	-
HCM 95th %tile Q(veh)	0	-	0.2	-	-

HCM 6th Signalized Intersection Summary  
45: 8th Ave W & 10th St W

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	131	248	35	189	285	35	108	1245	139	89	703	58
Future Volume (veh/h)	131	248	35	189	285	35	108	1245	139	89	703	58
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1885	1900	1900	1900	1856	1900	1885	1885	1885	1900	1900
Adj Flow Rate, veh/h	135	256	36	195	294	36	111	1284	143	92	725	60
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Percent Heavy Veh, %	0	1	0	0	0	3	0	1	1	1	0	0
Cap, veh/h	215	280	39	250	337	41	352	1695	188	309	1737	144
Arrive On Green	0.07	0.17	0.17	0.10	0.20	0.20	0.09	1.00	1.00	0.01	0.17	0.17
Sat Flow, veh/h	1810	1617	227	1810	1660	203	1810	3251	361	1795	3376	279
Grp Volume(v), veh/h	135	0	292	195	0	330	111	705	722	92	388	397
Grp Sat Flow(s),veh/h/ln	1810	0	1844	1810	0	1863	1810	1791	1820	1795	1805	1850
Q Serve(g_s), s	9.1	0.0	23.3	13.2	0.0	25.7	4.4	0.0	0.0	3.6	28.8	28.8
Cycle Q Clear(g_c), s	9.1	0.0	23.3	13.2	0.0	25.7	4.4	0.0	0.0	3.6	28.8	28.8
Prop In Lane	1.00		0.12	1.00		0.11	1.00		0.20	1.00		0.15
Lane Grp Cap(c), veh/h	215	0	319	250	0	378	352	934	949	309	929	952
V/C Ratio(X)	0.63	0.00	0.92	0.78	0.00	0.87	0.31	0.75	0.76	0.30	0.42	0.42
Avail Cap(c_a), veh/h	224	0	349	253	0	378	455	934	949	387	929	952
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	0.33	0.33	0.33
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	47.7	0.0	61.0	46.4	0.0	57.9	18.0	0.0	0.0	16.3	42.2	42.2
Incr Delay (d2), s/veh	5.2	0.0	27.5	14.3	0.0	20.1	0.5	5.6	5.7	0.5	1.4	1.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	4.4	0.0	13.3	6.9	0.0	14.2	1.8	1.5	1.5	1.6	14.4	14.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	52.9	0.0	88.4	60.7	0.0	78.0	18.5	5.6	5.7	16.8	43.5	43.5
LnGrp LOS	D	A	F	E	A	E	B	A	A	B	D	D
Approach Vol, veh/h		427			525			1538			877	
Approach Delay, s/veh		77.2			71.6			6.6			40.7	
Approach LOS		E			E			A			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	12.5	83.2	21.8	32.5	11.5	84.2	17.3	37.0				
Change Period (Y+Rc), s	6.0	6.0	6.6	6.6	5.9	6.0	* 6.2	6.6				
Max Green Setting (Gmax), s	15.0	66.0	15.4	28.4	12.1	69.0	* 12	28.4				
Max Q Clear Time (g_c+I1), s	6.4	30.8	15.2	25.3	5.6	2.0	11.1	27.7				
Green Ext Time (p_c), s	0.2	11.4	0.0	0.6	0.1	35.6	0.0	0.2				

Intersection Summary

HCM 6th Ctrl Delay	34.6
HCM 6th LOS	C

Notes

- User approved pedestrian interval to be less than phase max green.
- \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Intersection						
Int Delay, s/veh	0.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		
Traffic Vol, veh/h	1	42	3	1412	797	12
Future Vol, veh/h	1	42	3	1412	797	12
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	0	0	0	1	0	0
Mvmt Flow	1	44	3	1486	839	13

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1595	426	852	0	-	0
Stage 1	846	-	-	-	-	-
Stage 2	749	-	-	-	-	-
Critical Hdwy	6.8	6.9	4.1	-	-	-
Critical Hdwy Stg 1	5.8	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.5	3.3	2.2	-	-	-
Pot Cap-1 Maneuver	100	582	795	-	-	-
Stage 1	386	-	-	-	-	-
Stage 2	433	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	98	582	795	-	-	-
Mov Cap-2 Maneuver	98	-	-	-	-	-
Stage 1	378	-	-	-	-	-
Stage 2	433	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	12.6	0.1	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	795	-	522	-	-
HCM Lane V/C Ratio	0.004	-	0.087	-	-
HCM Control Delay (s)	9.5	0.1	12.6	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0	-	0.3	-	-

Intersection												
Int Delay, s/veh	2.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	0	0	7	19	1	9	12	1419	20	1	876	3
Future Vol, veh/h	0	0	7	19	1	9	12	1419	20	1	876	3
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	97	97	97	97	97	97	97	97	97	97	97	97
Heavy Vehicles, %	0	0	0	0	0	0	8	0	0	0	1	0
Mvmt Flow	0	0	7	20	1	9	12	1463	21	1	903	3

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1663	2415	453	1952	2406	742	906	0	0	1484	0	0
Stage 1	907	907	-	1498	1498	-	-	-	-	-	-	-
Stage 2	756	1508	-	454	908	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.26	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.28	-	-	2.2	-	-
Pot Cap-1 Maneuver	65	33	559	39	34	363	710	-	-	459	-	-
Stage 1	301	357	-	131	187	-	-	-	-	-	-	-
Stage 2	371	185	-	560	357	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	57	30	559	36	31	363	710	-	-	459	-	-
Mov Cap-2 Maneuver	57	30	-	36	31	-	-	-	-	-	-	-
Stage 1	272	356	-	118	169	-	-	-	-	-	-	-
Stage 2	324	167	-	551	356	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	11.5	152.5	0.6	0
HCM LOS	B	F		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	710	-	-	559	50	459	-
HCM Lane V/C Ratio	0.017	-	-	0.013	0.598	0.002	-
HCM Control Delay (s)	10.2	0.5	-	11.5	152.5	12.9	0
HCM Lane LOS	B	A	-	B	F	B	A
HCM 95th %tile Q(veh)	0.1	-	-	0	2.3	0	-

HCM 6th Signalized Intersection Summary  
48: 8th Ave W & 17th St W

Existing PM  
09/06/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↕		↖	↗	
Traffic Volume (veh/h)	300	190	51	166	168	7	68	1176	179	22	676	142
Future Volume (veh/h)	300	190	51	166	168	7	68	1176	179	22	676	142
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1885	1885	1900	1885	1885	1900	1900	1885	1900	1900	1900	1900
Adj Flow Rate, veh/h	319	202	54	177	179	7	72	1251	190	23	719	151
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	1	1	0	1	1	0	0	1	0	0	0	0
Cap, veh/h	308	226	60	244	262	10	358	1709	258	288	1598	335
Arrive On Green	0.11	0.16	0.16	0.10	0.15	0.15	0.06	1.00	1.00	0.02	0.54	0.54
Sat Flow, veh/h	1795	1433	383	1795	1802	70	1810	3120	471	1810	2969	623
Grp Volume(v), veh/h	319	0	256	177	0	186	72	715	726	23	437	433
Grp Sat Flow(s),veh/h/ln	1795	0	1816	1795	0	1872	1810	1791	1800	1810	1805	1788
Q Serve(g_s), s	16.5	0.0	20.7	12.4	0.0	14.1	2.7	0.0	0.0	0.9	22.1	22.2
Cycle Q Clear(g_c), s	16.5	0.0	20.7	12.4	0.0	14.1	2.7	0.0	0.0	0.9	22.1	22.2
Prop In Lane	1.00		0.21	1.00		0.04	1.00		0.26	1.00		0.35
Lane Grp Cap(c), veh/h	308	0	286	244	0	273	358	981	986	288	971	962
V/C Ratio(X)	1.04	0.00	0.89	0.73	0.00	0.68	0.20	0.73	0.74	0.08	0.45	0.45
Avail Cap(c_a), veh/h	308	0	379	265	0	391	464	981	986	388	971	962
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	2.00	2.00	2.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	54.9	0.0	62.0	49.2	0.0	60.8	15.8	0.0	0.0	14.8	21.1	21.1
Incr Delay (d2), s/veh	61.3	0.0	18.8	8.8	0.0	3.0	0.3	4.7	4.9	0.1	1.5	1.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	8.9	0.0	11.1	6.2	0.0	7.0	1.1	1.3	1.3	0.4	9.7	9.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	116.2	0.0	80.7	58.0	0.0	63.8	16.0	4.7	4.9	14.9	22.6	22.6
LnGrp LOS	F	A	F	E	A	E	B	A	A	B	C	C
Approach Vol, veh/h		575			363			1513			893	
Approach Delay, s/veh		100.4			60.9			5.4			22.4	
Approach LOS		F			E			A			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	9.7	88.8	21.2	30.3	11.2	87.3	23.0	28.5				
Change Period (Y+Rc), s	6.6	6.6	* 6.7	* 6.7	6.4	6.6	6.5	* 6.7				
Max Green Setting (Gmax), s	11.4	64.4	* 16	* 31	13.6	62.4	16.5	* 31				
Max Q Clear Time (g_c+I1), s	2.9	2.0	14.4	22.7	4.7	24.2	18.5	16.1				
Green Ext Time (p_c), s	0.0	33.9	0.1	0.9	0.1	13.0	0.0	0.8				

Intersection Summary

HCM 6th Ctrl Delay	32.3
HCM 6th LOS	C

Notes

User approved ignoring U-Turning movement.

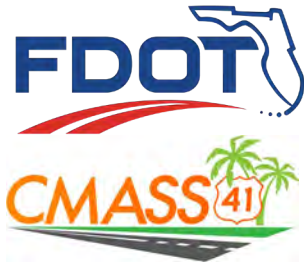
\* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.



## Appendix C: Stakeholder Coordination







US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
**Stakeholder/Agency Meeting (Virtual)**  
**Manasota Black Chamber of Commerce**

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## **Virtual Meeting via Microsoft Teams**

February 1, 2023, 11:00 a.m.

### [Meeting Summary](#)

A virtual stakeholder's meeting with the Manasota Black Chamber of Commerce was conducted via Microsoft Teams on Wednesday, February 1, 2023. The purpose of the meeting was to present information for FDOT's Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS) and conduct an open discussion between the study team and the Manasota Black Chamber of Commerce.

### [Attendees](#)

#### **Project Team**

RK&K: Matt Dockins, PE – Project Manager on behalf of FDOT  
VHB: Amy Sirmans, PE – Consultant Project Manager  
VHB: Heather Garcia – Project Team Member  
VHB: Kennedy Sumner-Snell – Project Team Member  
The Valerin Group: Catherine Winter, PE – Public Involvement Coordinator  
AECOM: Lauren Brooks, AICP – Connecting Communities Program Manager on behalf of FDOT

#### **Stakeholders**

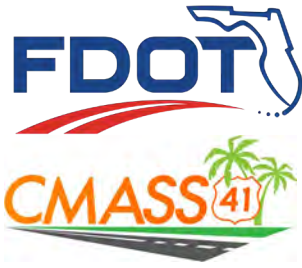
Manasota Back Chamber of Commerce: Tarnisha Cliatt – President, Founder, CEO

### [Project Overview](#)

The project team shared a presentation with meeting attendees providing information on the US 41 CMASS project including project description and background, goals and objectives, schedule, and public outreach. No capacity would be added to this project. The focus would be on how to more efficiently move vehicles, buses, bikes, and pedestrians through the corridor and may involve elements such as signal operations, the need for right-of-way for the addition of sidewalks or trails, and possible roadway resurfacing. A copy of the presentation is included with these notes.

The "Connecting Communities" concept discussed is inclusive of various Sarasota and Manatee County projects with the purpose of holistically enhancing the multi-modal transportation network within those counties. Five major projects of that initiative as being the Bradenton-Palmetto Connector ACE/PD&E Study, the DeSoto Bridge PD&E Study, the Palmetto Trails Network/PD&E Study, CMASS, and 10th Avenue Complete Streets. The project team clarified that not all area projects were listed in the presentation and encouraged stakeholders to advise the study team of any projects of which he felt they should be aware.

The project team used a digital whiteboard application (Mural) to capture and document stakeholder ideas and feedback. A copy of the Mural whiteboard with all feedback received to date from the individual stakeholder meetings is included with these meeting notes.



US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
**Stakeholder/Agency Meeting (Virtual)**  
**Manasota Black Chamber of Commerce**

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## Open Discussion

- Stakeholder Concerns
  - Congestion experience along the corridor.
  - Effects the project elements would have on businesses with specific mention of businesses on the south end of Bradenton heading into Sarasota, which have been struggling for years due to the traffic congestion.
  - Impacts that may be felt by what was described as some of the oldest African American churches in the community.
  - Current lack of grocers/food retailers along the route and whether project elements would further inhibit the establishment of those in the future.
  - Lack of pedestrian crossings.
  - Difficult to use turn lanes. Example shared was drivers attempting to maneuver to/from the USF campus, as well as the airport.
  - Expressed desire for aesthetic improvements/consistency along the route.
  - Implementation of safety improvement.
  - Important to consider planning improvements to address the homeless population.
- Connecting Communities Questionnaire distribution and further outreach efforts
  - Upcoming golf tournament as well as an event in October, to be conducted in partnership with the Manatee Convention Center.
  - Commemoration of Juneteenth, recognized throughout the month of June, would provide opportunities for the dissemination of project information.
  - Recommended the team utilize churches and other entities that would help with spreading information on the project.
  - Recommended that the team meet with area African American sororities.

---

## Attendees Email Addresses

Tarnisha Cliatt, [cliattt@manasotabcc.org](mailto:cliattt@manasotabcc.org); [cliattt@gmail.com](mailto:cliattt@gmail.com)

Matt Dockins, PE, [mdockins@hntb.com](mailto:mdockins@hntb.com)

Amy Sirmans, PE, [asirmans@vhb.com](mailto:asirmans@vhb.com)

Heather Garcia, [hgarcia@vhb.com](mailto:hgarcia@vhb.com)

Kennedy Sumner-Snell, [ksumner-snell@vhb.com](mailto:ksumner-snell@vhb.com)

Lauren Brooks, AICP, [lauren.brooks@aecom.com](mailto:lauren.brooks@aecom.com)

Catherine Winter, PE, [catherine@valerin-group.com](mailto:catherine@valerin-group.com)



**FDOT**

**District One  
Environmental  
Management Office**

***Sarasota/Manatee US 41  
Corridor Mobility and  
Safety Study (CMASS)***

*From University Parkway to 17<sup>th</sup> Street W*

**February 2023**



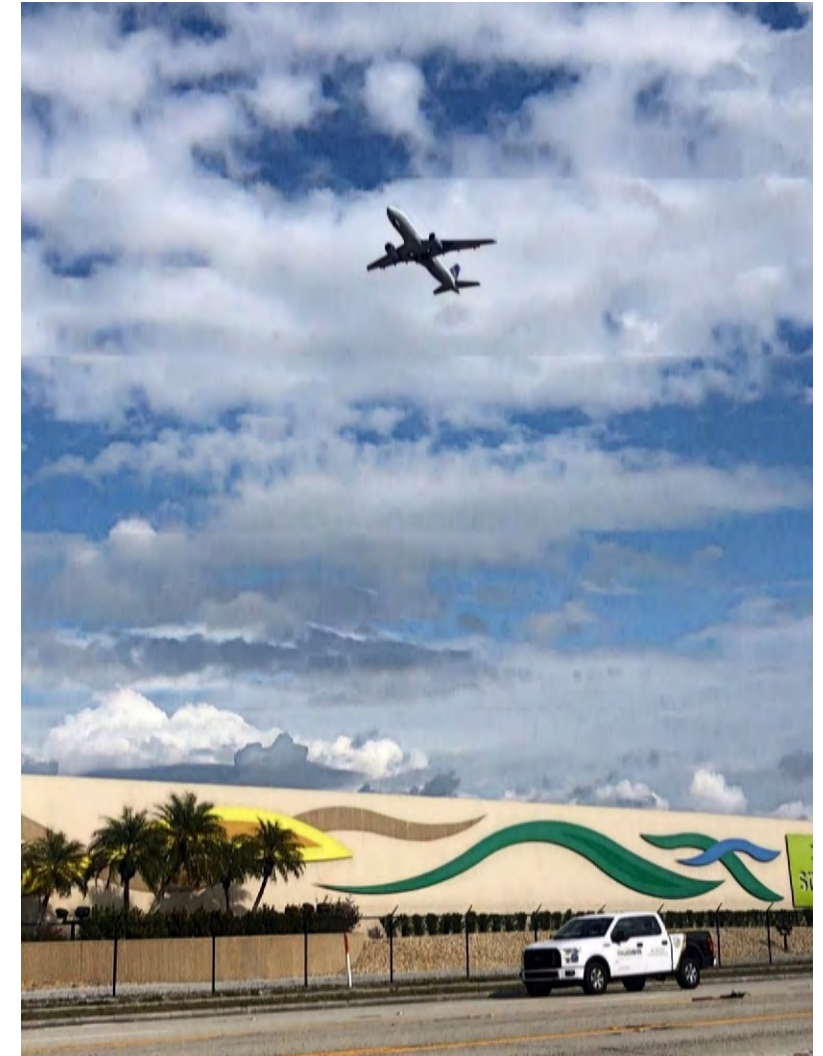
# Manasota Black Chamber of Commerce

February 1, 2023



# Agenda

- Introductions
- Project Description and Background
- US 41 Today: Existing Conditions
- Public Outreach
- Open Discussion: We Want Your Input



# Introductions



**Abra Horne**  
**Administrator**  
District Environmental  
Management Office  
FDOT  
[Abra.Horne@dot.state.fl.us](mailto:Abra.Horne@dot.state.fl.us)  
(863) 519-2239



**Matt Dockins, P.E.**  
**Project Manager**  
RK&K  
[mdockins@rkk.com](mailto:mdockins@rkk.com)  
(813) 386-1460



**Jeffrey James**  
**Environmental Manager**  
District Environmental  
Management Office  
FDOT  
[Jeffrey.James@dot.state.fl.us](mailto:Jeffrey.James@dot.state.fl.us)  
(863) 519-2625

# Introductions



**Lauren Brooks, AICP**  
**Project Manager**  
**Senior Transportation Planner**  
**AECOM**  
[lauren.brooks@aecom.com](mailto:lauren.brooks@aecom.com)  
(813) 636-2162



**Amy Sirmans, P.E.**  
**Project Manager**  
**VHB**  
[asirmans@vhb.com](mailto:asirmans@vhb.com)  
(407) 839-4006



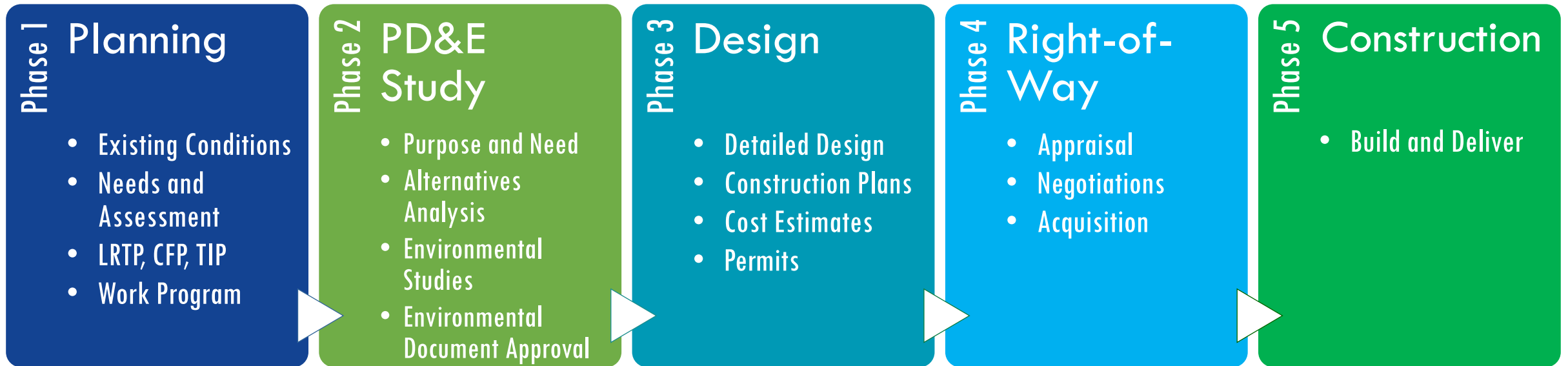
**Catherine Winter, P.E.**  
**Public Involvement Manager**  
**The Valerin Group**  
[Catherine@valerin-group.com](mailto:Catherine@valerin-group.com)  
(813) 431-0919

# Project Description and Background





# Transportation Project Development Process

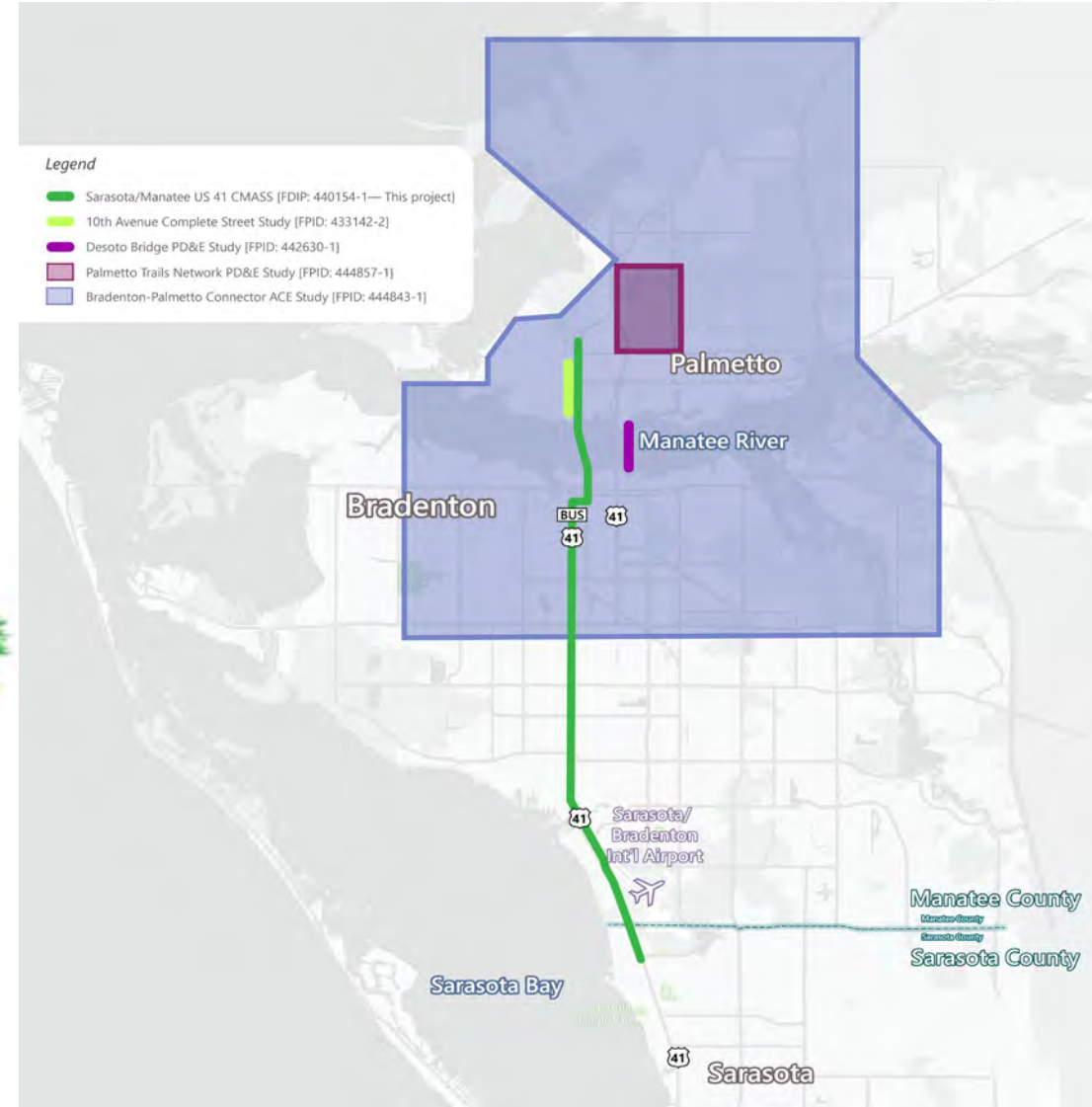


# Connecting Communities

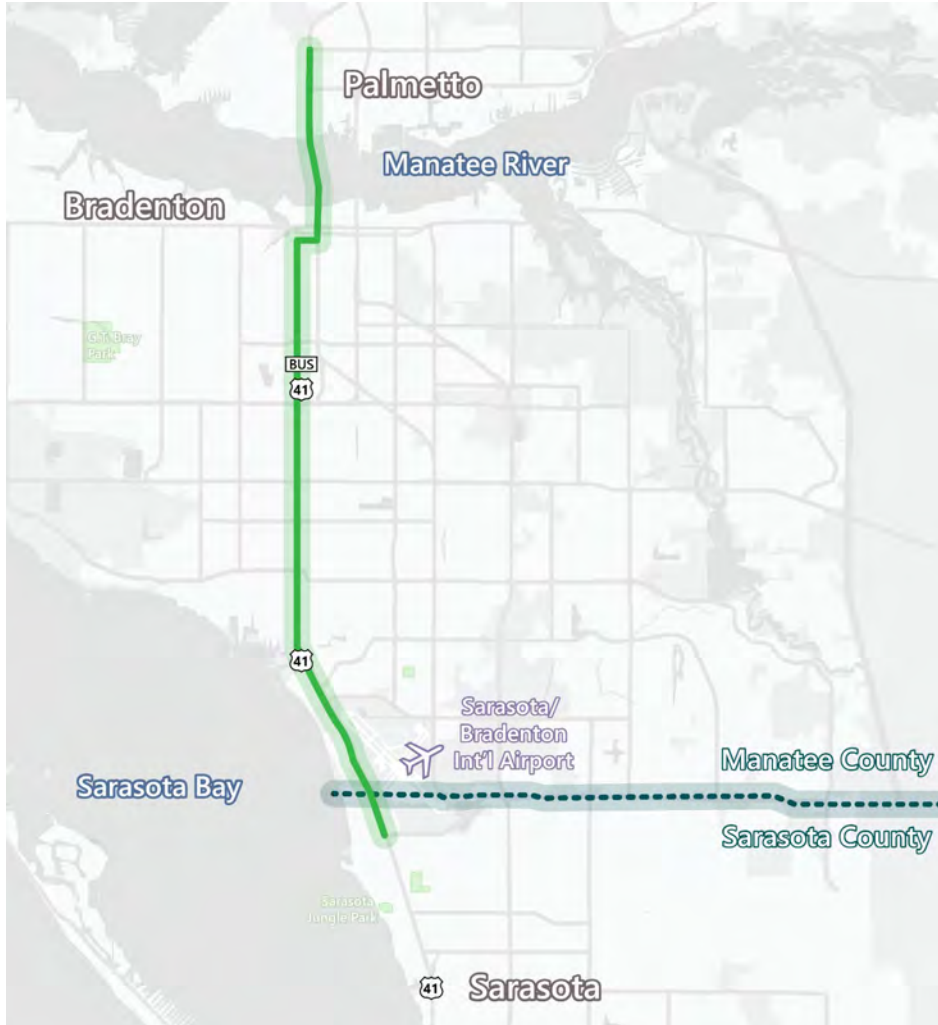
**Purpose:** Holistically enhance the multimodal transportation network within Sarasota and Manatee Counties.



## DeSoto Bridge PD&E Study



# Project Description



**Project Limits:** US 41/US 41 Business corridor from University Parkway (Sarasota) to 17<sup>th</sup> Street W (Palmetto)

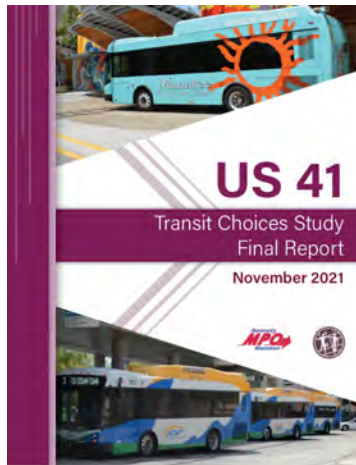
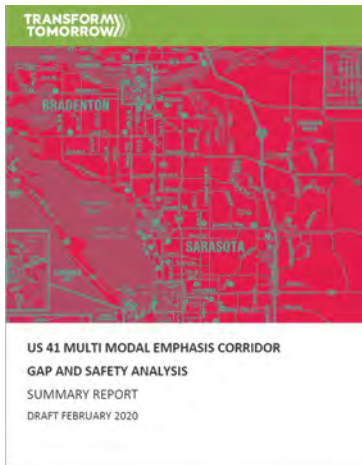
**Financial Project ID:** 440154-1-21-01

**Project Manager:** Matt Dockins

**Status:** Feasibility Study – Phase 1

# We are not starting from scratch...

- US 41 Multi-Modal Emphasis Corridor (MMEC)
- US 41 Transit Choices Study
- US 41 Complete Streets Corridor Planning Study
- Arterial Safety Study
- Access Management Study
- Intersection Safety Studies

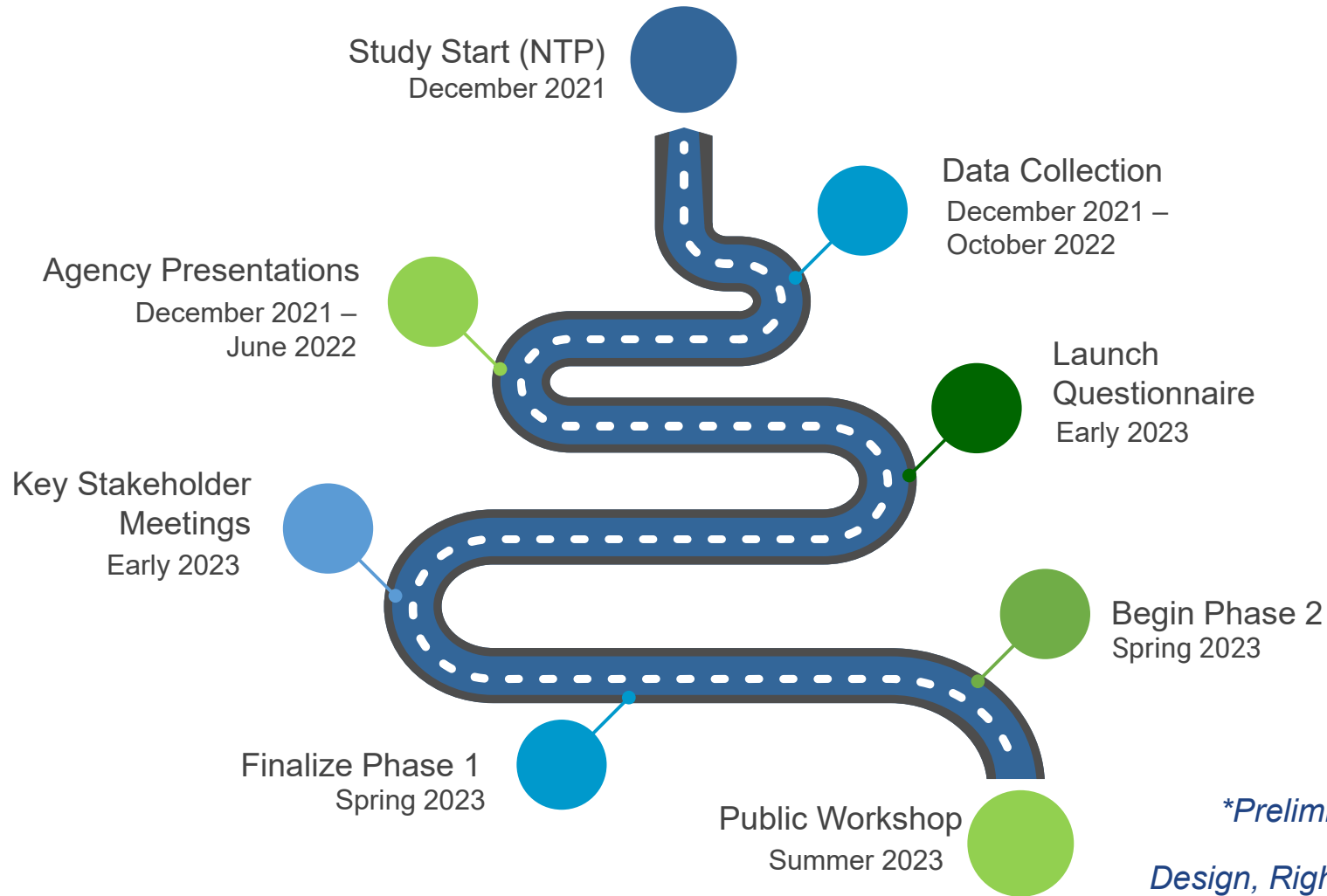


# Goals & Objectives

- Enhance multimodal mobility and safety for all users
- Advance transit service/operation strategies along the study corridor
- Improve transportation network access and interconnectivity within:
  - Neighborhoods
  - Adjacent areas
  - Transit centers



# Project Schedule



*\*Preliminary Schedule – subject to change\**  
*Design, Right-of-Way, & Construction are not funded.*

# US 41 Corridor Today



# US 41 Corridor Today

## What are the characteristics of the corridor?

- FDOT priority multimodal corridor
- **2-3 travel lanes in each direction**
- Separated by median, traffic separator, or center left turn lane
- Continuous sidewalks on both sides
- **Right of way varies with more constraints at northern end**
- **Posted speed limits vary from 25 to 45 mph**
- Longest travel time is northbound in evening peak
- Average travel speeds are lower than posted speed limit south of the Green Bridge
- Highest congestion occurs between 63<sup>rd</sup> Avenue and Manatee Avenue

## Who is using the corridor?

- Residents
- Visitors
- Students
- Businesses

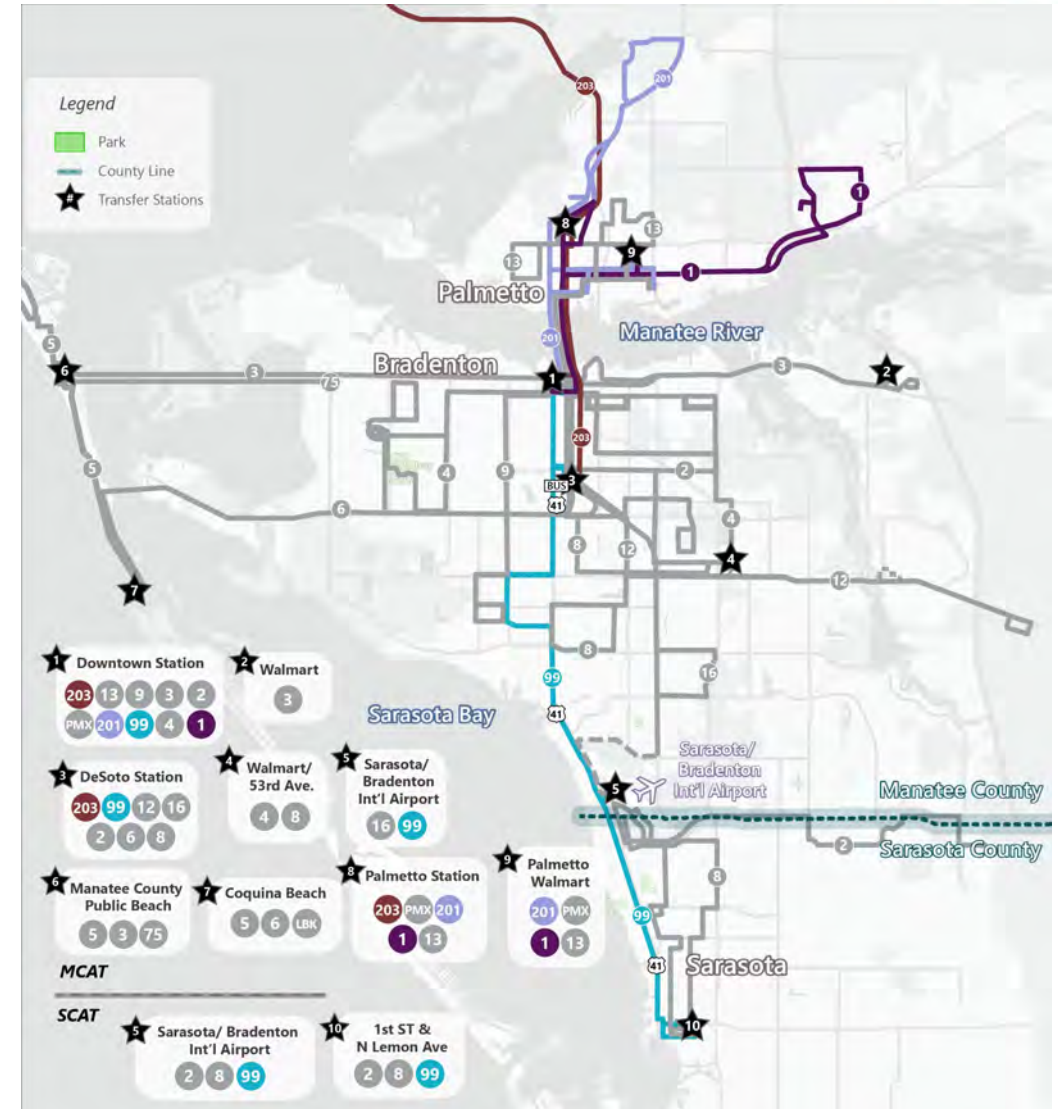
## How are they travelling?

- Single Occupancy Vehicles (cars/trucks, etc.)
- Pedestrians
- Bicyclists
- Several transit (bus) routes



# Area Transit Routes

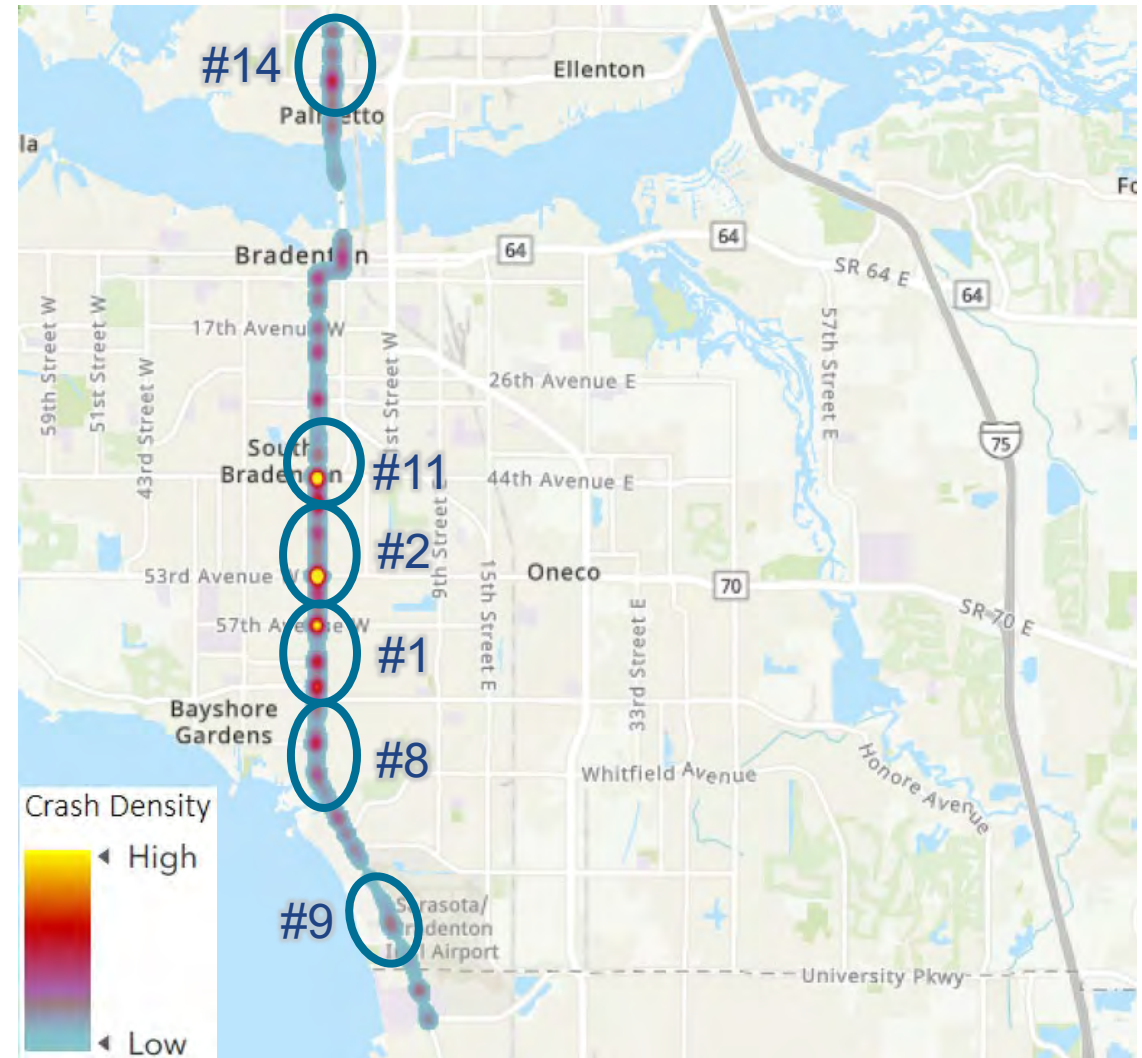
- Route 99 Bradenton-Sarasota
  - Runs length of study area
  - 20-minute AM and PM Peak Period service started Dec 2021
  - New Sunday service proposed starting in 2026
- Stations Near Project
  - Palmetto Station
  - Bradenton Downtown Station
  - DeSoto Station
  - Sarasota-Bradenton International Airport Station



# Safety and Crash Analysis

## Six segments are listed as SW Florida Fatal Top 20 Segments

- 63<sup>rd</sup> Ave W to 55<sup>th</sup> Ave W; Ranked #1
- 55<sup>th</sup> Ave W to Orlando Ave; Ranked #2
- Magellan Dr to 63<sup>rd</sup> Ave W; Ranked #8
- County line to Scott Ave; Ranked #9
- Orlando Ave to 30<sup>th</sup> Ave W; Ranked #11
- 10<sup>th</sup> St W to N of 23<sup>rd</sup> St W; Ranked #14



# Public Outreach



The cover page features a collage of images related to transportation and mobility. At the top, there are five circular icons representing different modes of transport: a car, a person walking, a bus, a person using a wheelchair, and a bicycle. Below these are several circular and rectangular images: a 'PUSH BUTTON FOR' sign with a pedestrian icon, a line of cars in traffic, a street view with palm trees and buildings, a green and white bus, and a traffic light. The text on the page includes the project title, logos for FDOT and CMASS 41, the date, and an approval signature.

**Sarasota | Manatee  
 U.S. 41 Corridor Mobility  
 and Safety Study  
 Communications Plan**

**FDOT** **CMASS 41**

September 2022 FPID #440154-1

Approved By: Abra Horne Date: 9/2/2022  
 Abra Horne, Planning and Environmental Administrator

# Public Outreach

## Outreach to Date:

### September 2021

- Sarasota/Manatee MPO Staff Meetings

### December 2021

- Sarasota/Manatee MPO Staff Meeting
- Pinellas Suncoast Transit Authority Meeting
- SunRunner BRT Meeting

### February 2022

- Sarasota/Manatee MPO Staff Meeting
- Sarasota/Manatee MPO Board Meeting
- Sarasota/Manatee MPO TAC Meeting

### April 2022

- Sarasota/Manatee MPO BPTAC Meeting

### May 2022

- Sarasota/Manatee MPO CAN Meeting
- Sarasota/Manatee MPO TAC Meeting
- Sarasota/Manatee MPO Board Meeting

### June 2022

- City of Bradenton City Council Meeting
- City of Palmetto City Council Meeting
- Manatee County Commission Meeting

## Upcoming Outreach:

### Early 2023

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire

### Summer 2023

- Hold Public Workshop - Summer 2023

### Throughout 2023

- Continued coordination with all agencies and stakeholders

# We Want to Hear From YOU!

Complete the questionnaire:

[swflroads.com/connectingcommunities/](http://swflroads.com/connectingcommunities/)



<http://www.swflroads.com/project/440154-1>



January 2023 - April 2023

## WE WANT TO HEAR FROM YOU!

The Florida Department of Transportation, District One, is conducting several projects in partnership with the Sarasota/Manatee Metropolitan Planning Organization (MPO) and local governments to holistically enhance the multimodal transportation network within Sarasota and Manatee Counties as part of the Connecting Communities Program. Major Connecting Communities Program projects include Bradenton-Palmetto Connector, Desoto Bridge, Sarasota/Manatee US 41 Corridor Mobility and Safety Study, Palmetto Trails Network, and 10th Avenue Complete Street. For a map and full list of current area projects included as part of the Connecting Communities Program visit [www.swflroads.com/connectingcommunities](http://www.swflroads.com/connectingcommunities).



Get involved today!

Scan to take our online questionnaire.

Complete our brief online questionnaire to provide valuable feedback for enhancing the multimodal transportation network.



For more information on the Connecting Communities Program, visit [www.swflroads.com/connectingcommunities](http://www.swflroads.com/connectingcommunities).




# Open Discussion

CMASS Stakeholder Meeting ↶ ↷ 📄 🔄 All changes saved! 👤 📄 Share

## CMASS Stakeholder Meeting January 2023

**Connecting Communities**  
The purpose of the Connecting Communities program is to holistically enhance the multimodal transportation network within Sarasota and Manatee Counties.



**NEXT STEPS**

- ★ Launch and Summarize Questionnaire
- ★ Finalize Goals & Objectives and Measures of Effectiveness
- ★ Finalize Corridor Neighborhood Strategies
- ★ Finalize Phase 1 - Spring 2023
- ★ Hold Public Workshops - Summer 2023
- ★ Begin Phase 2 Develop Concepts for Implementation

**ATTENDEES**

Name

Agency

Title

**US 41 CMASS - Goals & Objectives**

- Enhance multimodal mobility and safety for all users
- Advance transit service/operation strategies along the study corridor
- Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

**STAKEHOLDER INPUT**

Enhance multimodal mobility and safety for all users

Advance transit service/operation strategies along the study corridor

Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

😊 👤 +

# Next Steps

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire
- Finalize Goals & Objectives and Measures of Effectiveness
- Finalize Corridor/Neighborhood Strategies
- Finalize Phase 1 – Spring 2023
- Hold Public Workshop - Summer 2023
- Begin Phase 2: Develop Concepts for Implementation

**Complete the questionnaire:**

[swfroads.com/connectingcommunities/](http://swfroads.com/connectingcommunities/)





# Safety, Questions and Contact Information

**Matt Dockins, PE**  
FDOT Project Manager  
FDOT District 1  
[info@us41cmass.com](mailto:info@us41cmass.com)

**Amy Sirmans, PE**  
Consultant Project Manager  
VHB  
[asirmans@vhb.com](mailto:asirmans@vhb.com)



## PEDESTRIANS AND BICYCLISTS

Nationally, Florida had the **HIGHEST NUMBER OF BICYCLIST FATALITIES** in 2018

**Pedestrians** account for more than **22%** of traffic fatalities in Florida





# STAKEHOLDER INPUT



## Enhance multimodal mobility and safety for all users

Multimodal Mobility & Safety is #1 concern. Increased number of accidents occurring at US41 and General Spaatz Blvd in front of New College, including accidents involving students.

This project will help improve multimodal mobility. Crosswalk improvements/safety for school students would be beneficial

Suggestion to implement pedestrian crossings and enhance accessibility to nearby restaurants/facilities. There are currently no crosswalks on US41 at USF

Existing conditions include a basic sidewalk. Suggestion for greenway improvements

Concern for students crossing areas near Ringling College and the Airport

Have been discussions about providing safer pedestrian access near the airport

Suggestion to enhance pedestrian safety along DeSoto Bridge

Lack of crosswalks along corridor. Does not allow for recreation or exercise

## Advance transit service/operation strategies along the study corridor

Enhance transit along US 41 - Route 99 is best performing route along corridor

Many students use the pedestrian overpass rather than crossing US41 at the intersection. Bike traffic is more common than pedestrian traffic

Keep transit stops away from Runway 4 to enhance safety for pedestrians

Keeping the median opening at Dolphin would be beneficial for future development

Adding bus stops along corridor to benefit local community

Chamber focusing on cost effective methods for transportation including buses, on demand transit, and transit accessibility

Pedestrian/Scoter/Bike traffic around colleges. May not be necessary for a dedicated SCAT lane due to low ridership/trip

USF provides free transit (MCAT and SCAT) for students on campus. Bus stops are not on campus, but adjacent.

Having a stop at USF campus would provide enhanced accessibility for students

Congestion near universities does not allow for safe or efficient traffic flow

Transportation is barrier for employment. Enhanced transportation will benefit the community and employers

Transit timing is inefficient and poorly designed for workforce

## Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

Suggestion to enhance access to activity center located at US41 and 44th Ave W to benefit local community

Suggestion to make transit more easily accessible by adjacent neighborhoods

Area near Seagate Drive (Property 6) to be developed. Would benefit from enhanced pedestrian facilities. Will be developed as as parking lot for interim use

Not many businesses/restaurants surrounding New College. Students/Faculty rely on cars as main form of transportation. (~700 students residing on campus.)

Chamber considering best way to provide accessible transit for all

Future developments to increase campus activity, enhancing the opportunity for transit use.

Some students will use transit to get to other areas in Sarasota and Bradenton, however most use their own vehicle.

Consideration for businesses would be appreciated as they have been recently affected

Route to nearby businesses is not safe for pedestrians

Inefficient access to transit. Workforce living in southern Sarasota County travel to employment centers in northern Manatee County.

# MEETINGS TO DATE



- Bradenton Area Convention and Visitors Bureau
- Manasota Black Chamber of Commerce
- Manatee County Public Works
- New College of Florida
- Sarasota Manatee Airport Authority
- The Greater Sarasota Chamber of Commerce
- University of South Florida Sarasota-Manatee
- Manatee NAACP
- Bradenton Area Economic Development Corporation

## What improvements would you like to see for the US 41 corridor?

Enhanced lighting near Crosley Estate and USF Campus

Pedestrian facilities (Continous bike/ped) throughout the corridor

Consistency in roadway and surrounding facilities (aesthetics). The roadway and surrounding facilities are very different.

Opportunity for more bicycle lanes along US41 in both directions (N/S)

Enhanced safety at the intersection of US41 and General Spaatz Blvd

Lighting near runway zones is something to be considered. May need approval from FAA

Good wayfinding signs on US41 for Airport - beneficial to incorporate style/colors into signage

Bike/Ped connections between Sarasota and Manatee County

Coordination of red lights along the corridor

Aesthetic improvements along corridor

Continous sidewalks/pedestrian lighting throughout corridor to enhance safety

Adding bike lanes throughout the corridor.

Crosswalks along corridor and safety improvements when traveling to nearby businesses

Aesthetic improvements along corridor. Current aesthetics do not reflect the area well

## Are there any specific area of concern you would like to discuss?

Pedestrian activity walking to New College.

Difficulty for pedestrians to access transit stations

Insufficient lighting near New College

Ability to cross the road.

Traffic congestion between University and New College intersection in AM peak time

Varying speeds/high speed travelers at US 41 and New College entrance (especially northbound)

Lack of pedestrian access between New College and USF.

Pedestrians crossing the road in busier areas. Suggestion for a median that would enforce students/pedestrians to use designated crosswalks.

Captain Brian's Seafood has expressed concern for losing driveway access (involving resurfacing project) - could be beneficial to include shared access with Airport

High speeds along the corridor

Concern for homeless population in the area. Would like to see an intentional plan in place to accommodate the homeless population along the corridor

Concern for future stacking issues at Seagate Drive (highest entrance and exit counts here). Potential for increased congestion with coming development. Includes buses as well.

High speeds in the area. Speed control

Future increased traffic volume to cause issues along corridor

Upholding integrity for African American-owned churches and businesses along the corridor

Congestion and sustainability of the corridor.

Aesthetics and traffic flow along corridor.

Inconsistent aesthetics when traveling across the Sarasota/Manatee County line. Varying differences between the two counties, crossing University Parkway

## What issues do you notice when traveling the US 41 corridor?

Safety hazards (crashes, frequent stopping, roadway conditions, etc.)

Excessive vehicular speeding

Insufficient lighting

Difficulty entering/exiting properties along the corridor

Quality of sidewalks

Quality of bicycle facilities

Quality of transit stops

Lack of transit reliability

Traffic congestion

Inconsistency throughout the corridor including the examples above and number of lanes.

Turning into or out of USF onto US41 (Safety)

Lack of safety / rate of speed / congestion

Travelers unfamiliar with the area are unaware of surrounding conditions

Increased traffic flow

Lack of access management near Airport

Confusion with traffic flow patterns and turning lanes, especially considering ongoing projects

## What is your biggest complaint about traveling along the US 41 Corridor?

Poor lighting along the corridor for motorized traffic and pedestrians

High speeds near Crosley Estate

High pedestrian fatalities due to dangerous conditions

Lack of lighting and high traffic speeds

Bike/Ped safety along the corridor.

Complaints about the left turn signal at General Spaatz Blvd. Long wait times, congestion, queuing issues

Unsafe conditions along the corridor for pedestrians, especially near colleges

Lack of safety and aesthetics along corridor.

Lack of aesthetics along corridor.

Lack of pedestrian safety, especially near colleges

**Does your agency/organization have any planned improvements / projects / events that the project team should know about?**

Frequent Trolley Services from Crosley Estate to nearby for approximately 200 events per year.

Similar transportation methods at Ringling and New College.

Use of Trolley Service encouraged

Frequent events on campus, but most impacts are within the campus

Manatee County is working on Mobility Plan. The County will increase focus on capital project needs and prioritizations

Manatee County working on Trails Master Plan

Few events a year that may impact traffic. Winter events keep University traffic steady

Ground Transportation Center project to begin construction in coming months. Intended to improve MCAT and SCAT drop-off and pick-up. Larger area will accommodate 4 buses rather than 2.

Working with Manatee County to remove Ponce De Leon St once the land becomes the Airport's property

On-campus housing plans in development

Future commercial development anticipated in Property 5 and 6 near airport

Several developers interested in converting motels along the corridor into affordable housing, etc., which would provide a need for enhanced transit services

Future plan to construct nursing program will increase attendance

Annual Golf Tournament & Fall event partnered with Manatee Convention Center with 200+ in attendance

Juneteenth Event with local library

Planned improvements near airport and colleges are anticipated to bring growth in the next 10 years.

**How would you like to continue to be involved in the project?**

Meet with me again

Join Mailing List

Become a project champion

Additional meeting with New College.

Update meeting with Airport Authority

Join mailing list. Hold update meeting with governmental issues council (when appropriate). Can participate as a project champion

Continued meetings to keep up to date on the planning process. Mailing list updates.

Continued meetings to be updated on project process

Continued meetings and engagement with the African American Community

Project updates and meetings when appropriate

**Are there others from your organization that should be included?**

Add names and contact info

Joe Hembree - Commercial Realtor on Governmental Issues Board

James Golden - Legal Rep (NAACP)/ Manatee County Resident & Economic Development Chair.

Sororities and Fraternities - ASHAL Group and local churches

Potential to share with Bradenton Area EDC Board

**Can you help us distribute the questionnaire / project information? What methods?**

Newsletters

Mailing List

In-person flyers posted throughout New College campus

Post flyers throughout airport. Can ask IT to post to website.

Include in weekly and monthly newsletter

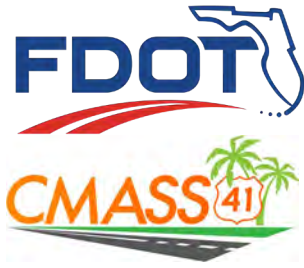
Town Hall Meetings, pass out flyers, email/ billboards

Will distribute to contact list.

Email blast and in-person depending on scheduled events

Electronic distribution

Suggestion for project team to look to local churches and childcare centers for distribution in addition to creating a spanish and creole version of the questionnaire.



US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
**Stakeholder/Agency Meeting (Virtual)**  
**Bradenton Area Convention and Visitors Bureau**

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## **Virtual Meeting via Microsoft Teams**

January 17, 2023, 1:00 p.m.

### [Meeting Summary](#)

A virtual stakeholder's meeting with the Bradenton Area Convention and Visitors Bureau was conducted via Microsoft Teams on Tuesday, January 17, 2023. The purpose of the meeting was to present information for FDOT's Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS) and conduct an open discussion between the study team and the Bradenton Area Convention and Visitors Bureau.

### [Attendees](#)

#### **Project Team**

FDOT: Jeff James – Environmental Manager  
RK&K: Matt Dockins, PE – Project Manager on behalf of FDOT  
VHB: Amy Sirmans, PE – Consultant Project Manager  
VHB: Heather Garcia – Project Team Member  
VHB: Kennedy Sumner-Snell – Project Team Member  
The Valerin Group: Catherine Winter, PE – Public Involvement Coordinator  
AECOM: Lauren Brooks, AICP – Connecting Communities Program Manager on behalf of FDOT

#### **Stakeholders**

Bradenton Area Convention and Visitors Bureau: Anna Pohl – General Manager  
Bradenton Area Convention and Visitors Bureau: Sandra Guerra – Operations and Events Manager  
Manatee County: Clarke Davis – Traffic Management Deputy Director, Public Works

Mitch Conner, Client Services Manager for the Bradenton Area Convention and Visitors Bureau, and Chad Butzow, Manatee County Public Works Director, were invited but unable to attend the meeting.

Anna Pohl stated that she managed several facilities, including the Bradenton Area Convention Center and the Powel Crosley Estate which is located behind the University of South Florida (USF) Sarasota-Manatee campus.

### [Project Overview](#)

The project team shared a presentation with meeting attendees providing information on the US 41 CMASS project including project description and background, goals and objectives, schedule, and public outreach. A copy of the presentation is included with these notes.

The “Connecting Communities” concept discussed is inclusive of various Sarasota and Manatee County projects with the purpose of holistically enhancing the multi-modal transportation network within those



US 41 CMASS  
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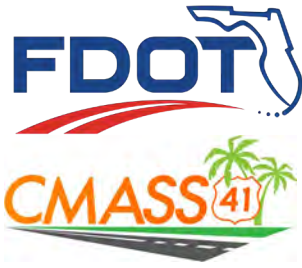
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counties. The Connecting Communities program addresses project overlap and helps ensure important and effective connections and coordination to avoid the need to “reinvent the wheel”, leveraging work that had already been completed. Five major projects of that initiative as being the Bradenton-Palmetto Connector ACE/PD&E Study, the DeSoto Bridge PD&E Study, the Palmetto Trails Network/PD&E Study, US 41 CMASS, and 10th Avenue Complete Streets. The project team clarified that not all area projects were listed in the presentation and encouraged stakeholders to advise the study team of any projects of which he felt they should be aware.

The project team used a digital whiteboard application (Mural) to capture and document stakeholder ideas and feedback. A copy of the Mural whiteboard with all feedback received to date from the individual stakeholder meetings is included with these meeting notes.

### Open Discussion

- Stakeholder concerns
  - Safety at 44<sup>th</sup> Avenue, in the areas of 26<sup>th</sup> Avenue and 63<sup>rd</sup> Avenue.
  - Safety along the southern portion of the corridor.
  - Absence of pedestrian accommodations along the DeSoto Bridge.
  - Lack of lighting for vehicles, pedestrians, and bicyclists area along the corridor and near the Crosley Estate and USF.
  - Difficulty for pedestrians to cross the street and access bus stops.
  - Speed of vehicle traffic in the evening.
- Stakeholder suggestions and additional input
  - Explore speed management and establish better connections between US 41 and the neighborhoods behind it. Safety and accessibility would aid in better performance of the transit system.
  - Continuous bicycle and pedestrian features throughout the corridor.
  - Approximately 75 to 150 people use trolley/buses to the Crosley Estate. Trolleys can run any day of the week, any day of the year, and typically operate anywhere from 1 p.m. to 3 a.m. Approximately 200 events per year at the Estate, half of which utilize the trolley service in addition to being used by those from New College and the Ringling Museum.
  - Manatee County is working on their Mobility Plan, focusing on Capital Improvement Project needs and prioritizations, and the Trails Master Plan, which could be useful to the US 41 CMASS team once available.
- Connecting Communities Questionnaire distribution
  - Flyer attached to newsletter, post on the convention center marquee and social media platforms.
- Continued stakeholder involvement
  - No need for follow-up meetings at this time.



US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
Stakeholder/Agency Meeting (Virtual)  
Bradenton Area Convention and Visitors Bureau

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#### Attendees Email Addresses

Anna Pohl, [anna.pohl@bacvb.com](mailto:anna.pohl@bacvb.com)  
Sandra Guerra, [sandra.guerra@bacvb.com](mailto:sandra.guerra@bacvb.com)  
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Jeff James, [jeffrey.james@dot.state.fl.us](mailto:jeffrey.james@dot.state.fl.us)  
Matt Dockins, PE, [mdockins@hntb.com](mailto:mdockins@hntb.com)  
Amy Sirmans, PE, [asirmans@vhb.com](mailto:asirmans@vhb.com)  
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Catherine Winter, PE, [catherine@valerin-group.com](mailto:catherine@valerin-group.com)  
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# District One Environmental Management Office

## *Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS)*

*From University Parkway to 17<sup>th</sup> Street W*

January 2023



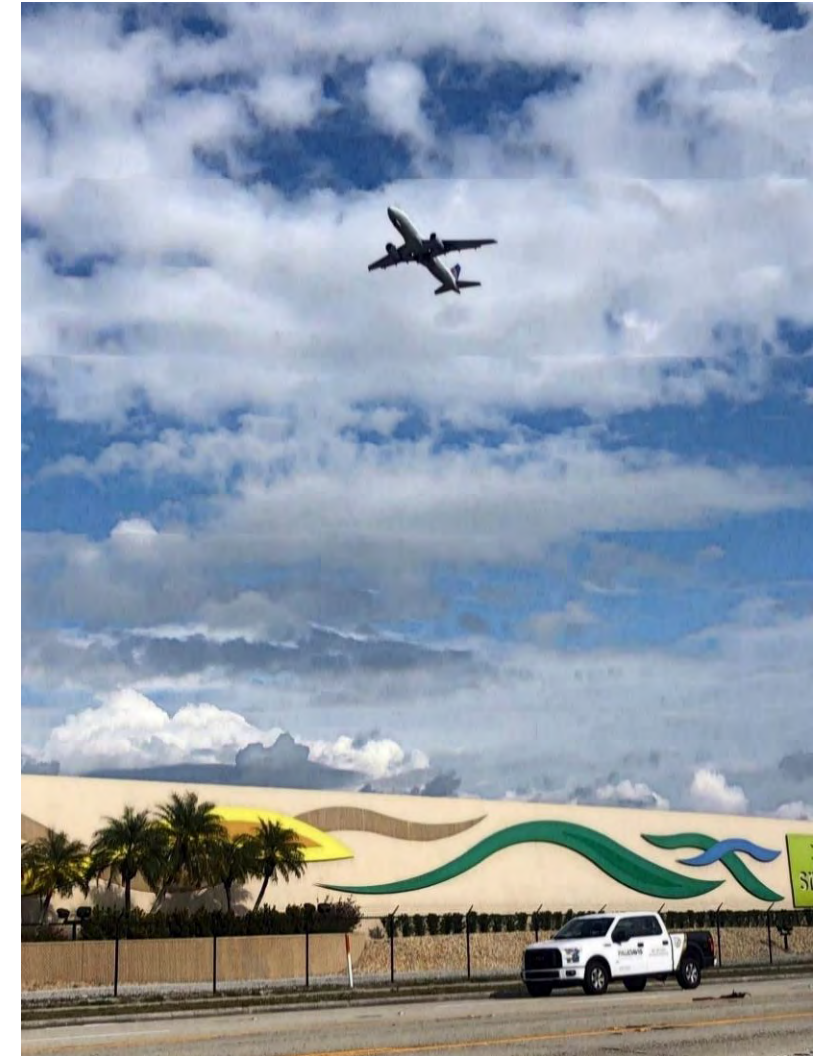
# Bradenton Area Convention and Visitors Bureau

January 17, 2023



# Agenda

- Introductions
- Project Description and Background
- US 41 Today: Existing Conditions
- Public Outreach
- Open Discussion: We Want Your Input





# Introductions



**Abra Horne**  
**Administrator**  
District Environmental  
Management Office  
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**Environmental Manager**  
District Environmental  
Management Office  
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(863) 519-2625

# Introductions



**Lauren Brooks, AICP**  
**Project Manager**  
**Senior Transportation Planner**  
**AECOM**  
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**Amy Sirmans, P.E.**  
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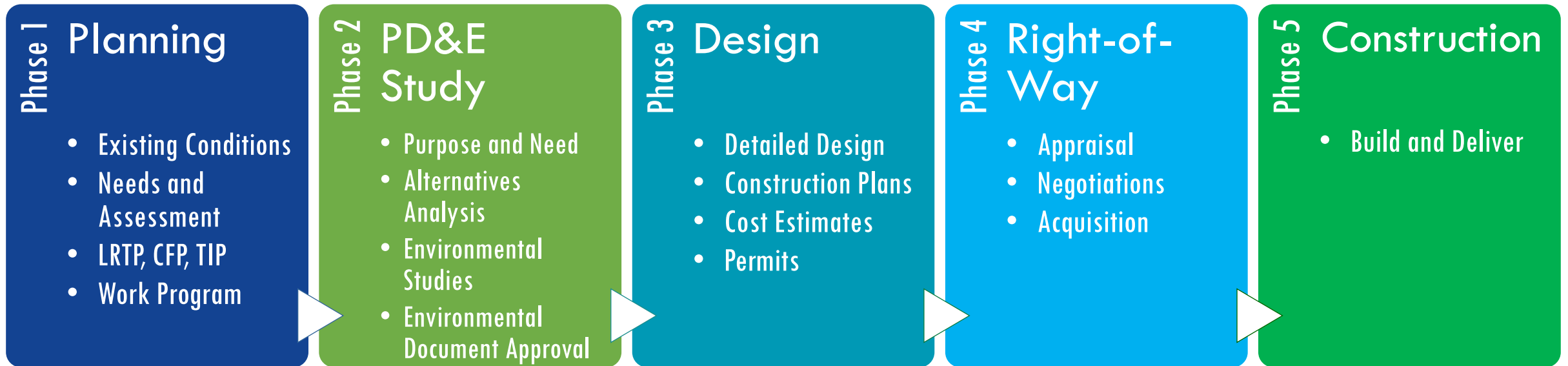


**Catherine Winter, P.E.**  
**Public Involvement Manager**  
**The Valerin Group**  
[Catherine@valerin-group.com](mailto:Catherine@valerin-group.com)  
(813) 431-0919

# Project Description and Background



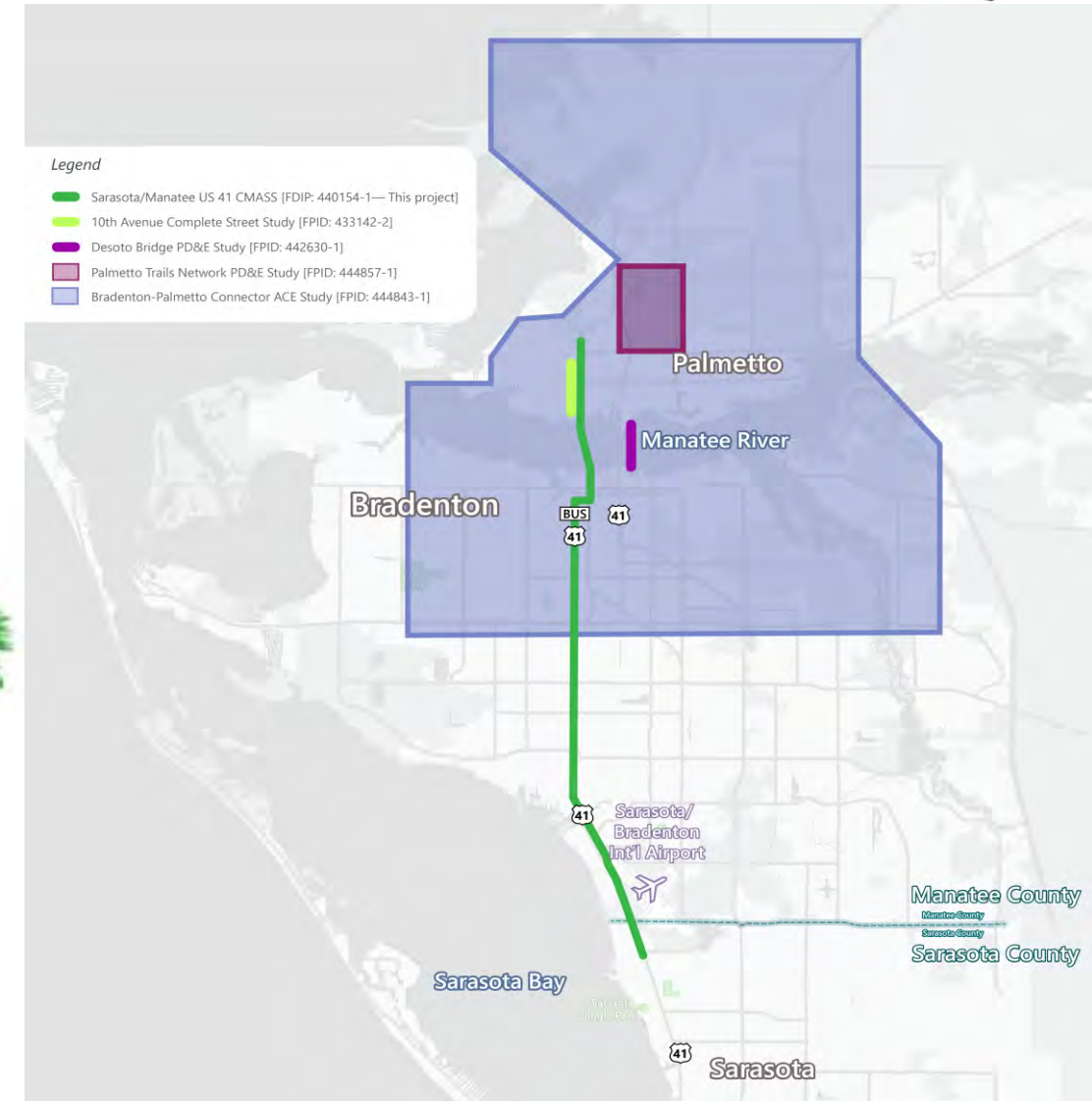
# Transportation Project Development Process



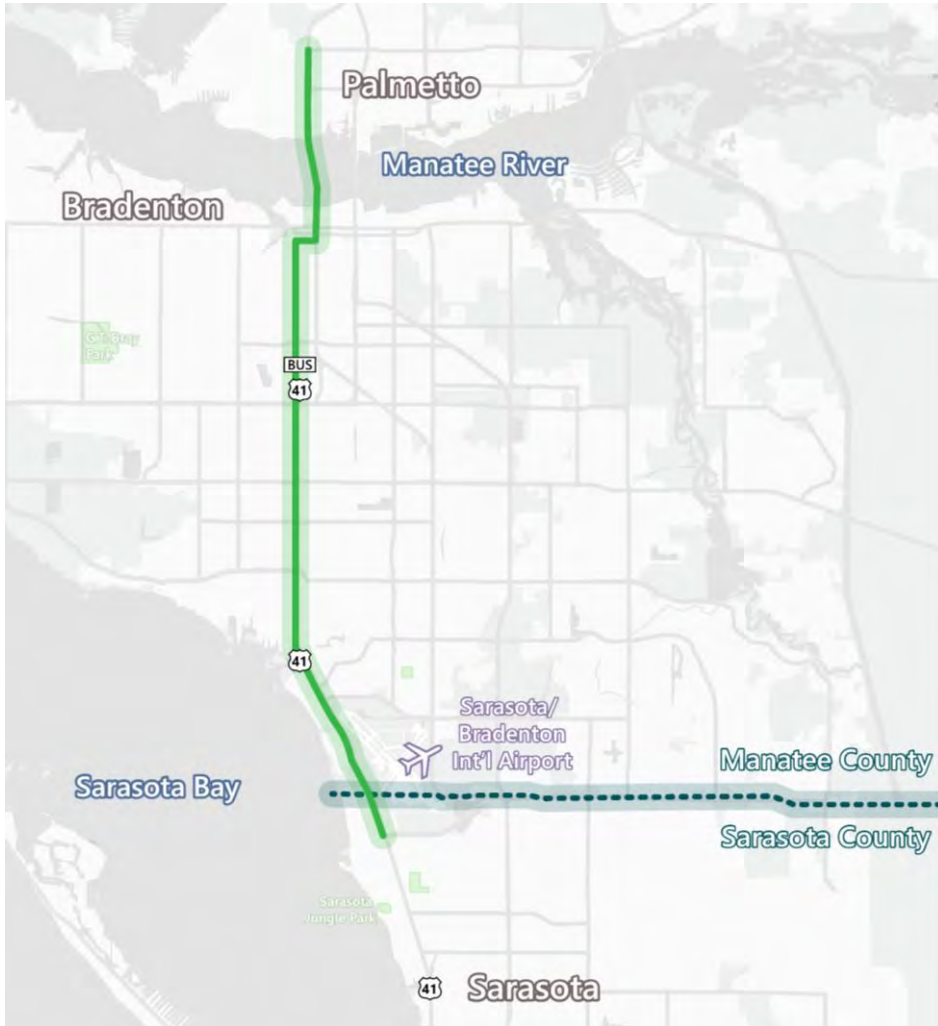
# Connecting Communities

**Purpose:** Holistically enhance the multimodal transportation network within Sarasota and Manatee Counties.

## Desoto Bridge PD&E Study



# Project Description



**Project Limits:** US 41/US 41 Business corridor from University Parkway (Sarasota) to 17<sup>th</sup> Street W (Palmetto)

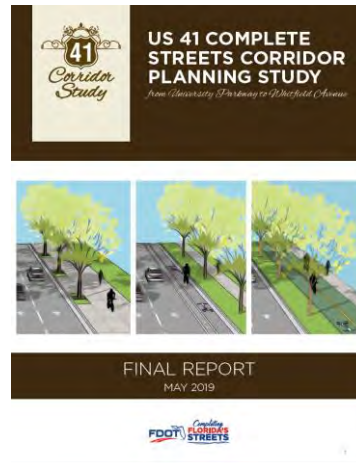
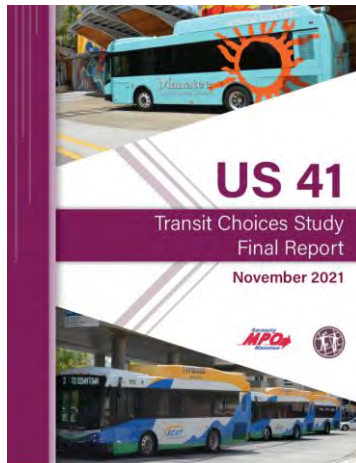
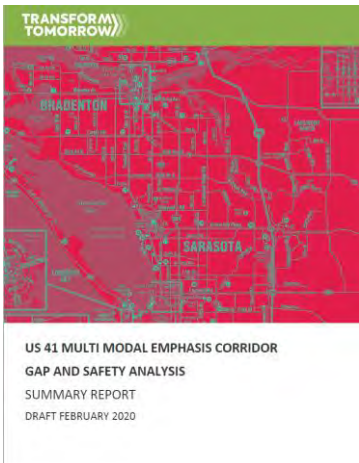
**Financial Project ID:** 440154-1-21-01

**Project Manager:** Matt Dockins

**Status:** Feasibility Study – Phase 1

# We are not starting from scratch...

- US 41 Multi-Modal Emphasis Corridor (MMEC)
- US 41 Transit Choices Study
- US 41 Complete Streets Corridor Planning Study
- Arterial Safety Study
- Access Management Study
- Intersection Safety Studies



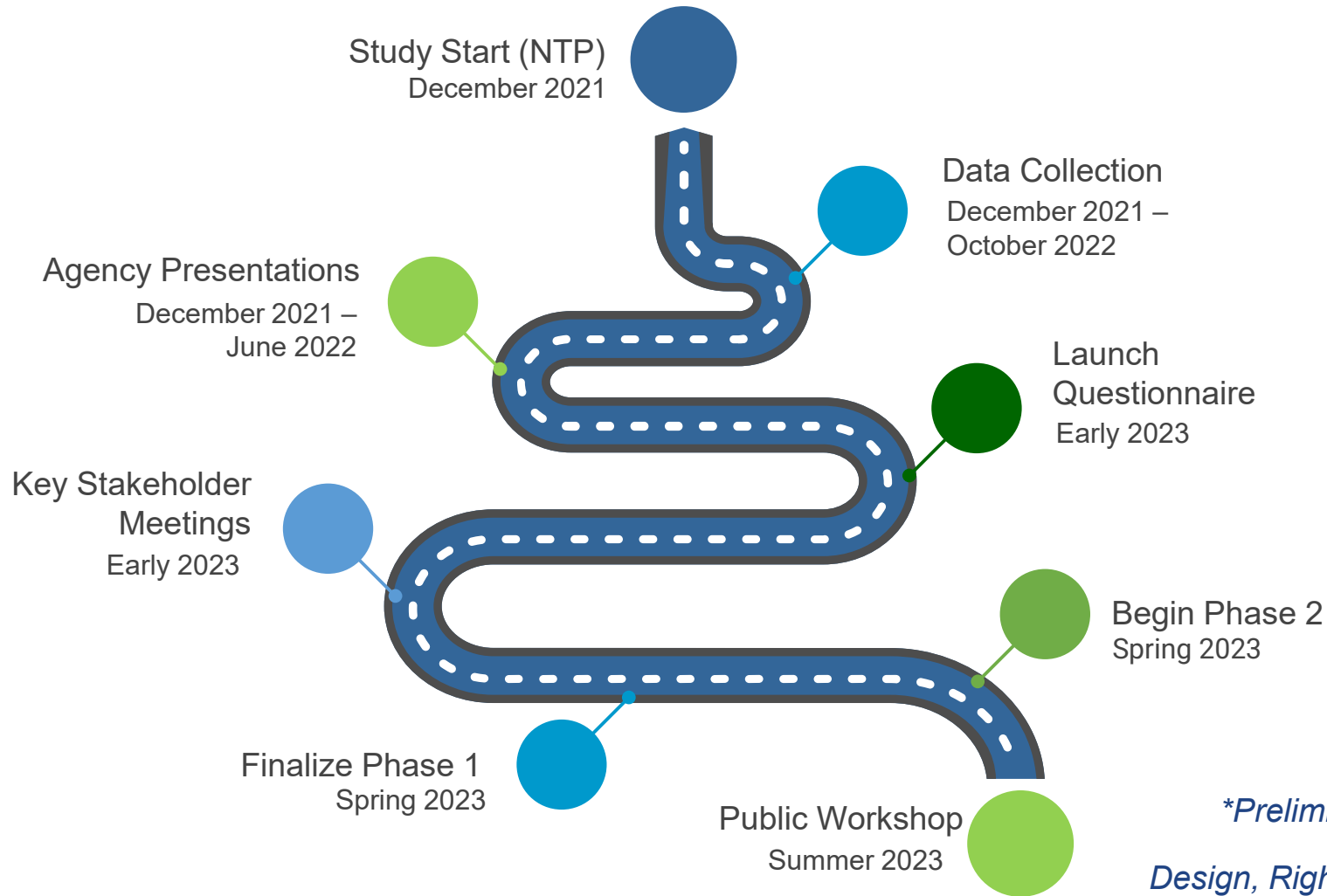
# Goals & Objectives

- Enhance multimodal mobility and safety for all users
- Advance transit service/operation strategies along the study corridor
- Improve transportation network access and interconnectivity within:
  - Neighborhoods
  - Adjacent areas
  - Transit centers





# Project Schedule



*\*Preliminary Schedule – subject to change\**

*Design, Right-of-Way, & Construction are not funded.*

# US 41 Corridor Today



# US 41 Corridor Today

## What are the characteristics of the corridor?

- FDOT priority multimodal corridor
- **2-3 travel lanes in each direction**
- Separated by median, traffic separator, or center left turn lane
- Continuous sidewalks on both sides
- **Right of way varies with more constraints at northern end**
- **Posted speed limits vary from 25 to 45 mph**
- Longest travel time is northbound in evening peak
- Average travel speeds are lower than posted speed limit south of the Green Bridge
- Highest congestion occurs between 63<sup>rd</sup> Avenue and Manatee Avenue

## Who is using the corridor?

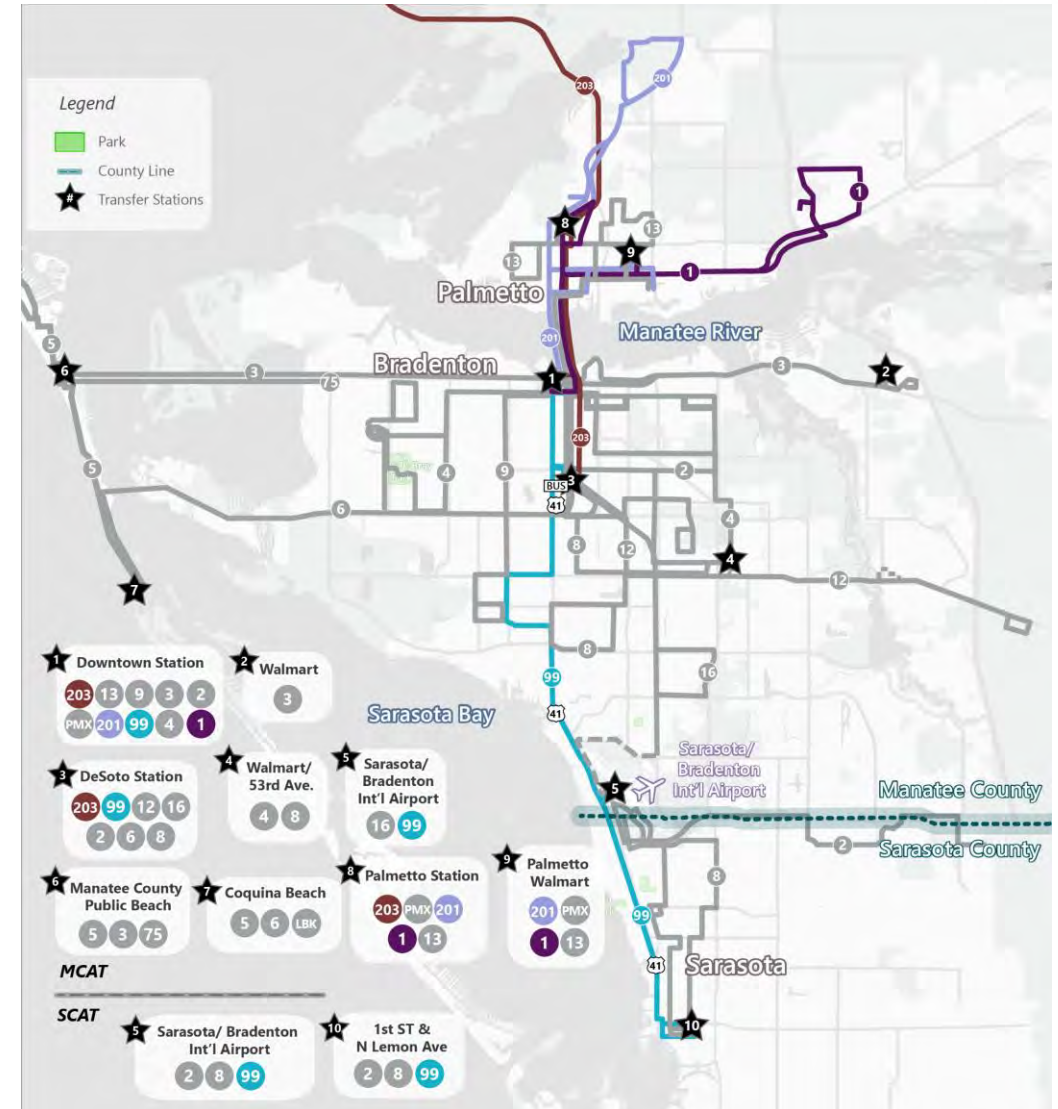
- Residents
- Visitors
- Students
- Businesses

## How are they travelling?

- Single Occupancy Vehicles (cars/trucks, etc.)
- Pedestrians
- Bicyclists
- Several transit (bus) routes

# Area Transit Routes

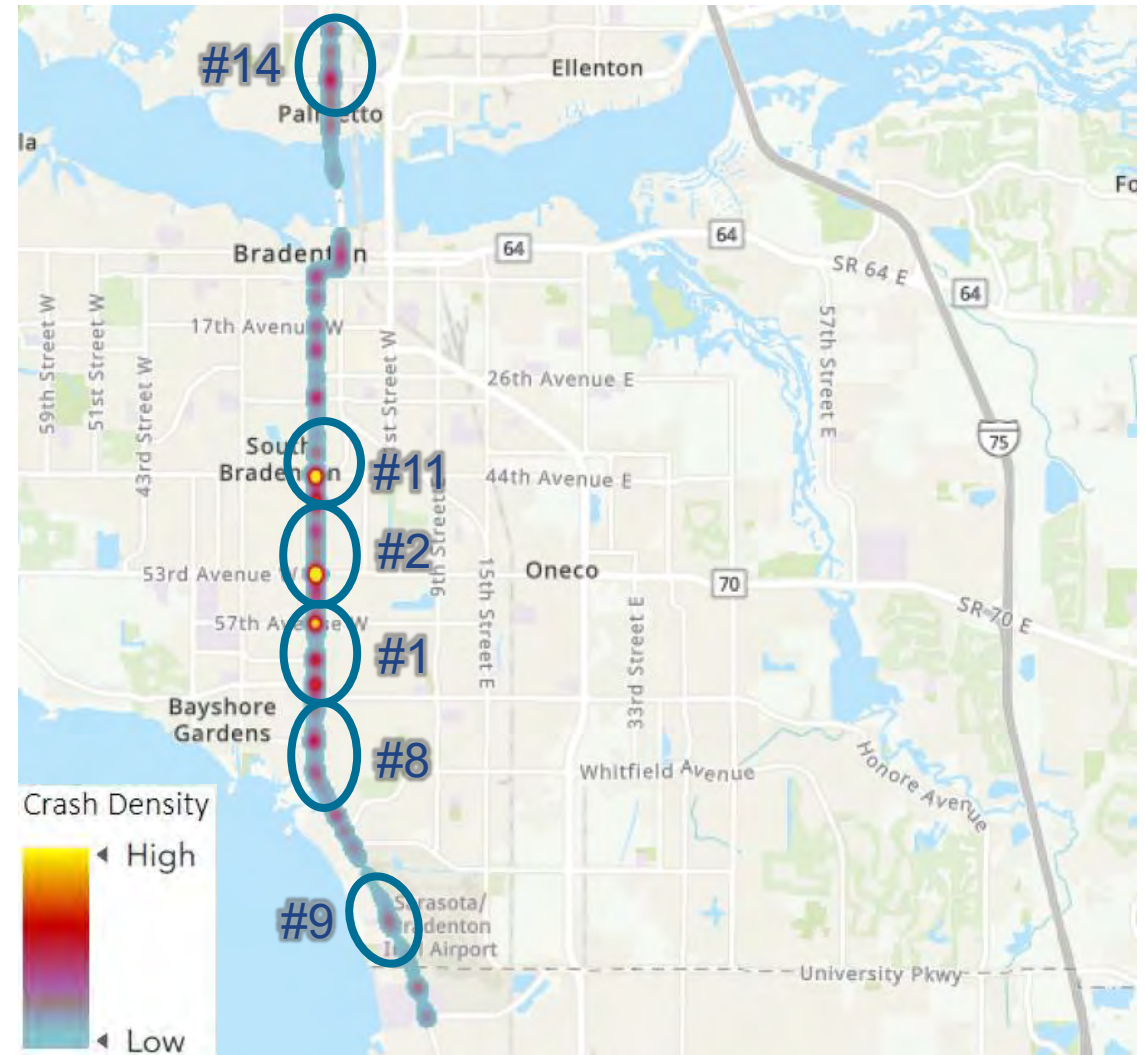
- Route 99 Bradenton-Sarasota
  - Runs length of study area
  - 20-minute AM and PM Peak Period service started Dec 2021
  - New Sunday service proposed starting in 2026
- Stations Near Project
  - Palmetto Station
  - Bradenton Downtown Station
  - DeSoto Station
  - Sarasota-Bradenton International Airport Station



# Safety and Crash Analysis

## Six segments are listed as SW Florida Fatal Top 20 Segments

- 63<sup>rd</sup> Ave W to 55<sup>th</sup> Ave W; Ranked #1
- 55<sup>th</sup> Ave W to Orlando Ave; Ranked #2
- Magellan Dr to 63<sup>rd</sup> Ave W; Ranked #8
- County line to Scott Ave; Ranked #9
- Orlando Ave to 30<sup>th</sup> Ave W; Ranked #11
- 10<sup>th</sup> St W to N of 23<sup>rd</sup> St W; Ranked #14



# Public Outreach

The graphic features a blue background with several circular and rectangular images. At the top, there are five icons representing different modes of transport: a car, a wheelchair, a bus, a person with a cane, and a bicycle. Below these are images of a 'PUSH BUTTON FOR' sign with a pedestrian icon, a line of cars, a street view with palm trees and buildings, a bus, and a traffic light. The text 'Sarasota | Manatee U.S. 41 Corridor Mobility and Safety Study Communications Plan' is prominently displayed in the center-right. At the bottom, it includes the logos for FDOT and CMASS 41, the date 'September 2022', and the project ID 'FPID #440154-1'. A signature line at the bottom right shows 'Approved By: Abra Home, Planning and Environmental Administrator' and 'Date: 9/2/2022'.

Sarasota | Manatee  
U.S. 41 Corridor Mobility  
and Safety Study  
Communications Plan

FDOT CMASS 41

September 2022 FPID #440154-1

Approved By: Abra Home Date: 9/2/2022  
Abra Home, Planning and Environmental Administrator

# Public Outreach

## Outreach to Date:

### September 2021

- Sarasota/Manatee MPO Staff Meetings

### December 2021

- Sarasota/Manatee MPO Staff Meeting
- Pinellas Suncoast Transit Authority Meeting
- SunRunner BRT Meeting

### February 2022

- Sarasota/Manatee MPO Staff Meeting
- Sarasota/Manatee MPO Board Meeting
- Sarasota/Manatee MPO TAC Meeting

### April 2022

- Sarasota/Manatee MPO BPTAC Meeting

### May 2022

- Sarasota/Manatee MPO CAN Meeting
- Sarasota/Manatee MPO TAC Meeting
- Sarasota/Manatee MPO Board Meeting

### June 2022

- City of Bradenton City Council Meeting
- City of Palmetto City Council Meeting
- Manatee County Commission Meeting

## Upcoming Outreach:

### Early 2023

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire

### Summer 2023

- Hold Public Workshop - Summer 2023

### Throughout 2023

- Continued coordination with all agencies and stakeholders

# We Want to Hear From YOU!

Complete the questionnaire:

[swfroads.com/connectingcommunities/](http://swfroads.com/connectingcommunities/)



<http://www.swfroads.com/project/440154-1>

A promotional poster for the FDOT Connecting Communities Program. At the top left is the FDOT logo. At the top right is the 'CONNECTING COMMUNITIES' logo with a stylized 'e' icon and the text 'Manatee | Sarasota Counties'. Below the logos is a date range: 'January 2023 - April 2023'. The main heading is 'WE WANT TO HEAR FROM YOU!'. The text describes the program's goal to enhance the multimodal transportation network in Sarasota and Manatee Counties through various projects. Below the text is a collage of images and circular callouts: 'Complete Streets' (showing a city street), 'Safety Improvements' (showing a road with a guardrail), 'Corridor Visioning' (showing a bus), 'Bridge Upgrades' (showing a bridge), and 'Multi-use Trails' (showing a path). At the bottom, there is a 'Get involved today!' section with a QR code and a call to action to scan and take the online questionnaire. A footer at the very bottom of the poster provides more information and the website URL.

**FDOT**

**CONNECTING COMMUNITIES**  
Manatee | Sarasota Counties

January 2023 - April 2023

## WE WANT TO HEAR FROM YOU!

The Florida Department of Transportation, District One, is conducting several projects in partnership with the Sarasota/Manatee Metropolitan Planning Organization (MPO) and local governments to holistically enhance the multimodal transportation network within Sarasota and Manatee Counties as part of the Connecting Communities Program. Major Connecting Communities Program projects include Bradenton-Palmetto Connector, Desoto Bridge, Sarasota/Manatee US 41 Corridor Mobility and Safety Study, Palmetto Trails Network, and 10th Avenue Complete Street. For a map and full list of current area projects included as part of the Connecting Communities Program visit [www.swfroads.com/connectingcommunities](http://www.swfroads.com/connectingcommunities).

**Complete Streets**

**Safety Improvements**

**Corridor Visioning**

**Bridge Upgrades**

**Multi-use Trails**

**Get involved today!**  
Scan to take our online questionnaire.  
Complete our brief online questionnaire to provide valuable feedback for enhancing the multimodal transportation network.

For more information on the Connecting Communities Program, visit [www.swfroads.com/connectingcommunities](http://www.swfroads.com/connectingcommunities).





# Next Steps

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire
- Finalize Goals & Objectives and Measures of Effectiveness
- Finalize Corridor/Neighborhood Strategies
- Finalize Phase 1 – Spring 2023
- Hold Public Workshop - Summer 2023
- Begin Phase 2: Develop Concepts for Implementation

**Complete the questionnaire:**

[swfroads.com/connectingcommunities/](http://swfroads.com/connectingcommunities/)





# Safety, Questions and Contact Information

**Matt Dockins, PE**  
FDOT Project Manager  
FDOT District 1  
[info@us41cmass.com](mailto:info@us41cmass.com)

**Amy Sirmans, PE**  
Consultant Project Manager  
VHB  
[asirmans@vhb.com](mailto:asirmans@vhb.com)



**PEDESTRIANS  
AND BICYCLISTS**

Nationally, Florida had the **HIGHEST NUMBER OF BICYCLIST FATALITIES** in 2018

**Pedestrians** account for more than **22%** of traffic fatalities in Florida



# STAKEHOLDER INPUT



## Enhance multimodal mobility and safety for all users

Multimodal Mobility & Safety is #1 concern. Increased number of accidents occurring at US41 and General Spaatz Blvd in front of New College, including accidents involving students.

This project will help improve multimodal mobility. Crosswalk improvements/safety for school students would be beneficial

Suggestion to implement pedestrian crossings and enhance accessibility to nearby restaurants/facilities. There are currently no crosswalks on US41 at USF

Existing conditions include a basic sidewalk. Suggestion for greenway improvements

Concern for students crossing areas near Ringling College and the Airport

Have been discussions about providing safer pedestrian access near the airport

Suggestion to enhance pedestrian safety along DeSoto Bridge

Lack of crosswalks along corridor. Does not allow for recreation or exercise

## Advance transit service/operation strategies along the study corridor

Enhance transit along US 41 - Route 99 is best performing route along corridor

Many students use the pedestrian overpass rather than crossing US41 at the intersection. Bike traffic is more common than pedestrian traffic

Keep transit stops away from Runway 4 to enhance safety for pedestrians

Keeping the median opening at Dolphin would be beneficial for future development

Adding bus stops along corridor to benefit local community

Chamber focusing on cost effective methods for transportation including buses, on demand transit, and transit accessibility

Pedestrian/Scoter/Bike traffic around colleges. May not be necessary for a dedicated SCAT lane due to low ridership/trip

USF provides free transit (MCAT and SCAT) for students on campus. Bus stops are not on campus, but adjacent.

Having a stop at USF campus would provide enhanced accessibility for students

Congestion near universities does not allow for safe or efficient traffic flow

Transportation is barrier for employment. Enhanced transportation will benefit the community and employers

Transit timing is inefficient and poorly designed for workforce

## Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

Suggestion to enhance access to activity center located at US41 and 44th Ave W to benefit local community

Suggestion to make transit more easily accessible by adjacent neighborhoods

Area near Seagate Drive (Property 6) to be developed. Would benefit from enhanced pedestrian facilities. Will be developed as as parking lot for interim use

Not many businesses/restaurants surrounding New College. Students/Faculty rely on cars as main form of transportation. (~700 students residing on campus.)

Chamber considering best way to provide accessible transit for all

Future developments to increase campus activity, enhancing the opportunity for transit use.

Some students will use transit to get to other areas in Sarasota and Bradenton, however most use their own vehicle.

Consideration for businesses would be appreciated as they have been recently affected

Route to nearby businesses is not safe for pedestrians

Inefficient access to transit. Workforce living in southern Sarasota County travel to employment centers in northern Manatee County.

# MEETINGS TO DATE



- Bradenton Area Convention and Visitors Bureau
- Manasota Black Chamber of Commerce
- Manatee County Public Works
- New College of Florida
- Sarasota Manatee Airport Authority
- The Greater Sarasota Chamber of Commerce
- University of South Florida Sarasota-Manatee
- Manatee NAACP
- Bradenton Area Economic Development Corporation

## What improvements would you like to see for the US 41 corridor?

Enhanced lighting near Crosley Estate and USF Campus

Pedestrian facilities (Continous bike/ped) throughout the corridor

Consistency in roadway and surrounding facilities (aesthetics). The roadway and surrounding facilities are very different.

Opportunity for more bicycle lanes along US41 in both directions (N/S)

Enhanced safety at the intersection of US41 and General Spaatz Blvd

Lighting near runway zones is something to be considered. May need approval from FAA

Good wayfinding signs on US41 for Airport - beneficial to incorporate style/colors into signage

Bike/Ped connections between Sarasota and Manatee County

Coordination of red lights along the corridor

Aesthetic improvements along corridor

Continous sidewalks/pedestrian lighting throughout corridor to enhance safety

Adding bike lanes throughout the corridor.

Crosswalks along corridor and safety improvements when traveling to nearby businesses

Aesthetic improvements along corridor. Current aesthetics do not reflect the area well

## Are there any specific area of concern you would like to discuss?

Pedestrian activity walking to New College.

Difficulty for pedestrians to access transit stations

Insufficient lighting near New College

Ability to cross the road.

Traffic congestion between University and New College intersection in AM peak time

Varying speeds/high speed travelers at US 41 and New College entrance (especially northbound)

Lack of pedestrian access between New College and USF.

Pedestrians crossing the road in busier areas. Suggestion for a median that would enforce students/pedestrians to use designated crosswalks.

Captain Brian's Seafood has expressed concern for losing driveway access (involving resurfacing project) - could be beneficial to include shared access with Airport

High speeds along the corridor

Concern for homeless population in the area. Would like to see an intentional plan in place to accommodate the homeless population along the corridor

Concern for future stacking issues at Seagate Drive (highest entrance and exit counts here). Potential for increased congestion with coming development. Includes buses as well.

High speeds in the area. Speed control

Future increased traffic volume to cause issues along corridor

Upholding integrity for African American-owned churches and businesses along the corridor

Congestion and sustainability of the corridor.

Aesthetics and traffic flow along corridor.

Inconsistent aesthetics when traveling across the Sarasota/Manatee County line. Varying differences between the two counties, crossing University Parkway

## What issues do you notice when traveling the US 41 corridor?

Safety hazards (crashes, frequent stopping, roadway conditions, etc.)

Excessive vehicular speeding

Insufficient lighting

Difficulty entering/exiting properties along the corridor

Quality of sidewalks

Quality of bicycle facilities

Quality of transit stops

Lack of transit reliability

Traffic congestion

Inconsistency throughout the corridor including the examples above and number of lanes.

Turning into or out of USF onto US41 (Safety)

Lack of safety / rate of speed / congestion

Travelers unfamiliar with the area are unaware of surrounding conditions

Increased traffic flow

Lack of access management near Airport

Confusion with traffic flow patterns and turning lanes, especially considering ongoing projects

## What is your biggest complaint about traveling along the US 41 Corridor?

Poor lighting along the corridor for motorized traffic and pedestrians

High speeds near Crosley Estate

High pedestrian fatalities due to dangerous conditions

Lack of lighting and high traffic speeds

Bike/Ped safety along the corridor.

Complaints about the left turn signal at General Spaatz Blvd. Long wait times, congestion, queuing issues

Unsafe conditions along the corridor for pedestrians, especially near colleges

Lack of safety and aesthetics along corridor.

Lack of aesthetics along corridor.

Lack of pedestrian safety, especially near colleges

**Does your agency/organization have any planned improvements / projects / events that the project team should know about?**

Frequent Trolley Services from Crosley Estate to nearby for approximately 200 events per year.

Similar transportation methods at Ringling and New College.

Use of Trolley Service encouraged

Frequent events on campus, but most impacts are within the campus

Manatee County is working on Mobility Plan. The County will increase focus on capital project needs and prioritizations

Manatee County working on Trails Master Plan

Few events a year that may impact traffic. Winter events keep University traffic steady

Ground Transportation Center project to begin construction in coming months. Intended to improve MCAT and SCAT drop-off and pick-up. Larger area will accommodate 4 buses rather than 2.

Working with Manatee County to remove Ponce De Leon St once the land becomes the Airport's property

On-campus housing plans in development

Future commercial development anticipated in Property 5 and 6 near airport

Several developers interested in converting motels along the corridor into affordable housing, etc., which would provide a need for enhanced transit services

Future plan to construct nursing program will increase attendance

Annual Golf Tournament & Fall event partnered with Manatee Convention Center with 200+ in attendance

Juneteenth Event with local library

Planned improvements near airport and colleges are anticipated to bring growth in the next 10 years.

**How would you like to continue to be involved in the project?**

Meet with me again

Join Mailing List

Become a project champion

Additional meeting with New College.

Update meeting with Airport Authority

Join mailing list. Hold update meeting with governmental issues council (when appropriate). Can participate as a project champion

Continued meetings to keep up to date on the planning process. Mailing list updates.

Continued meetings to be updated on project process

Continued meetings and engagement with the African American Community

Project updates and meetings when appropriate

**Are there others from your organization that should be included?**

Add names and contact info

Joe Hembree - Commercial Realtor on Governmental Issues Board

James Golden - Legal Rep (NAACP)/ Manatee County Resident & Economic Development Chair.

Sororities and Fraternities - ASHAL Group and local churches

Potential to share with Bradenton Area EDC Board

**Can you help us distribute the questionnaire / project information? What methods?**

Newsletters

Mailing List

In-person flyers posted throughout New College campus

Post flyers throughout airport. Can ask IT to post to website.

Include in weekly and monthly newsletter

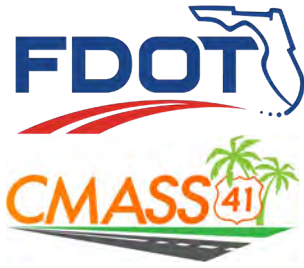
Town Hall Meetings, pass out flyers, email/ billboards

Will distribute to contact list.

Email blast and in-person depending on scheduled events

Electronic distribution

Suggestion for project team to look to local churches and childcare centers for distribution in addition to creating a spanish and creole version of the questionnaire.



US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
**Stakeholder/Agency Meeting (Virtual)**  
**Bradenton Area Economic Development Corporation**

---

## **Virtual Meeting via Microsoft Teams**

February 3, 2023, 10:00 a.m.

### [Meeting Summary](#)

A virtual stakeholder's meeting with the Bradenton Area Economic Development Corporation was conducted via Microsoft Teams on Friday, February 3, 2023. The purpose of the meeting was to present information for FDOT's Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS) and conduct an open discussion between the study team and the Bradenton Area Economic Development Corporation.

### [Attendees](#)

#### **Project Team**

FDOT: Jeff James – Environmental Manager  
RK&K: Matt Dockins, PE – Project Manager on behalf of FDOT  
VHB: Amy Sirmans, PE – Consultant Project Manager  
VHB: Heather Garcia – Project Team Member  
VHB: Kennedy Sumner-Snell – Project Team Member  
The Valerin Group: Catherine Winter, PE – Public Involvement Coordinator  
AECOM: Lauren Brooks, AICP – Connecting Communities Program Manager on behalf of FDOT

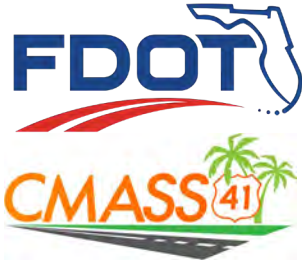
#### **Stakeholders**

Bradenton Area Economic Development Corporation – Sharon Hillstrom, President & CEO  
Bradenton Area Economic Development Corporation – Max Stewart, Vice President Economic Development

### [Project Overview](#)

The project team shared a presentation with meeting attendees providing information on the US 41 CMASS project including project description and background, goals and objectives, schedule, and public outreach. A copy of the presentation is included with these notes.

The “Connecting Communities” concept discussed is inclusive of various Sarasota and Manatee County projects with the purpose of holistically enhancing the multi-modal transportation network within those counties. Five major projects of that initiative as being the Bradenton-Palmetto Connector ACE/PD&E Study, the DeSoto Bridge PD&E Study, the Palmetto Trails Network/PD&E Study, US 41 CMASS, and 10<sup>th</sup> Avenue Complete Streets. The project team clarified that not all area projects were listed in the presentation and encouraged stakeholders to advise the study team of any projects of which he felt they should be aware.



US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
**Stakeholder/Agency Meeting (Virtual)**  
**Bradenton Area Economic Development Corporation**

---

The project team used a digital whiteboard application (Mural) to capture and document stakeholder ideas and feedback. A copy of the Mural whiteboard with all feedback received to date from the individual stakeholder meetings is included with these meeting notes.

### Open Discussion

- Stakeholder comments and suggestions
    - In strong agreement with the expressed need to advance transit service and operation strategies along the route, noting inefficiencies and the time it takes commuters to arrive to work given the existing transit schedules and access challenges.
    - Suggest reaching out to area childcare centers, churches, and educational institutions.
    - Need for aesthetic improvements along the corridor to reflect the vibrancy of Manatee County.
    - Anticipate future improvements of the area near the airport as well as at higher educational institutions will add to the need for safety considerations.
    - Need for improvements moving north into Manatee County as there is a significant difference in the area along the corridor beyond University Parkway.
    - Need to manage the turn lanes along the corridor.
    - Need for improvements in pedestrian safety, noting modes of travel of students along with the high rates of speed at which commuters travel along the route.
  - Continued stakeholder involvement
    - Request to receive updates on project activities and are interested in more one-on-one meetings once the project moves forward into future phases.
    - Would like to bring project information to their board in the future.
  - Connecting Communities Questionnaire distribution
    - Will distribute electronically.
    - Suggest materials being available in other languages, such as Spanish and Creole.
- 

### Attendees Email Addresses

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Lauren Brooks, AICP, [lauren.brooks@aecom.com](mailto:lauren.brooks@aecom.com)



# District One Environmental Management Office

## *Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS)*

*From University Parkway to 17<sup>th</sup> Street W*

February 2023





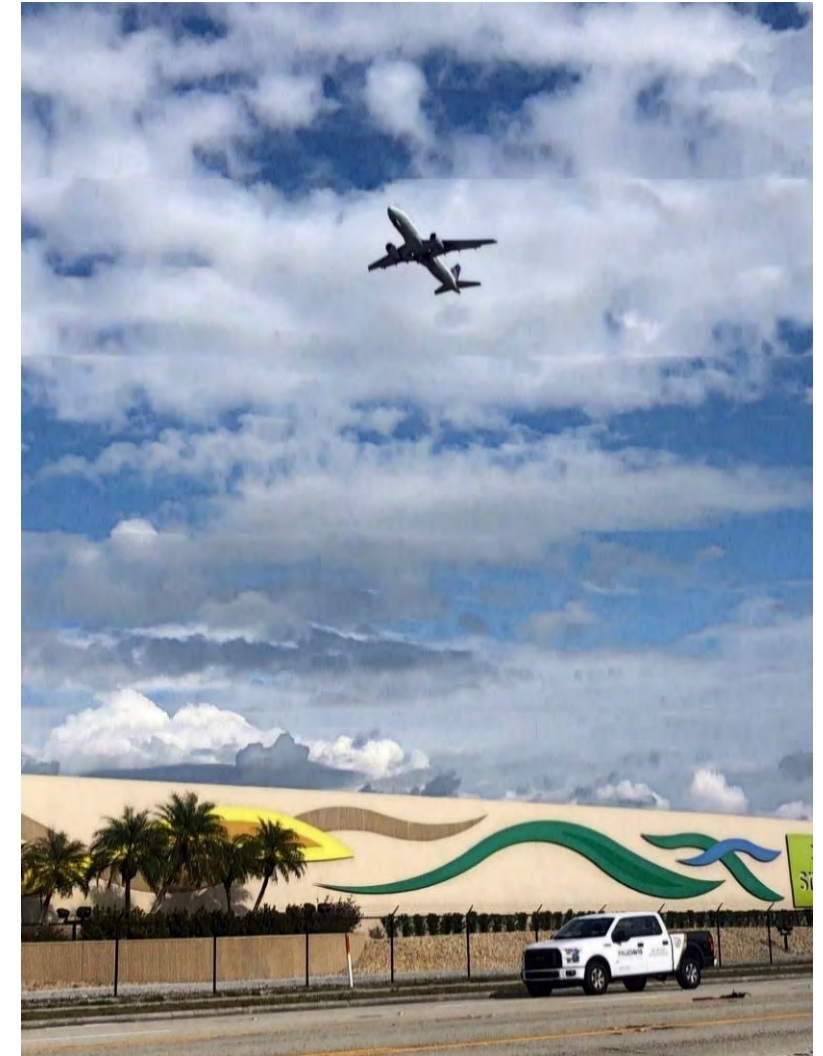
# Bradenton Area Economic Development Corporation

February 3, 2023



# Agenda

- Introductions
- Project Description and Background
- US 41 Today: Existing Conditions
- Public Outreach
- Open Discussion: We Want Your Input



# Introductions



**Abra Horne**  
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**Environmental Manager**  
District Environmental  
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# Introductions



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**Project Manager**  
**Senior Transportation Planner**  
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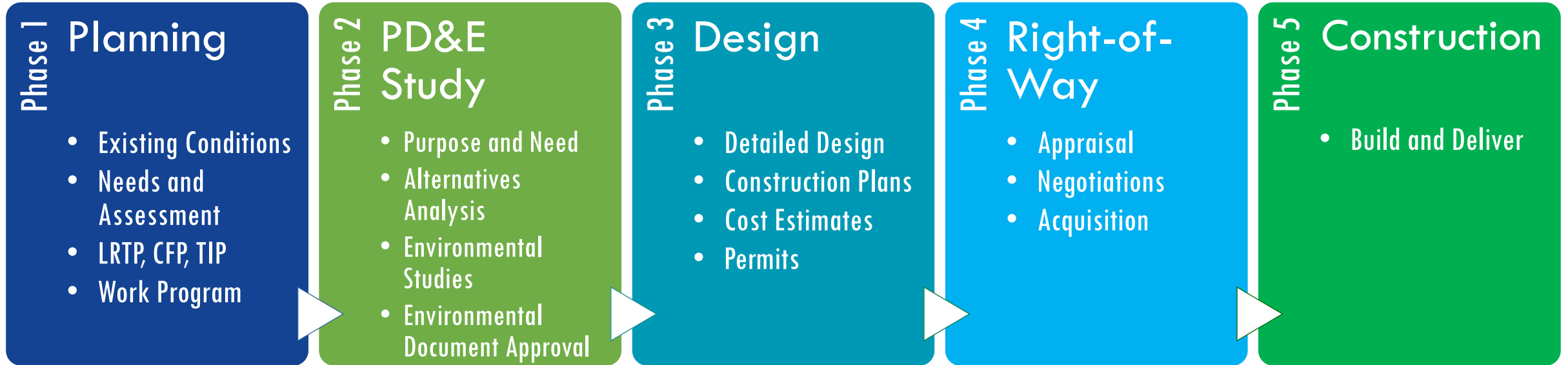


**Catherine Winter, P.E.**  
**Public Involvement Manager**  
The Valerin Group  
[Catherine@valerin-group.com](mailto:Catherine@valerin-group.com)  
(813) 431-0919

# Project Description and Background



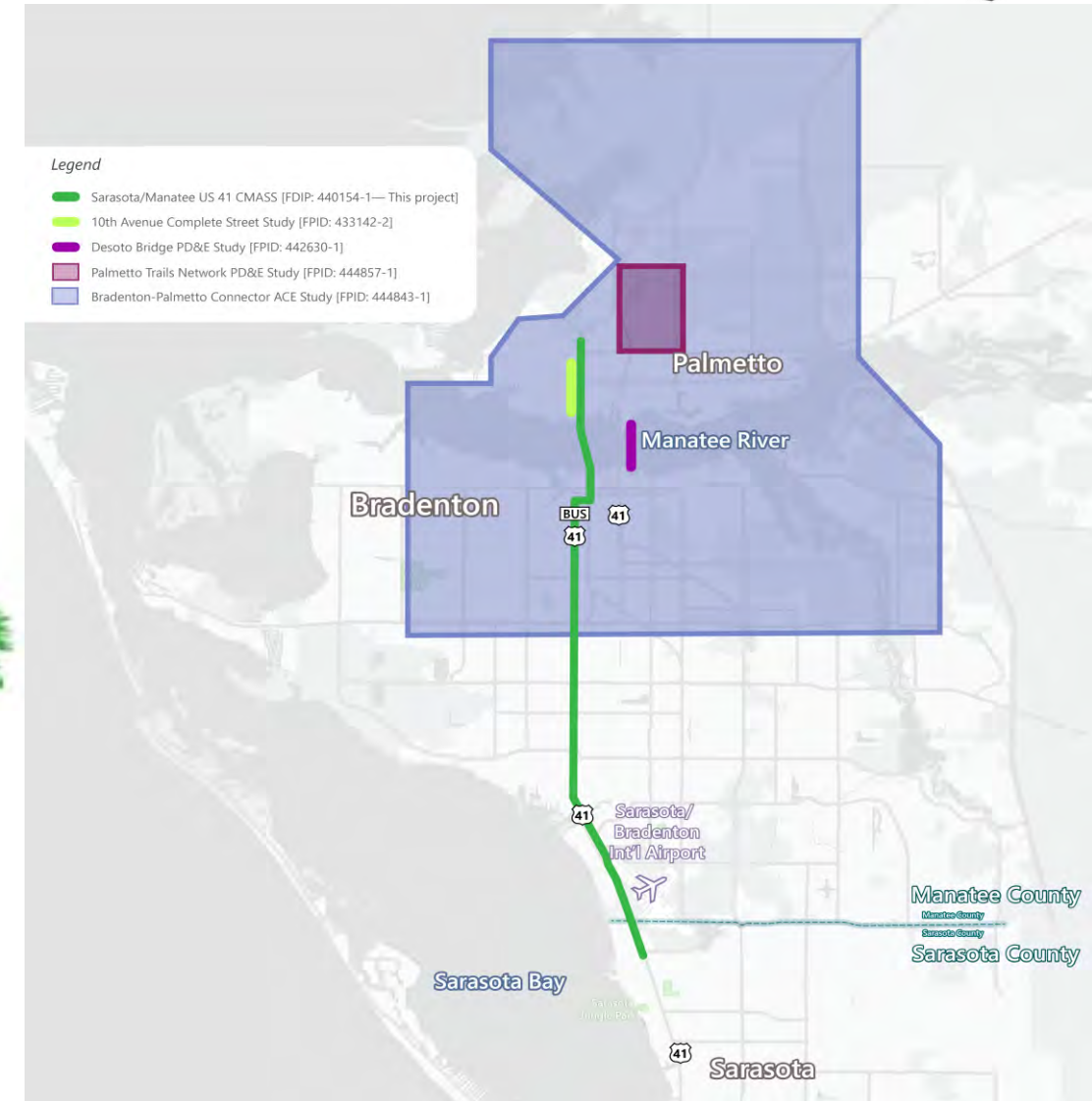
# Transportation Project Development Process



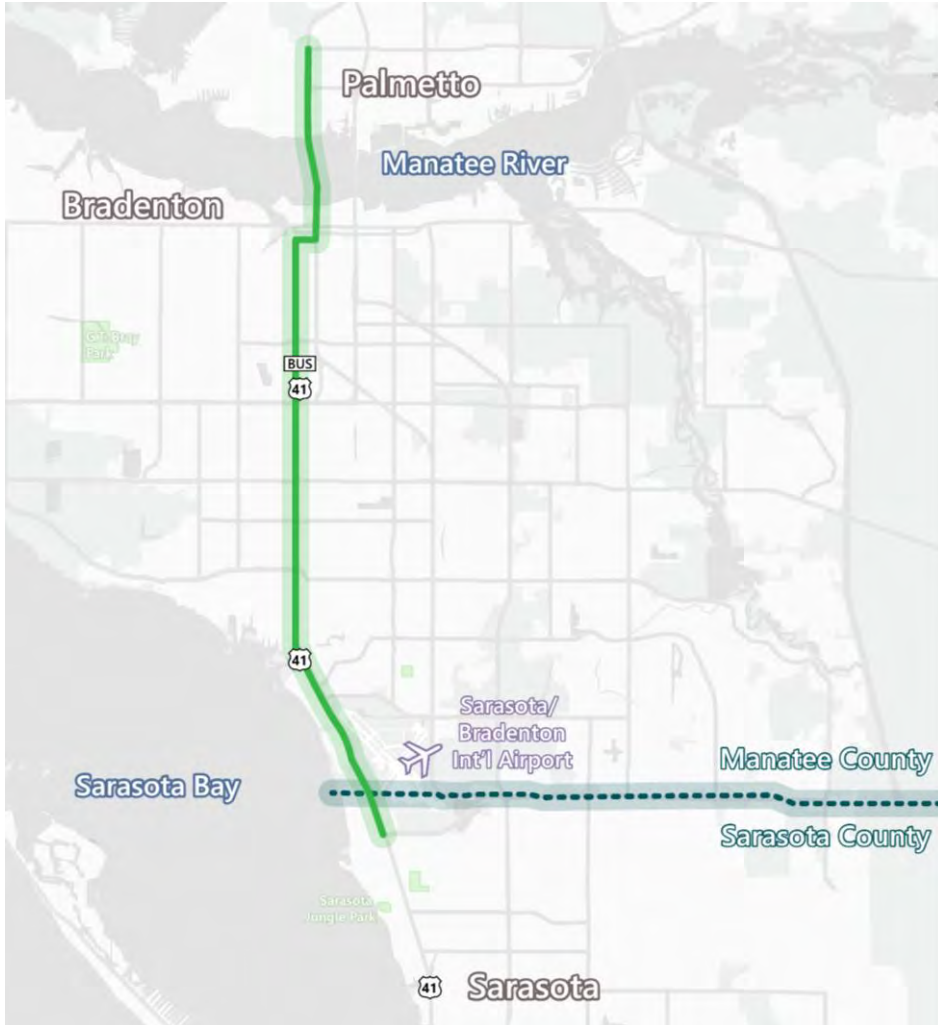
# Connecting Communities

**Purpose:** Holistically enhance the multimodal transportation network within Sarasota and Manatee Counties.

## DeSoto Bridge PD&E Study



# Project Description



**Project Limits:** US 41/US 41 Business corridor from University Parkway (Sarasota) to 17<sup>th</sup> Street W (Palmetto)

**Financial Project ID:** 440154-1-21-01

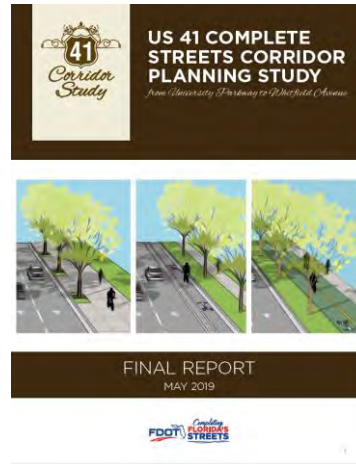
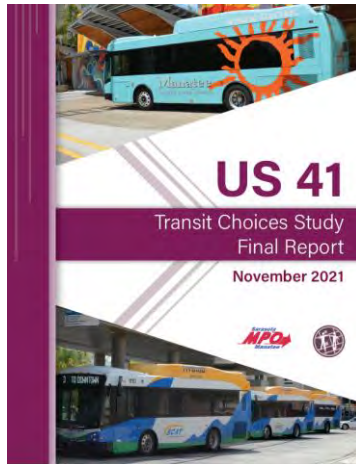
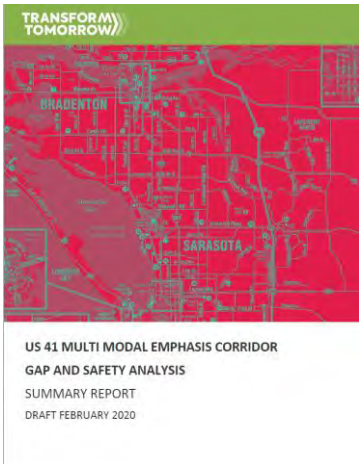
**Project Manager:** Matt Dockins

**Status:** Feasibility Study – Phase 1



# We are not starting from scratch...

- US 41 Multi-Modal Emphasis Corridor (MMEC)
- US 41 Transit Choices Study
- US 41 Complete Streets Corridor Planning Study
- Arterial Safety Study
- Access Management Study
- Intersection Safety Studies

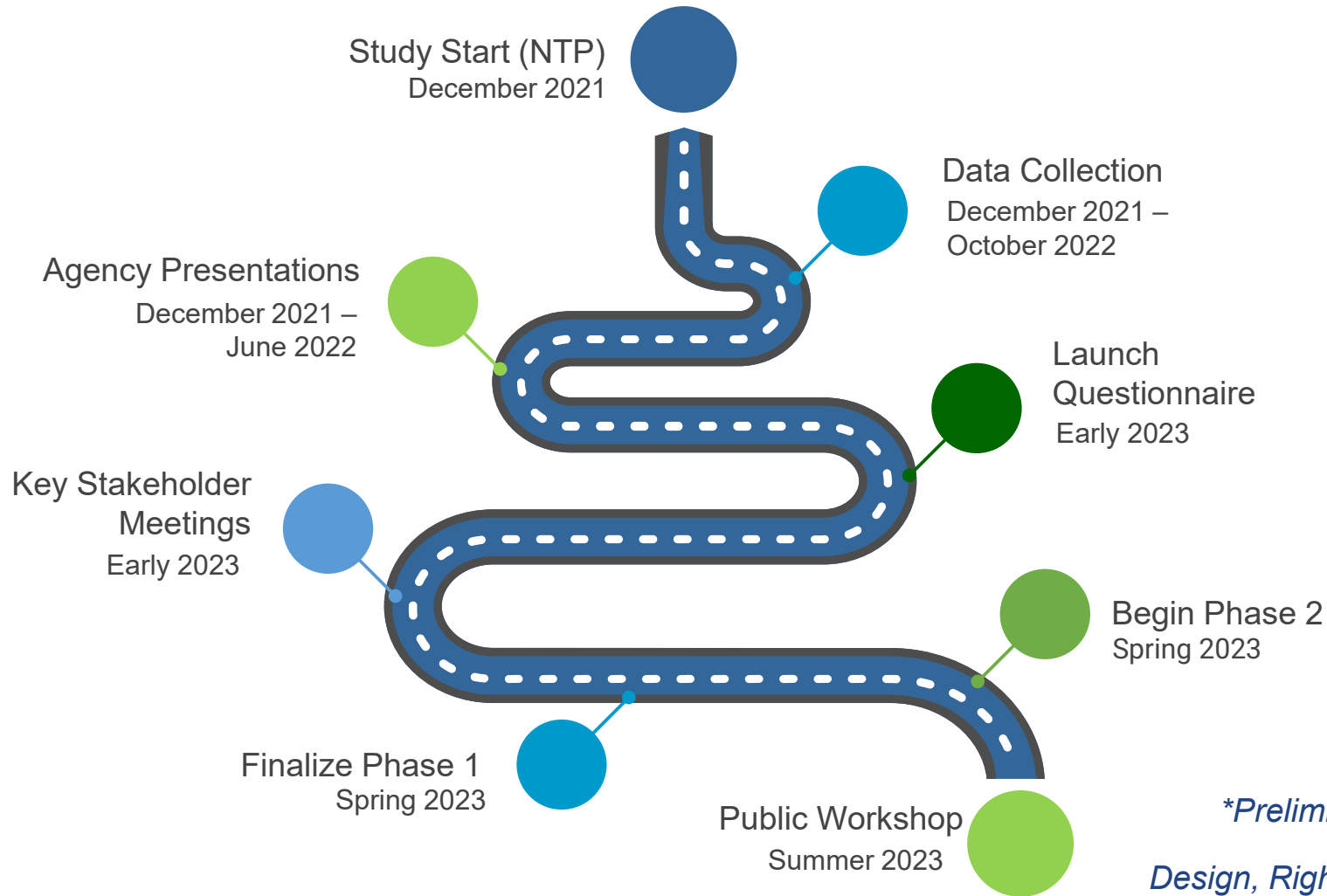


# Goals & Objectives

- Enhance multimodal mobility and safety for all users
- Advance transit service/operation strategies along the study corridor
- Improve transportation network access and interconnectivity within:
  - Neighborhoods
  - Adjacent areas
  - Transit centers



# Project Schedule



*\*Preliminary Schedule – subject to change\**  
*Design, Right-of-Way, & Construction are not funded.*

# US 41 Corridor Today



# US 41 Corridor Today

## What are the characteristics of the corridor?

- FDOT priority multimodal corridor
- **2-3 travel lanes in each direction**
- Separated by median, traffic separator, or center left turn lane
- Continuous sidewalks on both sides
- **Right of way varies with more constraints at northern end**
- **Posted speed limits vary from 25 to 45 mph**
- Longest travel time is northbound in evening peak
- Average travel speeds are lower than posted speed limit south of the Green Bridge
- Highest congestion occurs between 63<sup>rd</sup> Avenue and Manatee Avenue

## Who is using the corridor?

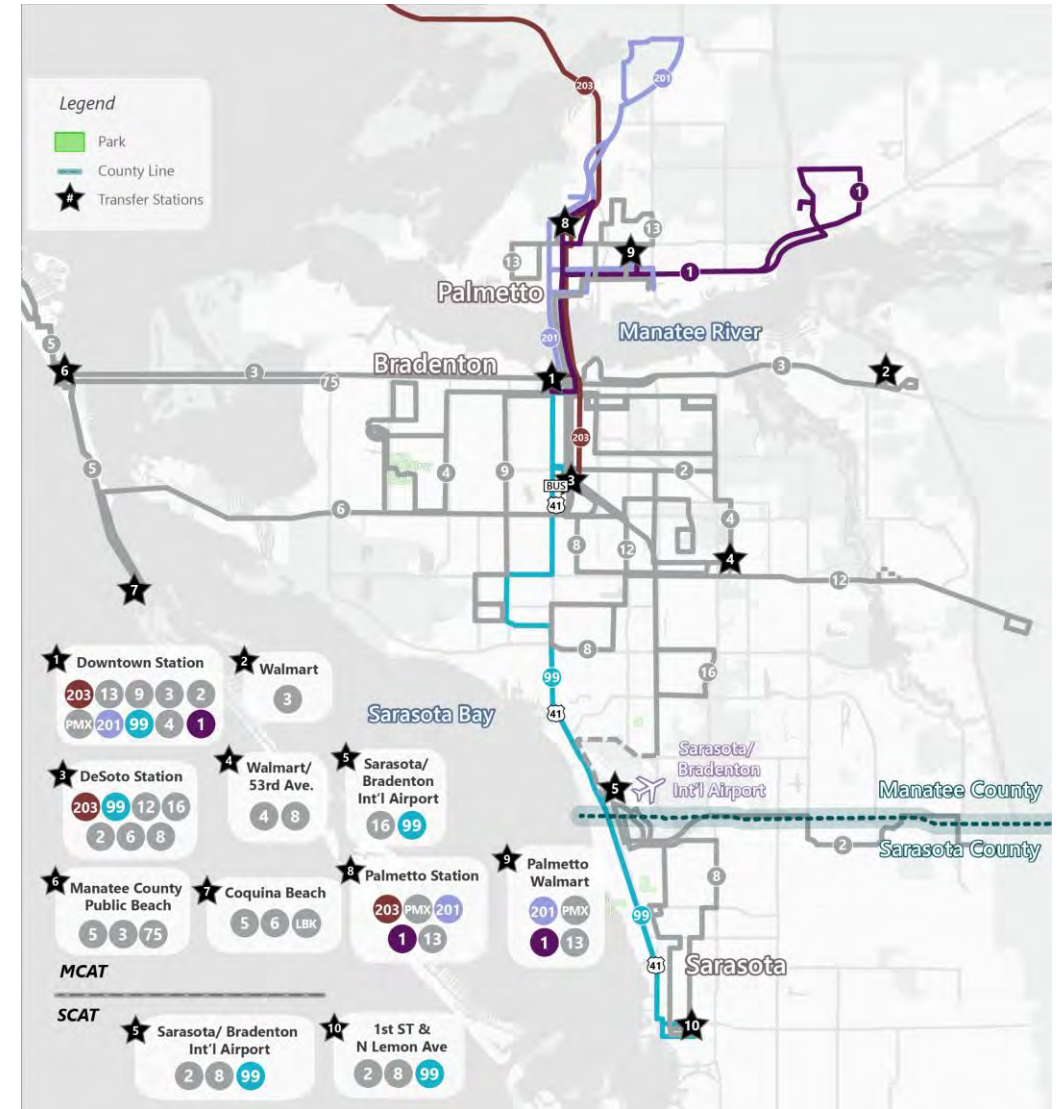
- Residents
- Visitors
- Students
- Businesses

## How are they travelling?

- Single Occupancy Vehicles (cars/trucks, etc.)
- Pedestrians
- Bicyclists
- Several transit (bus) routes

# Area Transit Routes

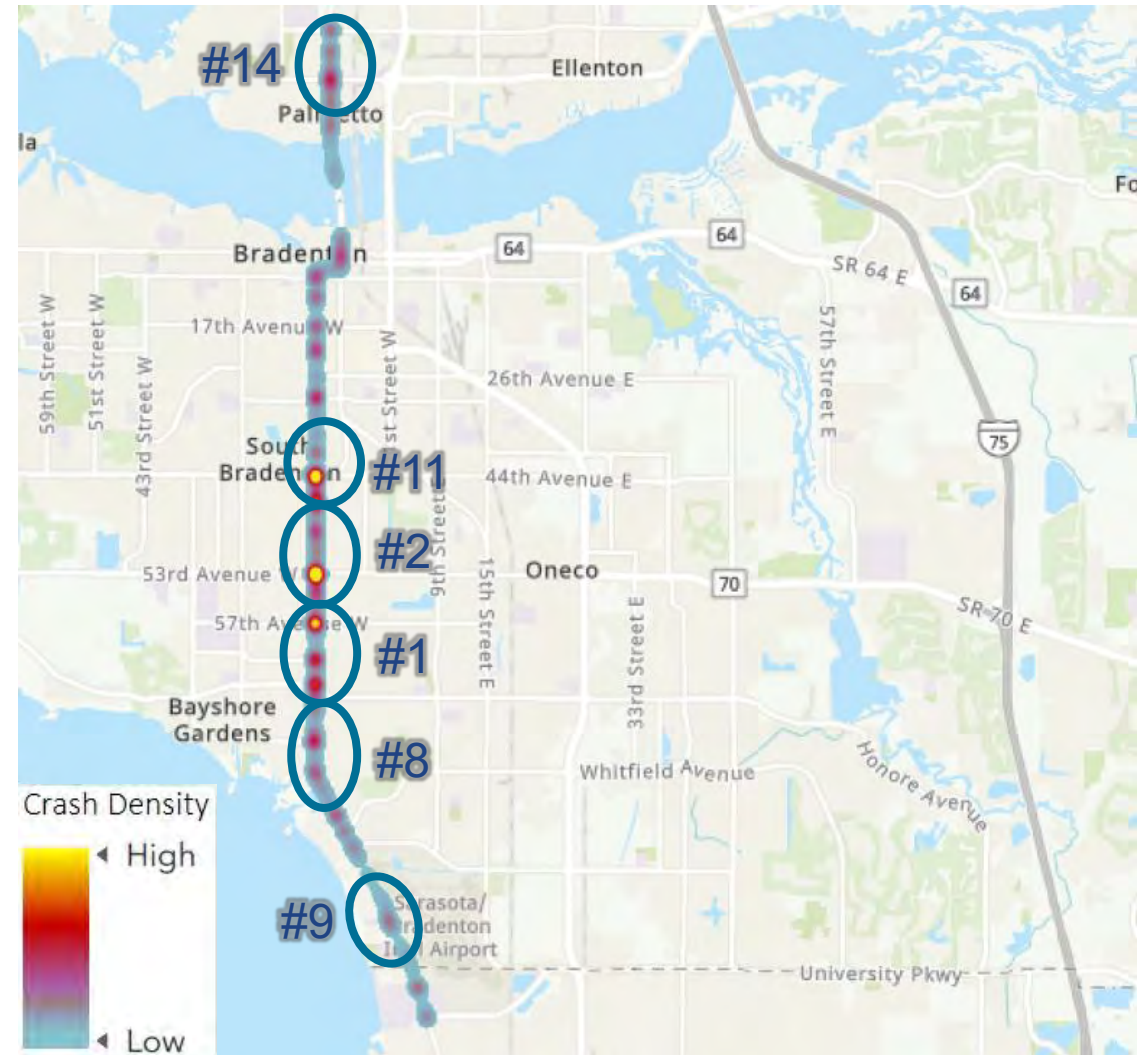
- Route 99 Bradenton-Sarasota
  - Runs length of study area
  - 20-minute AM and PM Peak Period service started Dec 2021
  - New Sunday service proposed starting in 2026
- Stations Near Project
  - Palmetto Station
  - Bradenton Downtown Station
  - DeSoto Station
  - Sarasota-Bradenton International Airport Station



# Safety and Crash Analysis

## Six segments are listed as SW Florida Fatal Top 20 Segments

- 63<sup>rd</sup> Ave W to 55<sup>th</sup> Ave W; Ranked #1
- 55<sup>th</sup> Ave W to Orlando Ave; Ranked #2
- Magellan Dr to 63<sup>rd</sup> Ave W; Ranked #8
- County line to Scott Ave; Ranked #9
- Orlando Ave to 30<sup>th</sup> Ave W; Ranked #11
- 10<sup>th</sup> St W to N of 23<sup>rd</sup> St W; Ranked #14



# Public Outreach

The graphic features a blue background with several circular and rectangular images. At the top, there are five icons: a car, a person with a cane, a bus, a person with a stroller, and a bicycle. The images include: a 'PUSH BUTTON FOR' sign with a pedestrian icon, a line of cars in traffic, a street view with palm trees and buildings, a green and white bus, a traffic light, and a street sign for 'NORTH BUSINESS 41 TAMiami TRAIL'. At the bottom, there are logos for 'FDOT' and 'CMASS 41', the title 'Sarasota | Manatee U.S. 41 Corridor Mobility and Safety Study Communications Plan', the date 'September 2022', and the project ID 'FPID #440154-1'. A signature line reads 'Approved By: Abra Horne Date: 9/2/2022' with the title 'Abra Horne, Planning and Environmental Administrator' below it.

Sarasota | Manatee  
 U.S. 41 Corridor Mobility  
 and Safety Study  
 Communications Plan

FDOT CMASS 41

September 2022 FPID #440154-1

Approved By: Abra Horne Date: 9/2/2022  
 Abra Horne, Planning and Environmental Administrator



# Public Outreach

## Outreach to Date:

### September 2021

- Sarasota/Manatee MPO Staff Meetings

### December 2021

- Sarasota/Manatee MPO Staff Meeting
- Pinellas Suncoast Transit Authority Meeting
- SunRunner BRT Meeting

### February 2022

- Sarasota/Manatee MPO Staff Meeting
- Sarasota/Manatee MPO Board Meeting
- Sarasota/Manatee MPO TAC Meeting

### April 2022

- Sarasota/Manatee MPO BPTAC Meeting

### May 2022

- Sarasota/Manatee MPO CAN Meeting
- Sarasota/Manatee MPO TAC Meeting
- Sarasota/Manatee MPO Board Meeting

### June 2022

- City of Bradenton City Council Meeting
- City of Palmetto City Council Meeting
- Manatee County Commission Meeting

## Upcoming Outreach:

### Early 2023

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire

### Summer 2023

- Hold Public Workshop - Summer 2023

### Throughout 2023

- Continued coordination with all agencies and stakeholders

# We Want to Hear From YOU!

Complete the questionnaire:

[swflroads.com/connectingcommunities/](http://swflroads.com/connectingcommunities/)



<http://www.swflroads.com/project/440154-1>

A promotional poster for the FDOT Connecting Communities Program. At the top left is the FDOT logo. At the top right is the 'CONNECTING COMMUNITIES' logo with a stylized 'e' icon and the text 'Manatee | Sarasota Counties'. Below the logos is a date range: 'January 2023 - April 2023'. The main heading is 'WE WANT TO HEAR FROM YOU!'. The text describes the program's goal to enhance the multimodal transportation network in Sarasota and Manatee Counties through various projects. Below the text is a collage of images showing different transportation modes and infrastructure, with circular callouts for 'Complete Streets', 'Safety Improvements', 'Corridor Visioning', 'Bridge Upgrades', and 'Multi-use Trails'. At the bottom, there is a 'Get involved today!' section with a QR code and a call to action to scan and take the online questionnaire. A footer at the very bottom provides more information and the website URL.

**FDOT**

**CONNECTING COMMUNITIES**  
Manatee | Sarasota Counties

January 2023 - April 2023

## WE WANT TO HEAR FROM YOU!

The Florida Department of Transportation, District One, is conducting several projects in partnership with the Sarasota/Manatee Metropolitan Planning Organization (MPO) and local governments to holistically enhance the multimodal transportation network within Sarasota and Manatee Counties as part of the Connecting Communities Program. Major Connecting Communities Program projects include Bradenton-Palmetto Connector, Desoto Bridge, Sarasota/Manatee US 41 Corridor Mobility and Safety Study, Palmetto Trails Network, and 10th Avenue Complete Street. For a map and full list of current area projects included as part of the Connecting Communities Program visit [www.swflroads.com/connectingcommunities](http://www.swflroads.com/connectingcommunities).

Complete Streets

Safety Improvements

Corridor Visioning

Bridge Upgrades

Multi-use Trails

**Get involved today!**

Scan to take our online questionnaire.

Complete our brief online questionnaire to provide valuable feedback for enhancing the multimodal transportation network.

For more information on the Connecting Communities Program, visit [www.swflroads.com/connectingcommunities](http://www.swflroads.com/connectingcommunities).

# Open Discussion

CMASS Stakeholder Meeting | All changes saved!

## CMASS Stakeholder Meeting | January 2023

**Connecting Communities**

The purpose of the Connecting Communities program is to holistically enhance the multimodal transportation network within Sarasota and Manatee Counties.

**US 41 CMASS - Goals & Objectives**

- Enhance multimodal mobility and safety for all users
- Advance transit service/operation strategies along the study corridor
- Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

**NEXT STEPS**

- Launch and commence development
- Finalize Goals & Objectives and Measures of Effectiveness
- Finalize Corridor Neighborhood Strategies
- Finalize Phase 1 - Spring 2022
- Hold Final Workshop - Summer 2022
- Begin Phase 2 Develop Concepts for Implementation

**ATTENDEES**

Name: [Redacted]

Agency: [Redacted]

Title: [Redacted]

**STAKEHOLDER INPUT**

Enhance multimodal mobility and safety for all users

[Green sticky notes]

Advance transit service/operation strategies along the study corridor

[Purple sticky notes]

Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

[Yellow sticky notes]

# Next Steps

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire
- Finalize Goals & Objectives and Measures of Effectiveness
- Finalize Corridor/Neighborhood Strategies
- Finalize Phase 1 – Spring 2023
- Hold Public Workshop - Summer 2023
- Begin Phase 2: Develop Concepts for Implementation

**Complete the questionnaire:**

[swfroads.com/connectingcommunities/](http://swfroads.com/connectingcommunities/)





# Safety, Questions and Contact Information

**Matt Dockins, PE**  
FDOT Project Manager  
FDOT District 1  
[info@us41cmass.com](mailto:info@us41cmass.com)

**Amy Sirmans, PE**  
Consultant Project Manager  
VHB  
[asirmans@vhb.com](mailto:asirmans@vhb.com)



**PEDESTRIANS  
AND BICYCLISTS**

Nationally, Florida had the **HIGHEST NUMBER OF BICYCLIST FATALITIES** in 2018

**Pedestrians** account for more than **22%** of **traffic fatalities** in Florida



# STAKEHOLDER INPUT



## Enhance multimodal mobility and safety for all users

Multimodal Mobility & Safety is #1 concern. Increased number of accidents occurring at US41 and General Spaatz Blvd in front of New College, including accidents involving students.

This project will help improve multimodal mobility. Crosswalk improvements/safety for school students would be beneficial

Suggestion to implement pedestrian crossings and enhance accessibility to nearby restaurants/facilities. There are currently no crosswalks on US41 at USF

Existing conditions include a basic sidewalk. Suggestion for greenway improvements

Concern for students crossing areas near Ringling College and the Airport

Have been discussions about providing safer pedestrian access near the airport

Suggestion to enhance pedestrian safety along DeSoto Bridge

Lack of crosswalks along corridor. Does not allow for recreation or exercise

## Advance transit service/operation strategies along the study corridor

Enhance transit along US 41 - Route 99 is best performing route along corridor

Many students use the pedestrian overpass rather than crossing US41 at the intersection. Bike traffic is more common than pedestrian traffic

Keep transit stops away from Runway 4 to enhance safety for pedestrians

Keeping the median opening at Dolphin would be beneficial for future development

Adding bus stops along corridor to benefit local community

Chamber focusing on cost effective methods for transportation including buses, on demand transit, and transit accessibility

Pedestrian/Scoter/Bike traffic around colleges. May not be necessary for a dedicated SCAT lane due to low ridership/trip

USF provides free transit (MCAT and SCAT) for students on campus. Bus stops are not on campus, but adjacent.

Having a stop at USF campus would provide enhanced accessibility for students

Congestion near universities does not allow for safe or efficient traffic flow

Transportation is barrier for employment. Enhanced transportation will benefit the community and employers

Transit timing is inefficient and poorly designed for workforce

## Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

Suggestion to enhance access to activity center located at US41 and 44th Ave W to benefit local community

Suggestion to make transit more easily accessible by adjacent neighborhoods

Area near Seagate Drive (Property 6) to be developed. Would benefit from enhanced pedestrian facilities. Will be developed as as parking lot for interim use

Not many businesses/restaurants surrounding New College. Students/Faculty rely on cars as main form of transportation. (~700 students residing on campus.)

Chamber considering best way to provide accessible transit for all

Future developments to increase campus activity, enhancing the opportunity for transit use.

Some students will use transit to get to other areas in Sarasota and Bradenton, however most use their own vehicle.

Consideration for businesses would be appreciated as they have been recently affected

Route to nearby businesses is not safe for pedestrians

Inefficient access to transit. Workforce living in southern Sarasota County travel to employment centers in northern Manatee County.

# MEETINGS TO DATE



- Bradenton Area Convention and Visitors Bureau
- Manasota Black Chamber of Commerce
- Manatee County Public Works
- New College of Florida
- Sarasota Manatee Airport Authority
- The Greater Sarasota Chamber of Commerce
- University of South Florida Sarasota-Manatee
- Manatee NAACP
- Bradenton Area Economic Development Corporation

## What improvements would you like to see for the US 41 corridor?

Enhanced lighting near Crosley Estate and USF Campus

Pedestrian facilities (Continous bike/ped) throughout the corridor

Consistency in roadway and surrounding facilities (aesthetics). The roadway and surrounding facilities are very different.

Opportunity for more bicycle lanes along US41 in both directions (N/S)

Enhanced safety at the intersection of US41 and General Spaatz Blvd

Lighting near runway zones is something to be considered. May need approval from FAA

Good wayfinding signs on US41 for Airport - beneficial to incorporate style/colors into signage

Bike/Ped connections between Sarasota and Manatee County

Coordination of red lights along the corridor

Aesthetic improvements along corridor

Continous sidewalks/pedestrian lighting throughout corridor to enhance safety

Adding bike lanes throughout the corridor.

Crosswalks along corridor and safety improvements when traveling to nearby businesses

Aesthetic improvements along corridor. Current aesthetics do not reflect the area well

## Are there any specific area of concern you would like to discuss?

Pedestrian activity walking to New College.

Difficulty for pedestrians to access transit stations

Insufficient lighting near New College

Ability to cross the road.

Traffic congestion between University and New College intersection in AM peak time

Varying speeds/high speed travelers at US 41 and New College entrance (especially northbound)

Lack of pedestrian access between New College and USF.

Pedestrians crossing the road in busier areas. Suggestion for a median that would enforce students/pedestrians to use designated crosswalks.

Captain Brian's Seafood has expressed concern for losing driveway access (involving resurfacing project) - could be beneficial to include shared access with Airport

High speeds along the corridor

Concern for homeless population in the area. Would like to see an intentional plan in place to accommodate the homeless population along the corridor

Concern for future stacking issues at Seagate Drive (highest entrance and exit counts here). Potential for increased congestion with coming development. Includes buses as well.

High speeds in the area. Speed control

Future increased traffic volume to cause issues along corridor

Upholding integrity for African American-owned churches and businesses along the corridor

Congestion and sustainability of the corridor.

Aesthetics and traffic flow along corridor.

Inconsistent aesthetics when traveling across the Sarasota/Manatee County line. Varying differences between the two counties, crossing University Parkway

## What issues do you notice when traveling the US 41 corridor?

Safety hazards (crashes, frequent stopping, roadway conditions, etc.)

Excessive vehicular speeding

Insufficient lighting

Difficulty entering/exiting properties along the corridor

Quality of sidewalks

Quality of bicycle facilities

Quality of transit stops

Lack of transit reliability

Traffic congestion

Inconsistency throughout the corridor including the examples above and number of lanes.

Turning into or out of USF onto US41 (Safety)

Lack of safety / rate of speed / congestion

Travelers unfamiliar with the area are unaware of surrounding conditions

Increased traffic flow

Lack of access management near Airport

Confusion with traffic flow patterns and turning lanes, especially considering ongoing projects

## What is your biggest complaint about traveling along the US 41 Corridor?

Poor lighting along the corridor for motorized traffic and pedestrians

High speeds near Crosley Estate

High pedestrian fatalities due to dangerous conditions

Lack of lighting and high traffic speeds

Bike/Ped safety along the corridor.

Complaints about the left turn signal at General Spaatz Blvd. Long wait times, congestion, queuing issues

Unsafe conditions along the corridor for pedestrians, especially near colleges

Lack of safety and aesthetics along corridor.

Lack of aesthetics along corridor.

Lack of pedestrian safety, especially near colleges

**Does your agency/organization have any planned improvements / projects / events that the project team should know about?**

Frequent Trolley Services from Crosley Estate to nearby for approximately 200 events per year.

Similar transportation methods at Ringling and New College.

Use of Trolley Service encouraged

Frequent events on campus, but most impacts are within the campus

Manatee County is working on Mobility Plan. The County will increase focus on capital project needs and prioritizations

Manatee County working on Trails Master Plan

Few events a year that may impact traffic. Winter events keep University traffic steady

Ground Transportation Center project to begin construction in coming months. Intended to improve MCAT and SCAT drop-off and pick-up. Larger area will accommodate 4 buses rather than 2.

Working with Manatee County to remove Ponce De Leon St once the land becomes the Airport's property

On-campus housing plans in development

Future commercial development anticipated in Property 5 and 6 near airport

Several developers interested in converting motels along the corridor into affordable housing, etc., which would provide a need for enhanced transit services

Future plan to construct nursing program will increase attendance

Annual Golf Tournament & Fall event partnered with Manatee Convention Center with 200+ in attendance

Juneteenth Event with local library

Planned improvements near airport and colleges are anticipated to bring growth in the next 10 years.

**How would you like to continue to be involved in the project?**

Meet with me again

Join Mailing List

Become a project champion

Additional meeting with New College.

Update meeting with Airport Authority

Join mailing list. Hold update meeting with governmental issues council (when appropriate). Can participate as a project champion

Continued meetings to keep up to date on the planning process. Mailing list updates.

Continued meetings to be updated on project process

Continued meetings and engagement with the African American Community

Project updates and meetings when appropriate

**Are there others from your organization that should be included?**

Add names and contact info

Joe Hembree - Commercial Realtor on Governmental Issues Board

James Golden - Legal Rep (NAACP)/ Manatee County Resident & Economic Development Chair.

Sororities and Fraternities - ASHAL Group and local churches

Potential to share with Bradenton Area EDC Board

**Can you help us distribute the questionnaire / project information? What methods?**

Newsletters

Mailing List

In-person flyers posted throughout New College campus

Post flyers throughout airport. Can ask IT to post to website.

Include in weekly and monthly newsletter

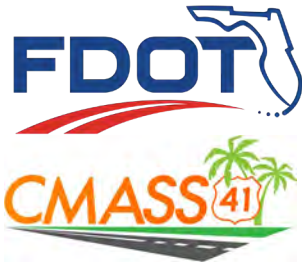
Town Hall Meetings, pass out flyers, email/ billboards

Will distribute to contact list.

Email blast and in-person depending on scheduled events

Electronic distribution

Suggestion for project team to look to local churches and childcare centers for distribution in addition to creating a spanish and creole version of the questionnaire.



US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
**Stakeholder/Agency Meeting (Virtual)**  
**Manatee NAACP**

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## **Virtual Meeting via Microsoft Teams**

January 24, 2023, 1:00 p.m.

### [Meeting Summary](#)

A virtual stakeholder’s meeting with the Manatee NAACP was conducted via Microsoft Teams on Tuesday, January 24, 2023. The purpose of the meeting was to present information for FDOT’s Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS) and conduct an open discussion between the study team and the Manatee NAACP.

### [Attendees](#)

#### **Project Team**

FDOT Abra Horne – District Administrator  
FDOT: Jeff James – Environmental Manager  
RK&K: Matt Dockins, PE – Project Manager on behalf of FDOT  
VHB: Amy Sirmans, PE – Consultant Project Manager  
VHB: Heather Garcia – Project Team Member  
VHB: Kennedy Sumner-Snell – Project Team Member  
The Valerin Group: Catherine Winter, PE – Public Involvement Coordinator  
AECOM: Lauren Brooks, AICP – Connecting Communities Program Manager on behalf of FDOT

#### **Stakeholders**

Manatee NAACP: Luther Wilkins – President, attended by phone and was unable to view the presentation

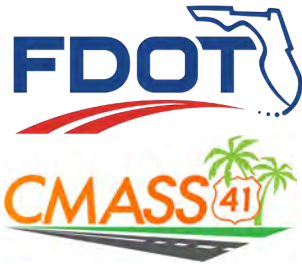
### [Project Overview](#)

Because Mr. Wilkins was attending by phone only, the team described the graphics in the presentation as the meeting progressed and shared that the presentation would be included with the meeting notes.

The project presentation provided information on the US 41 CMASS project including project description and background, goals and objectives, schedule, and public outreach. A copy of the presentation is included with these notes.

The “Connecting Communities” concept discussed is inclusive of various Sarasota and Manatee County projects with the purpose of holistically enhancing the multi-modal transportation network within those counties. Five major projects of that initiative as being the Bradenton-Palmetto Connector ACE/PD&E Study, the DeSoto Bridge PD&E Study, the Palmetto Trails Network/PD&E Study, CMASS, and 10<sup>th</sup> Avenue Complete Streets. The project team clarified that not all area projects were listed in the presentation and encouraged stakeholders to advise the study team of any projects of which he felt they should be aware.





US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
**Stakeholder/Agency Meeting (Virtual)**  
**Manatee NAACP**

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The project team used a digital whiteboard application (Mural) to capture and document stakeholder ideas and feedback. A copy of the Mural whiteboard with all feedback received to date from the individual stakeholder meetings is included with these meeting notes.

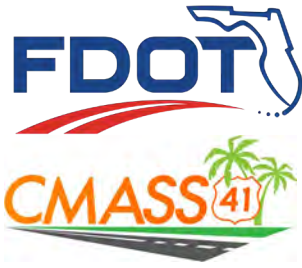
### Open Discussion

- Transportation Project Development Process
  - Explained “Concept to Concrete”, how FDOT goes through Phase 1 -Planning where the project needs are developed then through the five steps to Phase 5 – Construction.
- Other projects discussed
  - Typical resurfacing improvements include short-term solutions like sidewalk enhancements and lighting improvements and long-term improvements such as safety and bike-ped travel lanes and crossings, repurposing of outer lanes, lowering of speed limits, etc.
- Stakeholder concerns
  - Lighting needed to illuminate all sidewalk areas along the corridor.
  - Impacts to bus stops.
- Stakeholder suggestions and additional input
  - Expressed interest in the bike lanes element of the project and asked to be informed of the location once determined.
  - No project cost estimates have been developed at this time because no specific recommendations have been determined yet.
  - Expressed interest in learning about anticipated drawbacks of the project as recommendations are developed.
- Additional Outreach
  - The team asked if there were suggestions on other stakeholders to include in the FDOT outreach effort. Once the presentation is sent to Mr. Wilkins, he said could better identify those he feels should be included in future outreach initiatives.
- Connecting Communities Questionnaire Distribution
  - Stakeholder agreed to post on website.
  - Responses to questionnaire anticipated to be complete early next year.

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### Attendees Email Addresses

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Matt Dockins, PE, [mdockins@hntb.com](mailto:mdockins@hntb.com)  
Amy Sirmans, PE, [asirmans@vhb.com](mailto:asirmans@vhb.com)  
Heather Garcia, [hgarcia@vhb.com](mailto:hgarcia@vhb.com)



US 41 CMASS  
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**Stakeholder/Agency Meeting (Virtual)**  
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---

Kennedy Sumner-Snell, [ksumner-snell@vhb.com](mailto:ksumner-snell@vhb.com)  
Lauren Brooks, AICP, [lauren.brooks@aecom.com](mailto:lauren.brooks@aecom.com)  
Catherine Winter, PE, [catherine@valerin-group.com](mailto:catherine@valerin-group.com)



# District One Environmental Management Office

## *Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS)*

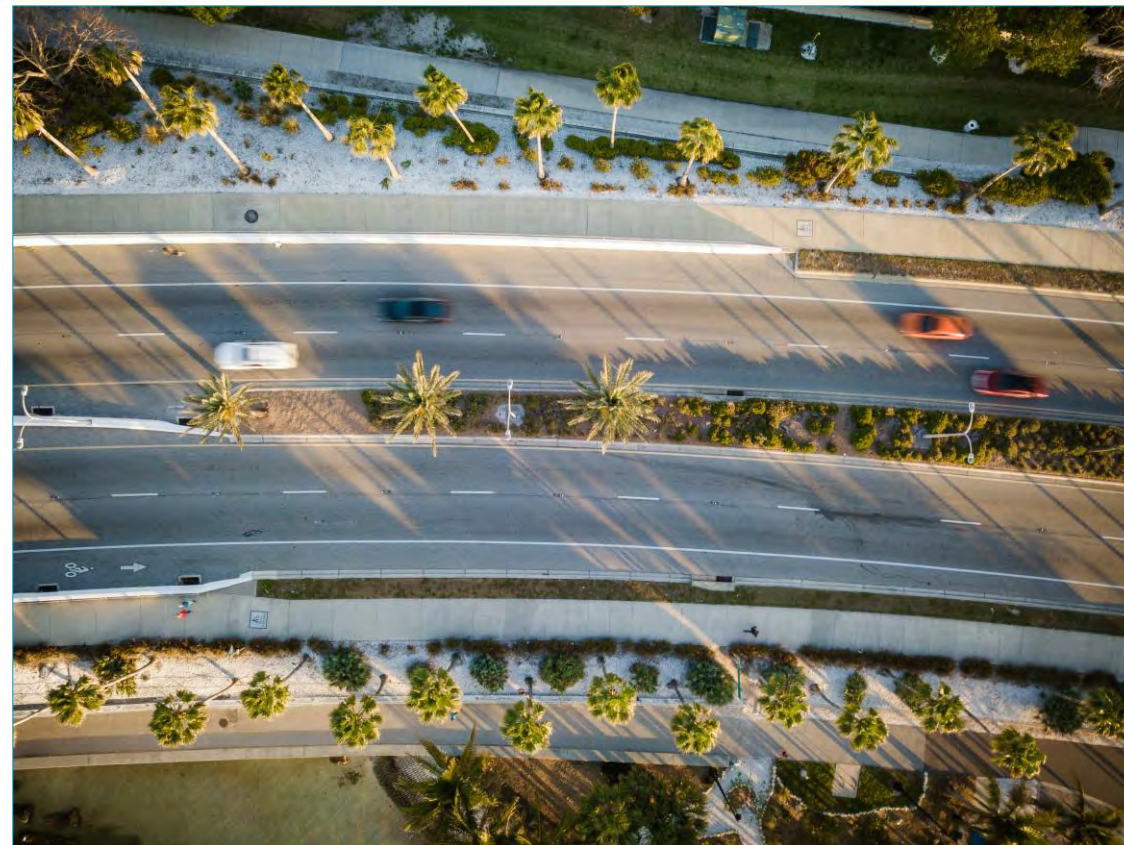
*From University Parkway to 17<sup>th</sup> Street W*

January 2023



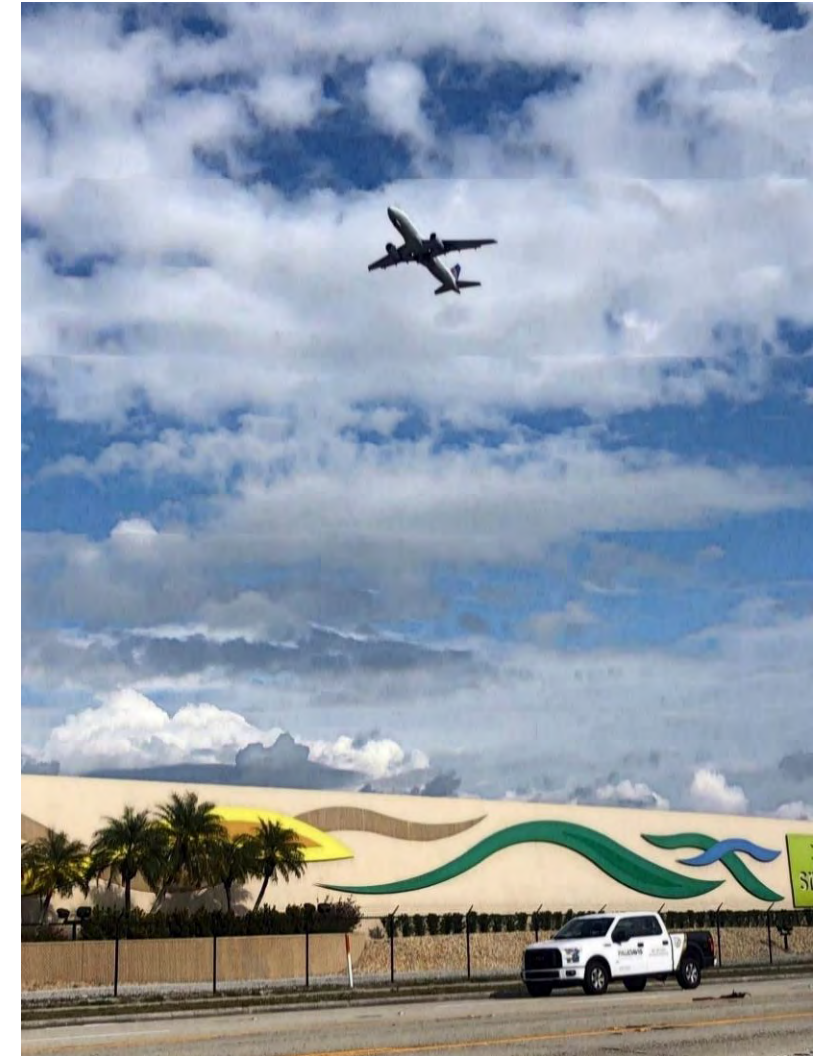
# Manatee County NAACP

January 24, 2023



# Agenda

- Introductions
- Project Description and Background
- US 41 Today: Existing Conditions
- Public Outreach
- Open Discussion: We Want Your Input



# Introductions



**Abra Horne**  
**Administrator**  
District Environmental  
Management Office  
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**Matt Dockins, P.E.**  
**Project Manager**  
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**Jeffrey James**  
**Environmental Manager**  
District Environmental  
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# Introductions



**Lauren Brooks, AICP**  
**Project Manager**  
**Senior Transportation Planner**  
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[asirmans@vhb.com](mailto:asirmans@vhb.com)  
(407) 839-4006



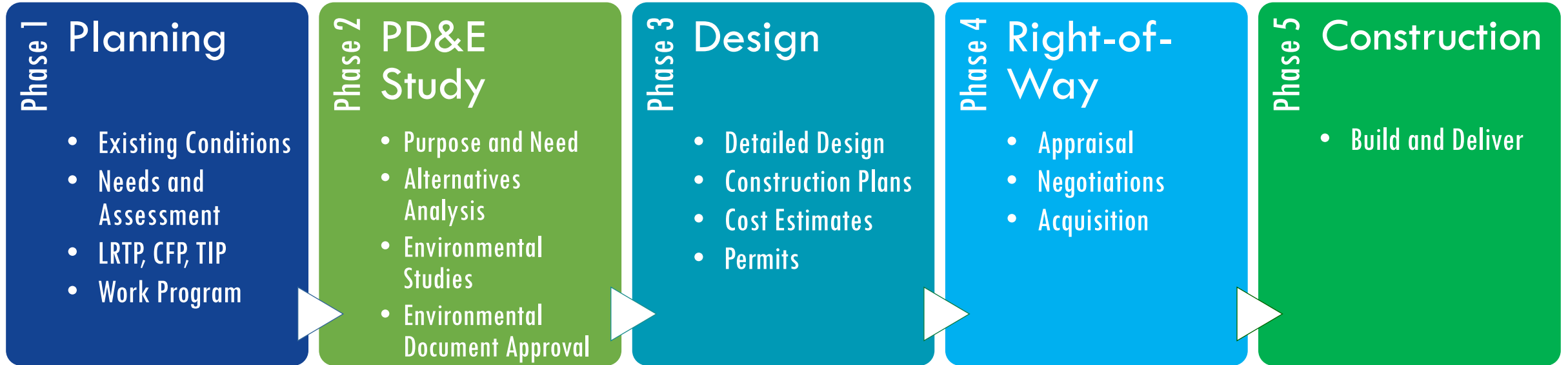
**Catherine Winter, P.E.**  
**Public Involvement Manager**  
**The Valerin Group**  
[Catherine@valerin-group.com](mailto:Catherine@valerin-group.com)  
(813) 431-0919

# Project Description and Background





# Transportation Project Development Process

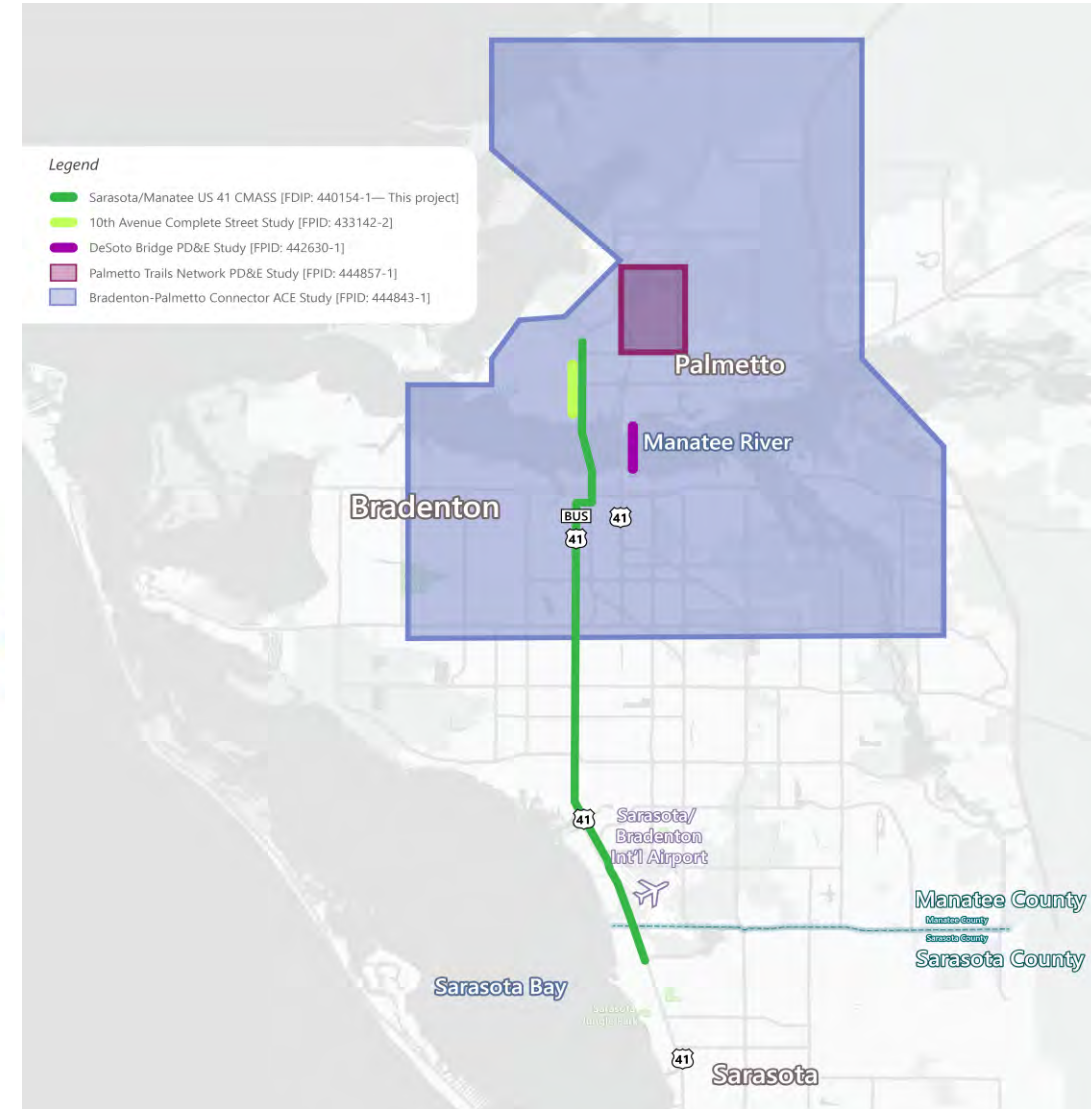


# Connecting Communities

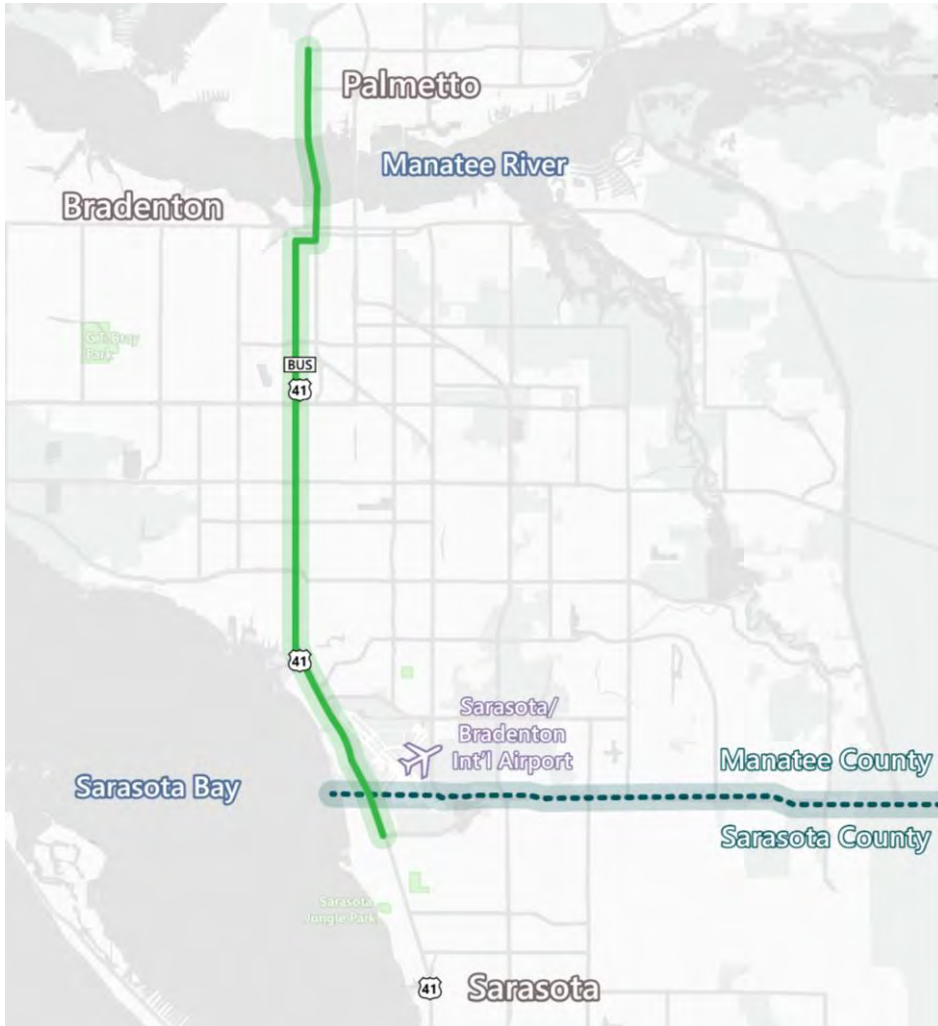
**Purpose:** Holistically enhance the multimodal transportation network within Sarasota and Manatee Counties.



## DeSoto Bridge PD&E Study



# Project Description



**Project Limits:** US 41/US 41 Business corridor from University Parkway (Sarasota) to 17<sup>th</sup> Street W (Palmetto)

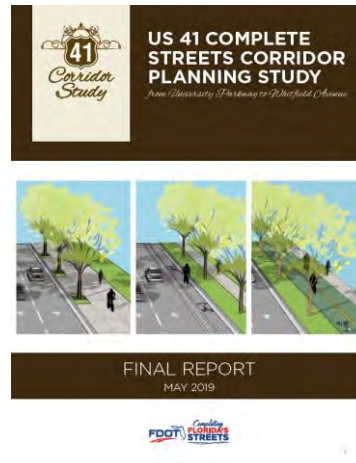
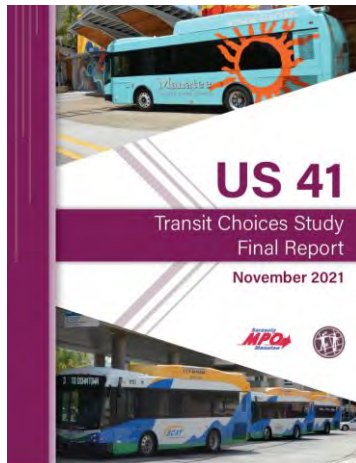
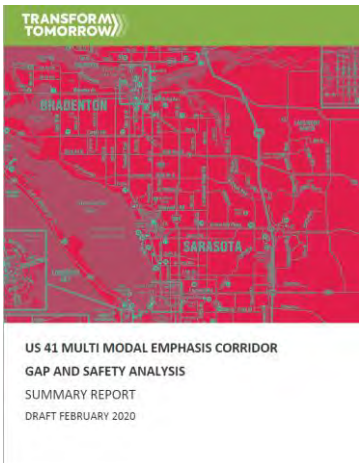
**Financial Project ID:** 440154-1-21-01

**Project Manager:** Matt Dockins

**Status:** Feasibility Study – Phase 1

# We are not starting from scratch...

- US 41 Multi-Modal Emphasis Corridor (MMEC)
- US 41 Transit Choices Study
- US 41 Complete Streets Corridor Planning Study
- Arterial Safety Study
- Access Management Study
- Intersection Safety Studies

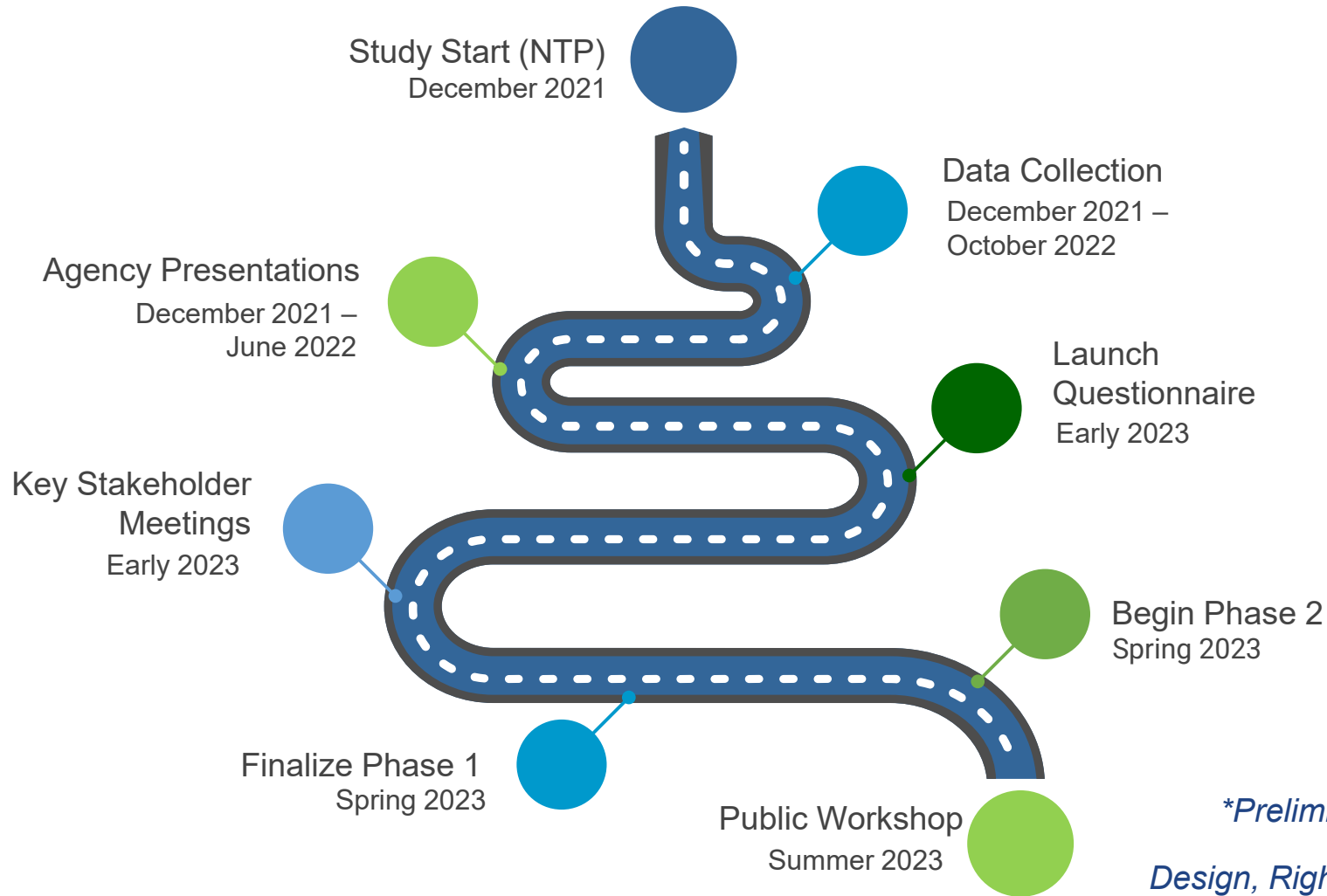


# Goals & Objectives

- Enhance multimodal mobility and safety for all users
- Advance transit service/operation strategies along the study corridor
- Improve transportation network access and interconnectivity within:
  - Neighborhoods
  - Adjacent areas
  - Transit centers



# Project Schedule



*\*Preliminary Schedule – subject to change\**  
*Design, Right-of-Way, & Construction are not funded.*

# US 41 Corridor Today



# US 41 Corridor Today

## What are the characteristics of the corridor?

- FDOT priority multimodal corridor
- **2-3 travel lanes in each direction**
- Separated by median, traffic separator, or center left turn lane
- Continuous sidewalks on both sides
- **Right of way varies with more constraints at northern end**
- **Posted speed limits vary from 25 to 45 mph**
- Longest travel time is northbound in evening peak
- Average travel speeds are lower than posted speed limit south of the Green Bridge
- Highest congestion occurs between 63<sup>rd</sup> Avenue and Manatee Avenue

## Who is using the corridor?

- Residents
- Visitors
- Students
- Businesses

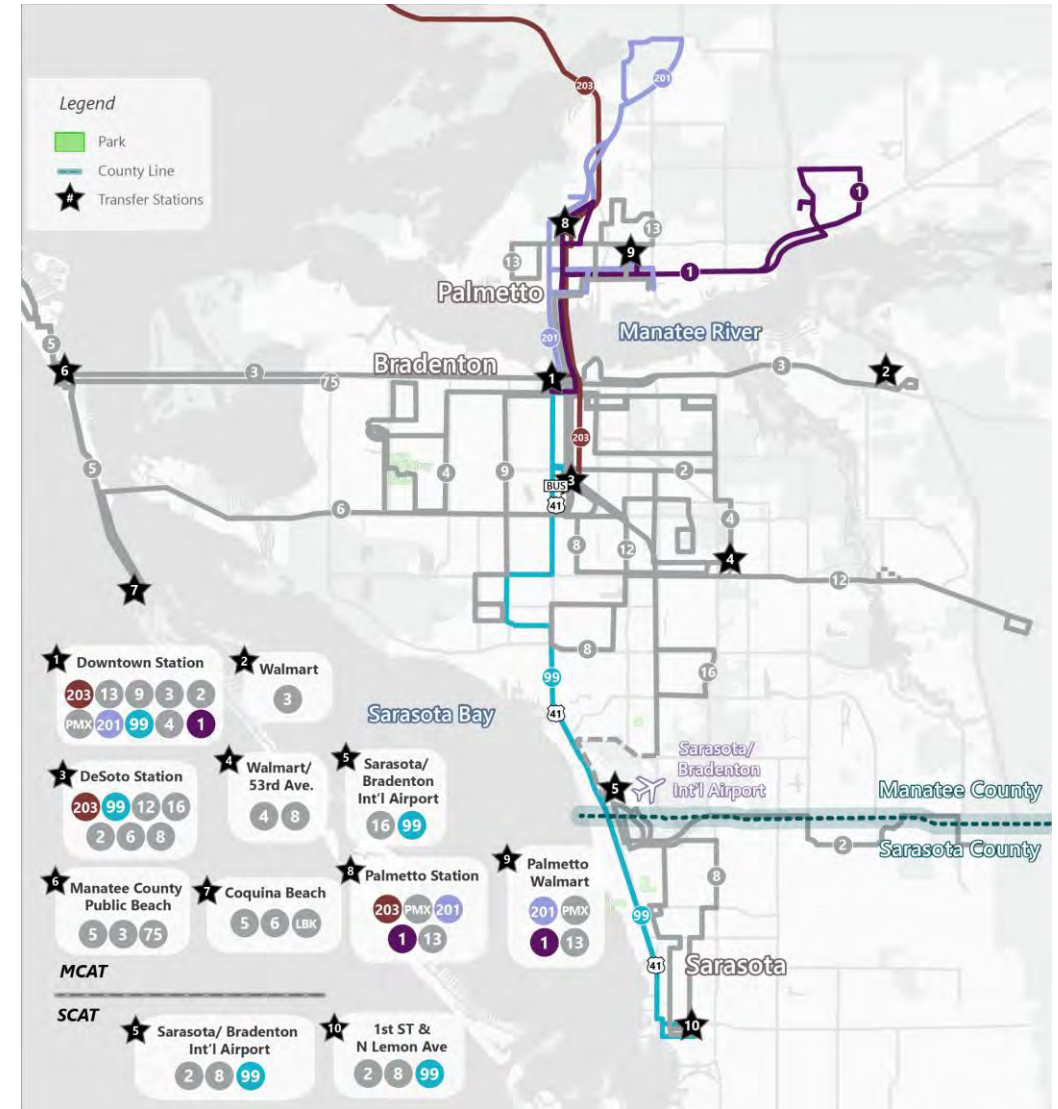
## How are they travelling?

- Single Occupancy Vehicles (cars/trucks, etc.)
- Pedestrians
- Bicyclists
- Several transit (bus) routes



# Area Transit Routes

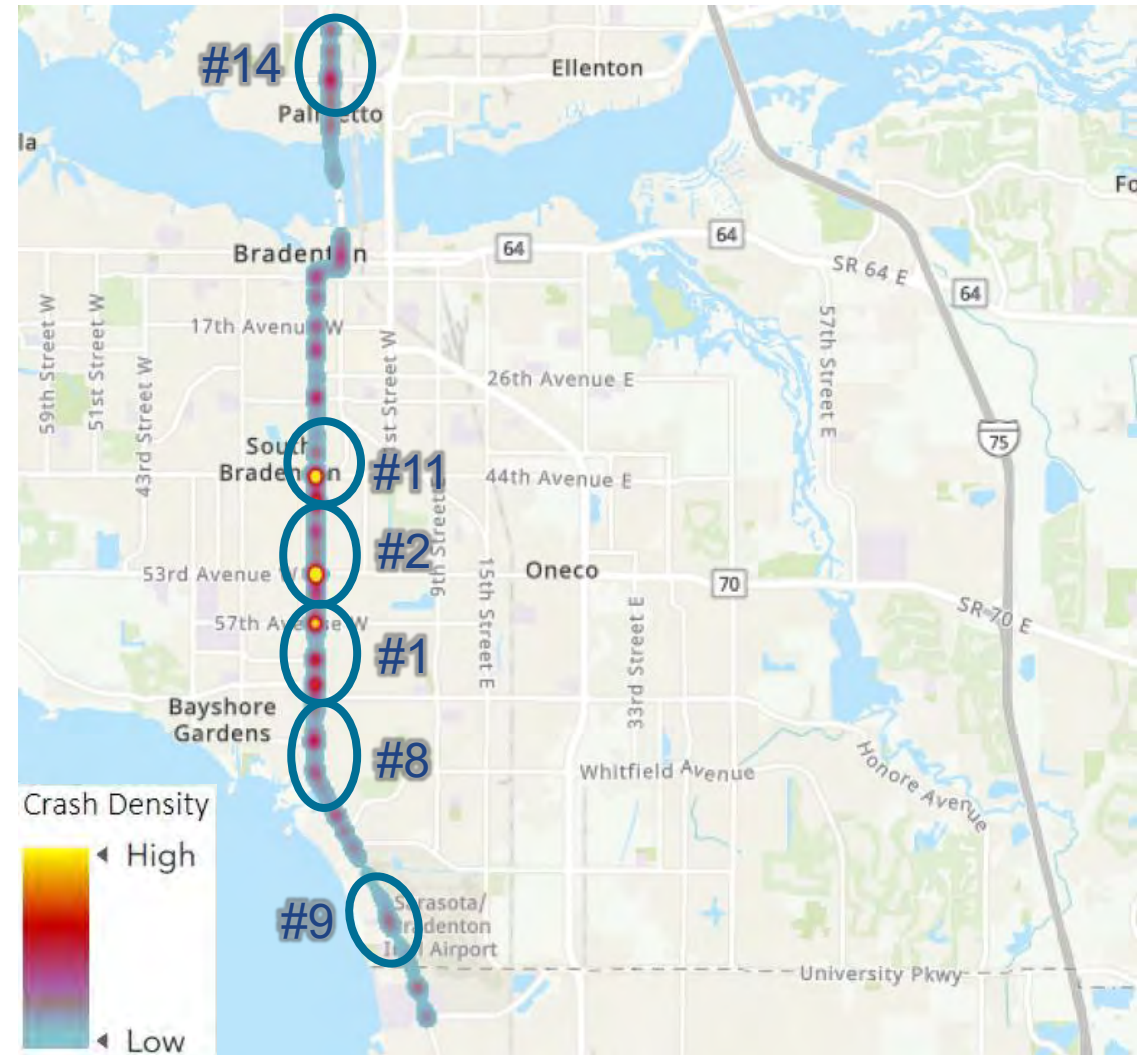
- Route 99 Bradenton-Sarasota
  - Runs length of study area
  - 20-minute AM and PM Peak Period service started Dec 2021
  - New Sunday service proposed starting in 2026
- Stations Near Project
  - Palmetto Station
  - Bradenton Downtown Station
  - DeSoto Station
  - Sarasota-Bradenton International Airport Station



# Safety and Crash Analysis

## Six segments are listed as SW Florida Fatal Top 20 Segments

- 63<sup>rd</sup> Ave W to 55<sup>th</sup> Ave W; Ranked #1
- 55<sup>th</sup> Ave W to Orlando Ave; Ranked #2
- Magellan Dr to 63<sup>rd</sup> Ave W; Ranked #8
- County line to Scott Ave; Ranked #9
- Orlando Ave to 30<sup>th</sup> Ave W; Ranked #11
- 10<sup>th</sup> St W to N of 23<sup>rd</sup> St W; Ranked #14



# Public Outreach

A graphic for a public outreach communications plan. At the top, there are five circular icons representing different modes of transport: a car, a person with a cane, a bus, a person with a stroller, and a bicycle. Below these are several circular images: a 'PUSH BUTTON FOR' sign with a pedestrian icon, a line of cars in traffic, a street view with palm trees and buildings, a green and white bus, and a traffic light. The text on the graphic includes the project title, logos for FDOT and CMASS 41, the date 'September 2022', the project ID 'FPID #440154-1', and an approval signature line.

**Sarasota | Manatee  
U.S. 41 Corridor Mobility  
and Safety Study  
Communications Plan**

**FDOT** **CMASS 41**

September 2022 FPID #440154-1

Approved By: Abra Horne Date: 9/2/2022  
Abra Horne, Planning and Environmental Administrator

# Public Outreach



## Outreach to Date:

### September 2021

- Sarasota/Manatee MPO Staff Meetings

### December 2021

- Sarasota/Manatee MPO Staff Meeting
- Pinellas Suncoast Transit Authority Meeting
- SunRunner BRT Meeting

### February 2022

- Sarasota/Manatee MPO Staff Meeting
- Sarasota/Manatee MPO Board Meeting
- Sarasota/Manatee MPO TAC Meeting

### April 2022

- Sarasota/Manatee MPO BPTAC Meeting

### May 2022

- Sarasota/Manatee MPO CAN Meeting
- Sarasota/Manatee MPO TAC Meeting
- Sarasota/Manatee MPO Board Meeting

### June 2022

- City of Bradenton City Council Meeting
- City of Palmetto City Council Meeting
- Manatee County Commission Meeting

## Upcoming Outreach:

### Early 2023

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire

### Summer 2023

- Hold Public Workshop - Summer 2023

### Throughout 2023

- Continued coordination with all agencies and stakeholders

# We Want to Hear From YOU!

Complete the questionnaire:

[swfroads.com/connectingcommunities/](http://swfroads.com/connectingcommunities/)



<http://www.swfroads.com/project/440154-1>

A flyer for the Connecting Communities Program. At the top left is the FDOT logo. To its right is the 'CONNECTING COMMUNITIES' logo with a stylized 'e' icon and the text 'Manatee | Sarasota Counties'. Below the logos is a date range: 'January 2023 - April 2023'. The main heading is 'WE WANT TO HEAR FROM YOU!'. The text describes the program's goal to enhance the multimodal transportation network in Sarasota and Manatee Counties through various projects. Below the text is a circular graphic with six overlapping circles, each containing an image and a label: 'Multi-use Trails' (with a person on a bike), 'Complete Streets' (with a city street), 'Safety Improvements' (with a road view), 'Corridor Visioning' (with a bus), 'Bridge Upgrades' (with a bridge), and 'TAMAMI TRAIL' (with a road sign). At the bottom, there is a 'Get involved today!' section with a QR code and text: 'Scan to take our online questionnaire. Complete our brief online questionnaire to provide valuable feedback for enhancing the multimodal transportation network.' Below this is a call to action: 'For more information on the Connecting Communities Program, visit www.swfroads.com/connectingcommunities.' The FDOT logo is also present in the bottom right corner of the flyer.

# Open Discussion

CMASS Stakeholder Meeting | All changes saved!

## CMASS Stakeholder Meeting | January 2023

**Connecting Communities**

The purpose of the Connecting Communities program is to holistically enhance the multimodal transportation network within Sarasota and Manatee Counties.

**US 41 CMASS - Goals & Objectives**

- Enhance multimodal mobility and safety for all users
- Advance transit service/operation strategies along the study corridor
- Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

**NEXT STEPS**

- Launch and commence development
- Finalize Goals & Objectives and Measures of Effectiveness
- Finalize Corridor Neighborhood Strategies
- Finalize Phase 1 - Spring 2022
- Hold Final Workshop - Summer 2022
- Begin Phase 2 Develop Concepts for Implementation

**ATTENDEES**

Name: [Redacted]

Agency: [Redacted]

Title: [Redacted]

**STAKEHOLDER INPUT**

Enhance multimodal mobility and safety for all users

[Green sticky notes]

Advance transit service/operation strategies along the study corridor

[Purple sticky notes]

Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

[Yellow sticky notes]

[User avatars and interaction icons]

# Next Steps

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire
- Finalize Goals & Objectives and Measures of Effectiveness
- Finalize Corridor/Neighborhood Strategies
- Finalize Phase 1 – Spring 2023
- Hold Public Workshop - Summer 2023
- Begin Phase 2: Develop Concepts for Implementation

**Complete the questionnaire:**

[swfroads.com/connectingcommunities/](http://swfroads.com/connectingcommunities/)





# Safety, Questions and Contact Information

**Matt Dockins, PE**  
FDOT Project Manager  
FDOT District 1  
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**Amy Sirmans, PE**  
Consultant Project Manager  
VHB  
[asirmans@vhb.com](mailto:asirmans@vhb.com)



**PEDESTRIANS  
AND BICYCLISTS**

Nationally, Florida had the **HIGHEST NUMBER OF BICYCLIST FATALITIES** in 2018

**Pedestrians** account for more than **22%** of traffic fatalities in Florida





# STAKEHOLDER INPUT



## Enhance multimodal mobility and safety for all users

Multimodal Mobility & Safety is #1 concern. Increased number of accidents occurring at US41 and General Spaatz Blvd in front of New College, including accidents involving students.

This project will help improve multimodal mobility. Crosswalk improvements/safety for school students would be beneficial

Suggestion to implement pedestrian crossings and enhance accessibility to nearby restaurants/facilities. There are currently no crosswalks on US41 at USF

Existing conditions include a basic sidewalk. Suggestion for greenway improvements

Concern for students crossing areas near Ringling College and the Airport

Have been discussions about providing safer pedestrian access near the airport

Suggestion to enhance pedestrian safety along DeSoto Bridge

Lack of crosswalks along corridor. Does not allow for recreation or exercise

## Advance transit service/operation strategies along the study corridor

Enhance transit along US 41 - Route 99 is best performing route along corridor

Many students use the pedestrian overpass rather than crossing US41 at the intersection. Bike traffic is more common than pedestrian traffic

Keep transit stops away from Runway 4 to enhance safety for pedestrians

Keeping the median opening at Dolphin would be beneficial for future development

Adding bus stops along corridor to benefit local community

Chamber focusing on cost effective methods for transportation including buses, on demand transit, and transit accessibility

Pedestrian/Scoter/Bike traffic around colleges. May not be necessary for a dedicated SCAT lane due to low ridership/trip

USF provides free transit (MCAT and SCAT) for students on campus. Bus stops are not on campus, but adjacent.

Having a stop at USF campus would provide enhanced accessibility for students

Congestion near universities does not allow for safe or efficient traffic flow

Transportation is barrier for employment. Enhanced transportation will benefit the community and employers

Transit timing is inefficient and poorly designed for workforce

## Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

Suggestion to enhance access to activity center located at US41 and 44th Ave W to benefit local community

Suggestion to make transit more easily accessible by adjacent neighborhoods

Area near Seagate Drive (Property 6) to be developed. Would benefit from enhanced pedestrian facilities. Will be developed as as parking lot for interim use

Not many businesses/restaurants surrounding New College. Students/Faculty rely on cars as main form of transportation. (~700 students residing on campus.)

Chamber considering best way to provide accessible transit for all

Future developments to increase campus activity, enhancing the opportunity for transit use.

Some students will use transit to get to other areas in Sarasota and Bradenton, however most use their own vehicle.

Consideration for businesses would be appreciated as they have been recently affected

Route to nearby businesses is not safe for pedestrians

Inefficient access to transit. Workforce living in southern Sarasota County travel to employment centers in northern Manatee County.

# MEETINGS TO DATE



- Bradenton Area Convention and Visitors Bureau
- Manasota Black Chamber of Commerce
- Manatee County Public Works
- New College of Florida
- Sarasota Manatee Airport Authority
- The Greater Sarasota Chamber of Commerce
- University of South Florida Sarasota-Manatee
- Manatee NAACP
- Bradenton Area Economic Development Corporation

## What improvements would you like to see for the US 41 corridor?

Enhanced lighting near Crosley Estate and USF Campus

Pedestrian facilities (Continous bike/ped) throughout the corridor

Consistency in roadway and surrounding facilities (aesthetics). The roadway and surrounding facilities are very different.

Opportunity for more bicycle lanes along US41 in both directions (N/S)

Enhanced safety at the intersection of US41 and General Spaatz Blvd

Lighting near runway zones is something to be considered. May need approval from FAA

Good wayfinding signs on US41 for Airport - beneficial to incorporate style/colors into signage

Bike/Ped connections between Sarasota and Manatee County

Coordination of red lights along the corridor

Aesthetic improvements along corridor

Continous sidewalks/pedestrian lighting throughout corridor to enhance safety

Adding bike lanes throughout the corridor.

Crosswalks along corridor and safety improvements when traveling to nearby businesses

Aesthetic improvements along corridor. Current aesthetics do not reflect the area well

## Are there any specific area of concern you would like to discuss?

Pedestrian activity walking to New College.

Difficulty for pedestrians to access transit stations

Insufficient lighting near New College

Ability to cross the road.

Traffic congestion between University and New College intersection in AM peak time

Varying speeds/high speed travelers at US 41 and New College entrance (especially northbound)

Lack of pedestrian access between New College and USF.

Pedestrians crossing the road in busier areas. Suggestion for a median that would enforce students/pedestrians to use designated crosswalks.

Captain Brian's Seafood has expressed concern for losing driveway access (involving resurfacing project) - could be beneficial to include shared access with Airport

High speeds along the corridor

Concern for homeless population in the area. Would like to see an intentional plan in place to accommodate the homeless population along the corridor

Concern for future stacking issues at Seagate Drive (highest entrance and exit counts here). Potential for increased congestion with coming development. Includes buses as well.

High speeds in the area. Speed control

Upholding integrity for African American-owned churches and businesses along the corridor

Congestion and sustainability of the corridor.

Inconsistent aesthetics when traveling across the Sarasota/Manatee County line. Varying differences between the two counties, crossing University Parkway

Aesthetics and traffic flow along corridor.

## What issues do you notice when traveling the US 41 corridor?

Safety hazards (crashes, frequent stopping, roadway conditions, etc.)

Excessive vehicular speeding

Insufficient lighting

Difficulty entering/exiting properties along the corridor

Quality of sidewalks

Quality of bicycle facilities

Quality of transit stops

Lack of transit reliability

Traffic congestion

Inconsistency throughout the corridor including the examples above and number of lanes.

Turning into or out of USF onto US41 (Safety)

Lack of safety / rate of speed / congestion

Travelers unfamiliar with the area are unaware of surrounding conditions

Increased traffic flow

Lack of access management near Airport

Confusion with traffic flow patterns and turning lanes, especially considering ongoing projects

## What is your biggest complaint about traveling along the US 41 Corridor?

Poor lighting along the corridor for motorized traffic and pedestrians

High speeds near Crosley Estate

High pedestrian fatalities due to dangerous conditions

Lack of lighting and high traffic speeds

Bike/Ped safety along the corridor.

Complaints about the left turn signal at General Spaatz Blvd. Long wait times, congestion, queuing issues

Unsafe conditions along the corridor for pedestrians, especially near colleges

Lack of safety and aesthetics along corridor.

Lack of aesthetics along corridor.

Lack of pedestrian safety, especially near colleges

**Does your agency/organization have any planned improvements / projects / events that the project team should know about?**

Frequent Trolley Services from Crosley Estate to nearby for approximately 200 events per year.

Similar transportation methods at Ringling and New College.

Use of Trolley Service encouraged

Frequent events on campus, but most impacts are within the campus

Manatee County is working on Mobility Plan. The County will increase focus on capital project needs and prioritizations

Manatee County working on Trails Master Plan

Few events a year that may impact traffic. Winter events keep University traffic steady

Ground Transportation Center project to begin construction in coming months. Intended to improve MCAT and SCAT drop-off and pick-up. Larger area will accommodate 4 buses rather than 2.

Working with Manatee County to remove Ponce De Leon St once the land becomes the Airport's property

On-campus housing plans in development

Future commercial development anticipated in Property 5 and 6 near airport

Several developers interested in converting motels along the corridor into affordable housing, etc., which would provide a need for enhanced transit services

Future plan to construct nursing program will increase attendance

Annual Golf Tournament & Fall event partnered with Manatee Convention Center with 200+ in attendance

Juneteenth Event with local library

Planned improvements near airport and colleges are anticipated to bring growth in the next 10 years.

**How would you like to continue to be involved in the project?**

Meet with me again

Join Mailing List

Become a project champion

Additional meeting with New College.

Update meeting with Airport Authority

Join mailing list. Hold update meeting with governmental issues council (when appropriate). Can participate as a project champion

Continued meetings to keep up to date on the planning process. Mailing list updates.

Continued meetings to be updated on project process

Continued meetings and engagement with the African American Community

Project updates and meetings when appropriate

**Are there others from your organization that should be included?**

Add names and contact info

Joe Hembree - Commercial Realtor on Governmental Issues Board

James Golden - Legal Rep (NAACP)/ Manatee County Resident & Economic Development Chair.

Sororities and Fraternities - ASHAL Group and local churches

Potential to share with Bradenton Area EDC Board

**Can you help us distribute the questionnaire / project information? What methods?**

Newsletters

Mailing List

In-person flyers posted throughout New College campus

Post flyers throughout airport. Can ask IT to post to website.

Include in weekly and monthly newsletter

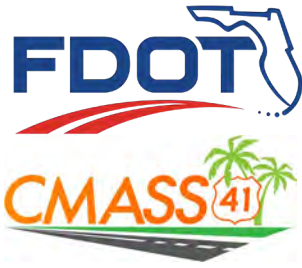
Town Hall Meetings, pass out flyers, email/ billboards

Will distribute to contact list.

Email blast and in-person depending on scheduled events

Electronic distribution

Suggestion for project team to look to local churches and childcare centers for distribution in addition to creating a spanish and creole version of the questionnaire.



US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
**Stakeholder/Agency Meeting (Virtual)**  
New College of Florida

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## **Virtual Meeting via Microsoft Teams**

January 18, 2023, 1:00 p.m.

### [Meeting Summary](#)

A virtual stakeholder's meeting with New College of Florida was conducted via Microsoft Teams on Wednesday, January 18, 2023. The purpose of the meeting was to present information for FDOT's Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS) and conduct an open discussion between the study team and New College of Florida.

### [Attendees](#)

#### **Project Team**

FDOT Abra Horne – District Environmental Administrator  
FDOT: Jeff James – District Environmental Manager  
RK&K: Matt Dockins, PE – Project Manager on behalf of FDOT  
VHB: Amy Sirmans, PE – Consultant Project Manager  
VHB: Heather Garcia – Project Team Member  
VHB: Kennedy Sumner-Snell – Project Team Member  
The Valerin Group: Catherine Winter, PE – Public Involvement Coordinator  
AECOM: Lauren Brooks, AICP – Connecting Communities Program Manager on behalf of FDOT

#### **Stakeholders**

New College of Florida: Itza Frisco – Interim Assistant Vice President, Facilities Management

### [Project Overview](#)

The project team shared a presentation with meeting attendees that provided information on the US 41 CMASS project including project description and background, goals and objectives, schedule, and public outreach. A copy of the presentation is included with these notes.

The project team used a digital whiteboard application (Mural) to capture and document stakeholder ideas and feedback. A copy of the Mural whiteboard with all feedback received to date from the individual stakeholder meetings is included with these meeting notes.

### [Open Discussion](#)

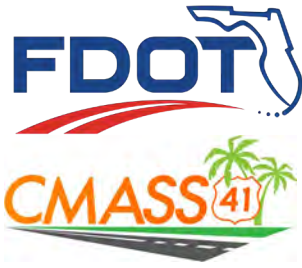
- Stakeholder comments and concerns
  - Enhancement of multimodal mobility and safety for all users is the number one issue
  - Suggested the addition of more bicycle and pedestrian accommodations
  - Biggest complaints concerning traveling the corridor are safety for pedestrian and bicycle traffic, lighting, and traffic speeds.



US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
Stakeholder/Agency Meeting (Virtual)  
New College of Florida

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- Increase in crashes over the past few years at the New College entrance on US 41 and General Spaatz Boulevard/College Drive
- Students use the overpass to avoid crossing the street at the intersection; however, those on bicycles sometimes cross at the intersection as opposed to those traveling on foot
- Some students use buses to get to other locations in Sarasota and Bradenton but stated most use their personal vehicles
- Less than 700 students on campus, all of whom are required to reside on campus and that faculty live in Bradenton or Sarasota with everyone using their cars
- Need for an increased number of bike lanes along US 41 in both directions, both north and south of the campus
- Need for safety considerations at the US 41/General Spaatz intersection
- Congestion between University Parkway and the New College entrance
- Varying speeds of southbound traffic at the New College entrance, who encounter their first traffic signal as they approach General Spaatz Boulevard and College Drive.
- Lighting issues from the New College entrance intersection moving northward
- USF Sarasota-Manatee
  - No accommodation for bicyclists going from New College to USF Sarasota-Manatee campus
  - Difficult to go north and get across the street at USF Sarasota-Manatee
  - Not a high number of students traveling between the campuses however, since both New College and USF are state universities, students should be able to take classes on both campuses
- Heavy congestion on northbound US 41 during peak hours accommodations.
- Most of the currently planned New College improvements/projects were on campus
- Events
  - Upcoming Board of Trustees meeting that will create a lot of traffic on campus but that would not attract a lot of people from outside
  - Occasional weddings on campus that do not typically generate a great deal of traffic
  - Upcoming challenge event focused on climate change that was expected to bring 50-60 people.
- Connecting Communities Questionnaire distribution
  - Offered to utilize their newsletter to help in distributing information to students and staff
  - Posters might also be helpful
  - FDOT will provide New College with posters and flyers, as well as FDOT social media posts
- Continued stakeholder involvement
  - Would like to be added to the mailing list
  - New College will identify others who should be involved in future meetings.



US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
Stakeholder/Agency Meeting (Virtual)  
New College of Florida

---

#### Attendees Email Addresses

Itza Frisco, [ifrisco@ncf.edu](mailto:ifrisco@ncf.edu)  
Abra Horne, [abra.horne@dot.state.fl.us](mailto:abra.horne@dot.state.fl.us)  
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Matt Dockins, PE, [mdockins@hntb.com](mailto:mdockins@hntb.com)  
Amy Sirmans, PE, [asirmans@vhb.com](mailto:asirmans@vhb.com)  
Heather Garcia, [hgarcia@vhb.com](mailto:hgarcia@vhb.com)  
Kennedy Sumner-Snell, [ksumner-snell@vhb.com](mailto:ksumner-snell@vhb.com)  
Lauren Brooks, AICP, [lauren.brooks@aecom.com](mailto:lauren.brooks@aecom.com)  
Catherine Winter, PE, [catherine@valerin-group.com](mailto:catherine@valerin-group.com)



# District One Environmental Management Office

## *Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS)*

*From University Parkway to 17<sup>th</sup> Street W*

January 2023



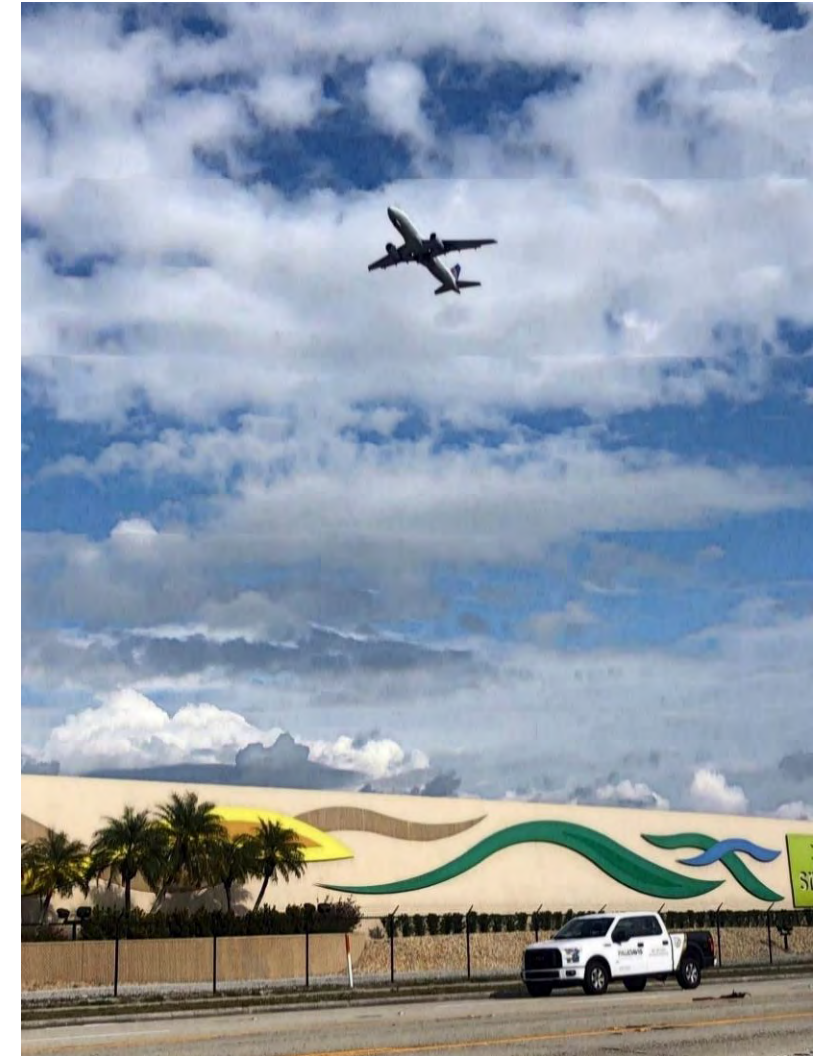
# New College of Florida

January 18, 2023



# Agenda

- Introductions
- Project Description and Background
- US 41 Today: Existing Conditions
- Public Outreach
- Open Discussion: We Want Your Input





# Introductions



**Abra Horne**  
**Administrator**  
District Environmental  
Management Office  
FDOT  
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**Matt Dockins, P.E.**  
**Project Manager**  
RK&K  
[mdockins@rkk.com](mailto:mdockins@rkk.com)  
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**Jeffrey James**  
**Environmental Manager**  
District Environmental  
Management Office  
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(863) 519-2625

# Introductions



**Lauren Brooks, AICP**  
**Project Manager**  
**Senior Transportation Planner**  
**AECOM**  
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**Amy Sirmans, P.E.**  
**Project Manager**  
**VHB**  
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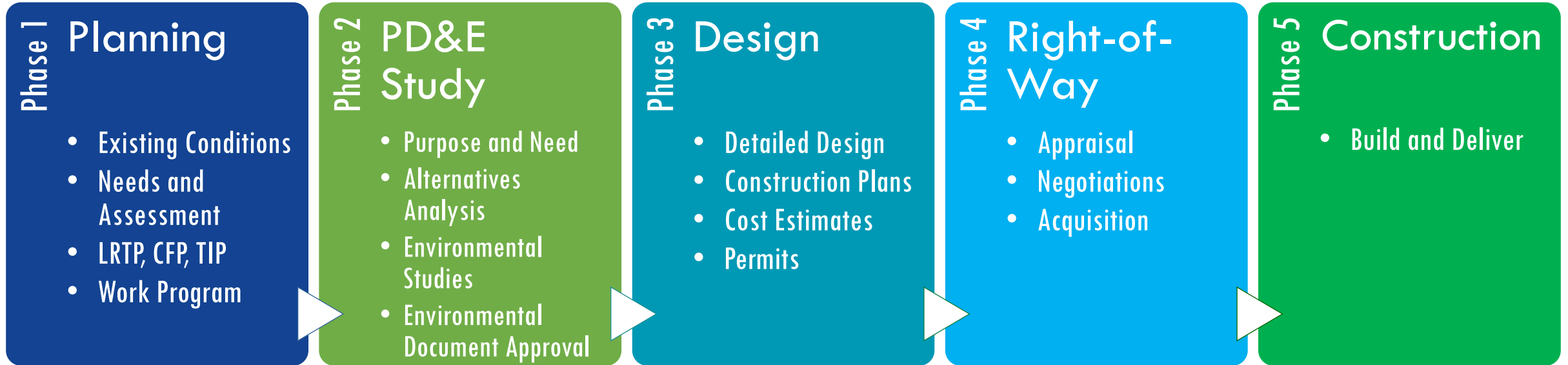


**Catherine Winter, P.E.**  
**Public Involvement Manager**  
**The Valerin Group**  
[Catherine@valerin-group.com](mailto:Catherine@valerin-group.com)  
(813) 431-0919

# Project Description and Background



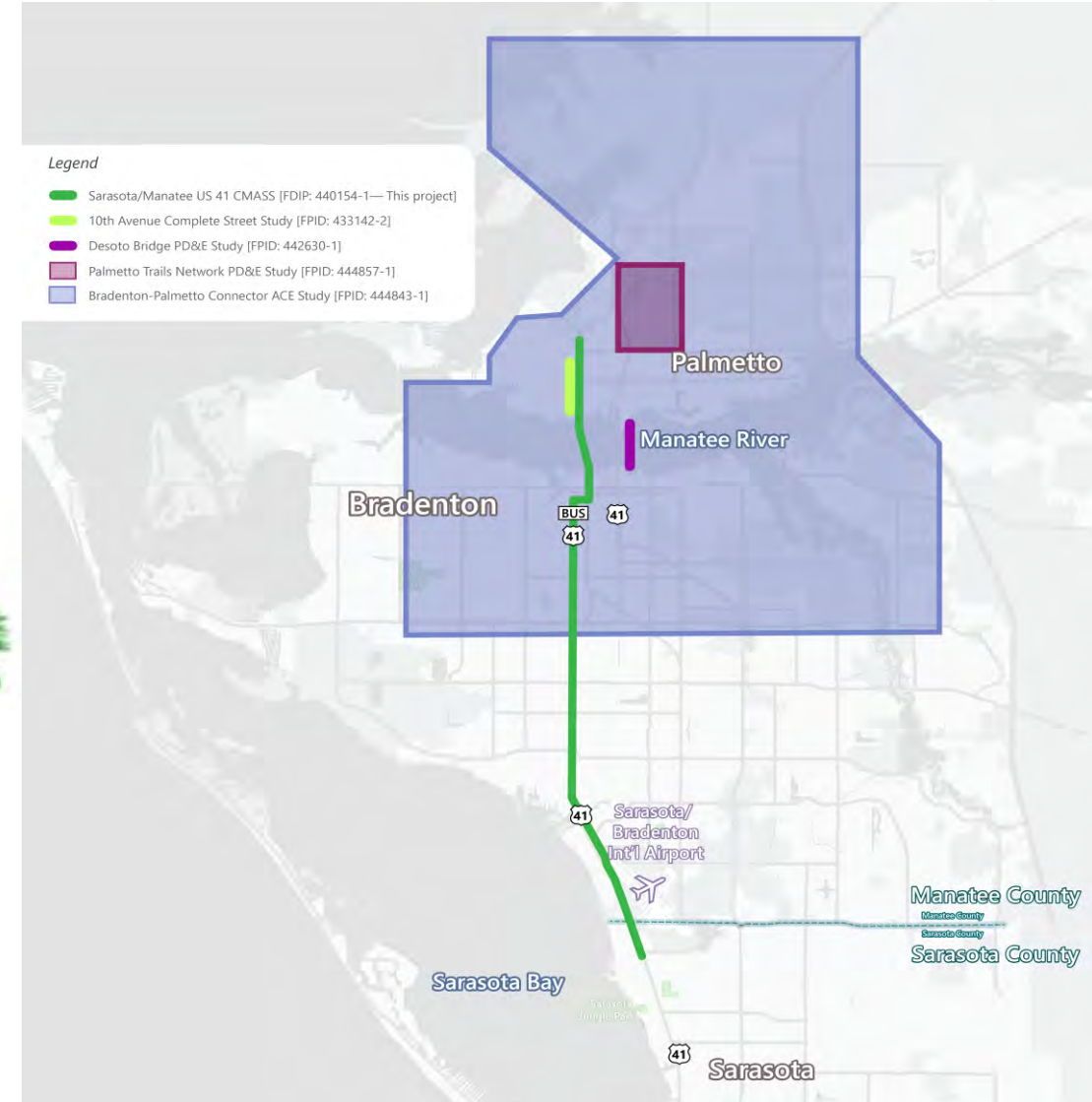
# Transportation Project Development Process



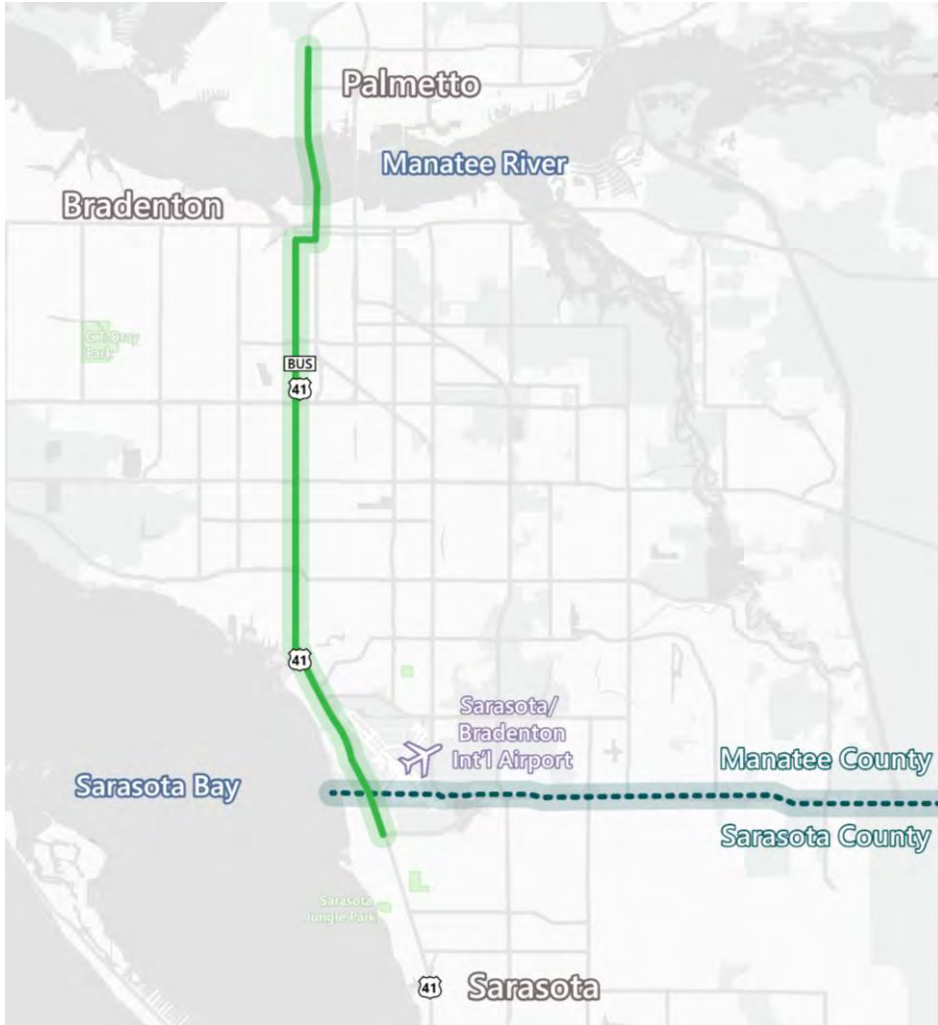
# Connecting Communities

**Purpose:** Holistically enhance the multimodal transportation network within Sarasota and Manatee Counties.

## DeSoto Bridge PD&E Study



# Project Description



**Project Limits:** US 41/US 41 Business corridor from University Parkway (Sarasota) to 17<sup>th</sup> Street W (Palmetto)

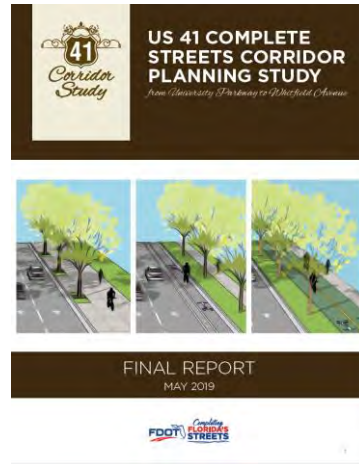
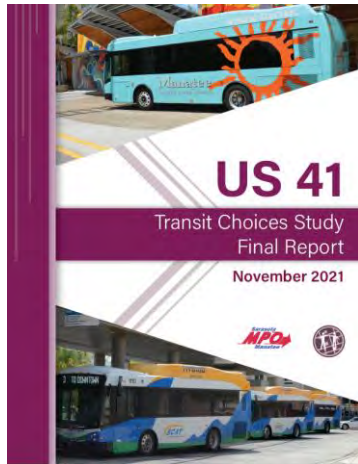
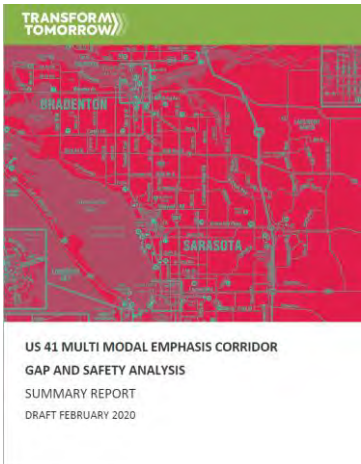
**Financial Project ID:** 440154-1-21-01

**Project Manager:** Matt Dockins

**Status:** Feasibility Study – Phase 1

# We are not starting from scratch...

- US 41 Multi-Modal Emphasis Corridor (MMEC)
- US 41 Transit Choices Study
- US 41 Complete Streets Corridor Planning Study
- Arterial Safety Study
- Access Management Study
- Intersection Safety Studies



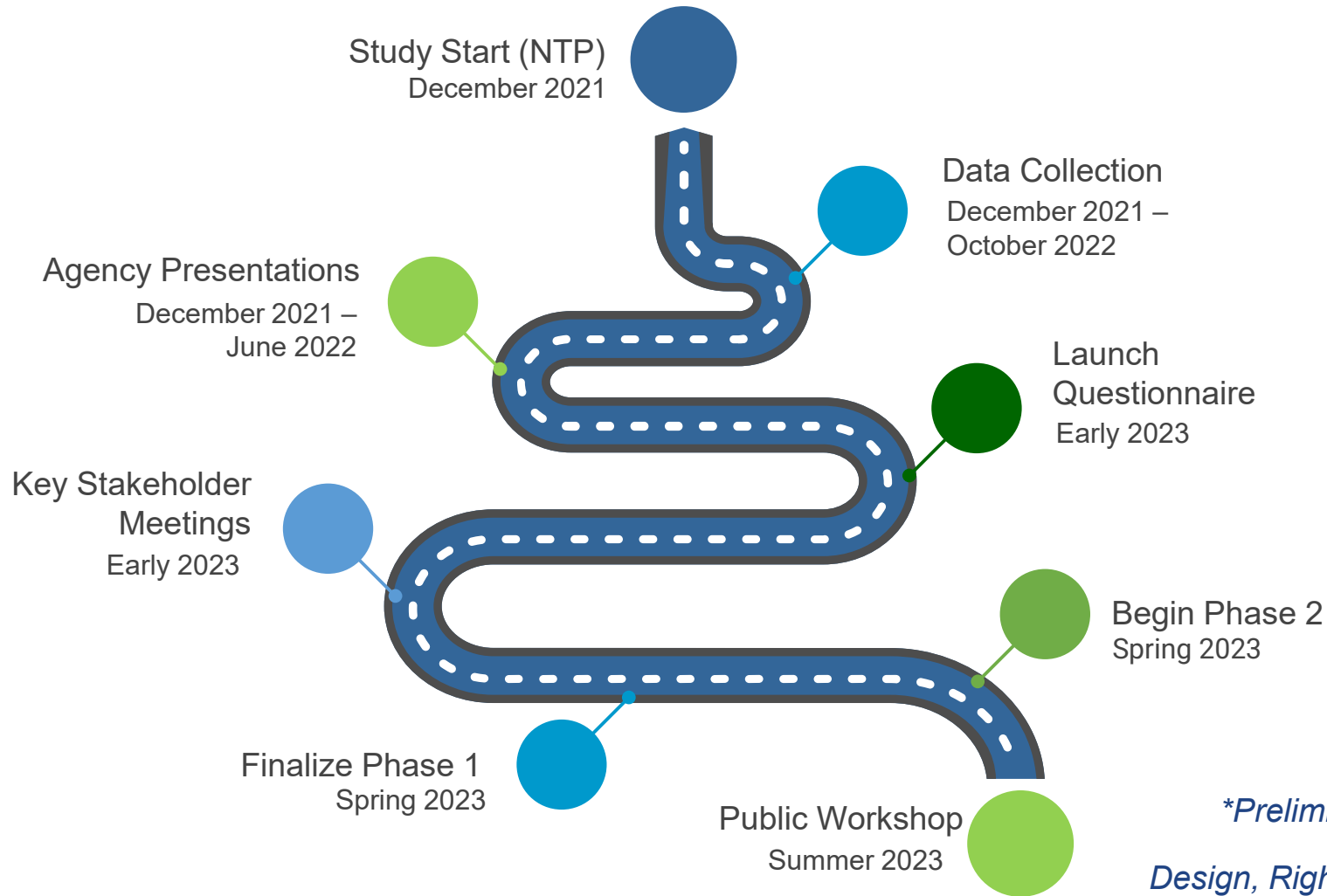
# Goals & Objectives

- Enhance multimodal mobility and safety for all users
- Advance transit service/operation strategies along the study corridor
- Improve transportation network access and interconnectivity within:
  - Neighborhoods
  - Adjacent areas
  - Transit centers





# Project Schedule



*\*Preliminary Schedule – subject to change\**

*Design, Right-of-Way, & Construction are not funded.*

# US 41 Corridor Today



# US 41 Corridor Today

## What are the characteristics of the corridor?

- FDOT priority multimodal corridor
- **2-3 travel lanes in each direction**
- Separated by median, traffic separator, or center left turn lane
- Continuous sidewalks on both sides
- **Right of way varies with more constraints at northern end**
- **Posted speed limits vary from 25 to 45 mph**
- Longest travel time is northbound in evening peak
- Average travel speeds are lower than posted speed limit south of the Green Bridge
- Highest congestion occurs between 63<sup>rd</sup> Avenue and Manatee Avenue

## Who is using the corridor?

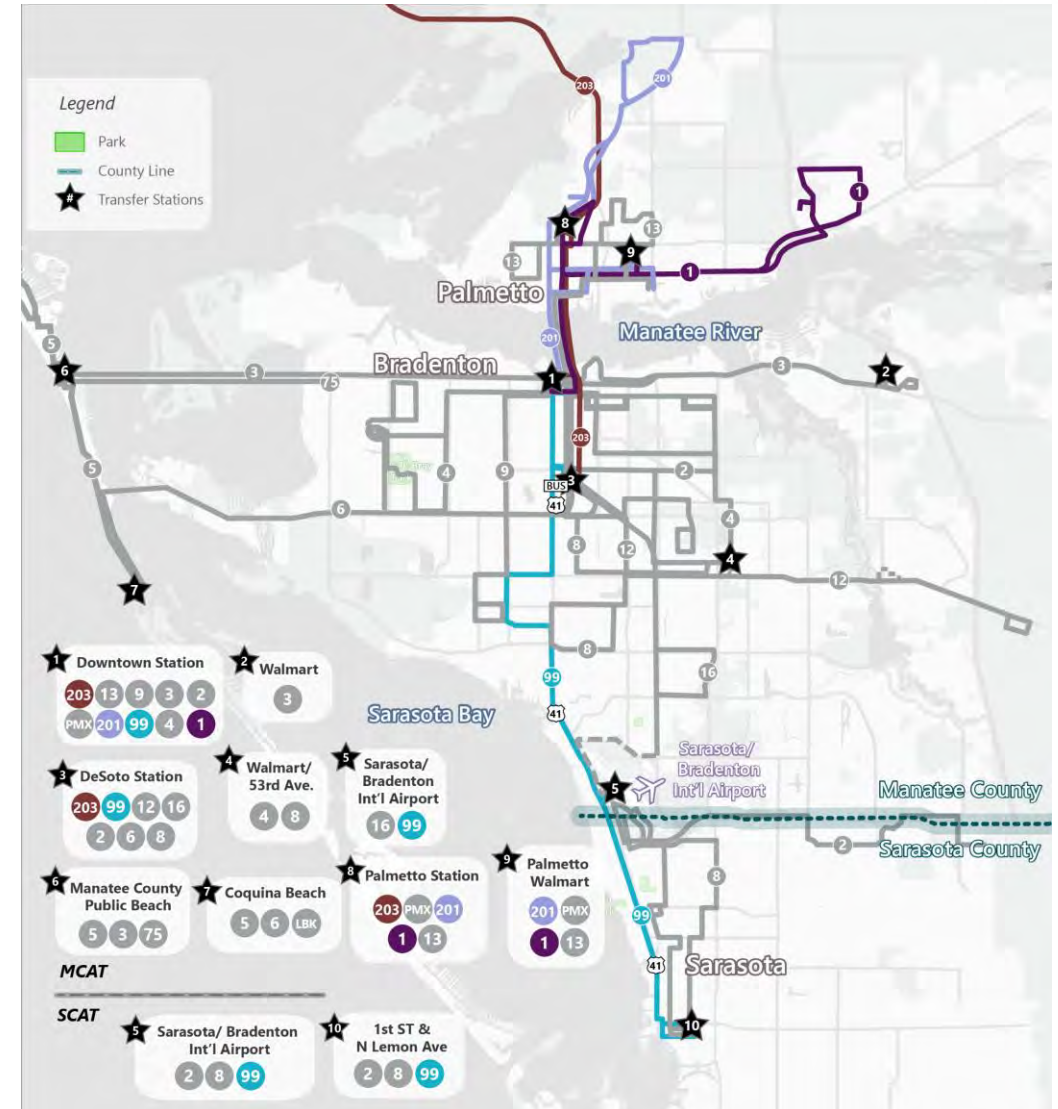
- Residents
- Visitors
- Students
- Businesses

## How are they travelling?

- Single Occupancy Vehicles (cars/trucks, etc.)
- Pedestrians
- Bicyclists
- Several transit (bus) routes

# Area Transit Routes

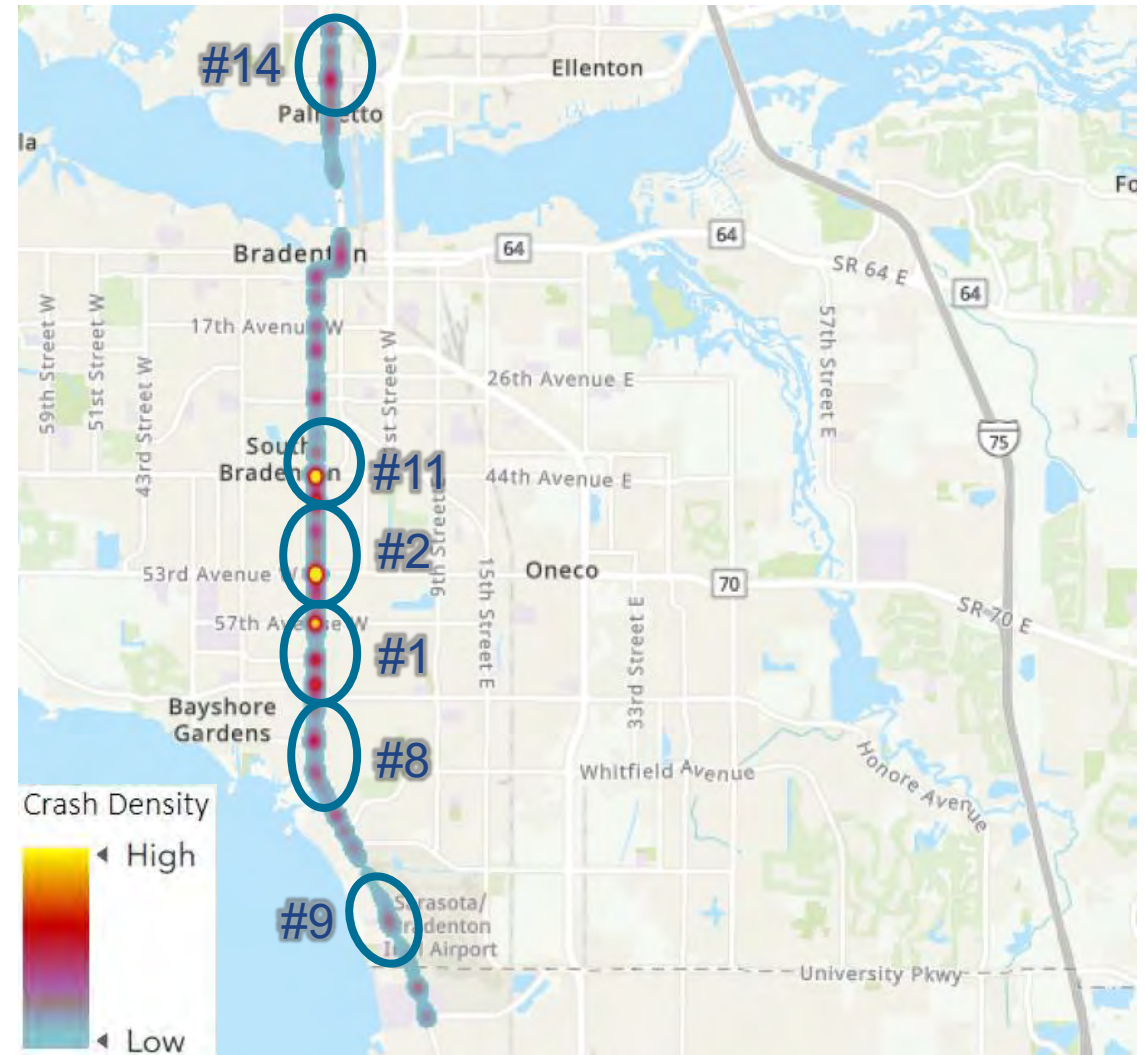
- Route 99 Bradenton-Sarasota
  - Runs length of study area
  - 20-minute AM and PM Peak Period service started Dec 2021
  - New Sunday service proposed starting in 2026
- Stations Near Project
  - Palmetto Station
  - Bradenton Downtown Station
  - DeSoto Station
  - Sarasota-Bradenton International Airport Station



# Safety and Crash Analysis

## Six segments are listed as SW Florida Fatal Top 20 Segments

- 63<sup>rd</sup> Ave W to 55<sup>th</sup> Ave W; Ranked #1
- 55<sup>th</sup> Ave W to Orlando Ave; Ranked #2
- Magellan Dr to 63<sup>rd</sup> Ave W; Ranked #8
- County line to Scott Ave; Ranked #9
- Orlando Ave to 30<sup>th</sup> Ave W; Ranked #11
- 10<sup>th</sup> St W to N of 23<sup>rd</sup> St W; Ranked #14



# Public Outreach

The graphic features a blue background with several circular and rectangular images. At the top, there are five icons representing different modes of transport: a car, a wheelchair, a bus, a person with a cane, and a bicycle. The images include: a 'PUSH BUTTON FOR' sign with a pedestrian icon; a line of cars in traffic; a street view with palm trees and buildings; a green and white bus; a traffic light; and a street sign for 'NORTH BUSINESS 41 TAMiami TRAIL'. The text at the bottom of the graphic reads: 'Sarasota | Manatee U.S. 41 Corridor Mobility and Safety Study Communications Plan', 'September 2022', 'FDOT CMASS 41', and 'FPID #440154-1'. There is also a signature line for 'Approved By: Abra Home, Planning and Environmental Administrator' and a date of '9/2/2022'.

Sarasota | Manatee  
U.S. 41 Corridor Mobility  
and Safety Study  
Communications Plan

September 2022

FDOT CMASS 41

FPID #440154-1

Approved By: Abra Home Date: 9/2/2022  
Abra Home, Planning and Environmental Administrator

# Public Outreach



## Outreach to Date:

### September 2021

- Sarasota/Manatee MPO Staff Meetings

### December 2021

- Sarasota/Manatee MPO Staff Meeting
- Pinellas Suncoast Transit Authority Meeting
- SunRunner BRT Meeting

### February 2022

- Sarasota/Manatee MPO Staff Meeting
- Sarasota/Manatee MPO Board Meeting
- Sarasota/Manatee MPO TAC Meeting

### April 2022

- Sarasota/Manatee MPO BPTAC Meeting

### May 2022

- Sarasota/Manatee MPO CAN Meeting
- Sarasota/Manatee MPO TAC Meeting
- Sarasota/Manatee MPO Board Meeting

### June 2022

- City of Bradenton City Council Meeting
- City of Palmetto City Council Meeting
- Manatee County Commission Meeting

## Upcoming Outreach:

### Early 2023

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire

### Summer 2023

- Hold Public Workshop - Summer 2023

### Throughout 2023

- Continued coordination with all agencies and stakeholders

# We Want to Hear From YOU!

Complete the questionnaire:

[swfroads.com/connectingcommunities/](http://swfroads.com/connectingcommunities/)



<http://www.swfroads.com/project/440154-1>

A flyer for the Connecting Communities Program. At the top left is the FDOT logo. At the top right is the 'CONNECTING COMMUNITIES' logo with a stylized 'e' icon and the text 'Manatee | Sarasota Counties'. Below the logo is a date range: 'January 2023 - April 2023'. The main heading is 'WE WANT TO HEAR FROM YOU!'. The text describes the program's goal to enhance the multimodal transportation network in Sarasota and Manatee Counties. It lists major projects: Bradenton-Palmetto Connector, Desoto Bridge, Sarasota/Manatee US 41 Corridor Mobility and Safety Study, Palmetto Trails Network, and 10th Avenue Complete Street. A URL is provided: [www.swfroads.com/connectingcommunities](http://www.swfroads.com/connectingcommunities). The central graphic features a collage of images with circular callouts: 'Complete Streets' (showing a city street), 'Safety Improvements' (showing a road with a guardrail), 'Corridor Visioning' (showing a bus), 'Bridge Upgrades' (showing a bridge), and 'Multi-use Trails' (showing a path). A 'NORTH BUSINESS 41 TAMAMI TRAIL' road sign is also visible. At the bottom, there is a 'Get involved today!' section with a QR code and text: 'Scan to take our online questionnaire. Complete our brief online questionnaire to provide valuable feedback for enhancing the multimodal transportation network.' Below this is a call to action: 'For more information on the Connecting Communities Program, visit [www.swfroads.com/connectingcommunities](http://www.swfroads.com/connectingcommunities).' The FDOT logo is in the bottom right corner.





# Next Steps

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire
- Finalize Goals & Objectives and Measures of Effectiveness
- Finalize Corridor/Neighborhood Strategies
- Finalize Phase 1 – Spring 2023
- Hold Public Workshop - Summer 2023
- Begin Phase 2: Develop Concepts for Implementation

**Complete the questionnaire:**

[swfroads.com/connectingcommunities/](http://swfroads.com/connectingcommunities/)





# Safety, Questions and Contact Information

**Matt Dockins, PE**  
FDOT Project Manager  
FDOT District 1  
[info@us41cmass.com](mailto:info@us41cmass.com)

**Amy Sirmans, PE**  
Consultant Project Manager  
VHB  
[asirmans@vhb.com](mailto:asirmans@vhb.com)



**PEDESTRIANS  
AND BICYCLISTS**

Nationally, Florida had the **HIGHEST NUMBER OF BICYCLIST FATALITIES** in 2018

**Pedestrians** account for more than **22%** of traffic fatalities in Florida



# STAKEHOLDER INPUT



## Enhance multimodal mobility and safety for all users

Multimodal Mobility & Safety is #1 concern. Increased number of accidents occurring at US41 and General Spaatz Blvd in front of New College, including accidents involving students.

This project will help improve multimodal mobility. Crosswalk improvements/safety for school students would be beneficial

Suggestion to implement pedestrian crossings and enhance accessibility to nearby restaurants/facilities. There are currently no crosswalks on US41 at USF

Existing conditions include a basic sidewalk. Suggestion for greenway improvements

Concern for students crossing areas near Ringling College and the Airport

Have been discussions about providing safer pedestrian access near the airport

Suggestion to enhance pedestrian safety along DeSoto Bridge

Lack of crosswalks along corridor. Does not allow for recreation or exercise

## Advance transit service/operation strategies along the study corridor

Enhance transit along US 41 - Route 99 is best performing route along corridor

Many students use the pedestrian overpass rather than crossing US41 at the intersection. Bike traffic is more common than pedestrian traffic

Keep transit stops away from Runway 4 to enhance safety for pedestrians

Keeping the median opening at Dolphin would be beneficial for future development

Adding bus stops along corridor to benefit local community

Chamber focusing on cost effective methods for transportation including buses, on demand transit, and transit accessibility

Pedestrian/Scoter/Bike traffic around colleges. May not be necessary for a dedicated SCAT lane due to low ridership/trip

USF provides free transit (MCAT and SCAT) for students on campus. Bus stops are not on campus, but adjacent.

Having a stop at USF campus would provide enhanced accessibility for students

Congestion near universities does not allow for safe or efficient traffic flow

Transportation is barrier for employment. Enhanced transportation will benefit the community and employers

Transit timing is inefficient and poorly designed for workforce

## Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

Suggestion to enhance access to activity center located at US41 and 44th Ave W to benefit local community

Suggestion to make transit more easily accessible by adjacent neighborhoods

Area near Seagate Drive (Property 6) to be developed. Would benefit from enhanced pedestrian facilities. Will be developed as as parking lot for interim use

Not many businesses/restaurants surrounding New College. Students/Faculty rely on cars as main form of transportation. (~700 students residing on campus.)

Chamber considering best way to provide accessible transit for all

Future developments to increase campus activity, enhancing the opportunity for transit use.

Some students will use transit to get to other areas in Sarasota and Bradenton, however most use their own vehicle.

Consideration for businesses would be appreciated as they have been recently affected

Route to nearby businesses is not safe for pedestrians

Inefficient access to transit. Workforce living in southern Sarasota County travel to employment centers in northern Manatee County.

# MEETINGS TO DATE



- Bradenton Area Convention and Visitors Bureau
- Manasota Black Chamber of Commerce
- Manatee County Public Works
- New College of Florida
- Sarasota Manatee Airport Authority
- The Greater Sarasota Chamber of Commerce
- University of South Florida Sarasota-Manatee
- Manatee NAACP
- Bradenton Area Economic Development Corporation

## What improvements would you like to see for the US 41 corridor?

Enhanced lighting near Crosley Estate and USF Campus

Pedestrian facilities (Continous bike/ped) throughout the corridor

Consistency in roadway and surrounding facilities (aesthetics). The roadway and surrounding facilities are very different.

Opportunity for more bicycle lanes along US41 in both directions (N/S)

Enhanced safety at the intersection of US41 and General Spaatz Blvd

Lighting near runway zones is something to be considered. May need approval from FAA

Good wayfinding signs on US41 for Airport - beneficial to incorporate style/colors into signage

Bike/Ped connections between Sarasota and Manatee County

Coordination of red lights along the corridor

Aesthetic improvements along corridor

Continous sidewalks/pedestrian lighting throughout corridor to enhance safety

Adding bike lanes throughout the corridor.

Crosswalks along corridor and safety improvements when traveling to nearby businesses

Aesthetic improvements along corridor. Current aesthetics do not reflect the area well

## Are there any specific area of concern you would like to discuss?

Pedestrian activity walking to New College.

Difficulty for pedestrians to access transit stations

Insufficient lighting near New College

Ability to cross the road.

Traffic congestion between University and New College intersection in AM peak time

Varying speeds/high speed travelers at US 41 and New College entrance (especially northbound)

Lack of pedestrian access between New College and USF.

Pedestrians crossing the road in busier areas. Suggestion for a median that would enforce students/pedestrians to use designated crosswalks.

Captain Brian's Seafood has expressed concern for losing driveway access (involving resurfacing project) - could be beneficial to include shared access with Airport

High speeds along the corridor

Concern for homeless population in the area. Would like to see an intentional plan in place to accommodate the homeless population along the corridor

Concern for future stacking issues at Seagate Drive (highest entrance and exit counts here). Potential for increased congestion with coming development. Includes buses as well.

High speeds in the area. Speed control

Upholding integrity for African American-owned churches and businesses along the corridor

Congestion and sustainability of the corridor.

Aesthetics and traffic flow along corridor.

Inconsistent aesthetics when traveling across the Sarasota/Manatee County line. Varying differences between the two counties, crossing University Parkway

## What issues do you notice when traveling the US 41 corridor?

Safety hazards (crashes, frequent stopping, roadway conditions, etc.)

Excessive vehicular speeding

Insufficient lighting

Difficulty entering/exiting properties along the corridor

Quality of sidewalks

Quality of bicycle facilities

Quality of transit stops

Lack of transit reliability

Traffic congestion

Inconsistency throughout the corridor including the examples above and number of lanes.

Turning into or out of USF onto US41 (Safety)

Lack of safety / rate of speed / congestion

Travelers unfamiliar with the area are unaware of surrounding conditions

Increased traffic flow

Lack of access management near Airport

Confusion with traffic flow patterns and turning lanes, especially considering ongoing projects

## What is your biggest complaint about traveling along the US 41 Corridor?

Poor lighting along the corridor for motorized traffic and pedestrians

High speeds near Crosley Estate

High pedestrian fatalities due to dangerous conditions

Lack of lighting and high traffic speeds

Bike/Ped safety along the corridor.

Complaints about the left turn signal at General Spaatz Blvd. Long wait times, congestion, queuing issues

Unsafe conditions along the corridor for pedestrians, especially near colleges

Lack of safety and aesthetics along corridor.

Lack of aesthetics along corridor.

Lack of pedestrian safety, especially near colleges

**Does your agency/organization have any planned improvements / projects / events that the project team should know about?**

Frequent Trolley Services from Crosley Estate to nearby for approximately 200 events per year.

Similar transportation methods at Ringling and New College.

Use of Trolley Service encouraged

Frequent events on campus, but most impacts are within the campus

Manatee County is working on Mobility Plan. The County will increase focus on capital project needs and prioritizations

Manatee County working on Trails Master Plan

Few events a year that may impact traffic. Winter events keep University traffic steady

Ground Transportation Center project to begin construction in coming months. Intended to improve MCAT and SCAT drop-off and pick-up. Larger area will accommodate 4 buses rather than 2.

Working with Manatee County to remove Ponce De Leon St once the land becomes the Airport's property

On-campus housing plans in development

Future commercial development anticipated in Property 5 and 6 near airport

Several developers interested in converting motels along the corridor into affordable housing, etc., which would provide a need for enhanced transit services

Future plan to construct nursing program will increase attendance

Annual Golf Tournament & Fall event partnered with Manatee Convention Center with 200+ in attendance

Juneteenth Event with local library

Planned improvements near airport and colleges are anticipated to bring growth in the next 10 years.

**How would you like to continue to be involved in the project?**

Meet with me again

Join Mailing List

Become a project champion

Additional meeting with New College.

Update meeting with Airport Authority

Join mailing list. Hold update meeting with governmental issues council (when appropriate). Can participate as a project champion

Continued meetings to keep up to date on the planning process. Mailing list updates.

Continued meetings to be updated on project process

Continued meetings and engagement with the African American Community

Project updates and meetings when appropriate

**Are there others from your organization that should be included?**

Add names and contact info

Joe Hembree - Commercial Realtor on Governmental Issues Board

James Golden - Legal Rep (NAACP)/ Manatee County Resident & Economic Development Chair.

Sororities and Fraternities - ASHAL Group and local churches

Potential to share with Bradenton Area EDC Board

**Can you help us distribute the questionnaire / project information? What methods?**

Newsletters

Mailing List

In-person flyers posted throughout New College campus

Post flyers throughout airport. Can ask IT to post to website.

Include in weekly and monthly newsletter

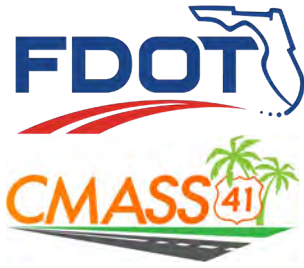
Town Hall Meetings, pass out flyers, email/ billboards

Will distribute to contact list.

Email blast and in-person depending on scheduled events

Electronic distribution

Suggestion for project team to look to local churches and childcare centers for distribution in addition to creating a spanish and creole version of the questionnaire.



US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
**Stakeholder/Agency Meeting (Virtual)**  
Greater Sarasota Chamber of Commerce

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## **Virtual Meeting via Microsoft Teams**

January 20, 2023, 9:00 a.m.

### [Meeting Summary](#)

A virtual stakeholder's meeting with the Greater Sarasota Chamber of Commerce was conducted via Microsoft Teams on Friday, January 20, 2023. The purpose of the meeting was to present information for FDOT's Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS) and conduct an open discussion between the study team and the Greater Sarasota Chamber of Commerce.

### [Attendees](#)

#### **Project Team**

FDOT Abra Horne – District Environmental Administrator  
FDOT: Jeff James – District Environmental Manager  
RK&K: Matt Dockins, PE – Project Manager on behalf of FDOT  
VHB: Amy Sirmans, PE – Consultant Project Manager  
VHB: Heather Garcia – Project Team Member  
VHB: Kennedy Sumner-Snell – Project Team Member  
The Valerin Group: Catherine Winter, PE – Public Involvement Coordinator  
AECOM: Lauren Brooks, AICP – Connecting Communities Program Manager on behalf of FDOT

#### **Stakeholders**

Greater Sarasota Chamber of Commerce: Heather Kasten, President

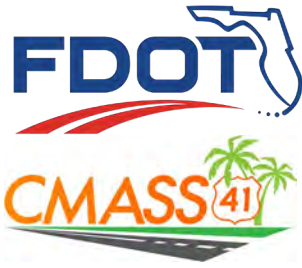
### [Project Overview](#)

The project team shared a presentation with meeting attendees that provided information on the US 41 CMASS project including project description and background, goals and objectives, schedule, and public outreach. A copy of the presentation is included with these notes.

The project team used a digital whiteboard application (Mural) to capture and document stakeholder ideas and feedback. A copy of the Mural whiteboard with all feedback received to date from the individual stakeholder meetings is included with these meeting notes.

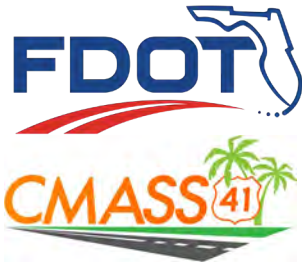
### [Open Discussion](#)

- Stakeholder comments and concerns
  - Supports this important project
  - Agreement with many of the comments reflected on the Mural application from those who had participated in previous stakeholder meetings - enhanced multimodal mobility as well as safety for students crossing US 41 near the Ringling, the colleges, and the airport
  - Large concentration of pedestrian, scooter, and bike traffic near the colleges



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- 
- Transit
    - Many people utilize SCAT's OnDemand ride service
    - Questioned whether dedicating an entire lane for a bus was warranted
    - The Chamber's and Governmental Issues Council have been focused on determining the most cost-effective way to transport people – whether smaller minivans providing on-demand services or million-dollar buses that might be transporting only a few passengers
    - Distance to bus stops, which, could be several miles is an issue
    - Hubs should also be considered - could be beneficial in workplace and cost-efficient living areas
  - Need for aesthetic consistency - the corridor features ranges of tight streets with very narrow sidewalks, to a multiple lane “speedway” near the airport and colleges, and then transitions back down to two lanes
  - Need for aesthetic improvements near US 41 at University Parkway - an “entryway” into the Sarasota/Manatee region
  - Concern relating to traffic speeds
  - Pedestrian safety is an issue - add medians near the colleges to may prevent students from crossing the street (something similar has been implemented near Florida State University in Tallahassee)
  - Several developers were looking at the older motels along the corridor and exploring the possible conversion of those hotels to affordable/attainable housing. If the redevelopment occurs, there would be a need for enhanced transit services and connectivity
  - Support this important project
  - Connecting Communities Questionnaire distribution
    - Will push information about the questionnaire out to their constituents – comprised of about 6,000 business professionals - in their monthly newsletters
    - FDOT will provide a copy of the flyer
  - Continued stakeholder involvement
    - Would like to be added to the mailing list
    - Requested that someone from the US 41 CMASS team provide updates to the Chamber's Governmental Issues Council and/or to the Chamber board.
    - Offered the Chamber's support within the community, serving as a project champion.
    - Joe C. Hembree (a commercial realtor and the head of the Governmental Issues Council) recommended as another point of contact. Heather can make an email introduction to connect him with the project team.



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#### Attendees Email Addresses

Heather Kasten, [hkasten@sarasotachamber.com](mailto:hkasten@sarasotachamber.com)  
Abra Horne, [abra.horne@dot.state.fl.us](mailto:abra.horne@dot.state.fl.us)  
Jeff James, [jeffrey.james@dot.state.fl.us](mailto:jeffrey.james@dot.state.fl.us)  
Matt Dockins, PE, [mdockins@hntb.com](mailto:mdockins@hntb.com)  
Amy Sirmans, PE, [asirmans@vhb.com](mailto:asirmans@vhb.com)  
Heather Garcia, [hgarcia@vhb.com](mailto:hgarcia@vhb.com)  
Kennedy Sumner-Snell, [ksumner-snell@vhb.com](mailto:ksumner-snell@vhb.com)  
Lauren Brooks, AICP, [lauren.brooks@aecom.com](mailto:lauren.brooks@aecom.com)  
Catherine Winter, PE, [catherine@valerin-group.com](mailto:catherine@valerin-group.com)





# District One Environmental Management Office

## *Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS)*

*From University Parkway to 17<sup>th</sup> Street W*

January 2023



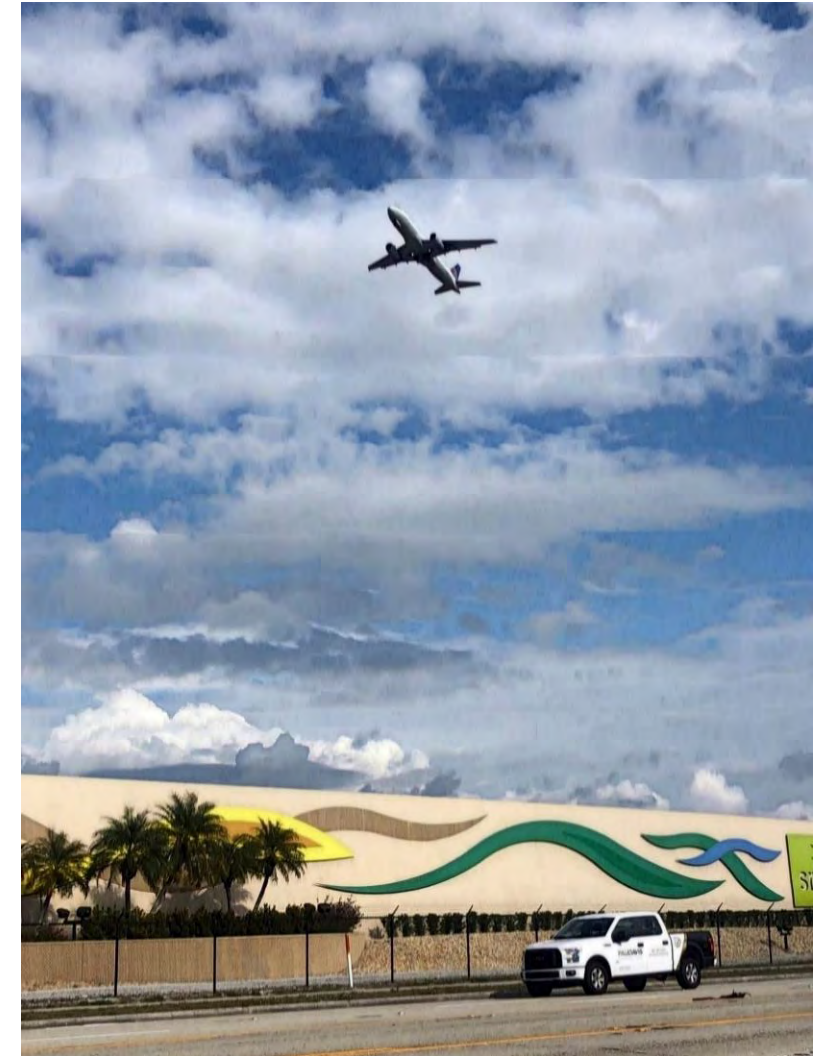
# Greater Sarasota Chamber of Commerce

January 20, 2023



# Agenda

- Introductions
- Project Description and Background
- US 41 Today: Existing Conditions
- Public Outreach
- Open Discussion: We Want Your Input



# Introductions



**Abra Horne**  
**Administrator**  
District Environmental  
Management Office  
FDOT  
[Abra.Horne@dot.state.fl.us](mailto:Abra.Horne@dot.state.fl.us)  
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**Matt Dockins, P.E.**  
**Project Manager**  
RK&K  
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**Jeffrey James**  
**Environmental Manager**  
District Environmental  
Management Office  
FDOT  
[Jeffrey.James@dot.state.fl.us](mailto:Jeffrey.James@dot.state.fl.us)  
(863) 519-2625

# Introductions



**Lauren Brooks, AICP**  
**Project Manager**  
**Senior Transportation Planner**  
**AECOM**  
[lauren.brooks@aecom.com](mailto:lauren.brooks@aecom.com)  
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**Amy Sirmans, P.E.**  
**Project Manager**  
**VHB**  
[asirmans@vhb.com](mailto:asirmans@vhb.com)  
(407) 839-4006

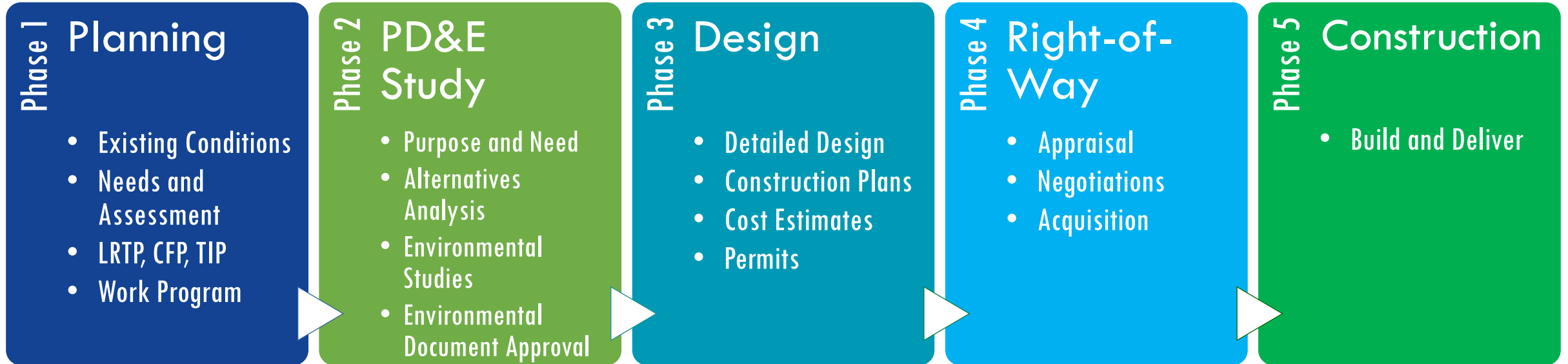


**Catherine Winter, P.E.**  
**Public Involvement Manager**  
**The Valerin Group**  
[Catherine@valerin-group.com](mailto:Catherine@valerin-group.com)  
(813) 431-0919

# Project Description and Background



# Transportation Project Development Process

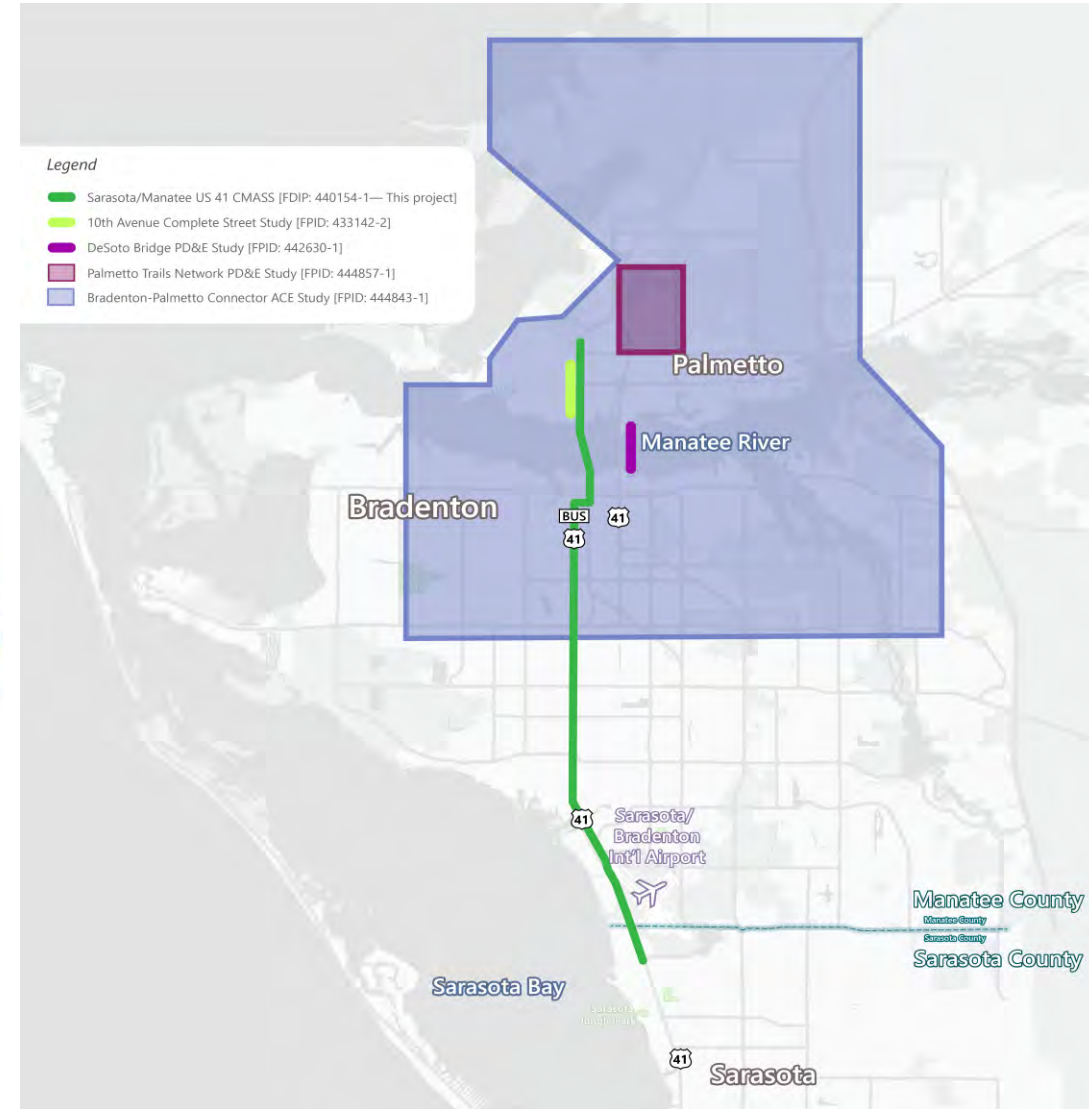


# Connecting Communities

**Purpose:** Holistically enhance the multimodal transportation network within Sarasota and Manatee Counties.

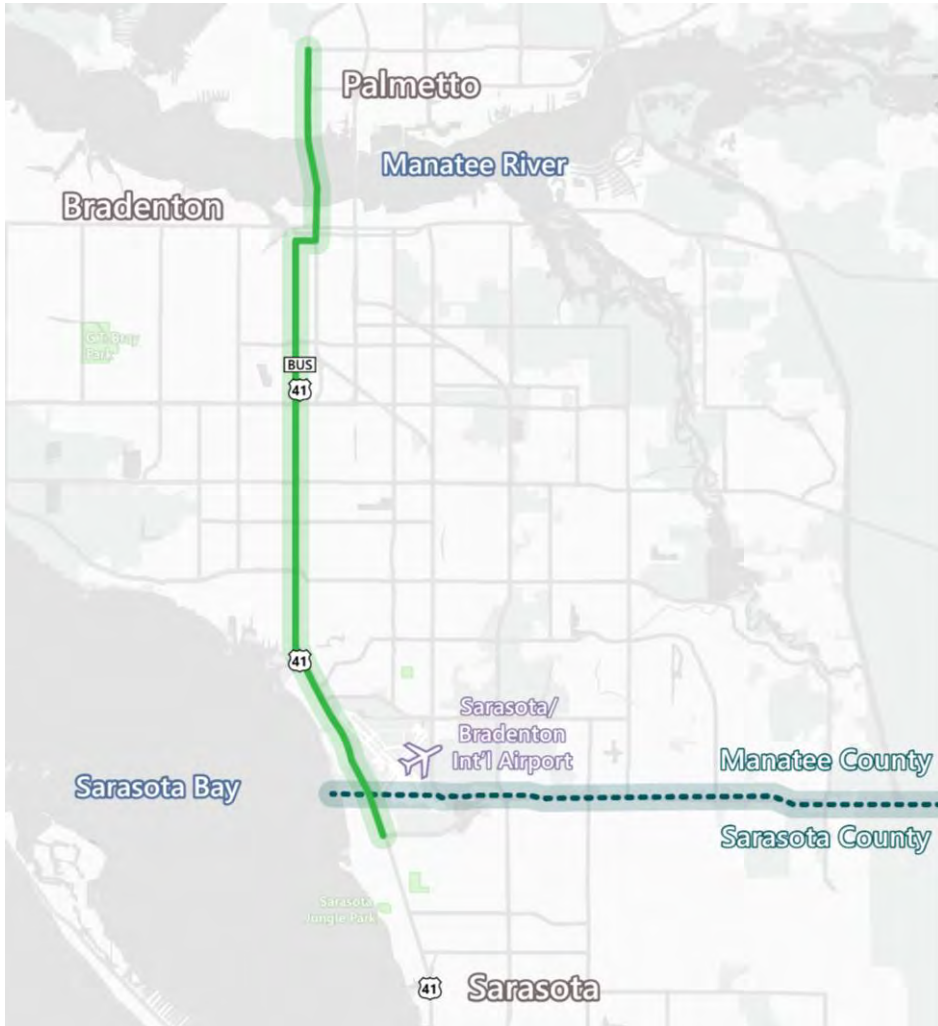


## DeSoto Bridge PD&E Study





# Project Description



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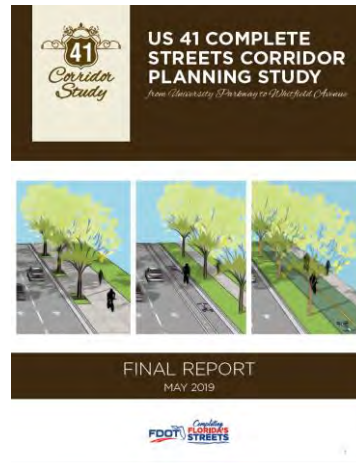
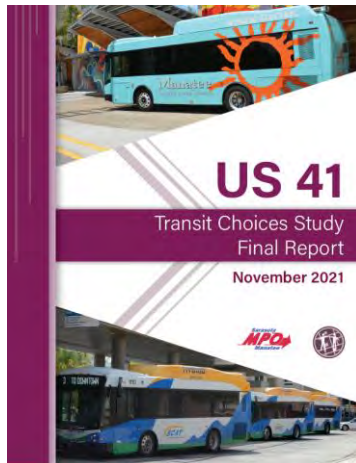
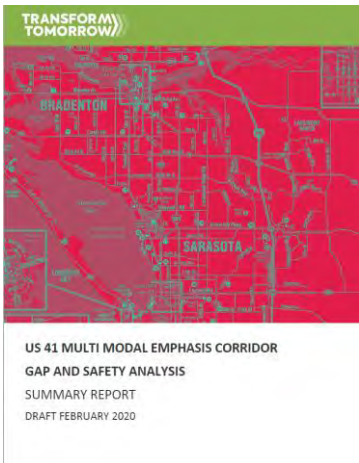
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**Project Manager:** Matt Dockins

**Status:** Feasibility Study – Phase 1

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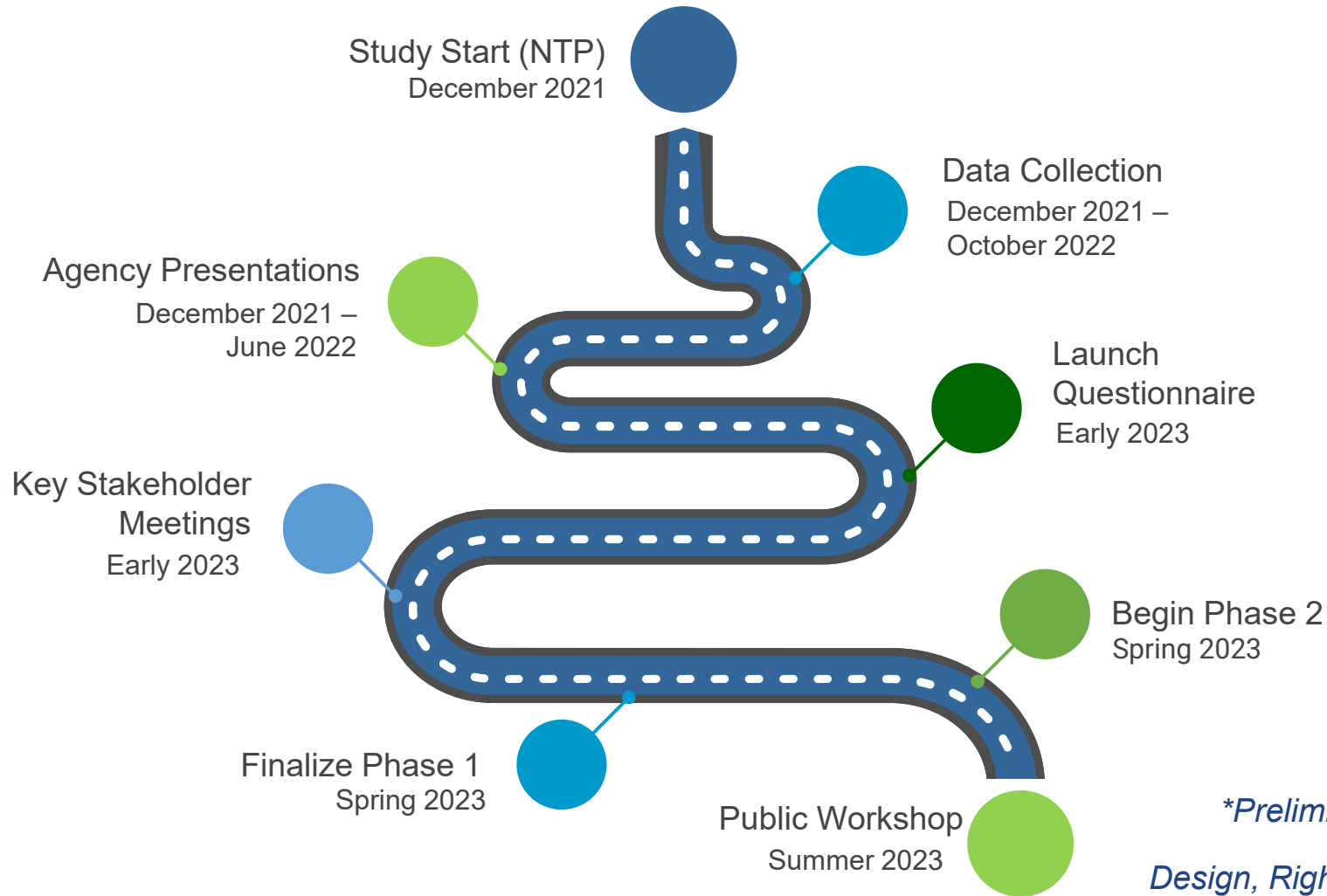


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# Project Schedule



*\*Preliminary Schedule – subject to change\**  
*Design, Right-of-Way, & Construction are not funded.*

# US 41 Corridor Today



# US 41 Corridor Today

## What are the characteristics of the corridor?

- FDOT priority multimodal corridor
- **2-3 travel lanes in each direction**
- Separated by median, traffic separator, or center left turn lane
- Continuous sidewalks on both sides
- **Right of way varies with more constraints at northern end**
- **Posted speed limits vary from 25 to 45 mph**
- Longest travel time is northbound in evening peak
- Average travel speeds are lower than posted speed limit south of the Green Bridge
- Highest congestion occurs between 63<sup>rd</sup> Avenue and Manatee Avenue

## Who is using the corridor?

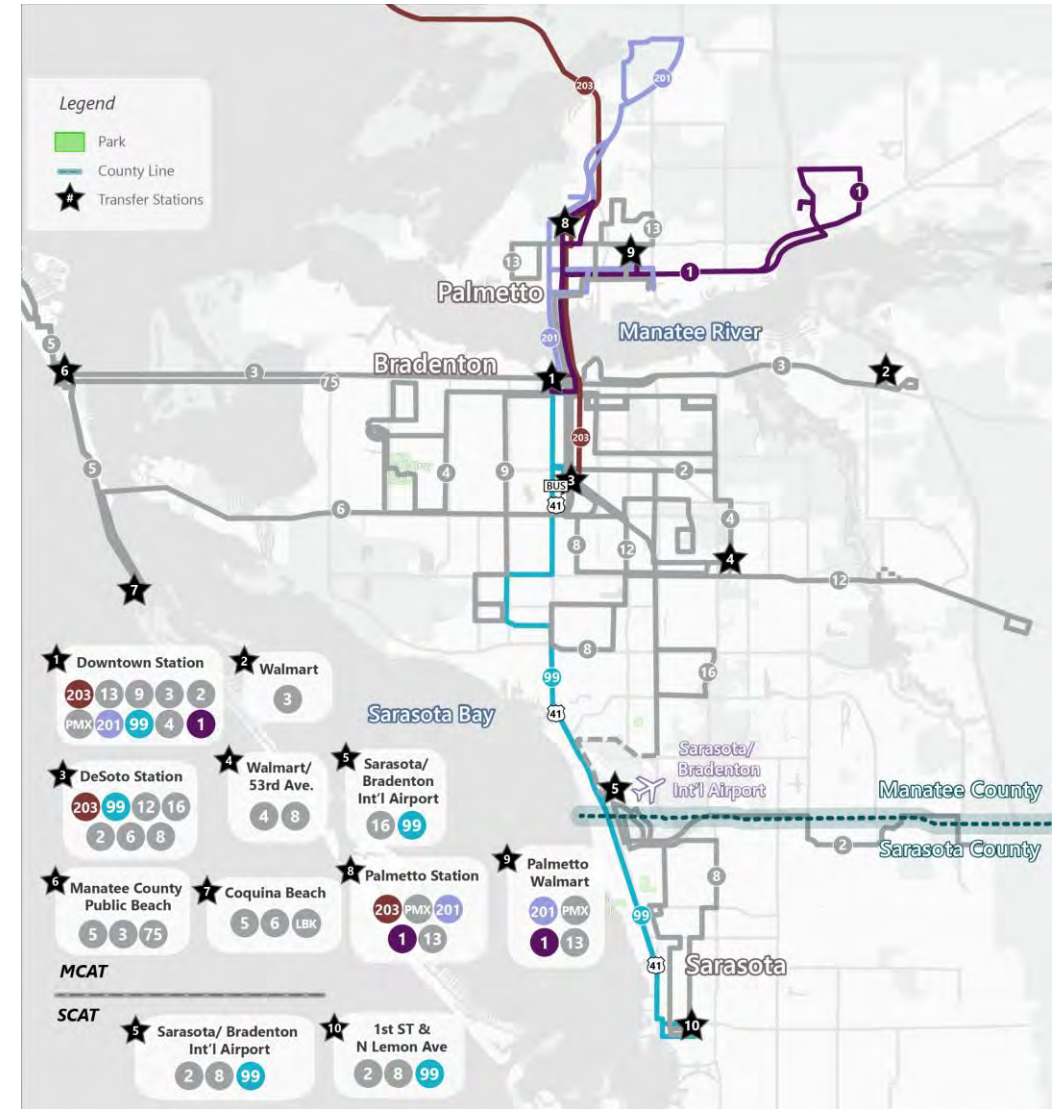
- Residents
- Visitors
- Students
- Businesses

## How are they travelling?

- Single Occupancy Vehicles (cars/trucks, etc.)
- Pedestrians
- Bicyclists
- Several transit (bus) routes

# Area Transit Routes

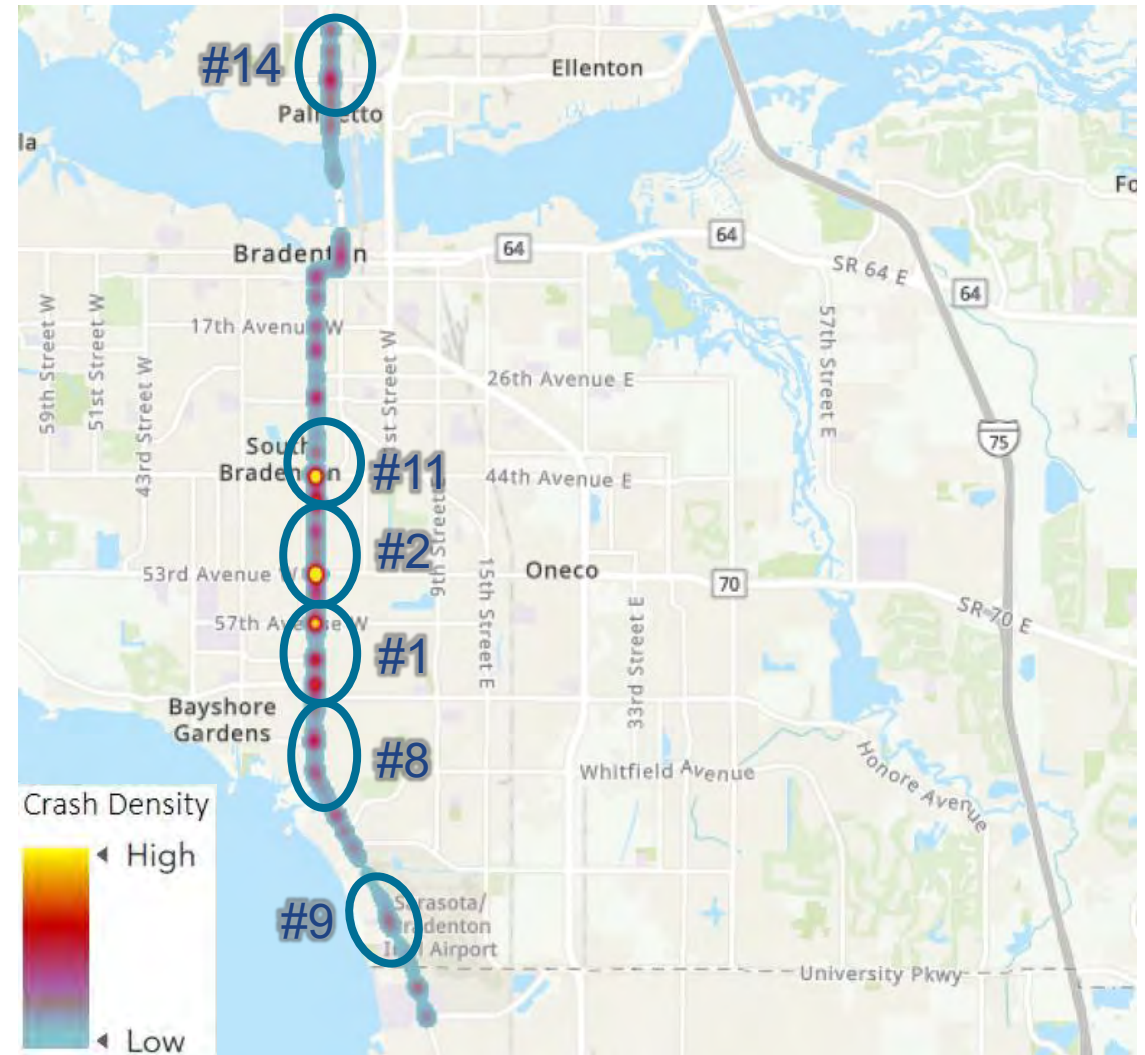
- Route 99 Bradenton-Sarasota
  - Runs length of study area
  - 20-minute AM and PM Peak Period service started Dec 2021
  - New Sunday service proposed starting in 2026
- Stations Near Project
  - Palmetto Station
  - Bradenton Downtown Station
  - DeSoto Station
  - Sarasota-Bradenton International Airport Station



# Safety and Crash Analysis

## Six segments are listed as SW Florida Fatal Top 20 Segments

- 63<sup>rd</sup> Ave W to 55<sup>th</sup> Ave W; Ranked #1
- 55<sup>th</sup> Ave W to Orlando Ave; Ranked #2
- Magellan Dr to 63<sup>rd</sup> Ave W; Ranked #8
- County line to Scott Ave; Ranked #9
- Orlando Ave to 30<sup>th</sup> Ave W; Ranked #11
- 10<sup>th</sup> St W to N of 23<sup>rd</sup> St W; Ranked #14





# Public Outreach

The graphic features a blue background with several circular and rectangular images. At the top, there are five icons: a car, a person with a cane, a bus, a person with a stroller, and a bicycle. The images include: a 'PUSH BUTTON FOR' sign with a pedestrian icon, a line of cars in traffic, a street view with palm trees and buildings, a green and white bus, and a traffic light. The text on the graphic reads: 'Sarasota | Manatee U.S. 41 Corridor Mobility and Safety Study Communications Plan'. It also includes the logos for 'FDOT' and 'CMASS 41', the date 'September 2022', and the project ID 'FPID #440154-1'. At the bottom, there is a signature line: 'Approved By: Abra Horne Date: 9/2/2022' and the title 'Abra Horne, Planning and Environmental Administrator'.

# Public Outreach



## Outreach to Date:

### September 2021

- Sarasota/Manatee MPO Staff Meetings

### December 2021

- Sarasota/Manatee MPO Staff Meeting
- Pinellas Suncoast Transit Authority Meeting
- SunRunner BRT Meeting

### February 2022

- Sarasota/Manatee MPO Staff Meeting
- Sarasota/Manatee MPO Board Meeting
- Sarasota/Manatee MPO TAC Meeting

### April 2022

- Sarasota/Manatee MPO BPTAC Meeting

### May 2022

- Sarasota/Manatee MPO CAN Meeting
- Sarasota/Manatee MPO TAC Meeting
- Sarasota/Manatee MPO Board Meeting

### June 2022

- City of Bradenton City Council Meeting
- City of Palmetto City Council Meeting
- Manatee County Commission Meeting

## Upcoming Outreach:

### Early 2023

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire

### Summer 2023

- Hold Public Workshop - Summer 2023

### Throughout 2023

- Continued coordination with all agencies and stakeholders

# We Want to Hear From YOU!

Complete the questionnaire:

[swfroads.com/connectingcommunities/](http://swfroads.com/connectingcommunities/)



<http://www.swfroads.com/project/440154-1>

**FDOT** **CONNECTING COMMUNITIES**  
Manatee | Sarasota Counties

January 2023 - April 2023

### WE WANT TO HEAR FROM YOU!

The Florida Department of Transportation, District One, is conducting several projects in partnership with the Sarasota/Manatee Metropolitan Planning Organization (MPO) and local governments to holistically enhance the multimodal transportation network within Sarasota and Manatee Counties as part of the Connecting Communities Program. Major Connecting Communities Program projects include Bradenton-Palmetto Connector, Desoto Bridge, Sarasota/Manatee US 41 Corridor Mobility and Safety Study, Palmetto Trails Network, and 10th Avenue Complete Street. For a map and full list of current area projects included as part of the Connecting Communities Program visit [www.swfroads.com/connectingcommunities](http://www.swfroads.com/connectingcommunities).

Complete Streets  
Safety Improvements  
Corridor Visioning  
Bridge Upgrades  
Multi-use Trails

**Get involved today!**  
**Scan to take our online questionnaire.**  
Complete our brief online questionnaire to provide valuable feedback for enhancing the multimodal transportation network.

For more information on the Connecting Communities Program, visit [www.swfroads.com/connectingcommunities](http://www.swfroads.com/connectingcommunities).

**FDOT**



# Next Steps

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire
- Finalize Goals & Objectives and Measures of Effectiveness
- Finalize Corridor/Neighborhood Strategies
- Finalize Phase 1 – Spring 2023
- Hold Public Workshop - Summer 2023
- Begin Phase 2: Develop Concepts for Implementation

**Complete the questionnaire:**

[swfroads.com/connectingcommunities/](http://swfroads.com/connectingcommunities/)





# Safety, Questions and Contact Information

**Matt Dockins, PE**  
FDOT Project Manager  
FDOT District 1  
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**Amy Sirmans, PE**  
Consultant Project Manager  
VHB  
[asirmans@vhb.com](mailto:asirmans@vhb.com)



**PEDESTRIANS  
AND BICYCLISTS**

Nationally, Florida had the **HIGHEST NUMBER OF BICYCLIST FATALITIES** in 2018

**Pedestrians** account for more than **22%** of **traffic fatalities** in Florida



# STAKEHOLDER INPUT



## Enhance multimodal mobility and safety for all users

Multimodal Mobility & Safety is #1 concern. Increased number of accidents occurring at US41 and General Spaatz Blvd in front of New College, including accidents involving students.

This project will help improve multimodal mobility. Crosswalk improvements/safety for school students would be beneficial

Suggestion to implement pedestrian crossings and enhance accessibility to nearby restaurants/facilities. There are currently no crosswalks on US41 at USF

Existing conditions include a basic sidewalk. Suggestion for greenway improvements

Concern for students crossing areas near Ringling College and the Airport

Have been discussions about providing safer pedestrian access near the airport

Suggestion to enhance pedestrian safety along DeSoto Bridge

Lack of crosswalks along corridor. Does not allow for recreation or exercise

## Advance transit service/operation strategies along the study corridor

Enhance transit along US 41 - Route 99 is best performing route along corridor

Many students use the pedestrian overpass rather than crossing US41 at the intersection. Bike traffic is more common than pedestrian traffic

Keep transit stops away from Runway 4 to enhance safety for pedestrians

Keeping the median opening at Dolphin would be beneficial for future development

Adding bus stops along corridor to benefit local community

Chamber focusing on cost effective methods for transportation including buses, on demand transit, and transit accessibility

Pedestrian/Scoter/Bike traffic around colleges. May not be necessary for a dedicated SCAT lane due to low ridership/trip

USF provides free transit (MCAT and SCAT) for students on campus. Bus stops are not on campus, but adjacent.

Having a stop at USF campus would provide enhanced accessibility for students

Congestion near universities does not allow for safe or efficient traffic flow

Transportation is barrier for employment. Enhanced transportation will benefit the community and employers

Transit timing is inefficient and poorly designed for workforce

## Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

Suggestion to enhance access to activity center located at US41 and 44th Ave W to benefit local community

Suggestion to make transit more easily accessible by adjacent neighborhoods

Area near Seagate Drive (Property 6) to be developed. Would benefit from enhanced pedestrian facilities. Will be developed as as parking lot for interim use

Not many businesses/restaurants surrounding New College. Students/Faculty rely on cars as main form of transportation. (~700 students residing on campus.)

Chamber considering best way to provide accessible transit for all

Future developments to increase campus activity, enhancing the opportunity for transit use.

Some students will use transit to get to other areas in Sarasota and Bradenton, however most use their own vehicle.

Consideration for businesses would be appreciated as they have been recently affected

Route to nearby businesses is not safe for pedestrians

Inefficient access to transit. Workforce living in southern Sarasota County travel to employment centers in northern Manatee County.

# MEETINGS TO DATE



- Bradenton Area Convention and Visitors Bureau
- Manasota Black Chamber of Commerce
- Manatee County Public Works
- New College of Florida
- Sarasota Manatee Airport Authority
- The Greater Sarasota Chamber of Commerce
- University of South Florida Sarasota-Manatee
- Manatee NAACP
- Bradenton Area Economic Development Corporation

## What improvements would you like to see for the US 41 corridor?

Enhanced lighting near Crosley Estate and USF Campus

Pedestrian facilities (Continous bike/ped) throughout the corridor

Consistency in roadway and surrounding facilities (aesthetics). The roadway and surrounding facilities are very different.

Opportunity for more bicycle lanes along US41 in both directions (N/S)

Enhanced safety at the intersection of US41 and General Spaatz Blvd

Lighting near runway zones is something to be considered. May need approval from FAA

Good wayfinding signs on US41 for Airport - beneficial to incorporate style/colors into signage

Bike/Ped connections between Sarasota and Manatee County

Coordination of red lights along the corridor

Aesthetic improvements along corridor

Continous sidewalks/pedestrian lighting throughout corridor to enhance safety

Adding bike lanes throughout the corridor.

Crosswalks along corridor and safety improvements when traveling to nearby businesses

Aesthetic improvements along corridor. Current aesthetics do not reflect the area well

## Are there any specific area of concern you would like to discuss?

Pedestrian activity walking to New College.

Difficulty for pedestrians to access transit stations

Insufficient lighting near New College

Ability to cross the road.

Traffic congestion between University and New College intersection in AM peak time

Varying speeds/high speed travelers at US 41 and New College entrance (especially northbound)

Lack of pedestrian access between New College and USF.

Pedestrians crossing the road in busier areas. Suggestion for a median that would enforce students/pedestrians to use designated crosswalks.

Captain Brian's Seafood has expressed concern for losing driveway access (involving resurfacing project) - could be beneficial to include shared access with Airport

High speeds along the corridor

Concern for homeless population in the area. Would like to see an intentional plan in place to accommodate the homeless population along the corridor

Concern for future stacking issues at Seagate Drive (highest entrance and exit counts here). Potential for increased congestion with coming development. Includes buses as well.

High speeds in the area. Speed control

Future increased traffic volume to cause issues along corridor

Upholding integrity for African American-owned churches and businesses along the corridor

Congestion and sustainability of the corridor.

Aesthetics and traffic flow along corridor.

Inconsistent aesthetics when traveling across the Sarasota/Manatee County line. Varying differences between the two counties, crossing University Parkway

## What issues do you notice when traveling the US 41 corridor?

Safety hazards (crashes, frequent stopping, roadway conditions, etc.)

Excessive vehicular speeding

Insufficient lighting

Difficulty entering/exiting properties along the corridor

Quality of sidewalks

Quality of bicycle facilities

Quality of transit stops

Lack of transit reliability

Traffic congestion

Inconsistency throughout the corridor including the examples above and number of lanes.

Turning into or out of USF onto US41 (Safety)

Lack of safety / rate of speed / congestion

Travelers unfamiliar with the area are unaware of surrounding conditions

Increased traffic flow

Lack of access management near Airport

Confusion with traffic flow patterns and turning lanes, especially considering ongoing projects

## What is your biggest complaint about traveling along the US 41 Corridor?

Poor lighting along the corridor for motorized traffic and pedestrians

High speeds near Crosley Estate

High pedestrian fatalities due to dangerous conditions

Lack of lighting and high traffic speeds

Bike/Ped safety along the corridor.

Complaints about the left turn signal at General Spaatz Blvd. Long wait times, congestion, queuing issues

Unsafe conditions along the corridor for pedestrians, especially near colleges

Lack of safety and aesthetics along corridor.

Lack of aesthetics along corridor.

Lack of pedestrian safety, especially near colleges

**Does your agency/organization have any planned improvements / projects / events that the project team should know about?**

Frequent Trolley Services from Crosley Estate to nearby for approximately 200 events per year.

Similar transportation methods at Ringling and New College.

Use of Trolley Service encouraged

Frequent events on campus, but most impacts are within the campus

Manatee County is working on Mobility Plan. The County will increase focus on capital project needs and prioritizations

Manatee County working on Trails Master Plan

Few events a year that may impact traffic. Winter events keep University traffic steady

Ground Transportation Center project to begin construction in coming months. Intended to improve MCAT and SCAT drop-off and pick-up. Larger area will accommodate 4 buses rather than 2.

Working with Manatee County to remove Ponce De Leon St once the land becomes the Airport's property

On-campus housing plans in development

Future commercial development anticipated in Property 5 and 6 near airport

Several developers interested in converting motels along the corridor into affordable housing, etc., which would provide a need for enhanced transit services

Future plan to construct nursing program will increase attendance

Annual Golf Tournament & Fall event partnered with Manatee Convention Center with 200+ in attendance

Juneteenth Event with local library

Planned improvements near airport and colleges are anticipated to bring growth in the next 10 years.

**How would you like to continue to be involved in the project?**

Meet with me again

Join Mailing List

Become a project champion

Additional meeting with New College.

Update meeting with Airport Authority

Join mailing list. Hold update meeting with governmental issues council (when appropriate). Can participate as a project champion

Continued meetings to keep up to date on the planning process. Mailing list updates.

Continued meetings to be updated on project process

Continued meetings and engagement with the African American Community

Project updates and meetings when appropriate

**Are there others from your organization that should be included?**

Add names and contact info

Joe Hembree - Commercial Realtor on Governmental Issues Board

James Golden - Legal Rep (NAACP)/ Manatee County Resident & Economic Development Chair.

Sororities and Fraternities - ASHAL Group and local churches

Potential to share with Bradenton Area EDC Board

**Can you help us distribute the questionnaire / project information? What methods?**

Newsletters

Mailing List

In-person flyers posted throughout New College campus

Post flyers throughout airport. Can ask IT to post to website.

Include in weekly and monthly newsletter

Town Hall Meetings, pass out flyers, email/ billboards

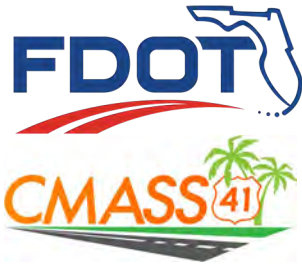
Will distribute to contact list.

Email blast and in-person depending on scheduled events

Electronic distribution

Suggestion for project team to look to local churches and childcare centers for distribution in addition to creating a spanish and creole version of the questionnaire.





US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
**Stakeholder/Agency Meeting (Virtual)**  
**Sarasota Manatee Airport Authority (SMAA)**

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## **Virtual Meeting via Microsoft Teams**

January 19, 2023, 10:00 a.m.

### [Meeting Summary](#)

A virtual stakeholder's meeting with the Sarasota Manatee Airport Authority (SMAA) was conducted via Microsoft Teams on Thursday, January 19, 2023. The purpose of the meeting was to present information for FDOT's Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS) and conduct an open discussion between the study team and SMAA.

### [Attendees](#)

#### **Project Team**

FDOT: Jeff James – Environmental Manager  
RK&K: Matt Dockins, PE – Project Manager on behalf of FDOT  
VHB: Amy Sirmans, PE – Consultant Project Manager  
VHB: Heather Garcia – Project Team Member  
VHB: Kennedy Sumner-Snell – Project Team Member  
The Valerin Group: Catherine Winter, PE – Public Involvement Coordinator  
AECOM: Lauren Brooks, AICP – Connecting Communities Program Manager on behalf of FDOT

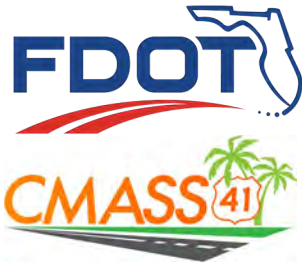
#### **Stakeholders**

SMAA: Kent Bontrager, Senior Vice President, Engineering, Planning and Facilities  
SMAA: Lionel Guilbert, Senior Vice President, Operations and Public Safety  
SMAA: Joe Filippelli, Senior Vice President, Real Estate Development and Properties

### [Project Overview](#)

The project team shared a presentation with meeting attendees that provided information on the US 41 CMASS project including project description and background, goals and objectives, schedule, and public outreach. A copy of the presentation is included with these notes.

The FDOT US 41 Complete Streets Corridor Planning Study was discussed as it relates to the US 41 CMASS study. The US 41 CMASS project team will take into consideration the potential improvements analyzed and identified in the Complete Streets Study. It was noted that the Complete Streets Study did not cover the entire length of US 41 CMASS, it only covered the portion along US 41 from the Sarasota/Manatee County line to north of the airport. The CMASS study is looking more holistically at the rest of the US 41 corridor. Recommendations from the Complete Streets Study will be a part of the US 41 CMASS baseline, but the CMASS team will need to see how those recommendations impact the rest of the network and the rest of the corridor.



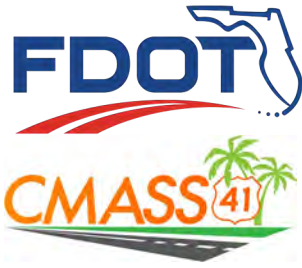
US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
**Stakeholder/Agency Meeting (Virtual)**  
**Sarasota Manatee Airport Authority (SMAA)**

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The project team used a digital whiteboard application (Mural) to capture and document stakeholder ideas and feedback. A copy of the Mural whiteboard with all feedback received to date from the individual stakeholder meetings is included with these meeting notes.

### Open Discussion

- Stakeholder comments and concerns
  - Better/safer pedestrian movement across US 41 near the airport was needed, especially with USF nearby.
  - The pedestrian overpass was sufficient for New College tract of land.
  - Property 6 (near Captain Brian's) would eventually be commercially developed and feature possible points of interest for students, necessitating crosswalks/access to the other side of the street.
  - The Dolphin hangar/FBO, being more aviation-related, would likely need vehicle access as opposed to that for pedestrians.
  - Vertical planes and a potential vertiport in that area which could result in an increase in traffic.
  - Need improved wayfinding signs along US 41.
  - Traffic speeding needs to be addressed.
  - Runway Protection Zone (RPZ) related
    - Need to minimize roadway (i.e., reducing lane width, etc.) near Runway 4, if possible, to avoid the airport's RPZ. Features such as bus stops and bike lanes should be avoided in that area due to proximity to the runway.
    - The situation is the same at Runway 14, but it's not as bad. The airport owns most of the property west of US 41 near Runway 14, including the vacant land. It would be ideal to push the roadway further to the west but recognized that it's probably cost-prohibitive for FDOT to do so. Improvements need to be coordinated through the FAA during the design process.
    - Indicated that FAR 77 surfaces (airspace/imaginary surfaces) would be more impacted in the RPZ areas than anywhere else, which would require exploration of sloping/elevations and the potential need for lower light poles, etc., to ensure proper clearance.
  - Property Access related
    - Property 5, a 6-acre tract of land, showing the southern entrance leading to the Dolphin FBO and the northern entrance at Braden Avenue, would be best served using the Braden Avenue traffic signal as opposed to going north or turning into Dolphin FBO.
    - Captain Brian's had expressed concern about the loss of their driveway.
    - A current FDOT resurfacing project underway is proposing the addition of medians along US 41 near the airport. The CMASS team will reach out to resurfacing team to obtain the most current plans showing median and driveway cuts.



US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
**Stakeholder/Agency Meeting (Virtual)**  
**Sarasota Manatee Airport Authority (SMAA)**

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- Construction was expected to begin on their Ground Transportation Center Project within the next month or two, during which they would be relocating the bus drop-off for MCAT and SCAT. The drop-off would be in the same area but would be slightly shifted and enlarged to accommodate four buses as opposed to two, with buses still able to access US 41.
  - Traffic signal timing at General Spaatz Boulevard.
  - Working with Manatee County to vacate Ponce de Leon Avenue, which would become airport property and involve removal of the road.
  - Connecting Communities Questionnaire distribution
    - Posting flyers/posters within airport, getting their IT department to include questionnaire on their website attached to newsletter.
  - Continued stakeholder involvement
    - Airport would like to continue with meeting as the project progresses.
- 

#### Attendees Email Addresses

Kent Bontrager, [kent.bontrager@flysrq.com](mailto:kent.bontrager@flysrq.com)

Lionel Guilbert, [lionel.guilbert@flysrq.com](mailto:lionel.guilbert@flysrq.com)

Joe Filippelli, [joseph.filippelli@flysrq.com](mailto:joseph.filippelli@flysrq.com)

Jeff James, [jeffrey.james@dot.state.fl.us](mailto:jeffrey.james@dot.state.fl.us)

Matt Dockins, PE, [mdockins@hntb.com](mailto:mdockins@hntb.com)

Amy Sirmans, PE, [asirmans@vhb.com](mailto:asirmans@vhb.com)

Heather Garcia, [hgarcia@vhb.com](mailto:hgarcia@vhb.com)

Kennedy Sumner-Snell, [ksumner-snell@vhb.com](mailto:ksumner-snell@vhb.com)

Lauren Brooks, AICP, [lauren.brooks@aecom.com](mailto:lauren.brooks@aecom.com)

Catherine Winter, PE, [catherine@valerin-group.com](mailto:catherine@valerin-group.com)



# District One Environmental Management Office

## *Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS)*

*From University Parkway to 17<sup>th</sup> Street W*

January 2023



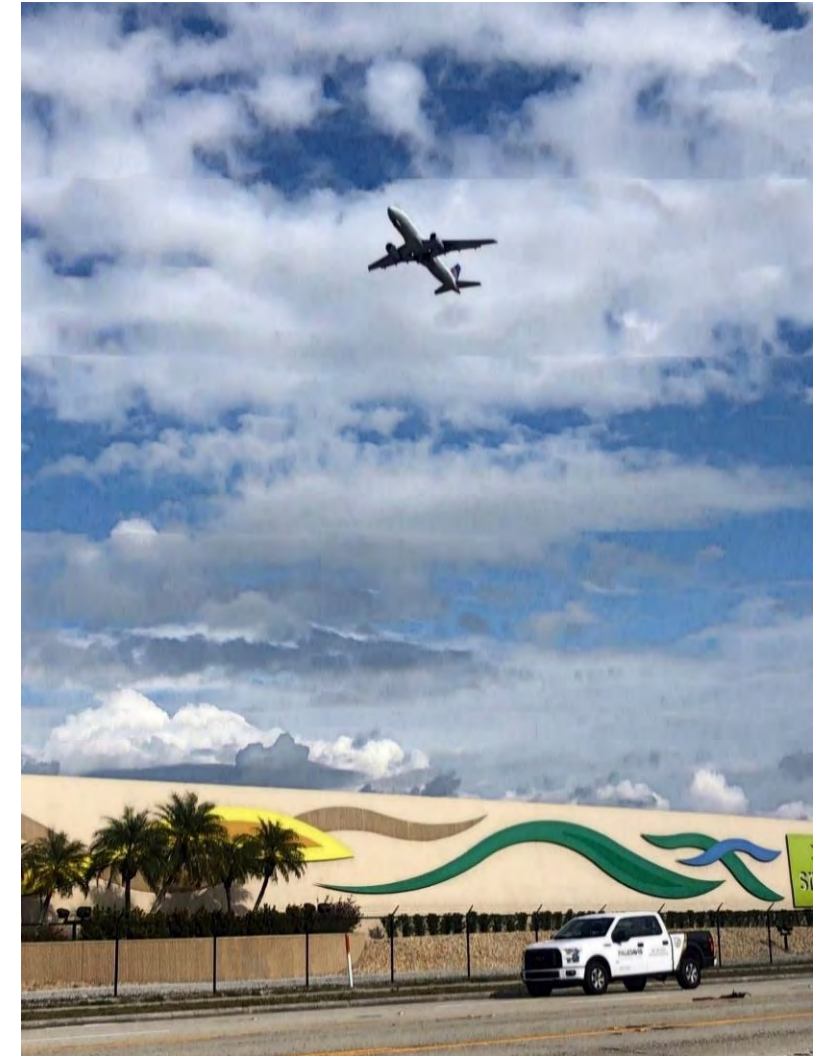
# Sarasota Manatee Airport Authority

January 19, 2023



# Agenda

- Introductions
- Project Description and Background
- US 41 Today: Existing Conditions
- Public Outreach
- Open Discussion: We Want Your Input



# Introductions



**Abra Horne**  
**Administrator**  
District Environmental  
Management Office  
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**Matt Dockins, P.E.**  
**Project Manager**  
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**Jeffrey James**  
**Environmental Manager**  
District Environmental  
Management Office  
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# Introductions



**Lauren Brooks, AICP**  
**Project Manager**  
**Senior Transportation Planner**  
**AECOM**  
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**Amy Sirmans, P.E.**  
**Project Manager**  
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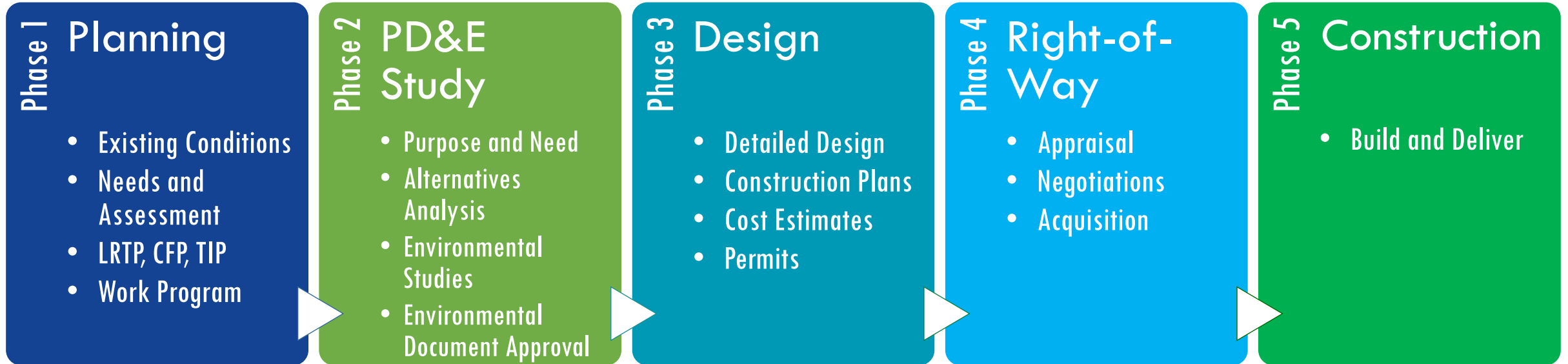
**Catherine Winter, P.E.**  
**Public Involvement Manager**  
**The Valerin Group**  
[Catherine@valerin-group.com](mailto:Catherine@valerin-group.com)  
(813) 431-0919



# Project Description and Background



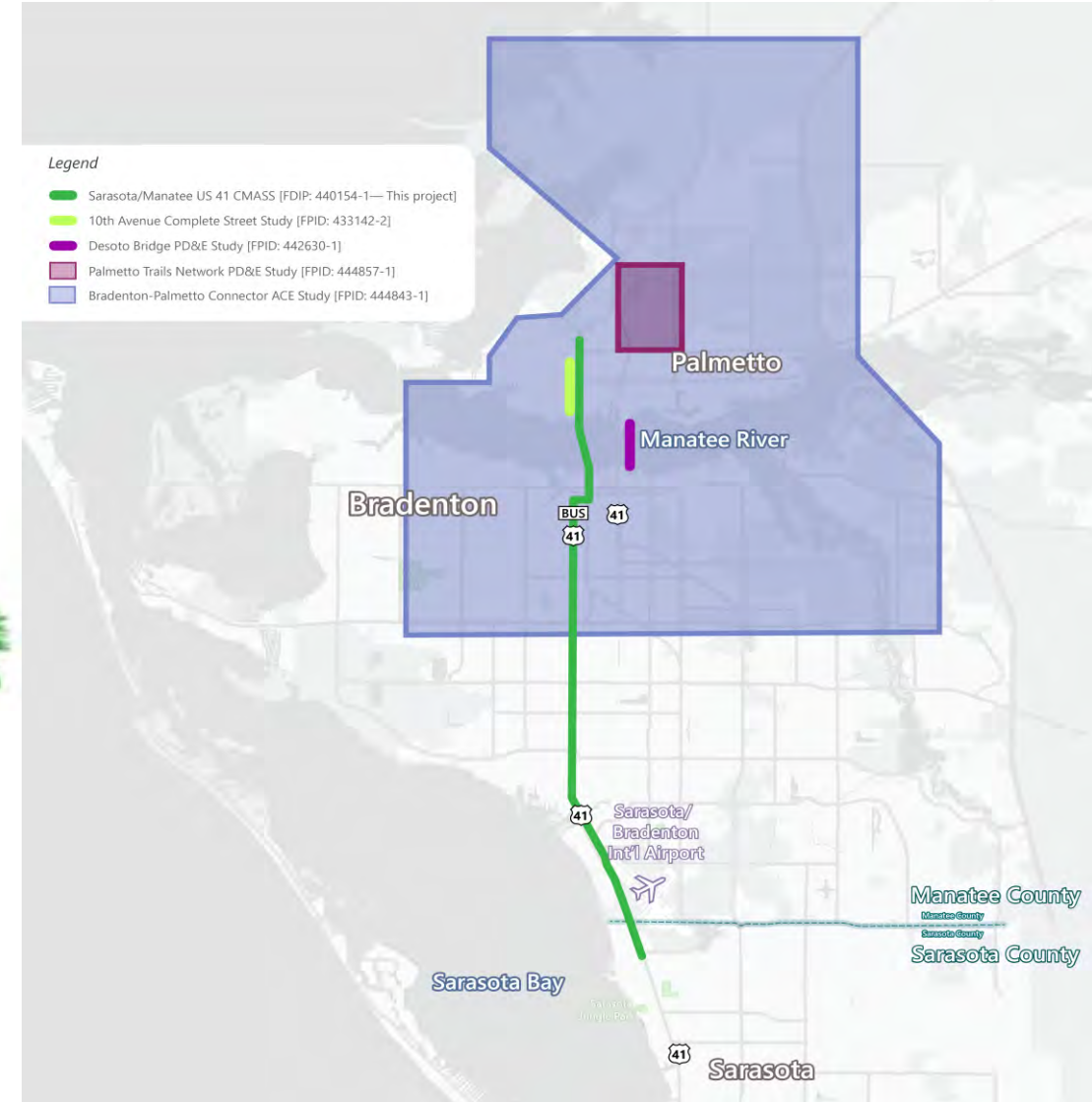
# Transportation Project Development Process



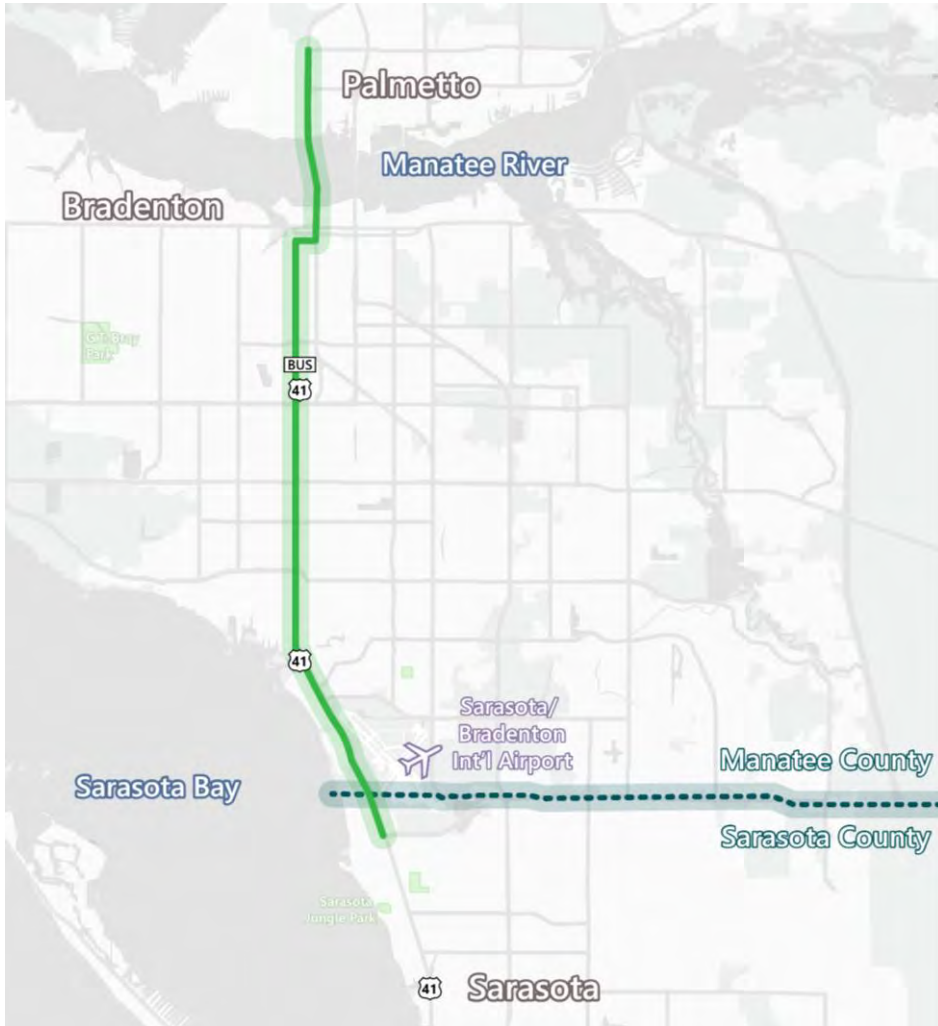
# Connecting Communities

**Purpose:** Holistically enhance the multimodal transportation network within Sarasota and Manatee Counties.

## DeSoto Bridge PD&E Study



# Project Description



**Project Limits:** US 41/US 41 Business corridor from University Parkway (Sarasota) to 17<sup>th</sup> Street W (Palmetto)

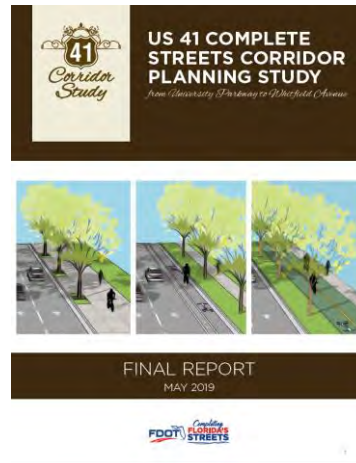
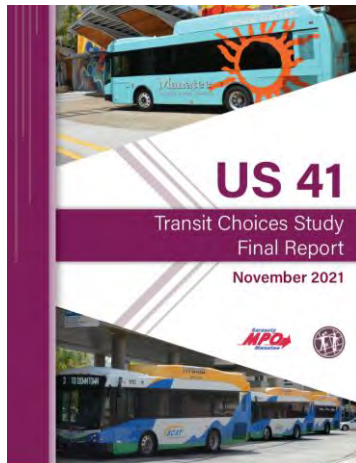
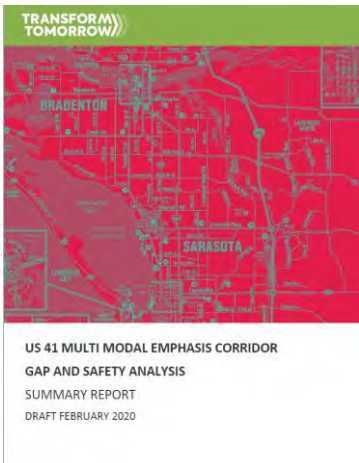
**Financial Project ID:** 440154-1-21-01

**Project Manager:** Matt Dockins

**Status:** Feasibility Study – Phase 1

# We are not starting from scratch...

- US 41 Multi-Modal Emphasis Corridor (MMEC)
- US 41 Transit Choices Study
- US 41 Complete Streets Corridor Planning Study
- Arterial Safety Study
- Access Management Study
- Intersection Safety Studies

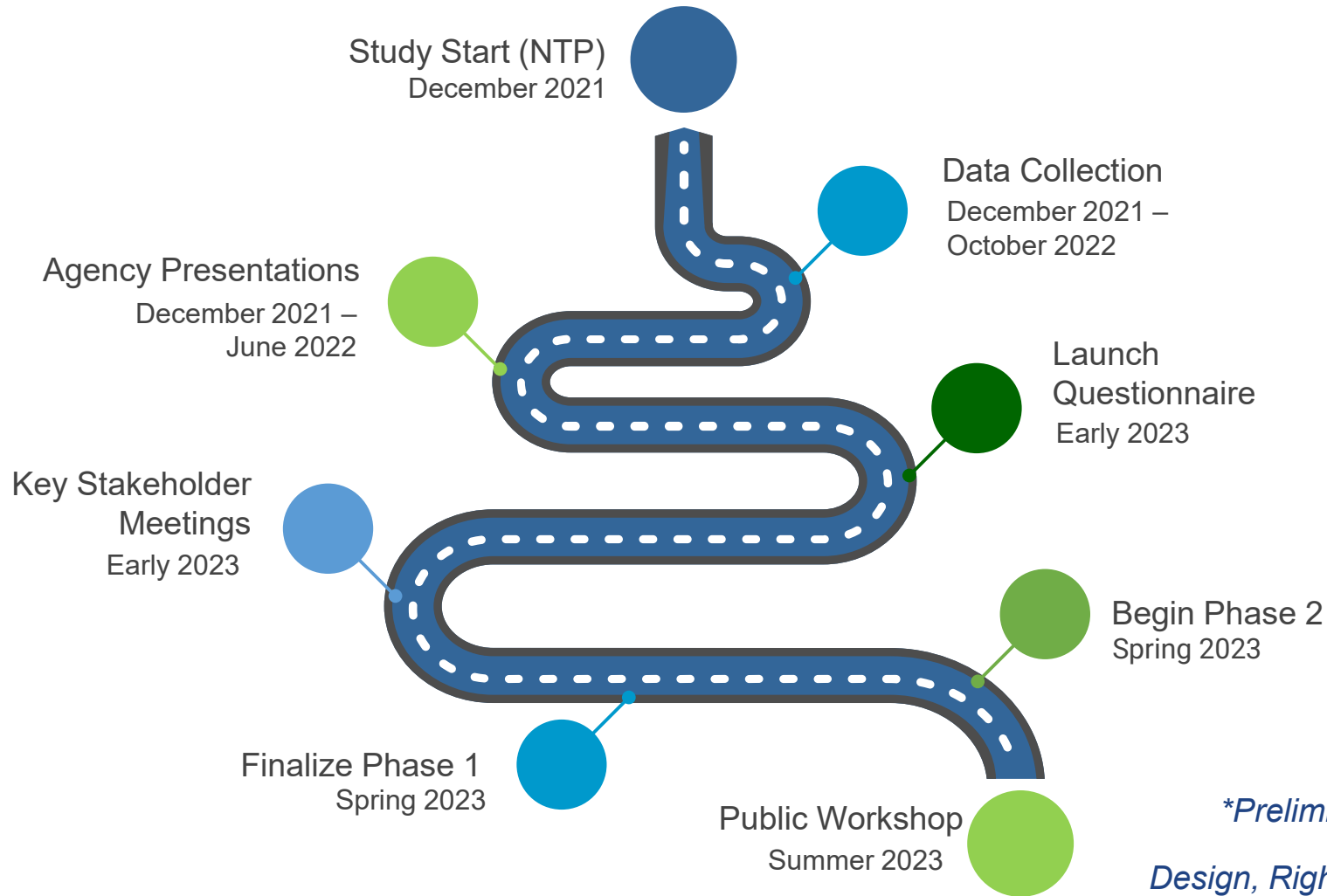


# Goals & Objectives

- Enhance multimodal mobility and safety for all users
- Advance transit service/operation strategies along the study corridor
- Improve transportation network access and interconnectivity within:
  - Neighborhoods
  - Adjacent areas
  - Transit centers



# Project Schedule



*\*Preliminary Schedule – subject to change\**  
*Design, Right-of-Way, & Construction are not funded.*

# US 41 Corridor Today





# US 41 Corridor Today

## What are the characteristics of the corridor?

- FDOT priority multimodal corridor
- **2-3 travel lanes in each direction**
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## Who is using the corridor?

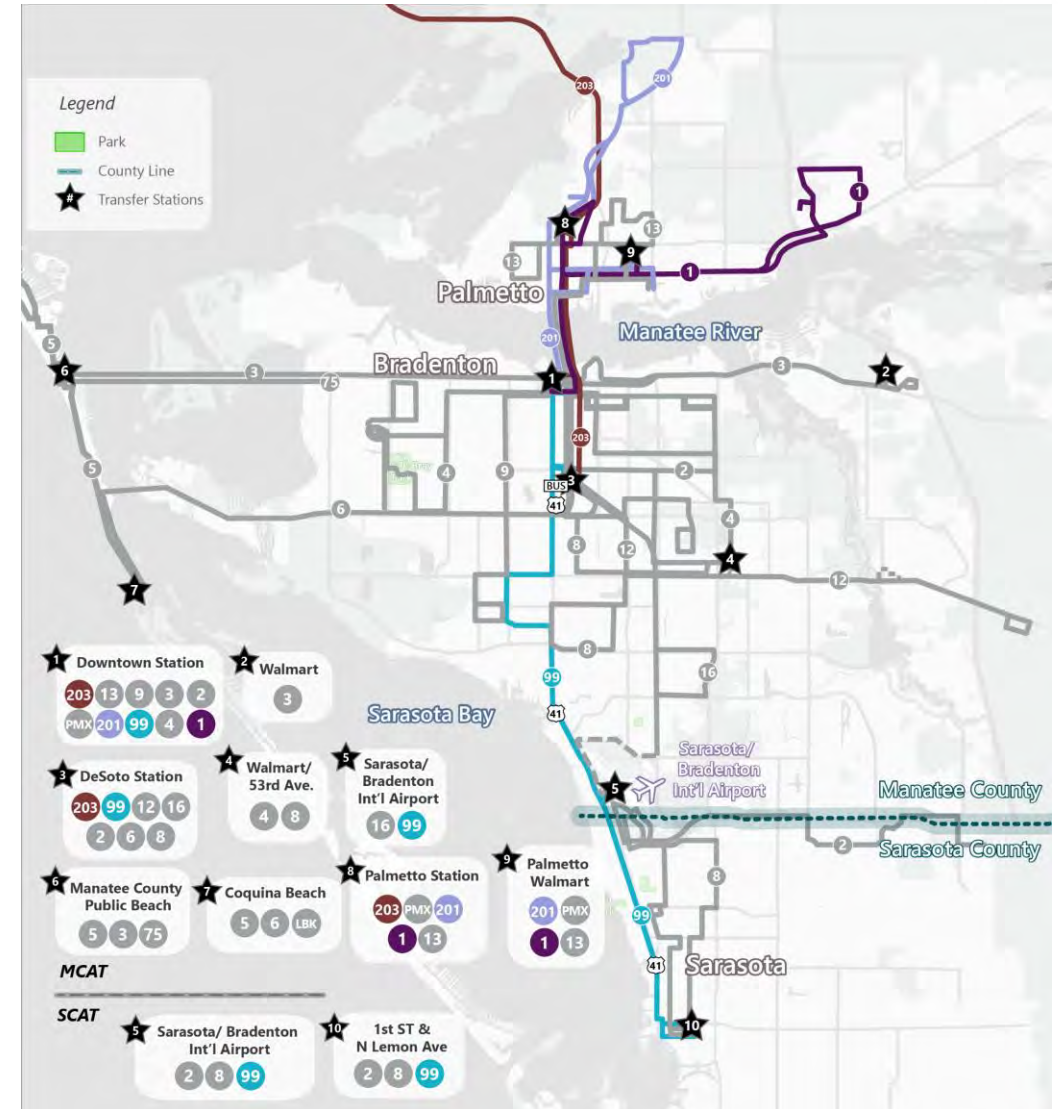
- Residents
- Visitors
- Students
- Businesses

## How are they travelling?

- Single Occupancy Vehicles (cars/trucks, etc.)
- Pedestrians
- Bicyclists
- Several transit (bus) routes

# Area Transit Routes

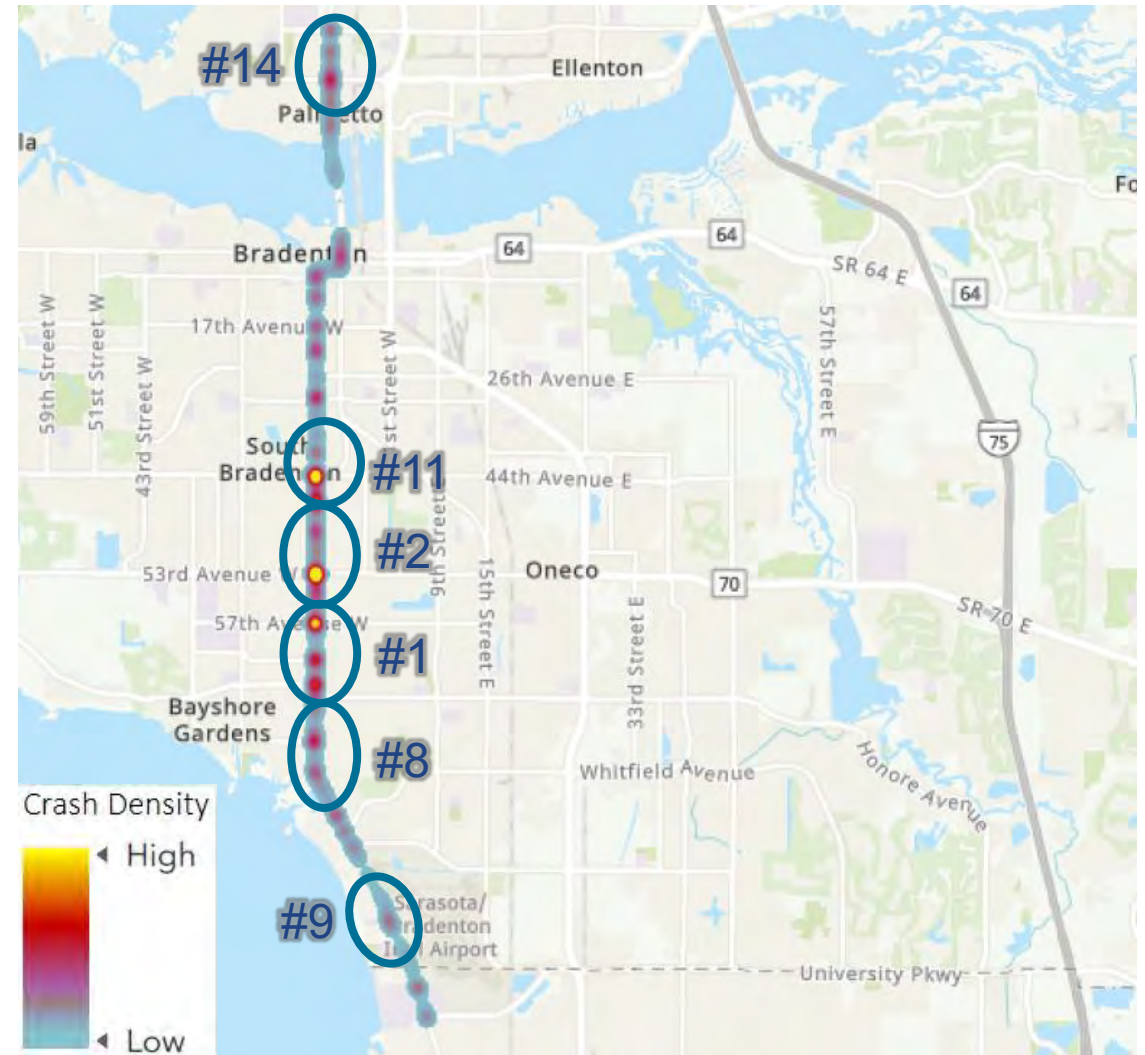
- Route 99 Bradenton-Sarasota
  - Runs length of study area
  - 20-minute AM and PM Peak Period service started Dec 2021
  - New Sunday service proposed starting in 2026
- Stations Near Project
  - Palmetto Station
  - Bradenton Downtown Station
  - DeSoto Station
  - Sarasota-Bradenton International Airport Station



# Safety and Crash Analysis

## Six segments are listed as SW Florida Fatal Top 20 Segments

- 63<sup>rd</sup> Ave W to 55<sup>th</sup> Ave W; Ranked #1
- 55<sup>th</sup> Ave W to Orlando Ave; Ranked #2
- Magellan Dr to 63<sup>rd</sup> Ave W; Ranked #8
- County line to Scott Ave; Ranked #9
- Orlando Ave to 30<sup>th</sup> Ave W; Ranked #11
- 10<sup>th</sup> St W to N of 23<sup>rd</sup> St W; Ranked #14



# Public Outreach

The graphic features a blue background with several circular and rectangular images. At the top, there are five icons: a car, a person with a cane, a bus, a person with a stroller, and a bicycle. The images include: a 'PUSH BUTTON FOR' sign with a pedestrian icon, a line of cars in traffic, a street view with palm trees and buildings, a green and white bus, and a traffic light. The text on the graphic reads: 'Sarasota | Manatee U.S. 41 Corridor Mobility and Safety Study Communications Plan'. It also includes the logos for 'FDOT' and 'CMASS 41', the date 'September 2022', and the project ID 'FPID #440154-1'. At the bottom, there is a signature line: 'Approved By: Abra Horne Date: 9/2/2022' and the title 'Abra Horne, Planning and Environmental Administrator'.

# Public Outreach



## Outreach to Date:

### September 2021

- Sarasota/Manatee MPO Staff Meetings

### December 2021

- Sarasota/Manatee MPO Staff Meeting
- Pinellas Suncoast Transit Authority Meeting
- SunRunner BRT Meeting

### February 2022

- Sarasota/Manatee MPO Staff Meeting
- Sarasota/Manatee MPO Board Meeting
- Sarasota/Manatee MPO TAC Meeting

### April 2022

- Sarasota/Manatee MPO BPTAC Meeting

### May 2022

- Sarasota/Manatee MPO CAN Meeting
- Sarasota/Manatee MPO TAC Meeting
- Sarasota/Manatee MPO Board Meeting

### June 2022

- City of Bradenton City Council Meeting
- City of Palmetto City Council Meeting
- Manatee County Commission Meeting

## Upcoming Outreach:

### Early 2023

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire

### Summer 2023

- Hold Public Workshop - Summer 2023

### Throughout 2023

- Continued coordination with all agencies and stakeholders

# We Want to Hear From YOU!

Complete the questionnaire:

[swflroads.com/connectingcommunities/](http://swflroads.com/connectingcommunities/)



**FDOT** **CONNECTING COMMUNITIES**  
Manatee | Sarasota Counties

January 2023 - April 2023

### WE WANT TO HEAR FROM YOU!

The Florida Department of Transportation, District One, is conducting several projects in partnership with the Sarasota/Manatee Metropolitan Planning Organization (MPO) and local governments to holistically enhance the multimodal transportation network within Sarasota and Manatee Counties as part of the Connecting Communities Program. Major Connecting Communities Program projects include Bradenton-Palmetto Connector, Desoto Bridge, Sarasota/Manatee US 41 Corridor Mobility and Safety Study, Palmetto Trails Network, and 10th Avenue Complete Street. For a map and full list of current area projects included as part of the Connecting Communities Program visit [www.swflroads.com/connectingcommunities](http://www.swflroads.com/connectingcommunities).

Complete Streets  
Safety Improvements  
Corridor Visioning  
Bridge Upgrades  
Multi-use Trails

**Get involved today!**  
**Scan to take our online questionnaire.**  
Complete our brief online questionnaire to provide valuable feedback for enhancing the multimodal transportation network.

For more information on the Connecting Communities Program, visit [www.swflroads.com/connectingcommunities](http://www.swflroads.com/connectingcommunities).

# Open Discussion

CMASS Stakeholder Meeting | All changes saved!

## CMASS Stakeholder Meeting

January 2023

**Connecting Communities**

The purpose of the Connecting Communities program is to holistically enhance the multimodal transportation network within Sarasota and Manatee Counties.

**US 41 CMASS - Goals & Objectives**

- Enhance multimodal mobility and safety for all users
- Advance transit service/operation strategies along the study corridor
- Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

**NEXT STEPS**

- Launch and commence development
- Finalize Goals & Objectives and Measures of Effectiveness
- Finalize Corridor Neighborhood Strategies
- Finalize Phase 1 - Spring 2022
- Hold Final Workshop - Summer 2022
- Begin Phase 2 Develop Concepts for Implementation

**ATTENDEES**

Name: [Redacted]

Agency: [Redacted]

Title: [Redacted]

**STAKEHOLDER INPUT**

Enhance multimodal mobility and safety for all users

[Green sticky notes]

Advance transit service/operation strategies along the study corridor

[Purple sticky notes]

Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

[Yellow sticky notes]

[User avatars and interaction icons]

# Next Steps

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire
- Finalize Goals & Objectives and Measures of Effectiveness
- Finalize Corridor/Neighborhood Strategies
- Finalize Phase 1 – Spring 2023
- Hold Public Workshop - Summer 2023
- Begin Phase 2: Develop Concepts for Implementation

**Complete the questionnaire:**

[swfroads.com/connectingcommunities/](http://swfroads.com/connectingcommunities/)







# Safety, Questions and Contact Information

**Matt Dockins, PE**  
FDOT Project Manager  
FDOT District 1  
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**Amy Sirmans, PE**  
Consultant Project Manager  
VHB  
[asirmans@vhb.com](mailto:asirmans@vhb.com)



**PEDESTRIANS  
AND BICYCLISTS**

Nationally, Florida had the **HIGHEST NUMBER OF BICYCLIST FATALITIES** in 2018

**Pedestrians** account for more than **22%** of **traffic fatalities** in Florida



# STAKEHOLDER INPUT



## Enhance multimodal mobility and safety for all users

Multimodal Mobility & Safety is #1 concern. Increased number of accidents occurring at US41 and General Spaatz Blvd in front of New College, including accidents involving students.

This project will help improve multimodal mobility. Crosswalk improvements/safety for school students would be beneficial

Suggestion to implement pedestrian crossings and enhance accessibility to nearby restaurants/facilities. There are currently no crosswalks on US41 at USF

Existing conditions include a basic sidewalk. Suggestion for greenway improvements

Concern for students crossing areas near Ringling College and the Airport

Have been discussions about providing safer pedestrian access near the airport

Suggestion to enhance pedestrian safety along DeSoto Bridge

Lack of crosswalks along corridor. Does not allow for recreation or exercise

## Advance transit service/operation strategies along the study corridor

Enhance transit along US 41 - Route 99 is best performing route along corridor

Many students use the pedestrian overpass rather than crossing US41 at the intersection. Bike traffic is more common than pedestrian traffic

Keep transit stops away from Runway 4 to enhance safety for pedestrians

Keeping the median opening at Dolphin would be beneficial for future development

Adding bus stops along corridor to benefit local community

Chamber focusing on cost effective methods for transportation including buses, on demand transit, and transit accessibility

Pedestrian/Scoter/Bike traffic around colleges. May not be necessary for a dedicated SCAT lane due to low ridership/trip

USF provides free transit (MCAT and SCAT) for students on campus. Bus stops are not on campus, but adjacent.

Having a stop at USF campus would provide enhanced accessibility for students

Congestion near universities does not allow for safe or efficient traffic flow

Transportation is barrier for employment. Enhanced transportation will benefit the community and employers

Transit timing is inefficient and poorly designed for workforce

## Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

Suggestion to enhance access to activity center located at US41 and 44th Ave W to benefit local community

Suggestion to make transit more easily accessible by adjacent neighborhoods

Area near Seagate Drive (Property 6) to be developed. Would benefit from enhanced pedestrian facilities. Will be developed as as parking lot for interim use

Not many businesses/restaurants surrounding New College. Students/Faculty rely on cars as main form of transportation. (~700 students residing on campus.)

Chamber considering best way to provide accessible transit for all

Future developments to increase campus activity, enhancing the opportunity for transit use.

Some students will use transit to get to other areas in Sarasota and Bradenton, however most use their own vehicle.

Consideration for businesses would be appreciated as they have been recently affected

Route to nearby businesses is not safe for pedestrians

Inefficient access to transit. Workforce living in southern Sarasota County travel to employment centers in northern Manatee County.

# MEETINGS TO DATE



- Bradenton Area Convention and Visitors Bureau
- Manasota Black Chamber of Commerce
- Manatee County Public Works
- New College of Florida
- Sarasota Manatee Airport Authority
- The Greater Sarasota Chamber of Commerce
- University of South Florida Sarasota-Manatee
- Manatee NAACP
- Bradenton Area Economic Development Corporation

## What improvements would you like to see for the US 41 corridor?

Enhanced lighting near Crosley Estate and USF Campus

Pedestrian facilities (Continous bike/ped) throughout the corridor

Consistency in roadway and surrounding facilities (aesthetics). The roadway and surrounding facilities are very different.

Opportunity for more bicycle lanes along US41 in both directions (N/S)

Enhanced safety at the intersection of US41 and General Spaatz Blvd

Lighting near runway zones is something to be considered. May need approval from FAA

Good wayfinding signs on US41 for Airport - beneficial to incorporate style/colors into signage

Bike/Ped connections between Sarasota and Manatee County

Coordination of red lights along the corridor

Aesthetic improvements along corridor

Continous sidewalks/pedestrian lighting throughout corridor to enhance safety

Adding bike lanes throughout the corridor.

Crosswalks along corridor and safety improvements when traveling to nearby businesses

Aesthetic improvements along corridor. Current aesthetics do not reflect the area well

## Are there any specific area of concern you would like to discuss?

Pedestrian activity walking to New College.

Difficulty for pedestrians to access transit stations

Insufficient lighting near New College

Ability to cross the road.

Traffic congestion between University and New College intersection in AM peak time

Varying speeds/high speed travelers at US 41 and New College entrance (especially northbound)

Lack of pedestrian access between New College and USF.

Pedestrians crossing the road in busier areas. Suggestion for a median that would enforce students/pedestrians to use designated crosswalks.

Captain Brian's Seafood has expressed concern for losing driveway access (involving resurfacing project) - could be beneficial to include shared access with Airport

High speeds along the corridor

Concern for homeless population in the area. Would like to see an intentional plan in place to accommodate the homeless population along the corridor

Concern for future stacking issues at Seagate Drive (highest entrance and exit counts here). Potential for increased congestion with coming development. Includes buses as well.

High speeds in the area. Speed control

Future increased traffic volume to cause issues along corridor

Upholding integrity for African American-owned churches and businesses along the corridor

Congestion and sustainability of the corridor.

Aesthetics and traffic flow along corridor.

Inconsistent aesthetics when traveling across the Sarasota/Manatee County line. Varying differences between the two counties, crossing University Parkway

## What issues do you notice when traveling the US 41 corridor?

Safety hazards (crashes, frequent stopping, roadway conditions, etc.)

Excessive vehicular speeding

Insufficient lighting

Difficulty entering/exiting properties along the corridor

Quality of sidewalks

Quality of bicycle facilities

Quality of transit stops

Lack of transit reliability

Traffic congestion

Inconsistency throughout the corridor including the examples above and number of lanes.

Turning into or out of USF onto US41 (Safety)

Lack of safety / rate of speed / congestion

Travelers unfamiliar with the area are unaware of surrounding conditions

Increased traffic flow

Lack of access management near Airport

Confusion with traffic flow patterns and turning lanes, especially considering ongoing projects

## What is your biggest complaint about traveling along the US 41 Corridor?

Poor lighting along the corridor for motorized traffic and pedestrians

High speeds near Crosley Estate

High pedestrian fatalities due to dangerous conditions

Lack of lighting and high traffic speeds

Bike/Ped safety along the corridor.

Complaints about the left turn signal at General Spaatz Blvd. Long wait times, congestion, queuing issues

Unsafe conditions along the corridor for pedestrians, especially near colleges

Lack of safety and aesthetics along corridor.

Lack of aesthetics along corridor.

Lack of pedestrian safety, especially near colleges

**Does your agency/organization have any planned improvements / projects / events that the project team should know about?**

Frequent Trolley Services from Crosley Estate to nearby for approximately 200 events per year.

Similar transportation methods at Ringling and New College.

Use of Trolley Service encouraged

Frequent events on campus, but most impacts are within the campus

Manatee County is working on Mobility Plan. The County will increase focus on capital project needs and prioritizations

Manatee County working on Trails Master Plan

Few events a year that may impact traffic. Winter events keep University traffic steady

Ground Transportation Center project to begin construction in coming months. Intended to improve MCAT and SCAT drop-off and pick-up. Larger area will accommodate 4 buses rather than 2.

Working with Manatee County to remove Ponce De Leon St once the land becomes the Airport's property

On-campus housing plans in development

Future commercial development anticipated in Property 5 and 6 near airport

Several developers interested in converting motels along the corridor into affordable housing, etc., which would provide a need for enhanced transit services

Future plan to construct nursing program will increase attendance

Annual Golf Tournament & Fall event partnered with Manatee Convention Center with 200+ in attendance

Juneteenth Event with local library

Planned improvements near airport and colleges are anticipated to bring growth in the next 10 years.

**How would you like to continue to be involved in the project?**

Meet with me again

Join Mailing List

Become a project champion

Additional meeting with New College.

Update meeting with Airport Authority

Join mailing list. Hold update meeting with governmental issues council (when appropriate). Can participate as a project champion

Continued meetings to keep up to date on the planning process. Mailing list updates.

Continued meetings to be updated on project process

Continued meetings and engagement with the African American Community

Project updates and meetings when appropriate

**Are there others from your organization that should be included?**

Add names and contact info

Joe Hembree - Commercial Realtor on Governmental Issues Board

James Golden - Legal Rep (NAACP)/ Manatee County Resident & Economic Development Chair.

Sororities and Fraternities - ASHAL Group and local churches

Potential to share with Bradenton Area EDC Board

**Can you help us distribute the questionnaire / project information? What methods?**

Newsletters

Mailing List

In-person flyers posted throughout New College campus

Post flyers throughout airport. Can ask IT to post to website.

Include in weekly and monthly newsletter

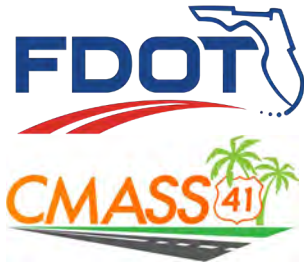
Town Hall Meetings, pass out flyers, email/ billboards

Will distribute to contact list.

Email blast and in-person depending on scheduled events

Electronic distribution

Suggestion for project team to look to local churches and childcare centers for distribution in addition to creating a spanish and creole version of the questionnaire.



US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
**Stakeholder/Agency Meeting (Virtual)**  
University of South Florida (USF) Sarasota-Manatee

## **Virtual Meeting via Microsoft Teams**

January 20, 2023, 1:00 p.m.

### [Meeting Summary](#)

A virtual stakeholder's meeting with the University of South Florida (USF) Sarasota-Manatee was conducted via Microsoft Teams on Friday, January 20, 2023. The purpose of the meeting was to present information for FDOT's Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS) and conduct an open discussion between the study team and USF Sarasota-Manatee.

### [Attendees](#)

#### **Project Team**

FDOT: Abra Horne – District Administrator  
RK&K: Matt Dockins, PE – Project Manager on behalf of FDOT  
VHB: Amy Sirmans, PE – Consultant Project Manager  
VHB: Heather Garcia – Project Team Member  
VHB: Kennedy Sumner-Snell – Project Team Member  
The Valerin Group: Catherine Winter, PE – Public Involvement Coordinator  
AECOM: Lauren Brooks, AICP – Connecting Communities Project Manager on behalf of FDOT

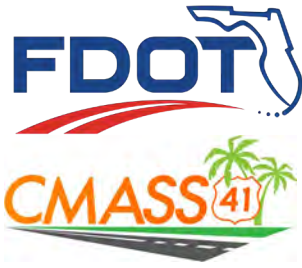
#### **Stakeholders**

USF Sarasota-Manatee: Daniel LaForge, Director of Facilities Planning & Management  
USF Sarasota-Manatee: Tom O'Loughlin, Associate Director of Facilities Planning & Management  
USF Sarasota-Manatee: Eddie Beauchamp, Regional Vice Chancellor for Business & Financial Affairs  
USF Sarasota-Manatee: Casey Welch, Assistant Vice President for External & Government Relations  
USF Sarasota-Manatee: Brent Kemker: Regional Vice Chancellor and Vice Provost, Academic Affairs and Student Success  
Core Construction: Jason Wilson, Project Director

### [Project Overview](#)

The project presentation provided information on the US 41 CMASS project including project description and background, goals and objectives, schedule, and public outreach. A copy of the presentation is included with these notes.

The project team used a digital whiteboard application (Mural) to capture and document stakeholder ideas and feedback. A copy of the Mural whiteboard with all feedback received to date from the individual stakeholder meetings is included with these meeting notes.

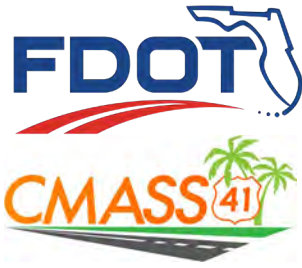


US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
**Stakeholder/Agency Meeting (Virtual)**  
**University of South Florida (USF) Sarasota-Manatee**

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## Open Discussion

- Other projects discussed
  - The US 41 CMASS project is a separate project than the US 41 median project which is currently underway.
- USF Growth
  - USF will break ground on the campus's first housing project in February, which will change the campus from a commuter to a residential campus; that more students are returning to campus post-Covid; and that 24/7 campus operations will be implemented. The project will add 200 beds to the campus.
  - Currently serving 7,000 to 9,000 Sarasota/Manatee students per semester.
- Transit related
  - USF now provides free access to transit in Manatee/Sarasota counties, adding that the program is expected to continue and expand as the campus grows. Data related to student transit use could be provided later.
  - The transit stop is on property adjacent to campus, creating possible accessibility concerns; the buses are currently unable to turn in and out of the campus.
  - The new residence hall and dorm will be further away from US 41 and current USF facilities, which may not be convenient for students with accessibility concerns.
  - USF has expressed to Manatee/Sarasota counties the importance of having a transit stop on campus, even more so with the addition of the new residence hall, to ensure the accessibility of multi-modal transportation for students.
- Stakeholder concerns and suggestions
  - Suggested a crosswalk on US 41 would provide for safe crossing by students traveling to restaurants across the street from campus, noting that most currently drive rather than attempting to cross the road on foot.
  - USF is in partnership with other educational institutions on the development of an aeronautical engineering program which anticipates a bigger need to move between USF and New College.
  - Currently there is not a safe option at USF's Sarasota-Manatee location to cross the road. This is a great concern especially in the winter, when populations are nearly doubled, and when it's dark.
  - Expressed the need for a pedestrian overpass crossing US 41, like at New College.
  - Suggested how a bike path connecting Sarasota/Manatee would be nice to have but recognize the high speeds in the area.
  - Need speed controlling measures.
  - Concern for likelihood of a traffic bottleneck developing at the campus's main entry/exit at Seagate Drive with the opening of the new residence hall in the fall of 2024.
  - Concerned about how the new medians being added would affect their campus entrances.
  - Concerned about how growth of USF and the airport will impact their properties and others in the area.



US 41 CMASS  
University Parkway to 17<sup>th</sup> Street West  
FPID: 440154-1  
Sarasota and Manatee Counties  
**Stakeholder/Agency Meeting (Virtual)**  
**University of South Florida (USF) Sarasota-Manatee**

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- Concern for personal safety when entering and leaving campus, especially for multi-modal users.
- Better coordination of traffic signals along the corridor.
- Continued stakeholder involvement
  - USF would like to continue meeting as the project progresses.
  - Add them to the project contact list so they can receive any project update emails
- Connecting Communities Questionnaire Distribution
  - Campus town hall meeting where the questionnaire and project information could be mentioned.
  - Flyers, poster and emails can be circulated by USF

#### Attendees Email Addresses

Daniel LaForge, [dlaforge@usf.edu](mailto:dlaforge@usf.edu)  
Tom O'Loughlin, [toloughlin@usf.edu](mailto:toloughlin@usf.edu)  
Eddie Beauchamp, [erb1@usf.edu](mailto:erb1@usf.edu)  
Casey Welch, [caseywelch@usf.edu](mailto:caseywelch@usf.edu)  
Brent Kemker, [kemker@usf.edu](mailto:kemker@usf.edu)  
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Abra Horne, [abra.horne@dot.state.fl.us](mailto:abra.horne@dot.state.fl.us)  
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Kennedy Sumner-Snell, [ksumner-snell@vhb.com](mailto:ksumner-snell@vhb.com)  
Lauren Brooks, AICP, [lauren.brooks@aecom.com](mailto:lauren.brooks@aecom.com)  
Catherine Winter, PE, [catherine@valerin-group.com](mailto:catherine@valerin-group.com)



# District One Environmental Management Office

## *Sarasota/Manatee US 41 Corridor Mobility and Safety Study (CMASS)*

*From University Parkway to 17<sup>th</sup> Street W*

January 2023



# University of South Florida Sarasota-Manatee

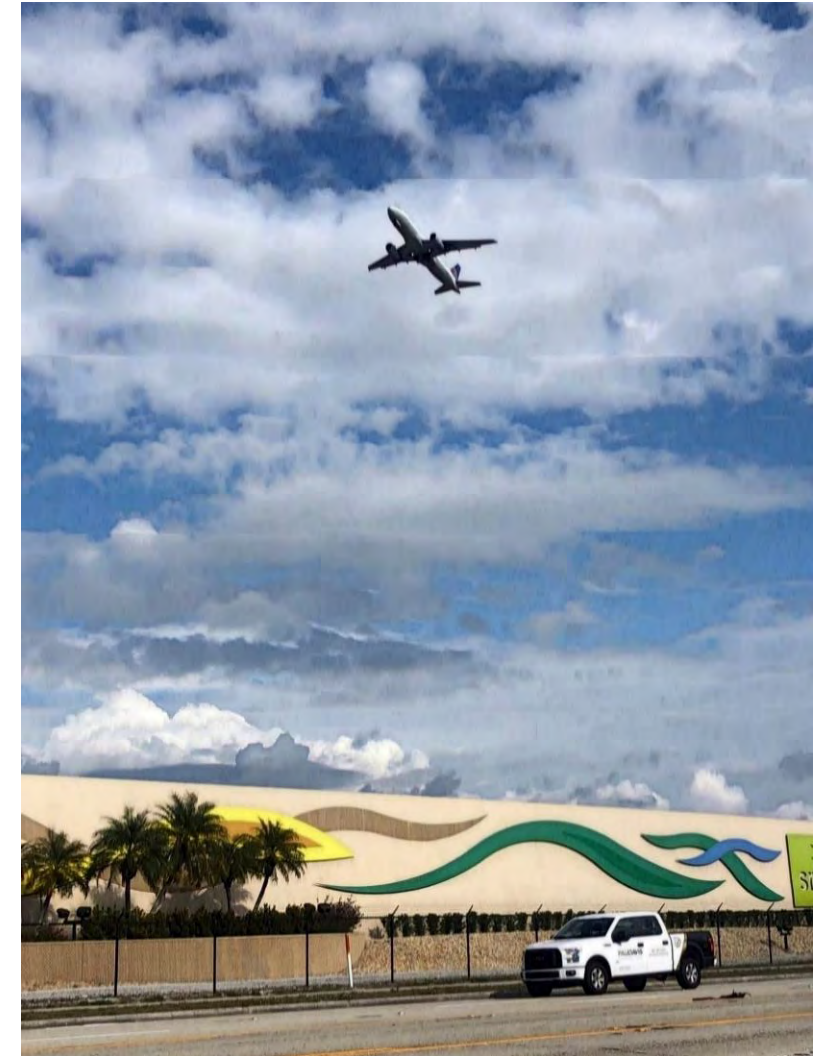
January 20, 2023





# Agenda

- Introductions
- Project Description and Background
- US 41 Today: Existing Conditions
- Public Outreach
- Open Discussion: We Want Your Input



# Introductions



**Abra Horne**  
**Administrator**  
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Management Office  
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**Matt Dockins, P.E.**  
**Project Manager**  
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**Jeffrey James**  
**Environmental Manager**  
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# Introductions



**Lauren Brooks, AICP**  
**Project Manager**  
**Senior Transportation Planner**  
**AECOM**  
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**Amy Sirmans, P.E.**  
**Project Manager**  
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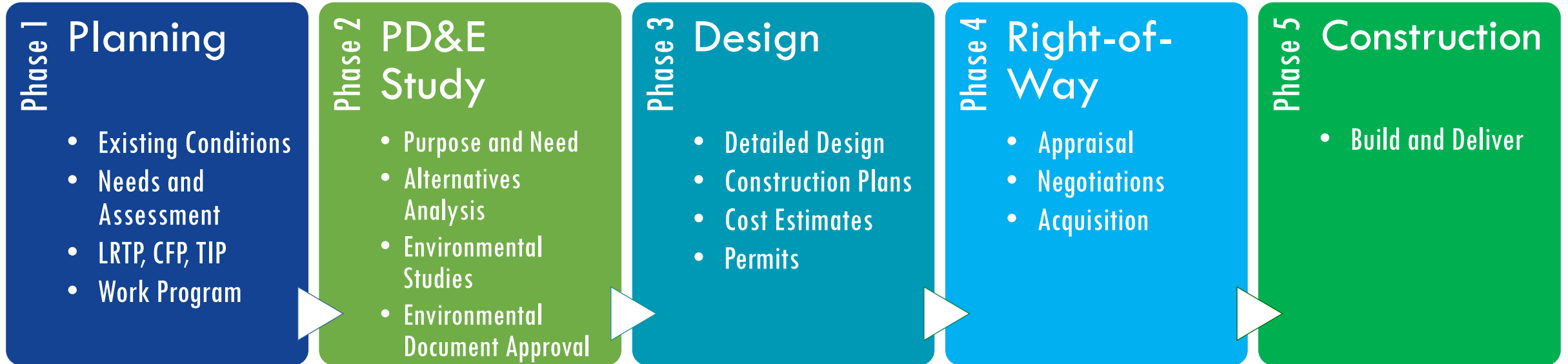


**Catherine Winter, P.E.**  
**Public Involvement Manager**  
**The Valerin Group**  
[Catherine@valerin-group.com](mailto:Catherine@valerin-group.com)  
(813) 431-0919

# Project Description and Background



# Transportation Project Development Process

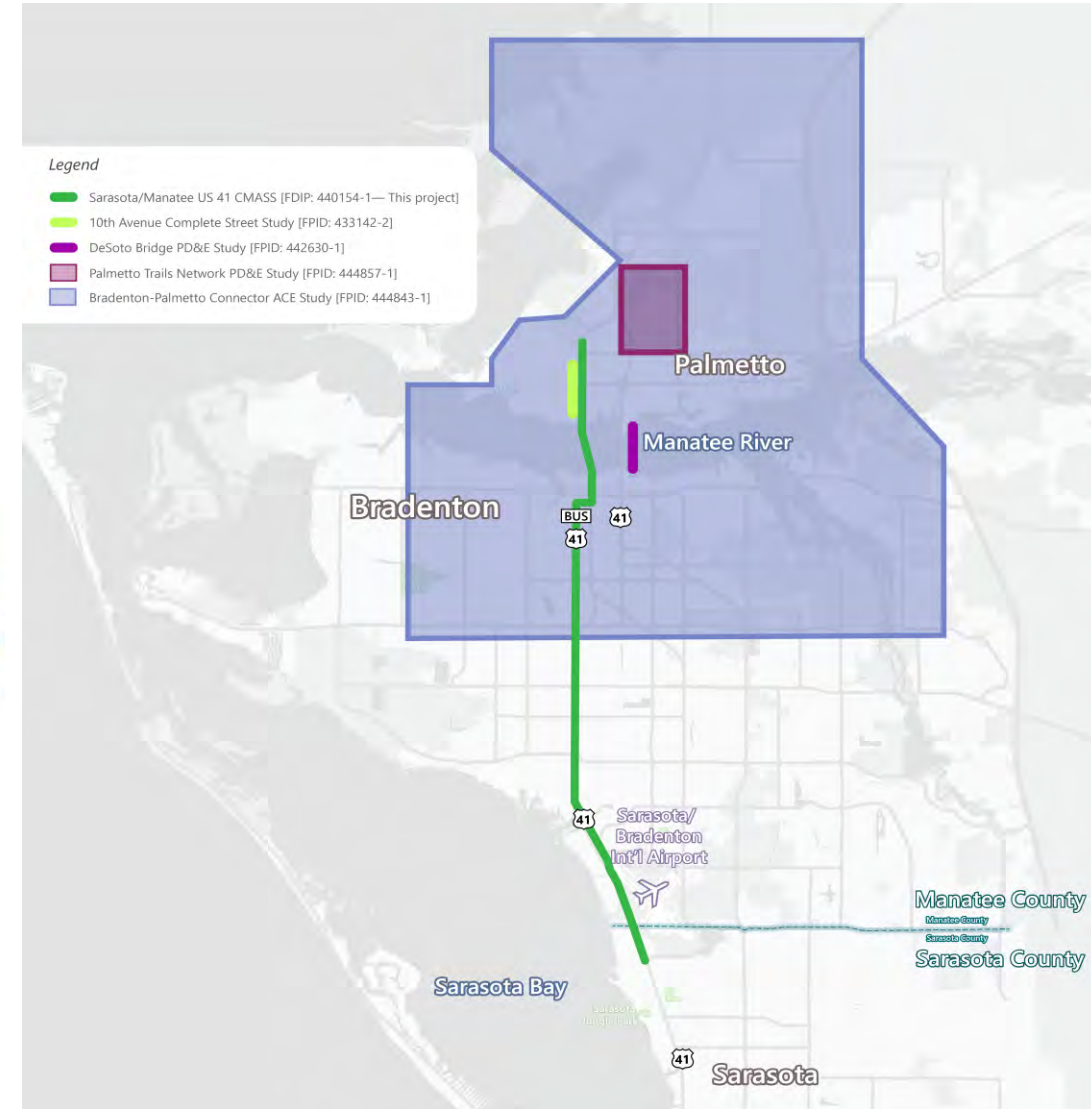


# Connecting Communities

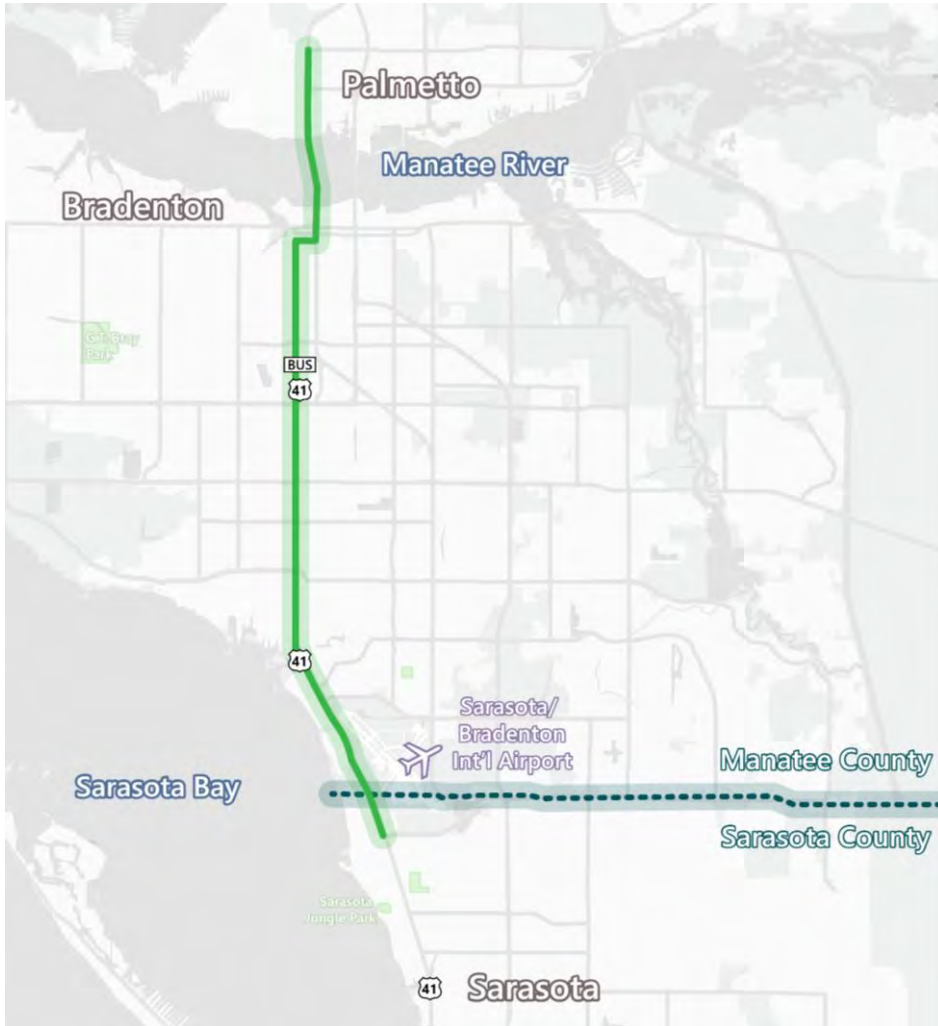
**Purpose:** Holistically enhance the multimodal transportation network within Sarasota and Manatee Counties.



## DeSoto Bridge PD&E Study



# Project Description



**Project Limits:** US 41/US 41 Business corridor from University Parkway (Sarasota) to 17<sup>th</sup> Street W (Palmetto)

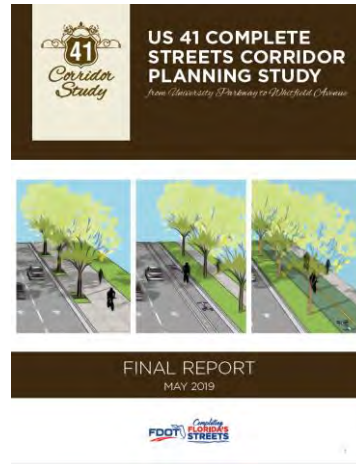
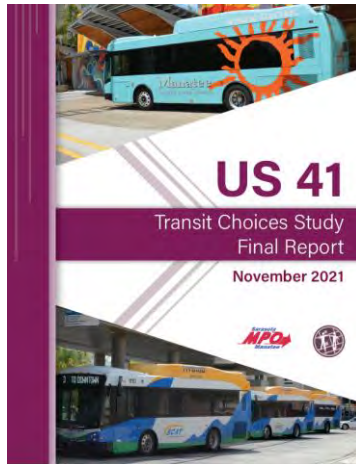
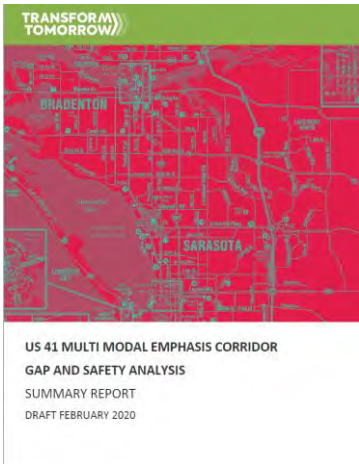
**Financial Project ID:** 440154-1-21-01

**Project Manager:** Matt Dockins

**Status:** Feasibility Study – Phase 1

# We are not starting from scratch...

- US 41 Multi-Modal Emphasis Corridor (MMEC)
- US 41 Transit Choices Study
- US 41 Complete Streets Corridor Planning Study
- Arterial Safety Study
- Access Management Study
- Intersection Safety Studies



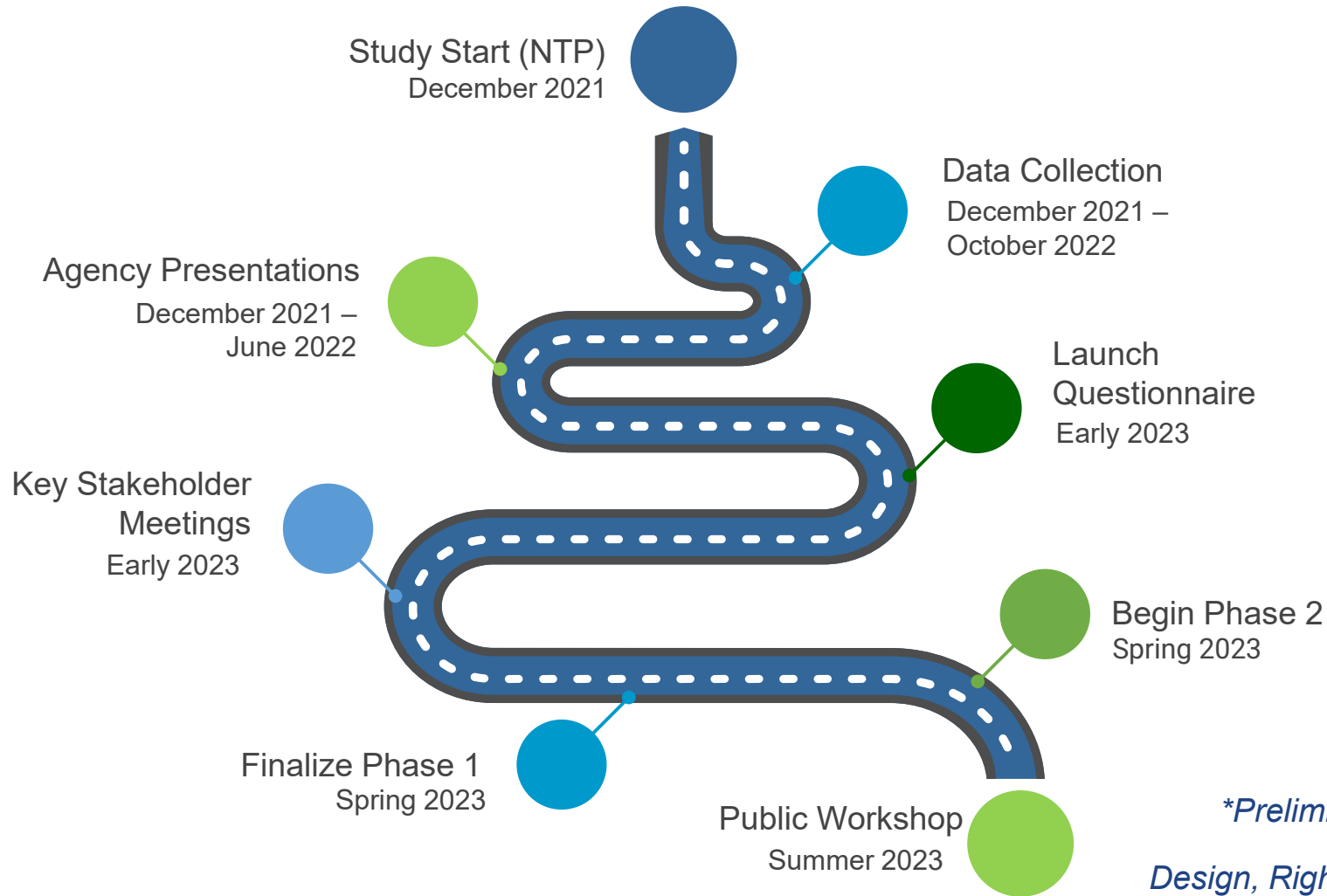


# Goals & Objectives

- Enhance multimodal mobility and safety for all users
- Advance transit service/operation strategies along the study corridor
- Improve transportation network access and interconnectivity within:
  - Neighborhoods
  - Adjacent areas
  - Transit centers



# Project Schedule



*\*Preliminary Schedule – subject to change\**

*Design, Right-of-Way, & Construction are not funded.*

# US 41 Corridor Today



# US 41 Corridor Today

## What are the characteristics of the corridor?

- FDOT priority multimodal corridor
- **2-3 travel lanes in each direction**
- Separated by median, traffic separator, or center left turn lane
- Continuous sidewalks on both sides
- **Right of way varies with more constraints at northern end**
- **Posted speed limits vary from 25 to 45 mph**
- Longest travel time is northbound in evening peak
- Average travel speeds are lower than posted speed limit south of the Green Bridge
- Highest congestion occurs between 63<sup>rd</sup> Avenue and Manatee Avenue

## Who is using the corridor?

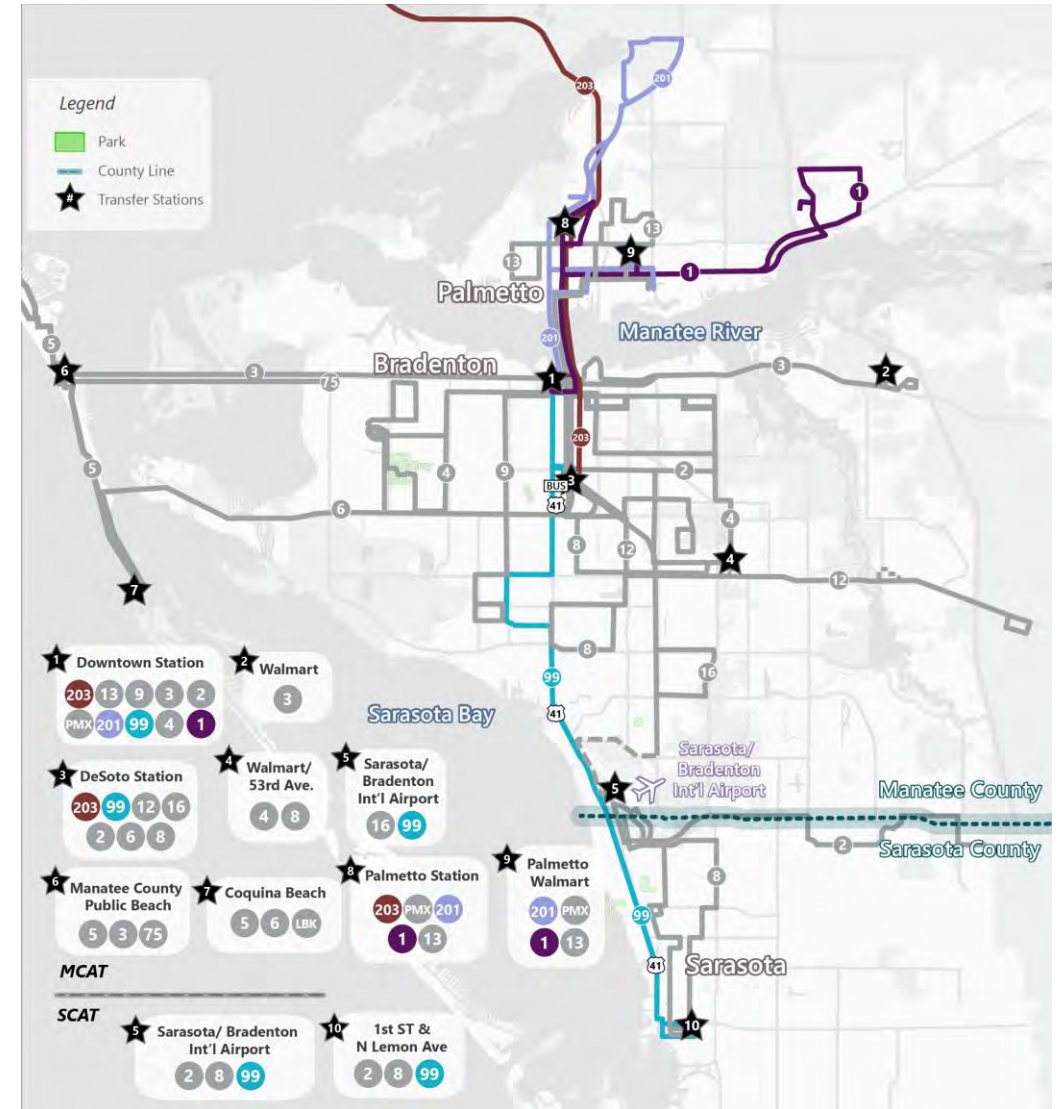
- Residents
- Visitors
- Students
- Businesses

## How are they travelling?

- Single Occupancy Vehicles (cars/trucks, etc.)
- Pedestrians
- Bicyclists
- Several transit (bus) routes

# Area Transit Routes

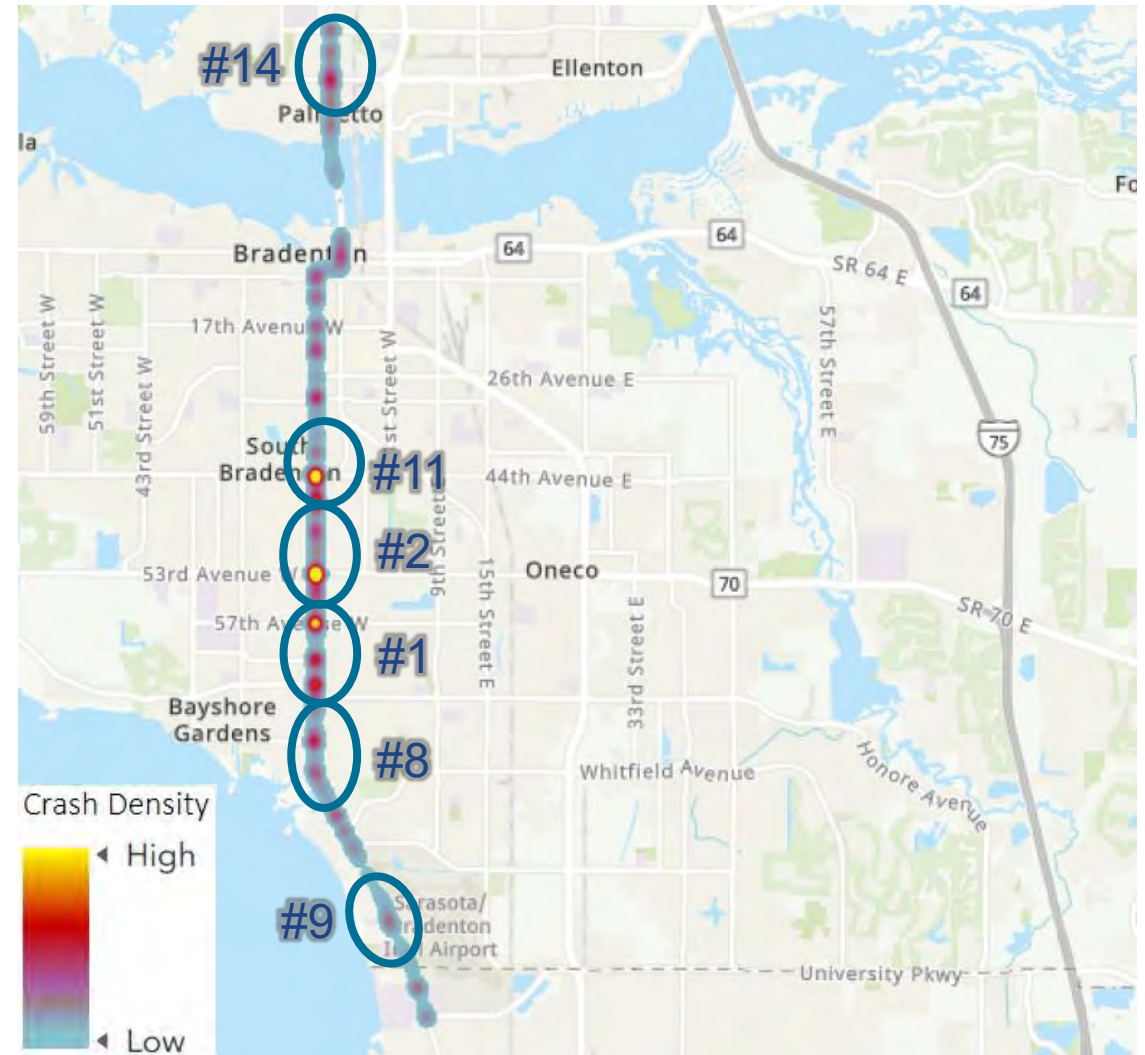
- Route 99 Bradenton-Sarasota
  - Runs length of study area
  - 20-minute AM and PM Peak Period service started Dec 2021
  - New Sunday service proposed starting in 2026
- Stations Near Project
  - Palmetto Station
  - Bradenton Downtown Station
  - DeSoto Station
  - Sarasota-Bradenton International Airport Station



# Safety and Crash Analysis

## Six segments are listed as SW Florida Fatal Top 20 Segments

- 63<sup>rd</sup> Ave W to 55<sup>th</sup> Ave W; Ranked #1
- 55<sup>th</sup> Ave W to Orlando Ave; Ranked #2
- Magellan Dr to 63<sup>rd</sup> Ave W; Ranked #8
- County line to Scott Ave; Ranked #9
- Orlando Ave to 30<sup>th</sup> Ave W; Ranked #11
- 10<sup>th</sup> St W to N of 23<sup>rd</sup> St W; Ranked #14



# Public Outreach

The graphic features a blue background with several circular and rectangular images. At the top, there are five icons: a car, a person with a cane, a bus, a person with a stroller, and a bicycle. The images include: a 'PUSH BUTTON FOR' sign with a pedestrian icon, a line of cars in traffic, a street view with palm trees and buildings, a green and white bus, a traffic light, and a street sign for 'NORTH BUSINESS 41 TAMiami TRAIL'. The text at the bottom right reads 'Sarasota | Manatee U.S. 41 Corridor Mobility and Safety Study Communications Plan'. Logos for 'FDOT' and 'CMASS 41' are also present.

**Sarasota | Manatee  
U.S. 41 Corridor Mobility  
and Safety Study  
Communications Plan**

September 2022 FPID #440154-1

Approved By: Abra Horne Date: 9/2/2022  
Abra Horne, Planning and Environmental Administrator

# Public Outreach



## Outreach to Date:

### September 2021

- Sarasota/Manatee MPO Staff Meetings

### December 2021

- Sarasota/Manatee MPO Staff Meeting
- Pinellas Suncoast Transit Authority Meeting
- SunRunner BRT Meeting

### February 2022

- Sarasota/Manatee MPO Staff Meeting
- Sarasota/Manatee MPO Board Meeting
- Sarasota/Manatee MPO TAC Meeting

### April 2022

- Sarasota/Manatee MPO BPTAC Meeting

### May 2022

- Sarasota/Manatee MPO CAN Meeting
- Sarasota/Manatee MPO TAC Meeting
- Sarasota/Manatee MPO Board Meeting

### June 2022

- City of Bradenton City Council Meeting
- City of Palmetto City Council Meeting
- Manatee County Commission Meeting

## Upcoming Outreach:

### Early 2023

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire

### Summer 2023

- Hold Public Workshop - Summer 2023

### Throughout 2023

- Continued coordination with all agencies and stakeholders



# We Want to Hear From YOU!

Complete the questionnaire:

[swfroads.com/connectingcommunities/](http://swfroads.com/connectingcommunities/)



<http://www.swfroads.com/project/440154-1>

**FDOT** **CONNECTING COMMUNITIES**  
Manatee | Sarasota Counties

January 2023 - April 2023

### WE WANT TO HEAR FROM YOU!

The Florida Department of Transportation, District One, is conducting several projects in partnership with the Sarasota/Manatee Metropolitan Planning Organization (MPO) and local governments to holistically enhance the multimodal transportation network within Sarasota and Manatee Counties as part of the Connecting Communities Program. Major Connecting Communities Program projects include Bradenton-Palmetto Connector, Desoto Bridge, Sarasota/Manatee US 41 Corridor Mobility and Safety Study, Palmetto Trails Network, and 10th Avenue Complete Street. For a map and full list of current area projects included as part of the Connecting Communities Program visit [www.swfroads.com/connectingcommunities](http://www.swfroads.com/connectingcommunities).

Complete Streets  
Safety Improvements  
Corridor Visioning  
Bridge Upgrades  
Multi-use Trails

**Get involved today!**  
**Scan to take our online questionnaire.**  
Complete our brief online questionnaire to provide valuable feedback for enhancing the multimodal transportation network.

For more information on the Connecting Communities Program, visit [www.swfroads.com/connectingcommunities](http://www.swfroads.com/connectingcommunities).

**FDOT**



# Next Steps

- Key Stakeholder Meetings
- Launch and Summarize Questionnaire
- Finalize Goals & Objectives and Measures of Effectiveness
- Finalize Corridor/Neighborhood Strategies
- Finalize Phase 1 – Spring 2023
- Hold Public Workshop - Summer 2023
- Begin Phase 2: Develop Concepts for Implementation

**Complete the questionnaire:**

[swfroads.com/connectingcommunities/](http://swfroads.com/connectingcommunities/)





# Safety, Questions and Contact Information

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**PEDESTRIANS  
AND BICYCLISTS**

Nationally, Florida had the **HIGHEST NUMBER OF BICYCLIST FATALITIES** in 2018

**Pedestrians** account for more than **22%** of traffic fatalities in Florida



# STAKEHOLDER INPUT



## Enhance multimodal mobility and safety for all users

Multimodal Mobility & Safety is #1 concern. Increased number of accidents occurring at US41 and General Spaatz Blvd in front of New College, including accidents involving students.

This project will help improve multimodal mobility. Crosswalk improvements/safety for school students would be beneficial

Suggestion to implement pedestrian crossings and enhance accessibility to nearby restaurants/facilities. There are currently no crosswalks on US41 at USF

Existing conditions include a basic sidewalk. Suggestion for greenway improvements

Concern for students crossing areas near Ringling College and the Airport

Have been discussions about providing safer pedestrian access near the airport

Suggestion to enhance pedestrian safety along DeSoto Bridge

Lack of crosswalks along corridor. Does not allow for recreation or exercise

## Advance transit service/operation strategies along the study corridor

Enhance transit along US 41 - Route 99 is best performing route along corridor

Many students use the pedestrian overpass rather than crossing US41 at the intersection. Bike traffic is more common than pedestrian traffic

Keep transit stops away from Runway 4 to enhance safety for pedestrians

Keeping the median opening at Dolphin would be beneficial for future development

Adding bus stops along corridor to benefit local community

Chamber focusing on cost effective methods for transportation including buses, on demand transit, and transit accessibility

Pedestrian/Scoter/Bike traffic around colleges. May not be necessary for a dedicated SCAT lane due to low ridership/trip

USF provides free transit (MCAT and SCAT) for students on campus. Bus stops are not on campus, but adjacent.

Having a stop at USF campus would provide enhanced accessibility for students

Congestion near universities does not allow for safe or efficient traffic flow

Transportation is barrier for employment. Enhanced transportation will benefit the community and employers

Transit timing is inefficient and poorly designed for workforce

## Improve transportation network access and interconnectivity within neighborhoods, adjacent areas, and transit centers

Suggestion to enhance access to activity center located at US41 and 44th Ave W to benefit local community

Suggestion to make transit more easily accessible by adjacent neighborhoods

Area near Seagate Drive (Property 6) to be developed. Would benefit from enhanced pedestrian facilities. Will be developed as as parking lot for interim use

Not many businesses/restaurants surrounding New College. Students/Faculty rely on cars as main form of transportation. (~700 students residing on campus.)

Chamber considering best way to provide accessible transit for all

Future developments to increase campus activity, enhancing the opportunity for transit use.

Some students will use transit to get to other areas in Sarasota and Bradenton, however most use their own vehicle.

Consideration for businesses would be appreciated as they have been recently affected

Route to nearby businesses is not safe for pedestrians

Inefficient access to transit. Workforce living in southern Sarasota County travel to employment centers in northern Manatee County.

# MEETINGS TO DATE



- Bradenton Area Convention and Visitors Bureau
- Manasota Black Chamber of Commerce
- Manatee County Public Works
- New College of Florida
- Sarasota Manatee Airport Authority
- The Greater Sarasota Chamber of Commerce
- University of South Florida Sarasota-Manatee
- Manatee NAACP
- Bradenton Area Economic Development Corporation

## What improvements would you like to see for the US 41 corridor?

Enhanced lighting near Crosley Estate and USF Campus

Pedestrian facilities (Continous bike/ped) throughout the corridor

Consistency in roadway and surrounding facilities (aesthetics). The roadway and surrounding facilities are very different.

Opportunity for more bicycle lanes along US41 in both directions (N/S)

Enhanced safety at the intersection of US41 and General Spaatz Blvd

Lighting near runway zones is something to be considered. May need approval from FAA

Good wayfinding signs on US41 for Airport - beneficial to incorporate style/colors into signage

Bike/Ped connections between Sarasota and Manatee County

Coordination of red lights along the corridor

Aesthetic improvements along corridor

Continous sidewalks/pedestrian lighting throughout corridor to enhance safety

Adding bike lanes throughout the corridor.

Crosswalks along corridor and safety improvements when traveling to nearby businesses

Aesthetic improvements along corridor. Current aesthetics do not reflect the area well

## Are there any specific area of concern you would like to discuss?

Pedestrian activity walking to New College.

Difficulty for pedestrians to access transit stations

Insufficient lighting near New College

Ability to cross the road.

Traffic congestion between University and New College intersection in AM peak time

Varying speeds/high speed travelers at US 41 and New College entrance (especially northbound)

Lack of pedestrian access between New College and USF.

Pedestrians crossing the road in busier areas. Suggestion for a median that would enforce students/pedestrians to use designated crosswalks.

Captain Brian's Seafood has expressed concern for losing driveway access (involving resurfacing project) - could be beneficial to include shared access with Airport

High speeds along the corridor

Concern for homeless population in the area. Would like to see an intentional plan in place to accommodate the homeless population along the corridor

Concern for future stacking issues at Seagate Drive (highest entrance and exit counts here). Potential for increased congestion with coming development. Includes buses as well.

High speeds in the area. Speed control

Future increased traffic volume to cause issues along corridor

Upholding integrity for African American-owned churches and businesses along the corridor

Congestion and sustainability of the corridor.

Aesthetics and traffic flow along corridor.

Inconsistent aesthetics when traveling across the Sarasota/Manatee County line. Varying differences between the two counties, crossing University Parkway

## What issues do you notice when traveling the US 41 corridor?

Safety hazards (crashes, frequent stopping, roadway conditions, etc.)

Excessive vehicular speeding

Insufficient lighting

Difficulty entering/exiting properties along the corridor

Quality of sidewalks

Quality of bicycle facilities

Quality of transit stops

Lack of transit reliability

Traffic congestion

Inconsistency throughout the corridor including the examples above and number of lanes.

Turning into or out of USF onto US41 (Safety)

Lack of safety / rate of speed / congestion

Travelers unfamiliar with the area are unaware of surrounding conditions

Increased traffic flow

Lack of access management near Airport

Confusion with traffic flow patterns and turning lanes, especially considering ongoing projects

## What is your biggest complaint about traveling along the US 41 Corridor?

Poor lighting along the corridor for motorized traffic and pedestrians

High speeds near Crosley Estate

High pedestrian fatalities due to dangerous conditions

Lack of lighting and high traffic speeds

Bike/Ped safety along the corridor.

Complaints about the left turn signal at General Spaatz Blvd. Long wait times, congestion, queuing issues

Unsafe conditions along the corridor for pedestrians, especially near colleges

Lack of safety and aesthetics along corridor.

Lack of aesthetics along corridor.

Lack of pedestrian safety, especially near colleges

**Does your agency/organization have any planned improvements / projects / events that the project team should know about?**

Frequent Trolley Services from Crosley Estate to nearby for approximately 200 events per year.

Similar transportation methods at Ringling and New College.

Use of Trolley Service encouraged

Frequent events on campus, but most impacts are within the campus

Manatee County is working on Mobility Plan. The County will increase focus on capital project needs and prioritizations

Manatee County working on Trails Master Plan

Few events a year that may impact traffic. Winter events keep University traffic steady

Ground Transportation Center project to begin construction in coming months. Intended to improve MCAT and SCAT drop-off and pick-up. Larger area will accommodate 4 buses rather than 2.

Working with Manatee County to remove Ponce De Leon St once the land becomes the Airport's property

On-campus housing plans in development

Future commercial development anticipated in Property 5 and 6 near airport

Several developers interested in converting motels along the corridor into affordable housing, etc., which would provide a need for enhanced transit services

Future plan to construct nursing program will increase attendance

Annual Golf Tournament & Fall event partnered with Manatee Convention Center with 200+ in attendance

Juneteenth Event with local library

Planned improvements near airport and colleges are anticipated to bring growth in the next 10 years.

**How would you like to continue to be involved in the project?**

Meet with me again

Join Mailing List

Become a project champion

Additional meeting with New College.

Update meeting with Airport Authority

Join mailing list. Hold update meeting with governmental issues council (when appropriate). Can participate as a project champion

Continued meetings to keep up to date on the planning process. Mailing list updates.

Continued meetings to be updated on project process

Continued meetings and engagement with the African American Community

Project updates and meetings when appropriate

**Are there others from your organization that should be included?**

Add names and contact info

Joe Hembree - Commercial Realtor on Governmental Issues Board

James Golden - Legal Rep (NAACP)/ Manatee County Resident & Economic Development Chair.

Sororities and Fraternities - ASHAL Group and local churches

Potential to share with Bradenton Area EDC Board

**Can you help us distribute the questionnaire / project information? What methods?**

Newsletters

Mailing List

In-person flyers posted throughout New College campus

Post flyers throughout airport. Can ask IT to post to website.

Include in weekly and monthly newsletter

Town Hall Meetings, pass out flyers, email/ billboards

Will distribute to contact list.

Email blast and in-person depending on scheduled events

Electronic distribution

Suggestion for project team to look to local churches and childcare centers for distribution in addition to creating a spanish and creole version of the questionnaire.